

Development Plan

Charles Sturt Council

Consolidated - 14 January 2021

Please refer to the Charles Sturt Council page at www.sa.gov.au/developmentplans to see any amendments not consolidated.



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Introduction

Section

Amendment Record Table

The following table is a record of authorised amendments and their consolidation dates for the Charles Sturt Council Development Plan since its inception on 4 September 2003. Further information on authorised amendments prior to this date may be researched through the relevant Council, the Planning and Land Use Services Division of the Attorney-General's Department or by viewing Gazette records.

Consolidated	Amendment – [Gazetted date]
4 September 2003	Hindmarsh & Woodville (City) and Henley & Grange (City) Development Plans - General PAR – [4 September 2003]
19 March 2004	Underdale Campus Master Plan - Design PAR (Interim) – [19 March 2004]
11 November 2004	Hindmarsh Urban Village PAR – [11 November 2004]
3 February 2005	Bulky Goods PAR (<i>Ministerial</i>) – [3 February 2005]
24 March 2005	Underdale Campus Master Plan Design PAR – [17 March 2005] Section 29(2)(a) Amendment – [24 March 2005]
6 October 2005	Waste Transfer Plan PAR – [6 October 2005] Editorial correction to page 97
22 December 2005	Centres Minor Amendments and Bulky Goods – [22 December 2005]
2 February 2006	District Centre (West Lakes) Zone Building Height and Design PAR (Interim) – [25 January 2006] Section 27(5) Amendment - Underdale Campus Master Plan Design PAR – [2 February 2006]
24 August 2006	Findon Urban Village PAR – [24 August 2006]
14 December 2006	District Centre (West Lakes) Zone Building Height and Design PAR – [14 December 2006]
25 January 2007	Sheridan Site PAR (<i>Ministerial</i>) – [25 January 2007]
19 April 2007	Section 29(2)(b)(ii) Amendment – [22 February 2007] Royal Park Bulky Goods PAR (<i>Ministerial</i>) – [19 April 2007]
28 February 2008	District Centre (Kilkenny) Zone DPA – [28 February 2008]
14 August 2008	Cheltenham Park Racecourse DPA (<i>Ministerial</i>) – [14 August 2008]
4 September 2008	Section 29(2)(b)(ii) Amendment – [4 September 2008]
13 August 2009	Better Development Plan (BDP) and General DPA – [13 August 2009]
10 June 2010	Statewide Bulky Goods DPA (<i>Ministerial</i>) (Interim) – [1 June 2010] West Lakes Council Depot DPA – [10 June 2010]
2 December 2010	Woodville West Neighbourhood Renewal DPA (<i>Ministerial</i>) (Interim) – [2 December 2010]
1 December 2011	Statewide Bulky Goods DPA (<i>Ministerial</i>) – [13 January 2011] Bowden Urban Village & Environs (Interim Policy) DPA – (<i>Ministerial</i>) (Interim) – [17 November 2011] Regulated Trees DPA (Interim) – [17 November 2011] Woodville West Neighbourhood Renewal DPA (<i>Ministerial</i>) – [1 December 2011]
5 July 2012	Bowden Urban Village & Environs DPA (<i>Ministerial</i>) – supersedes the Bowden Urban Village & Environs (Interim Policy) DPA – [5 July 2012]
16 May 2013	Regulated Trees DPA (<i>Ministerial</i>) – [15 November 2012] Heritage DPA (Interim) – [16 May 2013]

Charles Sturt Council
Introduction Section
Amendment Record Table

Consolidated	Amendment – [Gazetted date]
14 November 2013	Woodville Station DPA (<i>Ministerial</i>) – [8 November 2013] West Lakes (AAMI Stadium Precinct) DPA (<i>Ministerial</i>) – [8 November 2013]
12 December 2013	Editorial correction to District Centre Zone non-complying table
15 May 2014	Heritage DPA – [15 May 2014]
25 September 2014	Devon Park Residential DPA – [31 July 2014] Adelaide Shores DPA (<i>Ministerial</i>) – [25 September 2014]
5 November 2015	Section 29(2)(a) Amendment – [7 May 2015] Section 29(1) Amendment – [5 November 2015]
5 May 2016	Existing Activity Centres DPA (<i>Ministerial</i>) – [21 April 2016]
12 September 2017	Section 29(2)(b)(ii) Amendment – [2 February 2017] Heritage Places DPA (Interim) – [12 September 2017]
31 October 2017	Residential City-wide Policies DPA – [24 October 2017] Urban Employment Zone DPA – [31 October 2017]
12 December 2017	West Lakes (AAMI Stadium Precinct) Zoning and Policy Review DPA (<i>Ministerial</i>) – [12 December 2017]
30 January 2018	Privately Funded Seaton Mixed Use (Residential and Commercial) DPA – [30 January 2018]
13 September 2018	Section 29(2)(b)(i) and Section 29(2)(b)(ii) Amendments – [27 February 2018] Heritage Places DPA – [13 September 2018]
13 June 2019	Bowden–Brompton Mixed Use (Residential and Commercial) DPA (Privately Funded) – [13 June 2019]
25 July 2019	Grange Road Findon DPA (Privately Funded) – [25 July 2019]
13 February 2020	Findon Road, Kidman Park (North) Mixed Use (Residential and Commercial) DPA (Privately Funded) – [13 February 2020]
14 January 2021	St Clair Residential DPA (Trident Plastics Site) (Privately Funded) – [14 January 2021]

Consolidated: The date of which an authorised amendment to a Development Plan was consolidated (incorporated into the published Development Plan) pursuant to section 31 of the *Development Act 1993*.

Gazetted: The date of which an authorised amendment was authorised through the publication of a notice in the Government Gazette pursuant to Part 3 of the *Development Act 1993*.

Introduction to the Development Plan

Welcome to the Development Plan for the City of Charles Sturt.

This introduction has been prepared by the Attorney-General's Department as a guide to assist you in understanding this Development Plan.

For full details about your rights and responsibilities, you are advised to refer to the *Development Act 1993* and the associated *Development Regulations 2008* and/or consult your council.

A number of guides and additional information regarding South Australia's Planning and Development Assessment System are available via the website plan.sa.gov.au or by contacting the Planning and Land Use Services Division, Attorney-General's Department at Level 5, 50 Flinders Street, Adelaide, SA 5000.

Overview of the Planning System

South Australia has an integrated planning and development system, with three distinct but interrelated parts, these being:

- Legislation
- The Planning Strategy
- Development Plans.

The **legislative framework** establishing the planning and development system and setting out its statutory procedures is provided by the *Development Act 1993* and its associated *Development Regulations 2008*. The Development Act is the core legislation enacted by the South Australian Parliament to establish the planning and development system framework and many of the processes required to be followed within that framework (including processes for assessing development applications). The Regulations provide more details about the framework and are updated from time to time by the Governor (on the advice of the Minister for Planning).

The State Government's broad vision for sustainable land use and the built development of the state is outlined in the **Planning Strategy**. The relevant volume of the Planning Strategy for this Development Plan is The 30-Year Plan for Greater Adelaide.

The Planning Strategy, which covers a full range of social, economic and environmental issues, informs and guides policies both across Government and in local area Development Plans. The Planning Strategy is required under section 22 of the Development Act and is updated by the State Government every few years. Local councils also prepare strategic plans which guide the same matters but at a local level. These strategic plans are not, however, development assessment tools: that is the role of Development Plans.

Development Plans are the key on-the-ground development assessment documents in South Australia. They contain the rules that set out what can be done on any piece of land across the state, and the detailed criteria against which development applications will be assessed. Development Plans cover distinct and separate geographic areas of the state. There is a separate Development Plan for each one of the 68 local council areas, plus a handful of other Development Plans covering areas not situated within local government boundaries. Development Plans outline what sort of developments and land use are and are not envisaged for particular zones (eg residential, commercial, industrial), and various objectives, principles and policies further controlling and affecting the design and other aspects of proposed developments.

What is Development?

'Development' is defined in Section 4 of the *Development Act 1993* as:

- a change in the use of land or buildings
- the creation of new allotments through land division (including Strata and Community Title division)
- building work (including construction, demolition, alteration and associated excavation / fill)

- cutting, damaging or felling of significant trees
- specific work in relation to State and Local heritage places
- prescribed mining operations
- other acts or activities in relation to land as declared by the Development Regulations.

No development can be undertaken without an appropriate **Development Approval** being obtained from the relevant authority after an application and assessment process.

How does the Development Plan relate to other legislation?

The Development Plan is a self-contained policy document prepared under and given statutory recognition pursuant to *the Development Act 1993*.

It is generally independent of other legislation but is one of many mechanisms that control or manage the way that land and buildings are used.

The Development Act and Regulations contain a number of provisions to ensure that development applications are referred to other government agencies when appropriate.

What doesn't a Development Plan do?

Development Plans are applicable only when new development is being designed or assessed. They do not affect existing development (see above for a description of what constitutes 'development').

Once a Development Approval is issued, the details contained within the application and any conditions attached to that approval are binding.

Development Plan policies guide the point in time assessment of a development application but do not generally seek to control the on-going management of land, which is the role of other legislation (eg the Environment Protection Act, Natural Resources Management Act, Liquor Licensing Act).

When do you use the Development Plan?

The Development Plan should be used during a development application process. This may include:

- when undertaking or proposing to undertake 'development' (eg building a house or factory or converting an office into a shop)
- when assessing or determining a development proposal (eg by council staff, a Development Assessment Panel, or the Development Assessment Commission)
- when you believe you could be affected by a proposed development and you are given an opportunity to comment on it as part of the assessment process.

How to read the Development Plan

Development Plans are comprised of several sections as described below.

All sections and **all** relevant provisions within each section of the Development Plan must be considered in relation to a development proposal or application.

Development Plans use three text font colours:

- (a) Black text is used to identify all standard policy that forms the basis of all council Development Plans.
- (b) Green text is used to identify additional council-specific policy or variables that have been included in the Development Plan to reflect local circumstances.
- (c) Blue text illustrates hyperlinks to maps, overlays and tables in the Development Plan. These hyperlinks are operational only when viewing electronic versions of the Development Plan.

Development Plan Structure Overview

Advisory Section	Function
Table of Contents	Navigational aid to reference sections within the Development Plan by name and page number.
Amendment Record Table	Tabled information recording previously-authorized Development Plan amendments and their consolidation dates.
Introduction Overview of the Planning System What is Development? How does the Development Plan relate to other legislation? What doesn't a Development Plan do? When do you use the Development Plan? How to read the Development Plan?	A general overview of the context, purpose and way a Development Plan is set out (this section is advisory only and not used for development assessment purposes).
Strategic Setting State Strategic Setting <i>(Metro / Outer Metro / Regional Planning Strategy)</i> Council Strategic Setting <i>(Council Strategy)</i>	To be developed, but intended to reflect the relevant Planning Strategy (as it relates to the council area) and council's own local strategic investigations.
Council Preface Map	Map of the entire Development Plan boundary and its spatial relationship to other Development Plans' boundaries.
Assessment Section	Function
General Provisions Objectives Principles of Development Control	These policies apply across the whole council area and relate to a range of social, environmental, and economic development issues such as: <ul style="list-style-type: none"> ▪ site and design criteria ▪ access and vehicle parking requirements ▪ heritage and conservation measures ▪ environmental issues ▪ hazards ▪ infrastructure requirements ▪ land use specific requirements. They establish the development standards that apply to all forms of development and provide a yardstick against which the suitability of development proposals is measured.
Zone Provisions	These policies give greater certainty and direction about where certain forms of developments should be located. Maps are referenced within zones that show where land uses are suitable to be located. Generally, envisaged forms of development within a zone are identified and encouraged through carefully worded policies.

Assessment Section	Function
Desired Character Statements	These express a vision about how the zone should look and feel in the future. They may describe the valued elements of the neighbourhood or area to be retained and/or what level and nature of change is desired.
Objectives	These are the specific planning policies that determine what land uses are encouraged or discouraged in the zone. They often contain detailed provisions to further guide the scale and design of development.
Principles of Development Control	These also provide lists of complying and non-complying development and any public notification provisions that vary from those in the Development Regulations.
Policy Area	Policy areas apply to a portion of a zone and contain additional objectives, desired character statements and principles of development control for that portion.
Precincts	<p>Precincts are used to express policies for a small sub-area of a zone or a policy area.</p> <p>Precincts are used if additional site-specific principles of development control are needed to reflect particular circumstances associated with those sub-areas. They do not contain additional objectives or desired character statements.</p>
Procedural Matters	<p>All zones have a procedural matters section that identifies and lists complying, non-complying and public notification categories for various forms of development.</p> <p>Policy areas and/or precincts, which are a sub-set of the zone, share this procedural matters section. Their respective lists can be modified to accommodate policy area and precinct variations.</p>
Tables	<p>These tables provide detailed data for the assessment of certain elements of development, for example, numeric values for setbacks from road boundaries and car parking rates for certain types of development.</p> <p>Conditions for complying development are grouped into their respective tables.</p>
Mapping	
Structure Plan Maps	Structure Plan maps will commonly show the general arrangement and broad distribution of land uses; key spatial elements; and movement patterns throughout the council area and major urban areas.
Council Index Maps	<p>This is the first point of reference when determining the appropriate map(s) applying to a specific property.</p> <p>An enlargement index map may be included where needed, eg for large townships.</p>

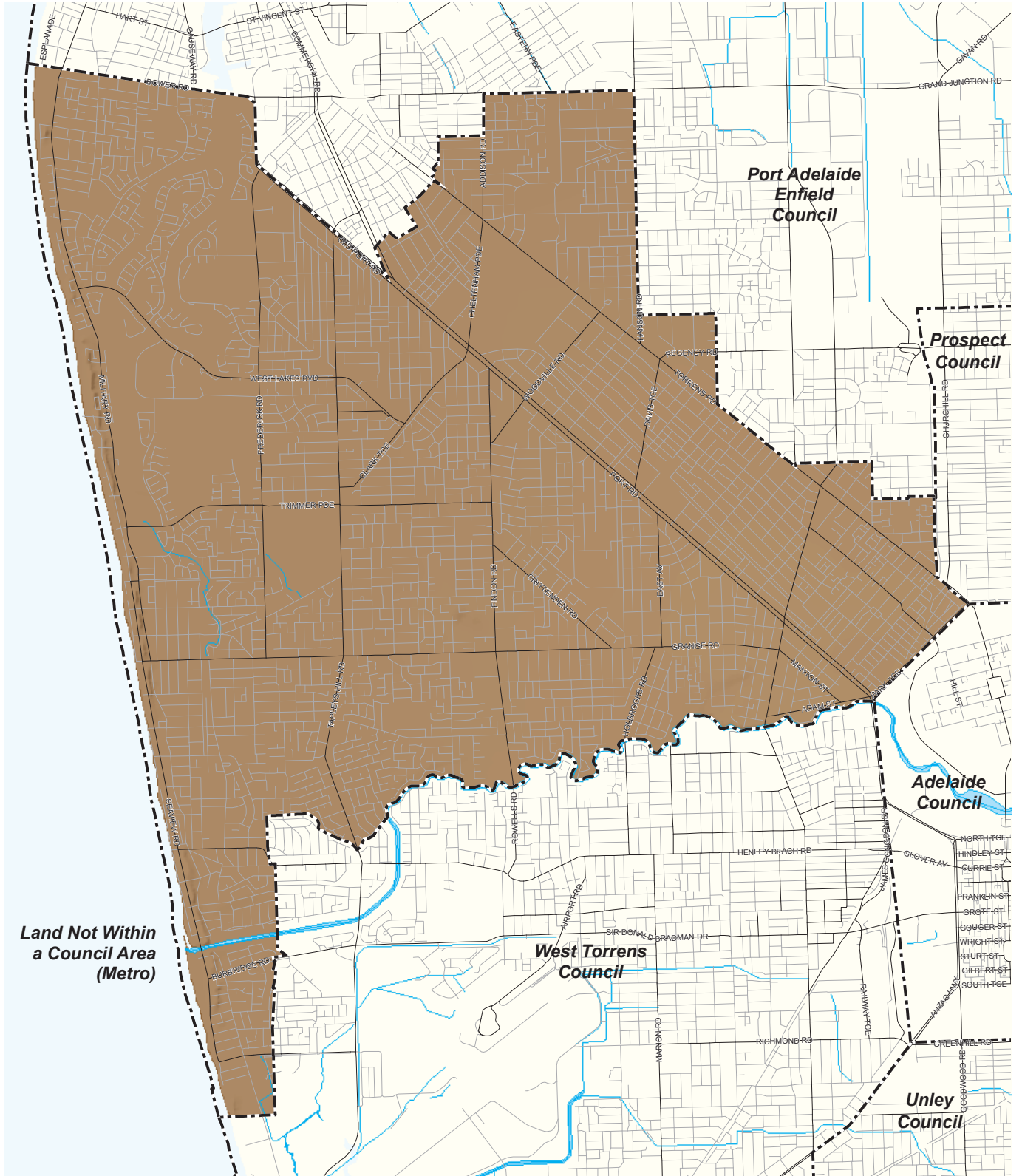
Assessment Section	Function
Extent Map Series Location Maps	<p>Individual overlay and spatial-based maps (based on the Council Index Maps) originate from a single Location Map and ‘drill down’ through relevant extent maps affecting that location.</p> <p><i>Note: the entire council area will always be represented as the first map in the extent map series and will commence as map 1.</i></p>
Overlay Maps	<p>Used to show issue areas or features that run across a number of zones, and are spatially defined to a cadastre, for example:</p> <ul style="list-style-type: none"> ▪ Transport ▪ Development Constraints ▪ Heritage ▪ Natural Resources. <p><i>Note: issues that are not spatially defined to a cadastre can appear in this section; however they will be presented as illustrative maps only.</i></p>
Zone Maps	Used to determine which zone applies to which land.
Policy Area Maps	Used to depict the presence and location of any applicable policy area.
Precinct Maps	Used to depict the presence and location of any applicable precincts.
Bushfire Maps (<i>where applicable</i>) Bushfire Protection Area BPA Maps - Bushfire Risk	Bushfire Protection Area - BPA Maps are used to determine the potential bushfire risk (high, medium or general), associated with an allotment located within an area prone to bushfires.
Concept Plan Maps	<p>Concept Plans are used to depict graphically key features and conceptual layouts of how specific areas should be developed.</p> <p>Concept Plans appear at the end of the extent map series as a separate section. Concept Plans are consecutively numbered, commencing with number 1.</p>

Further info

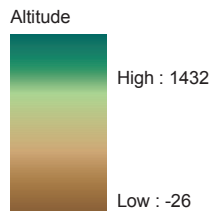
Contact the [City of Charles Sturt](#).

Visit the Planning and Land Use Services website: www.plan.sa.gov.au.

Discuss your matter with your planning consultant.



To identify the precise location of the Development Plan boundary refer to Council Index Map then select the relevant map number.



Council Preface Map

General Section

Advertisements

OBJECTIVES

- 1 Urban and rural landscapes that are not disfigured by advertisements and/or advertising hoardings.
- 2 Advertisements and/or advertising hoardings that do not create a hazard.
- 3 Advertisements and/or advertising hoardings designed to enhance the appearance of the building and locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Advertising and/or advertising hoardings should have regard to the suitable outdoor advertising types outlined in [Table ChSt/3 – Suitable Outdoor Advertising Types](#).
- 2 The location, siting, design, materials, size, and shape of advertisements and/or advertising hoardings should be:
 - (a) consistent with the predominant character of the urban or rural landscape
 - (b) in harmony with any buildings or sites of historic significance or heritage value in the area
 - (c) coordinated with and complement the architectural form and design of the building they are to be located on.
- 3 The number of advertisements and/or advertising hoardings associated with a development should be minimised to avoid:
 - (a) clutter
 - (b) disorder
 - (c) untidiness of buildings and their surrounds
 - (d) driver distraction.
- 4 Buildings occupied by a number of tenants should exhibit coordinated and complementary advertisements and/or advertising hoardings to identify the tenants and their type of business.
- 5 The content of advertisements should be limited to information relating to the legitimate use of the associated land.
- 6 Advertisements and/or advertising hoardings should:
 - (a) be completely contained within the boundaries of the subject allotment
 - (b) be sited to avoid damage to, or pruning or lopping of, on-site landscaping or street trees
 - (c) not obscure views to vistas or objects of high amenity value.
- 7 Advertisements and/or advertising hoardings should not be erected on:
 - (a) a public footpath or veranda post

- (b) a road, median strip or traffic island
 - (c) a vehicle adapted and exhibited primarily as an advertisement
 - (d) residential land.
- 8 Advertisements and/or advertising hoardings attached to buildings should not be sited on the roof or higher than the walls of a building, unless the advertisement or advertising hoarding is appropriately designed to form an integrated and complementary extension of the existing building.
- 9 Advertisements and/or advertising hoardings erected on a veranda or that project from a building wall should:
- (a) have a clearance over a footway to allow for safe and convenient pedestrian access
 - (b) where erected on the side of a veranda, not exceed the width of the veranda or project from the veranda
 - (c) where erected on the front of a veranda, not exceed the length of the veranda or project from the veranda
 - (d) where projecting from a wall, have the edge of the advertisement or advertising hoarding abutting the surface of the wall.
- 10 Advertisements should be designed to conceal their supporting advertising hoarding from view.
- 11 Advertisements should convey the owner/occupier and/or generic type of business, merchandise or services using simple, clear and concise language, symbols, print style and layout and a small number of colours.
- 12 Advertisements which perform a secondary role in identifying the business, goods or services should only be readable in the immediate vicinity of the site.

Safety

- 13 Advertisements and/or advertising hoardings should not create a hazard by:
- (a) being so highly illuminated as to cause discomfort to an approaching driver, or to create difficulty in the driver's perception of the road or persons or objects on the road
 - (b) being liable to interpretation by drivers as an official traffic sign, or convey to drivers information that might be confused with instructions given by traffic signals or other control devices, or impair the conspicuous nature of traffic signs or signals
 - (c) distracting drivers from the primary driving task at a location especially where the demands on driver concentration are high
 - (d) obscuring a driver's view of other road or rail vehicles at/or approaching level crossings, or of pedestrians or of features of the road that are potentially hazardous (eg junctions, bends, changes in width, traffic control devices).
- 14 Any internally illuminated advertising signs and/or advertising hoardings which utilise LED, LCD or other similar technologies should be located a minimum of 80 metres from traffic signals, level crossings and other important traffic control devices.

Freestanding Advertisements

- 15 Freestanding advertisements and/or advertising hoardings should be:
- (a) limited to only one primary advertisement per site or complex

- (b) of a scale and size in keeping with the desired character of the locality and compatible with the development on the site.
- 16 Freestanding advertisements and/or advertising hoardings for multiple-business tenancy complexes should:
- (a) incorporate the name or nature of each business or activity within the site or complex in a single advertisement
 - (b) be integrally designed and mounted below the more predominant main complex or site identity advertisement.
- 17 Portable, easel or A-frame advertisements should be displayed only where:
- (a) no other appropriate opportunity exists for an adequate coordinated and permanently erected advertisement and/or advertising hoarding
 - (b) they do not obstruct or compromise the safety of pedestrians or vehicle movement
 - (c) there is no unnecessary duplication or proliferation of advertising information
 - (d) there is no damage to, or removal of, any landscaping on the site.
- 18 Portable, easel or A-frame advertisements associated with a development should be displayed only during the hours the development is open for trading.

Flags, Bunting and Streamers

- 19 Advertisements and/or advertising hoardings incorporating any flags, bunting, streamers, or suspended objects should:
- (a) be placed or arranged to complement and accord with the scale of the associated development
 - (b) other than flags, not be positioned higher than the building they are attached or related to
 - (c) not be displayed in residential areas.

Advertising along Arterial Roads

- 20 Advertising and/or advertising hoardings should not be placed along arterial roads that have a speed limit of 80 km/h or more.

Advertising in Mixed Use and Corridor Zones

- 21 Advertisements or advertising hoardings should be:
- (a) no higher than the height of the finished floor level of the second storey of the building to which it relates
 - (b) where located below canopy level, flush with the wall or projecting horizontally
 - (c) where located at canopy level, in the form of a fascia sign
 - (d) where located above the canopy, flush with the wall and within the height of the parapet.
- 22 Advertisements or advertising hoardings should not exceed 25 per cent of the ground floor wall area on the façade the sign is placed.

Building near Airfields

OBJECTIVES

- 1 Development that ensures the long-term operational, safety and commercial aviation requirements of airfields (airports, airstrips and helicopter landing sites) continue to be met.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 The height and location of buildings and structures should not adversely affect the long-term operational, safety and commercial aviation requirements of airfields.
- 2 Buildings and structures that exceed the airport building heights shown on the *Overlay Maps - Development Constraints* should not be developed unless a safety analysis determines that the building / structure does not pose a hazard to aircraft operations.
- 3 Buildings should be sited and of a height which will not result in a breach of the Obstacle Limitation Surfaces (OLS) and navigational clearance zones (PANSOPS) associated with Airport runways.
- 4 Development in the vicinity of airfields should not create a risk to public safety, in particular through any of the following:
 - (a) lighting glare
 - (b) smoke
 - (c) air turbulence
 - (d) storage of flammable liquids
 - (e) attraction of birds
 - (f) materials that affect aircraft navigational aids.
- 5 Lighting within six kilometres of an airport should be designed so that it does not pose a hazard to aircraft operations.
- 6 Development that is likely to increase the attraction of birds should not be located within three kilometres of an airport used by commercial aircraft. If located closer than three kilometres the facility should incorporate bird control measures to minimise the risk of bird strikes to aircraft.
- 7 Development within areas affected by aircraft flightpath noise as shown on *Overlay Maps - Development Constraints* should be consistent with *Australian Standard AS2021– Acoustics - Aircraft Noise Intrusion - Building Siting and Construction*.

Centres and Retail Development

OBJECTIVES

- 1 Shopping, administrative, cultural, community, entertainment, educational, religious and recreational facilities located in integrated centres and mixed use zones.
- 2 Centres that ensure rational, economic and convenient provision of goods and services and provide:
 - (a) a focus for community life
 - (b) safe, permeable, pleasant and accessible walking and cycling networks.
- 3 The provision of a safe pedestrian environment within centres which gives high priority to pedestrians, public and community transport.
- 4 Increased vitality and activity in centres through the introduction and integration of housing.
- 5 Centres developed in accordance with a hierarchy based on function, so that each type of centre provides a proportion of the total requirement of goods and services commensurate with its role.
- 6 The central business district of the City of Adelaide providing the principal focus for the economic, social and political life of Greater Adelaide and the State.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development within centres should:
 - (a) integrate facilities within the zone
 - (b) allow for the multiple use of facilities and the sharing of utility spaces
 - (c) allow for the staging of development within the centre
 - (d) be integrated with public and community transport.
- 2 Development within centres should be designed to be compatible with adjoining areas. This should be promoted through landscaping, screen walls, centre orientation, location of access ways, buffer strips and transitional use areas.
- 3 Development within centres should provide:
 - (a) public spaces such as malls, plazas and courtyards
 - (b) street furniture, including lighting, signs, litter bins, seats and bollards, that is sited and designed to complement the desired character
 - (c) unobtrusive facilities for the storage and removal of waste materials
 - (d) public facilities including toilets, infant changing facilities for parents, telephones and community information boards
 - (e) access for public and community transport and sheltered waiting areas for passengers
 - (f) lighting for pedestrian paths, buildings and associated areas

- (g) a single landscaping theme
 - (h) safe and secure bicycle parking.
- 4 A single architectural theme should be established within centres through:
- (a) constructing additions or other buildings in a style complementary to the existing shopping complex
 - (b) renovating the existing shopping complex to complement new additions and other buildings within the centre
 - (c) employing a signage theme.

Arterial Roads

- 5 Centres should develop on one side of an arterial road or in one quadrant of an arterial road intersection.
- 6 Centre development straddling an arterial road should:
- (a) concentrate on one side of the arterial road or one quadrant of the arterial road intersection
 - (b) minimise the need for pedestrian and vehicular movement from one part of the centre to another across the arterial road.

Retail Development

- 7 A shop or group of shops located outside of zones that allow for retail development should:
- (a) be of a size and type that will not hinder the development, function or viability of any centre zone
 - (b) not demonstrably lead to the physical deterioration of any designated centre
 - (c) be developed taking into consideration its effect on adjacent development.
- 8 Bulky goods outlets located within centre zones should:
- (a) complement the overall provision of facilities
 - (b) be sited towards the periphery of those zones where the bulky goods outlet has a gross leasable area of 500 square metres or more.
- 9 Other than in Bulky Goods Policy Area 6, the incidental sale of foodstuffs, clothing, footwear or personal effects goods within any bulky goods outlet or service trade premises should be limited to:
- (a) 7.5 percent of the gross leasable area of the tenancy or 100 square metres, whichever is the lesser
 - (b) 100 square metres in relation to the sale of food and beverages for consumption on premises, provided that the gross leasable area of the bulky goods outlet or service trade premises is greater than 2000 square metres.

Coastal Areas

OBJECTIVES

- 1 The protection and enhancement of the natural coastal environment, including environmentally important features of coastal areas such as mangroves, wetlands, sand dunes, cliff-tops, native vegetation, wildlife habitat shore and estuarine areas.
- 2 Protection of the physical and economic resources of the coast from inappropriate development.
- 3 Preservation of areas of high landscape and amenity value including stands of vegetation, shores, exposed cliffs, headlands, islands and hill tops, and areas which form an attractive background to urban and tourist areas.
- 4 Development that maintains and/or enhances public access to coastal areas with minimal impact on the environment and amenity.
- 5 Development only undertaken on land which is not subject to or that can be protected from coastal hazards including inundation by storm tides or combined storm tides and stormwater, coastal erosion or sand drift, and probable sea level rise.
- 6 Development that can accommodate anticipated changes in sea level due to natural subsidence and probable climate change during the first 100 years of the development.
- 7 Development which will not require, now or in the future, public expenditure on protection of the development or the environment.
- 8 Management of development in coastal areas to sustain or enhance the remaining natural coastal environment.
- 9 Low intensity recreational uses located where environmental impacts on the coast will be minimal.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be compatible with the coastal environment in terms of built-form, appearance and landscaping including the use of walls and low pitched roofs of non-reflective texture and natural earth colours.

Environmental Protection

- 2 The coast should be protected from development that would adversely affect the marine and onshore coastal environment, whether by pollution, erosion, damage or depletion of physical or biological resources, interference with natural coastal processes or any other means.
- 3 Development should not be located in delicate or environmentally-sensitive coastal features such as sand dunes, cliff-tops, wetlands or substantially intact strata of native vegetation.
- 4 Development should not be undertaken where it will create or aggravate coastal erosion, or where it will require coast protection works which cause or aggravate coastal erosion.
- 5 Development should be designed so that solid / fluid wastes and stormwater runoff is disposed of in a manner that will not cause pollution or other detrimental impacts on the marine and on-shore environment of coastal areas.

- 6 Development that proposes to include or create confined coastal waters, as well as water subject to the ebb and flow of the tide should be designed to ensure the quality of such waters is maintained at an acceptable level.
- 7 Development should be designed and sited so that it does not prevent natural landform and ecological adjustment to changing climatic conditions and sea levels and should allow for the following:
 - (a) the unrestricted landward migration of coastal wetlands
 - (b) new areas to be colonised by mangroves, samphire and wetland species
 - (c) sand dune drift
 - (d) where appropriate, the removal of embankments that interfere with the abovementioned processes.

Maintenance of Public Access

- 8 Development should maintain or enhance public access to and along the foreshore.
- 9 Development should provide for a public thoroughfare between the development and any coastal reserve.
- 10 Other than small-scale infill development in a predominantly urban zone, development adjacent to the coast should not be undertaken unless it has, or incorporates an existing or proposed public reserve, not including a road or erosion buffer, of at least 50 metres width between the development and the landward toe of the frontal dune or the top edge of an escarpment. If an existing reserve is less than 50 metres wide, the development should incorporate an appropriate width of reserve to achieve a total 50 metres wide reserve.
- 11 Except where otherwise specified in a particular zone or policy area, buildings on land abutting coastal reserves should be set back either a distance of 8 metres from any boundary with the reserve or in line with adjacent development, whichever is the greater distance.
- 12 Development that abuts or includes a scenic, conservation or recreational coastal reserve should be sited and designed to be compatible with the purpose, management and amenity of the reserve, as well as to prevent inappropriate access to the reserve.
- 13 Development, including marinas and aquaculture, should be located and designed to ensure convenient public access along the waterfront to beaches and coastal reserves is maintained, and where possible enhanced through the provision of one or more of the following:
 - (a) pedestrian pathways and recreation trails
 - (b) coastal reserves and lookouts
 - (c) recreational use of the water and waterfront
 - (d) safe public boating facilities at selected locations
 - (e) vehicular access to points near beaches and points of interest
 - (f) car parking.
- 14 Where a development such as a marina creates new areas of waterfront, provision should be made for public access to, and recreational use of, the waterfront and the water.
- 15 Public access through sensitive coastal landforms, particularly sand dunes, wetlands and cliff faces, should be restricted to defined pedestrian paths constructed to minimise adverse environmental impact.

- 16 Access roads to the coast and lookouts should preferably be spur roads rather than through routes, other than tourist routes where they:
- (a) do not detract from the amenity or the environment
 - (b) are designed for slow moving traffic
 - (c) provide adequate car parking.

Hazard Risk Minimisation

- 17 Development and its site should be protected against the standard sea-flood risk level which is defined as the 1 in 100 year average return interval flood extreme sea level (tide, stormwater and associated wave effects combined), plus an allowance to accommodate land subsidence until the year 2100.
- 18 Development including associated roads and parking areas, other than minor structures unlikely to be adversely affected by flooding, should be protected from sea level rise by ensuring all of the following apply:
- (a) site levels are at least 0.3 metres above the standard sea-flood risk level
 - (b) building floor levels are at least 0.55 metres above the standard sea-flood risk level
 - (c) there are practical measures available to protect the development against an additional sea level rise of 0.7 metres, plus an allowance to accommodate land subsidence until the year 2100 at the site.
- 19 Buildings to be sited over tidal water or which are not capable of being raised or protected by flood protection measures in future, should have a floor level of at least 1.25 metres above the standard sea-flood risk level.
- 20 Development that requires protection measures against coastal erosion, sea or stormwater flooding, sand drift or the management of other coastal processes at the time of development, or in the future, should only be undertaken if all of the following apply:
- (a) the measures themselves will not have an adverse effect on coastal ecology, processes, conservation, public access and amenity.
 - (b) the measures do not nor will not require community resources, including land, to be committed.
 - (c) the risk of failure of measures such as sand management, levee banks, flood gates, valves or stormwater pumping, is acceptable relative to the potential hazard resulting from their failure.
 - (d) binding agreements are in place to cover future construction, operation, maintenance and management of the protection measures.
- 21 Unavoidable stormwater and effluent outfalls should be designed and located so as not to conflict with the objectives for coastal areas and if discharging across a beach do so at beach level from properly constructed pipes or channels.
- 22 Development should not compromise the structural integrity of any sea wall or levee bank adjacent to the foreshore, or compromise its capacity to protect against coastal flooding and erosion.

Erosion Buffers

- 23 Development should be set back a sufficient distance from the coast to provide an erosion buffer which will allow for at least 100 years of coastal retreat for single buildings or small scale developments, or 200 years of coastal retreat for large scale developments (ie new townships) unless either of the following applies:

- (a) the development incorporates appropriate private coastal protection measures to protect the development and public reserve from the anticipated erosion.
 - (b) the council is committed to protecting the public reserve and development from the anticipated coastal erosion.
- 24 Where a coastal reserve exists or is to be provided it should be increased in width by the amount of any required erosion buffer. The width of an erosion buffer should be based on the following:
- (a) the susceptibility of the coast to erosion
 - (b) local coastal processes
 - (c) the effect of severe storm events
 - (d) the effect of a 0.3 metres sea level rise over the next 50 years on coastal processes and storms
 - (e) the availability of practical measures to protect the development from erosion caused by a further sea level rise of 0.7 metres per 50 years thereafter.
- 25 Development should not occur where essential services cannot be economically provided and maintained having regard to flood risk and sea level rise, or where emergency vehicle access would be prevented by a 1 in 100 year average return interval flood event, adjusted for 100 years of sea level rise.

Land Division

- 26 Land in coastal areas should only be divided if:
- (a) it or the subsequent development and use of the land will not adversely affect the management of the land, adjoining land or the coast
 - (b) sand dunes, wetlands and substantially intact strata of native vegetation are maintained or consolidated within single allotments.
- 27 Land division in coastal areas outside of designated urban or settlement zones should not increase either of the following:
- (a) the number of allotments abutting the coast or a reserve
 - (b) the number of allotments, including community title allotments and those that incorporate rights of way, with direct access to the coast or a reserve.
- 28 Land should not be divided for commercial, industrial or residential purposes unless a layout can be achieved whereby roads, parking areas and development sites on each allotment are at least 0.3 metres above the standard sea-flood risk level, unless the land is, or can be provided with appropriate coastal protection measures.

Protection of Economic Resources

- 29 Development should be sited, designed and managed so as not to conflict with or jeopardise the continuance of an existing aquaculture development.

Development in Appropriate Locations

- 30 Development along the coast should be in the form of infill in existing developed areas or concentrated into appropriately chosen nodes and not be in a scattered or linear form.
- 31 Development of a kind or scale (eg commercial or large-scale retail) that does not require a coastal location and would not significantly contribute to the community's enjoyment of the coast should not be located in coastal areas.

Community Facilities

OBJECTIVES

- 1 Location of community facilities including social, health, welfare, education and recreation facilities where they are conveniently accessible to the population they serve.
- 2 The proper provision of public and community facilities including the reservation of suitable land in advance of need.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Community facilities should be sited and developed to be accessible by pedestrians, cyclists and public and community transport.
- 2 Community facilities should be integrated in their design to promote efficient land use.
- 3 Design of community facilities should encourage flexible and adaptable use of open space and facilities to meet the needs of a range of users over time.
- 4 Non-residential development of a local community nature such as health and welfare services, community centres, child care facilities, primary and secondary schools, recreation, public open space and similar facilities should only be developed within residential areas where they are of a nature and scale that serves a local area function and do not hinder the development and function of centres.

Crime Prevention

OBJECTIVES

- 1 A safe, secure, crime resistant environment where land uses are integrated and designed to facilitate community surveillance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be designed to maximise surveillance of public spaces through the incorporation of clear lines of sight, appropriate lighting and the use of visible permeable barriers wherever practicable.
- 2 Buildings should be designed to overlook public and communal streets and public open space to allow casual surveillance.
- 3 Development should provide a robust environment that is resistant to vandalism and graffiti.
- 4 Development should provide lighting in frequently used public spaces including those:
 - (a) along dedicated cyclist and pedestrian pathways, laneways and access routes
 - (b) around public facilities such as toilets, telephones, bus stops, seating, litter bins, automatic teller machines, taxi ranks and car parks.
- 5 Development, including car park facilities should incorporate signage and lighting that indicate the entrances and pathways to, from and within sites.
- 6 Landscaping should be used to assist in discouraging crime by:
 - (a) screen planting areas susceptible to vandalism
 - (b) planting trees or ground covers, rather than shrubs, alongside footpaths
 - (c) planting vegetation other than ground covers a minimum distance of two metres from footpaths to reduce concealment opportunities.
- 7 Buildings and street addresses should be easily identified to assist orientation.
- 8 Site planning, buildings, fences, landscaping and other features should clearly differentiate public, communal and private areas.
- 9 Buildings should be designed to minimise and discourage access between roofs, balconies and windows of adjoining dwellings.
- 10 Public toilets should be located, sited and designed:
 - (a) to promote the visibility of people entering and exiting the facility (eg by avoiding recessed entrances and dense shrubbery that obstructs passive surveillance)
 - (b) near public and community transport links and pedestrian and cyclist networks to maximise visibility.
- 11 Development should avoid pedestrian entrapment spots and movement predictors (eg routes or paths that are predictable or unchangeable and offer no choice to pedestrians).
- 12 Service lanes and alleyways should be designed and located to maximise community safety.
- 13 Development should provide a clear separation between the private and public domain.
- 14 Development should be designed and managed to ensure that users are aware of how to safely gain access to, around, and within the development, site or locality.

Design and Appearance

OBJECTIVES

- 1 Development of a high architectural standard and appearance that responds to and reinforces positive aspects of the local environment and built form.
- 2 Roads, open spaces, paths, buildings and land uses laid out and linked so that they are easy to understand and navigate.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Buildings should reflect the desired character of the locality while incorporating contemporary designs that have regard to the following:
 - (a) building height, mass and proportion
 - (b) external materials, patterns, colours and decorative elements
 - (c) roof form and pitch
 - (d) façade articulation and detailing
 - (e) verandas, eaves, parapets and window screens.
- 2 Where a building is sited on or close to a side or rear boundary, the boundary wall should minimise:
 - (a) the visual impact of the building as viewed from adjoining properties
 - (b) overshadowing of adjoining properties and allow adequate sunlight access to neighbouring buildings.
- 3 The external walls and roofs of buildings should not incorporate highly reflective materials which will result in glare to neighbouring properties, drivers or cyclists.
- 4 Structures located on the roofs of buildings to house plant and equipment should be screened from view and form an integral part of the building design in relation to external finishes, shaping and colours.
- 5 Balconies should:
 - (a) be integrated with the overall form and detail of the building
 - (b) include balustrade detailing that enables line of sight to the street
 - (c) be recessed where wind would otherwise make the space unusable
 - (d) be self-draining and plumbed to minimise runoff.
- 6 Transportable buildings and buildings which are elevated on stumps, posts, piers, columns or the like, should have their suspended footings enclosed around the perimeter of the building, and the use of verandas, pergolas and other suitable architectural detailing to give the appearance of a permanent structure.

Development Adjacent Heritage Places

- 7 The design of multi-storey buildings should not detract from the form and materials of adjacent State and Local heritage places listed in [Table ChSt/7 – State Heritage Places](#) or in [Table ChSt/6 – Local Heritage Places](#).
- 8 Development on land adjacent to a State or Local heritage place, as listed in [Table ChSt/7 – State Heritage Places](#) or in [Table ChSt/6 – Local Heritage Places](#), should be sited and designed to reinforce the historic character of the place and maintain its visual prominence.

Overshadowing

- 9 The design and location of buildings should enable direct winter sunlight into adjacent dwellings and private open space and minimise overshadowing of:
 - (a) windows of main internal living areas
 - (b) ground level private open space
 - (c) upper-level private balconies that provide the primary open space area for dwelling
 - (d) solar collectors (such as solar hot water systems and photovoltaic cells).
- 10 Except where specified in a zone, policy area or precinct, development should ensure that:
 - (a) north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9 am and 3 pm on the 21 June.
 - (b) ground level open space of existing buildings receives direct sunlight for a minimum of 2 hours between 9 am and 3 pm on 21 June to at least the smaller of the following:
 - (i) half of the existing ground level open space
 - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres).
 - (c) where overshadowing already exceeds the requirements contained in part (b), the area overshadowed should not increase by more than 20 per cent.

Visual Privacy

- 11 Development should minimise direct overlooking of habitable rooms and private open spaces of dwellings through one or more of the following measures:
 - (a) off-setting the location of balconies and windows of habitable rooms with those of other buildings so that views are oblique rather than direct
 - (b) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms
 - (c) screening devices (including fencing, obscure glazing, screens, external ventilation blinds, window hoods and shutters) that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity
 - (d) other than within the **Urban Core Zone**, sill heights of not less than 1.5 metres or permanent screens having a height of 1.5 metres above finished floor level.
- 12 Permanently fixed external screening devices should be designed and coloured to complement the associated building's external materials and finishes.

Relationship to the Street and Public Realm

- 13 Buildings (other than ancillary buildings, group dwellings or buildings on allotments with a battle axe configuration) should be designed so that their main façade faces the primary street frontage of the land on which they are situated.
- 14 Buildings, landscaping, paving and signage should have a coordinated appearance that maintains and enhances the visual attractiveness of the locality.
- 15 Buildings should be designed and sited to avoid extensive areas of uninterrupted walling facing areas exposed to public view.
- 16 Building design should emphasise pedestrian entry points to provide perceptible and direct access from public street frontages and vehicle parking areas.
- 17 The ground floors of mixed use buildings should comprise non-residential land uses.
- 18 In mixed use areas, development facing the street should be designed to activate the street frontage(s) by:
 - (a) including features that attract people to the locality such as frequent doors and display windows, retail shopfronts and outdoor eating or dining areas
 - (b) minimising the frontage for fire escapes, service doors, plant and equipment hatches
 - (c) avoiding undercroft or ground floor vehicle parking that is visible from the primary street frontage
 - (d) using colour, vertical and horizontal elements, roof overhangs and other design techniques to provide visual interest and reduced massing.
- 19 Where zero or minor setbacks are desirable, development should incorporate shelter over footpaths to enhance the quality of the pedestrian environment.

Outdoor Storage and Service Areas

- 20 Outdoor storage, loading and service areas should be:
 - (a) screened from public view by a combination of built form, solid fencing and/or landscaping.
 - (b) conveniently located and designed to enable the manoeuvring of service and delivery vehicles
 - (c) sited away from sensitive land uses.
- 21 Adequate access should be provided to the rear of any site for servicing purposes, especially where a building does not extend to the rear boundary.

Building Setbacks from Road Boundaries

- 22 Except in areas where a new character is desired, the setback of buildings from public roads should:
 - (a) be similar to, or compatible with, setbacks of buildings on adjoining land and other buildings in the locality
 - (b) contribute positively to the function, appearance and/ or desired character of the locality

- 23 Except where specified in a particular zone, policy area or precinct, the main face of a building should be set back from the primary road frontage in accordance with the following table:

Setback difference between buildings on adjacent allotments	Setback of new building
Up to 2 metres	The same setback as one of the adjacent buildings, as illustrated below:
<i>When $b - a \leq 2$, setback of new dwelling = a or b</i>	
Greater than 2 metres	At least the average setback of the adjacent buildings.

- 24 Except in areas where a new character is desired or where specified in a zone, policy area or precinct, the setback of development from a secondary street frontage should reflect the setbacks of the adjoining buildings and other buildings in the locality.
- 25 All setbacks from the road frontage should be additional to the road widening setback established under the *Metropolitan Adelaide Road Widening Plan Act 1972*.
- 26 No development should be undertaken which would impair, disfigure, or interfere with the amenity, aesthetic appearance or scenic beauty of any of the following:
- (a) the River Torrens
 - (b) land within 60 metres of the top of the bank of the River Torrens
 - (c) the landscape visible from the River Torrens.

Energy Efficiency

OBJECTIVES

- 1 Development designed and sited to conserve energy and minimise waste.
- 2 Development that provides for on-site power generation including photovoltaic cells and wind power.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should provide for efficient solar access to buildings and open space all year around.
- 2 Buildings should be sited and designed so that the open spaces associated with the main activity areas face north for exposure to winter sun.
- 3 Buildings should be sited and designed to ensure adequate natural light and winter sunlight is available to the main activity areas of adjacent buildings.
- 4 Roof pitches should facilitate the efficient use of solar hot water services and photovoltaic cells.
- 5 Development should be designed to minimise consumption of non-renewable energy through designing the roof of buildings with a north facing slope to accommodate solar collectors.
- 6 Public infrastructure, including lighting and telephones, should be designed to generate and use renewable energy.

Hazards

OBJECTIVES

- 1 Maintenance of the natural environment and systems by limiting development in areas susceptible to natural hazard risk.
- 2 Development located away from areas that are vulnerable to, and cannot be adequately and effectively protected from the risk of natural hazards.
- 3 Development located to minimise the threat and impact of bushfires on life and property.
- 4 Expansion of existing non-rural uses directed away from areas of high bushfire risk.
- 5 Critical community facilities such as hospitals, emergency control centres, major service infrastructure facilities, and emergency service facilities located where they are not exposed to natural hazard risks.
- 6 The environmental values and ecological health of receiving waterways and marine environments protected from the release of acid water resulting from the disturbance of acid sulphate soils.
- 7 Protection of human health and the environment wherever site contamination has been identified or suspected to have occurred.
- 8 Appropriate assessment and remediation of site contamination to ensure land is suitable for the proposed use and provides a safe and healthy living and working environment.
- 9 Minimisation of harm to life, property and the environment through appropriate location of development and appropriate storage, containment and handling of hazardous materials.
- 10 Development that does not cause land, air or water contamination.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should:
 - (a) be excluded from areas that are vulnerable to, and cannot be adequately and effectively protected from, the risk of natural hazards
 - (b) be sited, designed and undertaken with appropriate precautions being taken against fire, flood, coastal flooding, storm surge, landslip, earthquake, toxic emissions or other hazards such as vermin
 - (c) not occur on land where the risk of flooding is likely to be harmful to safety or damage property.
- 2 There should not be any significant interference with natural processes in order to reduce the exposure of development to the risk of natural hazards.
- 3 The location of critical community facilities or key infrastructure in areas of high natural hazard risk should be avoided.
- 4 Buildings should be erected on stable land.

Flooding

- 5 Development should not be undertaken in areas liable to inundation by tidal, drainage or flood waters unless the development can achieve all of the following:

- (a) it is developed with a public stormwater system capable of catering for a 1 in 100 year average return interval flood event
 - (b) buildings are designed and constructed to prevent the entry of floodwaters in a 1 in 100 year average return interval flood event.
- 6 Development, including earthworks associated with development, should not do any of the following:
- (a) impede the flow of floodwaters through the land or other surrounding land
 - (b) occur on land where the risk of flooding is unacceptable having regard to personal and public safety and to property damage
 - (c) increase the potential hazard risk to public safety of persons during a flood event
 - (d) aggravate the potential for erosion or siltation or lead to the destruction of vegetation during a flood
 - (e) cause any adverse effect on the floodway function
 - (f) increase the risk of flooding of other land
 - (g) obstruct a watercourse.

Salinity

- 7 Development should not increase the potential for, or result in an increase in, soil and water salinity.
- 8 Preservation, maintenance and restoration of locally indigenous plant species should be encouraged in areas affected by dry land salinity.
- 9 Irrigated horticulture and pasture should not increase groundwater-induced salinity.

Acid Sulfate Soils

- 10 Development and activities, including excavation and filling of land, that may lead to the disturbance of potential or actual acid sulfate soils should be avoided unless such disturbances are managed in a way that effectively avoids the potential for harm or damage to any of the following:
- (a) the marine and estuarine environment
 - (b) natural water bodies and wetlands
 - (c) agricultural or aquaculture activities
 - (d) buildings, structures and infrastructure
 - (e) public health.
- 11 Development, including primary production, aquaculture activities and infrastructure, should not proceed unless it can be demonstrated that the risk of releasing acid water resulting from the disturbance of acid sulfate soils is minimal.

Site Contamination

- 12 Development, including land division, should not occur on contaminated land or on potentially contaminated land unless either of the following applies:
- (a) remediation of the site is undertaken to a standard that makes it suitable and safe for the proposed use

- (b) the site will be maintained in a condition, or the development will be undertaken in a manner, that will not pose a threat to the health and safety of the environment or to occupiers of the site or land in the locality.

Containment of Chemical and Hazardous Materials

- 13 Hazardous materials should be stored and contained in a manner that minimises the risk to public health and safety and the potential for water, land or air contamination.
- 14 Development that involves the storage and handling of hazardous materials should ensure that these are contained in designated areas that are secure, readily accessible to emergency vehicles, impervious, protected from rain and stormwater intrusion and other measures necessary to prevent:
 - (a) discharge of polluted water from the site
 - (b) contamination of land
 - (c) airborne migration of pollutants
 - (d) potential interface impacts with sensitive land uses.

Landslip

- 15 Development, including associated cut and fill activities, should not lead to an increased danger from land surface instability or to the potential of landslip occurring on the site or on surrounding land.
- 16 Development on steep slopes should promote the retention and replanting of vegetation as a means of stabilising and reducing the possibility of surface movement or disturbance.
- 17 Development in areas susceptible to landslip should:
 - (a) incorporate split level designs to minimise cutting into the slope
 - (b) ensure that cut and fill and heights of faces are minimised
 - (c) ensure cut and fill is supported with engineered retaining walls or are battered to appropriate grades
 - (d) control any erosion that will increase the gradient of the slope and decrease stability
 - (e) ensure the siting and operation of an effluent drainage field does not contribute to landslip
 - (f) provide drainage measures to ensure surface stability is not compromised
 - (g) ensure natural drainage lines are not obstructed.

Heritage Conservation

OBJECTIVES

- 1 The conservation of areas, places and their settings of indigenous and non-indigenous cultural or natural significance.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should conserve and not adversely impact on the cultural or natural significance of places, areas, artefacts and shipwrecks that display any of the following values:
 - (a) aesthetic
 - (b) anthropological
 - (c) archaeological
 - (d) architectural
 - (e) ecological
 - (f) economic
 - (g) educational
 - (h) geological
 - (i) historic
 - (j) palaeontologic
 - (k) scientific
 - (l) social
 - (m) speleological
 - (n) spiritual
 - (o) technological.
- 2 Advertisements and/or advertising hoardings associated with culturally significant places and areas should:
 - (a) be of a size, colour, shape and materials that enhances the character of the locality
 - (b) not dominate or cause detracton from the prominence of any place and/or area of historic significance.

Heritage Places

OBJECTIVES

- 1 The conservation of State and local heritage places.
- 2 The continued use, or adaptive re-use of State and local heritage places that supports the conservation of their cultural significance.
- 3 Conservation of the setting of State and local heritage places.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 A heritage place spatially located on *Overlay Maps - Heritage* and more specifically identified in [Table ChSt/7 - State Heritage Places](#) or in [Table ChSt/6 - Local Heritage Places](#) should not be demolished, destroyed or removed, in total or in part, unless either of the following apply:
 - (a) that portion of the place to be demolished, destroyed or removed is excluded from the extent of the places identified in the Tables
 - (b) the structural condition of the place represents an unacceptable risk to public or private safety
 - (c) the demolition of the place is required for the implementation of the South Road/Port Road Upgrade Project.
- 2 Development of Local heritage places should be carried out, where applicable, in accordance with the design guidelines contained in [Table ChSt/4 - Development Guidelines for Residential Character Zone and Local Heritage Places](#).
- 3 Development of a State or local heritage place should retain those elements contributing to its heritage value, which may include (but not be limited to):
 - (a) principal elevations
 - (b) important vistas and views to and from the place
 - (c) setting and setbacks
 - (d) building materials
 - (e) outbuildings and walls
 - (f) trees and other landscaping elements
 - (g) access conditions (driveway form/width/material)
 - (h) architectural treatments
 - (i) the use of the place.
- 4 Development of a State or local heritage place should be compatible with the heritage value of the place.
- 5 Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places should be preserved, unpainted.

- 6 New buildings should not be placed or erected between the front street boundary and the façade of existing State or local heritage places.
- 7 Development that materially affects the context within which the heritage place is situated should be compatible with the heritage place. It is not necessary to replicate historic detailing, however design elements that should be compatible include, but are not limited to:
 - (a) scale and bulk
 - (b) width of frontage
 - (c) boundary setback patterns
 - (d) proportion and composition of design elements such as rooflines, openings, fencing and landscaping
 - (e) colour and texture of external materials.
- 8 Multi-storey additions to a State or local heritage place should be compatible with the heritage value of the place through a range of design solutions such as:
 - (a) extending into the existing roof space or to the rear of the building
 - (b) retaining the elements that contribute to the building's value
 - (c) distinguishing between the existing and new portion of the building
 - (d) stepping in parts of the building that are taller than the front facade.
- 9 The introduction of advertisements and signage to a State or local heritage place should:
 - (a) be placed on discrete elements of its architecture such as parapets and wall panels, below the canopy, or within fascias and infill end panels and windows
 - (b) not conceal or obstruct historical detailing of the heritage place
 - (c) not project beyond the silhouette or skyline of the heritage place
 - (d) not form a dominant element of the place.
- 10 The division of land adjacent to or containing a State or local heritage place should occur only where it will:
 - (a) create an allotment pattern that maintains or reinforces the integrity of the heritage place and the character of the surrounding area
 - (b) create an allotment or allotments of a size and dimension that can accommodate new development that will reinforce and complement the heritage place and the zone or policy area generally
 - (c) be of a size and dimension that will enable the siting and setback of new buildings from allotment boundaries so that they do not overshadow, dominate, encroach on or otherwise impact on the setting of the heritage place
 - (d) provide an area for landscaping of a size and dimension that complements the landscape setting of the heritage place and the landscape character of the locality
 - (e) enable the State or local heritage place to have a curtilage of a size sufficient to protect its setting.

Historic Conservation Area

Refer to *Overlay Maps - Heritage* (ChSt/4, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/20, ChSt/21, ChSt/22) to identify the Historic Conservation Area.

OBJECTIVES

- 1 The conservation of areas of historical significance.
- 2 Development that promotes, conserves and enhances the cultural significance and historic character of identified places and areas.
- 3 Development that complements the historic significance of the area.
- 4 The retention and conservation of places such as land, buildings, structures and landscape elements that contribute positively to the historic character of the area.
- 5 Development that contributes to the desired character.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the area.
- 2 Development within the Historic Conservation Area should be carried out, where applicable, in accordance with the design guidelines contained in [Table ChSt/4 - Development Guidelines for Residential Character Zone and Local Heritage Places](#).
- 3 Places such as land, buildings, structures and landscape elements that contribute to the historic character of the area identified on the *Overlay Maps – Heritage* and more specifically identified in the respective [Table ChSt/5 - Contributory items](#), should be retained and conserved.
- 4 The elements of a contributory item (including a fence or outbuilding or structure) that contribute to the historic character of the locality should not be demolished or removed unless one of the following applies:
 - (a) the integrity or prominence of the contributory item (when viewed from a public street or place) would not be affected
 - (b) their condition is seriously unsound and cannot reasonably be rehabilitated
 - (c) a replacement development can be erected which does not diminish the level of contribution to the historic character of the locality and the replacement has been approved, or is concurrently approved, by the planning authority.
- 5 Development should:
 - (a) restore and rehabilitate existing places of historic value
 - (b) incorporate compact extensions to buildings of a scale, bulk and external appearance that are sympathetic and compatible with the historic character of the places
 - (c) conserve, enhance and reinforce the historic significance of buildings and streetscapes.

- 6 Development of a contributory item should:
 - (a) not compromise its value to the historic significance of the area
 - (b) retain its present integrity or restore its original design features
 - (c) maintain or enhance the prominence of the original street facade
 - (d) facilitate or support the continued use or adaptive re use of existing contributory places in a manner that results in a scale, bulk and external appearance that is sympathetic to and reflective of the existing buildings and the surrounding area.
- 7 Additions to a contributory item should be screened by the original building and/or located to the rear of the original building.
- 8 Original unpainted plaster, brickwork, stonework, or other masonry of existing State or local heritage places or contributory items should be preserved, unpainted.
- 9 Development should take design cues from the existing historic built forms. In doing this, it is not necessary to replicate historic detailing; however design elements for consideration should be compatible with building and streetscape character and should include but not be limited to:
 - (a) scale and bulk
 - (b) width of frontage
 - (c) boundary setback patterns
 - (d) proportion and composition of design elements such as roof lines, pitches, openings, fencing and landscaping
 - (e) colour and texture of external materials
 - (f) visual interest.
- 10 Garages and carports should be designed to maintain the prominence of existing historic buildings in the policy area and to not dominate the appearance of the front elevation of the associated building.
- 11 New buildings should not be placed or erected between the front street boundary and the façade of existing State or local heritage places or contributory items.
- 12 Front fences, walls and gates should:
 - (a) reflect and conserve the traditional period, style and form of the associated building
 - (b) be no more than 1 metre in height unless they are of an open design to enable informal overlooking of public places and to enable the associated building to be viewed from the street.
- 13 Single storey dwellings may include any of the following elements in their design:
 - (a) a sympathetic two-storey addition that uses existing roof space or incorporates minor extensions to the roof space at the rear of the dwelling
 - (b) a second storey within the roof space, where the overall building height, scale and form is compatible with existing single-storey development in the policy area
 - (c) second storey windows having a total length less than 30 per cent of the total roof length along each elevation.

- 14 New residential development should include landscaped front garden areas that complement the desired character.
- 15 Advertisements and/or advertising hoardings associated with culturally significant places and areas should:
 - (a) be of a size, colour, shape and materials that enhances the character of the locality
 - (b) not dominate or detract from the prominence of any place and/or area of historic significance.
- 16 Development should respect the existing topography and the relationship of sites to street levels and to adjoining land and not involve substantial cut and/or fill or sites.
- 17 The division of land should occur only where it will maintain the traditional pattern and scale of allotments.

Industrial Development

OBJECTIVES

- 1 Industrial, warehouse, storage, commercial and transport distribution development on appropriately located land, integrated with transport networks and designed to minimise potential impact on these networks.
- 2 Industrially zoned allotments and uses protected from encroachment by adjoining uses that would reduce industrial development or expansion.
- 3 Industrial development occurring without adverse effects on the health and amenity of occupiers of land in adjoining zones.
- 4 Compatibility between industrial uses within industrial zones.
- 5 The improved amenity of industrial areas.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Offices and showrooms associated with industrial, warehouse, storage, commercial and transport development should be sited at the front of the building with direct and convenient pedestrian access from the main visitor parking area.
- 2 Industrial development should be adequately separated from adjoining land uses where the development is likely to cause significant adverse impact on adjoining land uses.
- 3 Industrial development should enable all vehicles to enter and exit the site in a forward direction.
- 4 Industrial development abutting an arterial road, a non-industrial zone boundary, or public open space should be developed in a manner that does not create adverse visual impacts on the locality.
- 5 Building facades facing a non-industrial zone, public road, or public open space should:
 - (a) use a variety of building finishes
 - (b) not consist solely of metal cladding
 - (c) contain materials of low reflectivity
 - (d) incorporate design elements to add visual interest
 - (e) avoid large expanses of blank walls.
- 6 Industrial development should minimise significant adverse impact on adjoining uses due to hours of operation, traffic, noise, fumes, smell, dust, paint or other chemical over-spray, vibration, glare or light spill, electronic interference, ash or other harmful or nuisance-creating impacts.
- 7 Landscaping should be incorporated as an integral element of industrial development along non-industrial zone boundaries.
- 8 Fencing (including colour-coated wire mesh fencing) adjacent to public roads should be set back in one of the following ways:
 - (a) in line with the building façade
 - (b) behind the building line

- (c) behind a landscaped area that softens its visual impact.
- 9 Marine aquaculture onshore storage, cooling and processing facilities should not impair the coastline and its visual amenity and should:
- (a) be sited, designed, landscaped and developed at a scale and using external materials that minimise any adverse visual impact on the coastal landscape
 - (b) be sited and designed with appropriate vehicular access arrangement
 - (c) include appropriate waste treatment and disposal.

Infrastructure

OBJECTIVES

- 1 Infrastructure provided in an economical and environmentally sensitive manner.
- 2 Infrastructure, including social infrastructure, provided in advance of need.
- 3 Suitable land for infrastructure identified and set aside in advance of need.
- 4 The visual impact of infrastructure facilities minimised.
- 5 The efficient and cost-effective use of existing infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not occur without the provision of adequate utilities and services, including:
 - (a) electricity supply
 - (b) water supply
 - (c) drainage and stormwater systems
 - (d) waste disposal
 - (e) effluent disposal systems
 - (f) formed all-weather public roads
 - (g) telecommunications services
 - (h) social infrastructure, community services and facilities
 - (i) gas services.
- 2 Development should only occur only where it provides, or has access to, relevant easements for the supply of infrastructure.
- 3 Development should incorporate provision for the supply of infrastructure services to be located within common service trenches where practicable.
- 4 Development should not take place until adequate and coordinated drainage of the land is assured.
- 5 Development in urban areas should not occur without provision of an adequate reticulated domestic quality mains water supply and an appropriate waste treatment system.
- 6 In areas where no reticulated water supply is available, buildings whose usage is reliant on a water supply should be equipped with an adequate and reliable on-site water storage system.
- 7 **Service** infrastructure should be designed and located to minimise its visual and environmental impacts.
- 8 In urban areas, electricity supply (except transmission lines) serving new development should be installed underground, excluding lines having a capacity greater than or equal to 33kV.

- 9 Utilities and services, including access roads and tracks, should be sited on areas already cleared of native vegetation. If this is not possible, their siting should cause minimal interference or disturbance to existing native vegetation and biodiversity.
- 10 Utility buildings and structures should be grouped with non-residential development where possible.
- 11 Development in proximity to infrastructure facilities should be sited and be of a scale to ensure adequate separation to protect people and property.
- 12 Incompatible uses should not encroach upon the easements of infrastructure corridors for existing and proposed transmission lines.
- 13 Provision should be made for new transmission and distribution substations and overhead major electricity line corridors (having a capacity greater than or equal to 33kV) in areas which have the required buffer distance to protect people and allow for adequate access.
- 14 Land division for the purpose of residential and other sensitive land uses should not occur within electricity line corridors or existing electricity easements unless the resultant allotments are large enough to enable accommodation of safe clearances and expected structures.

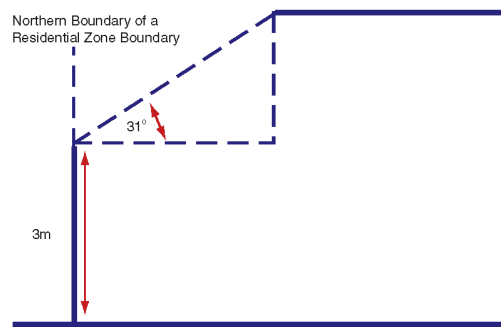
Interface between Land Uses

OBJECTIVES

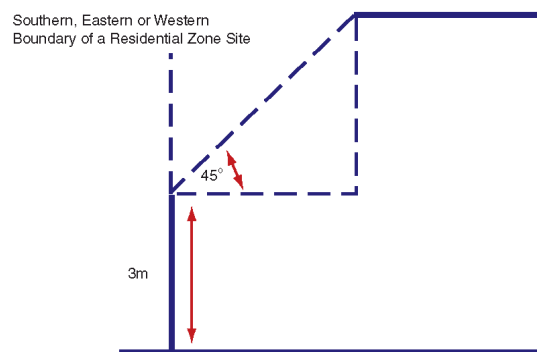
- 1 Development located and designed to minimise adverse impact and conflict between land uses.
- 2 Protect community health and amenity from adverse impacts of development.
- 3 Protect desired land uses from the encroachment of incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

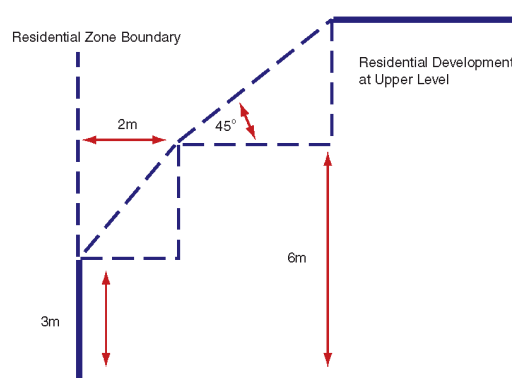
- 1 Development should not detrimentally affect the amenity of the locality or cause unreasonable interference through any of the following:
 - (a) the emission of effluent, odour, smoke, fumes, dust or other airborne pollutants
 - (b) noise
 - (c) vibration
 - (d) electrical interference
 - (e) light spill
 - (f) glare
 - (g) hours of operation
 - (h) traffic impacts.
- 2 Development should be sited and designed to minimise negative impact on existing and potential future land uses desired in the locality.
- 3 Development adjacent to a Residential Zone or residential area should be designed to minimise overlooking and overshadowing of adjacent dwellings and private open space.
- 4 Residential development adjacent to non-residential zones and land uses should be located, designed and/or sited to protect residents from potential adverse impacts from non-residential activities.
- 5 Sensitive uses likely to conflict with the continuation of lawfully existing developments and land uses desired for the zone should be designed to minimise negative impacts.
- 6 Non-residential development on land abutting a residential zone should be designed to minimise noise impacts to achieve adequate levels of compatibility between existing and proposed uses.
- 7 Where the site of a building located in a centre zone or **Suburban Activity Node Zone**:
 - (a) abuts the northern boundary of a site in a zone where residential development is envisaged, building development should not intrude into a plane angled 31 degrees above the horizontal into the site, as measured from a point 3 metres above the boundary, as shown in the figure below:



- (b) abuts the southern, eastern or western boundary of a site in a zone where residential development is envisaged, building development should not intrude into a plane angled 45 degrees above the horizontal into the site, as measured from a point 3 metres above the boundary, as shown in the figure below:



- (c) contains wholly residential use or residential use at an upper level and abuts the side boundary of a residentially zoned site, then notwithstanding (a) or (b) above, such upper level residential development should not intrude into an envelope measured from 3 metres above the boundary, extending to 6 metres above natural ground level at a point 2 metres from the boundary, and then extending in a plane angled 45 degrees above the horizontal into the site, as shown in the figure below:



Noise Generating Activities

- 8 Development that emits noise (other than music noise) should include noise attenuation measures that achieve the relevant *Environment Protection (Noise) Policy* criteria when assessed at the nearest existing noise sensitive premises.
- 9 Development with the potential to emit significant noise (e.g. industry) should incorporate noise attenuation measures that prevent noise from causing unreasonable interference with the amenity of noise sensitive premises.

- 10 Outdoor areas (such as beer gardens or dining areas) associated with licensed premises should be designed or sited to minimise adverse noise impacts on adjacent existing or future noise sensitive development.
- 11 Development proposing music should include noise attenuation measures that achieve the following desired noise levels:

Noise level assessment location	Desired noise level
Adjacent existing <i>noise sensitive development</i> property boundary	Less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and Less than 5 dB(A) above the level of background noise ($LA_{90,15min}$) for the overall (sum of all octave bands) A-weighted level.
Adjacent <i>land</i> property boundary	Less than 65dB(Lin) at 63Hz and 70dB(Lin) in all other octave bands of the sound spectrum or less than 8 dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum and 5 dB(A) overall (sum of all octave bands) A-weighted level.

Air quality

- 12 Development with the potential to emit harmful or nuisance-generating air pollution should incorporate air pollution control measures to prevent harm to human health or unreasonable interference with the amenity of sensitive uses within the locality.
- 13 Chimneys or exhaust flues associated with commercial development (including cafes, restaurants and fast food outlets) should be designed to ensure they do not cause a nuisance or health concerns to nearby sensitive receivers by:
- (a) incorporating appropriate treatment technology before exhaust emission are released to the atmosphere
 - (b) ensuring that the location and design of chimneys or exhaust flues maximises dispersion and takes into account the location of nearby sensitive uses.

Land Division

OBJECTIVES

- 1 Land division that occurs in an orderly sequence allowing efficient provision of new infrastructure and facilities and making optimum use of existing under utilised infrastructure and facilities.
- 2 Land division that creates allotments appropriate for the intended use.
- 3 Land division layout that is optimal for energy efficient building orientation.
- 4 Land division that is integrated with site features, including landscape and environmental features, adjacent land uses, the existing transport network and the availability of infrastructure.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 When land is divided:
 - (a) stormwater should be capable of being drained safely and efficiently from each proposed allotment and disposed of from the land in an environmentally sensitive manner
 - (b) a sufficient water supply should be made available for each allotment
 - (c) provision should be made for the disposal of wastewater, sewage and other effluent from each allotment without risk to health
 - (d) proposed roads should be graded, or be capable of being graded to connect safely and conveniently with an existing road or thoroughfare.
- 2 Land should not be divided if any of the following apply:
 - (a) the size, shape, location, slope or nature of the land makes any of the allotments unsuitable for the intended use
 - (b) any allotment will not have a frontage to an existing or proposed public road
 - (c) the intended use of the land would require excessive cut and fill
 - (d) the intended use, or the establishment of that use, is likely to lead to undue erosion of the subject land or land within the locality
 - (e) the area is unsewered and cannot accommodate an appropriate waste disposal system within the allotment to suit the intended development
 - (f) the intended use of the land would be contrary to the zone objectives
 - (g) any single allotments are created that sit within more than one zone
 - (h) the allotments unreasonably restrict access to publicly owned land such as recreation areas
 - (i) the satisfactory future division of the land or any part thereof cannot be achieved
 - (j) if it would cause an infringement of any provisions of the Building Code.

Design and Layout

- 3 Land divisions should be designed to ensure that areas of native vegetation and wetlands do not need to be cleared as a consequence of subsequent development or fragmented or reduced in size.
- 4 The design of a land division should incorporate:
 - (a) roads, thoroughfares and open space that result in safe and convenient linkages with the surrounding environment, including public and community transport facilities, and which, where necessary, facilitate the satisfactory future division of land and the inter-communication with neighbouring localities
 - (b) new road and allotment access points providing appropriate separation distances from existing road junctions or level crossings
 - (c) safe and convenient access from each allotment to an existing or proposed public road or thoroughfare
 - (d) areas to provide appropriate separation distances between potentially conflicting land uses and/or zones
 - (e) suitable land set aside for useable local open space
 - (f) public utility services within road reserves and where necessary within dedicated easements
 - (g) the preservation of significant natural, cultural or landscape features including State and local heritage places
 - (h) protection for existing vegetation and drainage lines
 - (i) where appropriate, the amalgamation of smaller allotments to ensure coordinated and efficient site development
 - (j) the preservation of significant trees.
- 5 Land division should result in allotments of a size suitable for their intended use.
- 6 Land division should facilitate optimum solar access for energy efficiency.
- 7 Allotments in the form of a battleaxe configuration should:
 - (a) have a site area of at least the minimum specified for the zone or policy area (excluding the area of the 'handle' of such an allotment)
 - (b) provide for an access onto a public road, with the driveway 'handle' being not less than
 - (i) 4 metres in width, which includes space for landscaping of not less than 1 metre in width
 - (ii) 5.5 metres for at least the first 6 metres of the driveway for an allotment accommodating four or more dwellings or where the allotment has frontage to Primary and Secondary Arterial Roads shown on *Overlay Map ChSt/1 – Transport*
 - (c) contain sufficient area on the allotment for a vehicle to turn around to enable it to egress the allotment in a forward direction
 - (d) not be created where it would lead to multiple access points onto a road which would dominate or adversely affect the amenity of the streetscape
 - (e) be avoided where their creation would be incompatible with the prevailing pattern of development.

- 8 Allotments should have an orientation, size and configuration to encourage development that:
 - (a) minimises the need for earthworks and retaining walls
 - (b) maintains natural drainage systems
 - (c) faces abutting streets and open spaces
 - (d) does not require the removal of existing native vegetation to facilitate that development
 - (e) will not overshadow, dominate, encroach on or otherwise detrimentally affect the setting of the surrounding locality.
- 9 The layout of a land division should provide for efficient solar access.
- 10 Where the land to be divided borders a river, lake, wetland or creek, the land adjoining the bank should become public open space and linked with an existing or proposed pedestrian or transport network.
- 11 Land division should make provision for a reserve or an area of open space that incorporates land within the 1 in 100 year average return interval flood event area and being in any event at least 30 metres wide from the top of the bank of a watercourse.
- 12 The layout of a land division should keep flood-prone land free from development.
- 13 The arrangement of roads, allotments, reserves and open space should enable the provision of a storm drainage system that:
 - (a) creates, contains and retains all watercourses, drainage lines and native vegetation
 - (b) incorporates retention and/or detention devices to maintain the volume and rate of run-off as near as possible to pre-development levels
 - (c) enhances amenity
 - (d) integrates with the open space system and surrounding area.

Roads and Access

- 14 Road reserves should be of a width and alignment that can:
 - (a) provide for safe and convenient movement and parking of projected volumes and type of vehicles and other users
 - (b) provide for footpaths, cycle lanes and shared-use paths for the safety and convenience of residents and visitors
 - (c) allow vehicles to enter or reverse from an allotment or site in a single movement allowing for a car parked on the opposite side of the street
 - (d) accommodate street tree planting, landscaping and street furniture
 - (e) accommodate the location, construction and maintenance of stormwater drainage and public utilities
 - (f) provide unobstructed, safe and efficient vehicular access to individual allotments and sites
 - (g) allow for the efficient movement of service and emergency vehicles.
 - (h) accommodate the provision of services underground.

- 15 The design of the land division should facilitate the most direct route to local facilities for pedestrians and cyclists and enable footpaths, cycle lanes and shared-use paths to be provided of a safe and suitable width and reasonable longitudinal gradient.
- 16 The layout of land divisions should result in roads designed and constructed to ensure:
 - (a) that traffic speeds and volumes are restricted where appropriate by limiting street length and/or the distance between bends and slow points
 - (b) there are adequate sight distances for motorists at intersections, junctions, pedestrian and cyclist crossings, and crossovers to allotments to ensure the safety of all road users and pedestrians
 - (c) that existing dedicated cycling and walking routes are not compromised.
- 17 The design of the land division should provide space sufficient for on-street visitor car parking for the number and size of allotments, taking account of:
 - (a) the size of proposed allotments and sites and opportunities for on-site parking
 - (b) the availability and frequency of public and community transport
 - (c) on-street parking demand likely to be generated by nearby uses
 - (d) except in the **Urban Core Zone**, at least one on-street visitor car parking space being provided for every two residential allotments
 - (e) on-street visitor car parking being located as close as is practicable to the allotments they serve.
- 18 The layout of land divisions should incorporate street patterns designed to enhance the efficient movement of traffic and minimise trip lengths.
- 19 Except in the **Urban Core Zone**, no residential allotment should be created where its primary frontage would be to a road reserve with an overall width less than 8 metres in width.

Landscaping, Fences and Walls

OBJECTIVES

- 1 The amenity of land and development enhanced with appropriate planting and other landscaping works, using locally indigenous plant species where possible.
- 2 Functional fences and walls that enhance the attractiveness of development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should incorporate open space and landscaping in order to:
 - (a) complement built form and reduce the visual impact of larger buildings (eg taller and broader plantings against taller and bulkier building components)
 - (b) enhance the appearance of road frontages
 - (c) screen service yards, loading areas and outdoor storage areas
 - (d) minimise maintenance and watering requirements
 - (e) enhance and define outdoor spaces, including car parking areas
 - (f) maximise shade and shelter
 - (g) assist in climate control within buildings
 - (h) maintain privacy
 - (i) maximise stormwater re-use
 - (j) complement existing, including native vegetation, vegetation
 - (k) contribute to the viability of ecosystems and species
 - (l) promote water and biodiversity conservation
 - (m) minimise heat absorption and reflection.
- 2 Landscaping should:
 - (a) include the planting of locally indigenous species where appropriate
 - (b) be oriented towards the street frontage
 - (c) result in the appropriate clearance from powerlines and other infrastructure being maintained.
- 3 Landscaping should not:
 - (a) unreasonably restrict solar access to adjoining development
 - (b) cause damage to buildings, paths and other landscaping from root invasion, soil disturbance or plant overcrowding

- (c) introduce pest plants
 - (d) increase the risk of bushfire
 - (e) remove opportunities for passive surveillance
 - (f) increase leaf fall in waterways
 - (g) increase the risk of weed invasion
 - (h) obscure driver sight lines
 - (i) create a hazard for train or tram drivers by obscuring sight lines at crossovers.
- 4 Fences and walls, including retaining walls, should:
- (a) not result in damage to neighbouring trees
 - (b) be compatible with the associated development and with existing predominant, attractive fences and walls in the locality
 - (c) enable some visibility of buildings from and to the street to enhance safety and allow casual surveillance
 - (d) incorporate articulation or other detailing where there is a large expanse of wall facing the street;
 - (e) assist in highlighting building entrances
 - (f) be sited and limited in height, to ensure adequate sight lines for motorists and pedestrians especially on corner sites
 - (g) in the case of side and rear boundaries, be of sufficient height to maintain privacy and/or security without adversely affecting the visual amenity or access to sunlight of adjoining land
 - (h) be constructed of non-flammable materials
 - (i) be incorporated as part of the development where the established character of the locality involves front fences.
- 5 Front fencing should be open in form to allow cross ventilation and access to sunlight.

Marine Aquaculture

OBJECTIVES

- 1 Development in marine waters including marine aquaculture development that ensures fair and equitable sharing of marine and coastal resources and minimises conflict with water-based and land-based uses.
- 2 Maintenance of marine navigational safety.
- 3 Ecologically sustainable development of the marine aquaculture industry.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Marine aquaculture and other offshore development should be ecologically sustainable and be located, designed, constructed and managed to:
 - (a) minimise adverse impacts on marine habitats and ecosystems, and public access to beaches, public watercourses or the foreshore
 - (b) take into account the requirements of traditional indigenous and commercial fishing grounds
 - (c) ensure satisfactory removal and disposal of litter, disused material, shells, debris, detritus, faecal matter and dead animals from the development
 - (d) prevent the build up of waste (except where waste can be removed).
- 2 In marine waters, marine aquaculture (other than inter tidal aquaculture) and other offshore development should be located a minimum of 100 metres seaward of the high-water mark.
- 3 Marine aquaculture development should not significantly obstruct or adversely affect any of the following:
 - (a) areas of high public use
 - (b) areas established for recreational activities
 - (c) areas of outstanding visual, environmental, commercial or tourism value
 - (d) sites, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports.
- 4 Marine aquaculture should be sited, designed, constructed and managed to minimise interference and obstruction to the natural processes of the coastal and marine environment.
- 5 Marine aquaculture should be developed in areas where an adequate water current exists to disperse sediments and be sited a sufficient height above the sea floor to:
 - (a) prevent the fouling of waters, publicly owned wetlands or the nearby coastline
 - (b) minimise seabed damage.
- 6 Racks, floats and other farm structures associated with marine aquaculture or other offshore development should where practicable be visually unobtrusive from the shoreline.

- 7 Marine aquaculture development should:
 - (a) use feed hoppers that are painted in subdued colours and suspended as low as possible above the water
 - (b) position structures to protrude the minimum distance practicable above water
 - (c) avoid the use of shelters and structures above cages and platforms.
- 8 Marine aquaculture should be developed to maintain existing rights of way within or adjacent to a site.
- 9 Marine aquaculture access, launching and maintenance facilities should:
 - (a) where possible, use existing and established roads, tracks, ramps and paths to or from the sea
 - (b) be developed cooperatively, and co-located to serve the needs of the industry and community as a whole.
- 10 Marine aquaculture and other offshore development should be located at least:
 - (a) 550 metres from a proclaimed shipwreck
 - (b) 1000 metres seaward from the boundary of any reserve under the *National Parks and Wildlife Act*, unless a lesser distance is agreed with the Minister responsible for that *Act*.
- 11 Aquaculture development should be located so as not to obstruct nor interfere with navigation channels, access channels, frequently used natural launching sites, safe anchorage areas, known diving areas, commercial shipping lanes or activities associated with existing jetties and wharves.
- 12 Marine aquaculture development should contribute to navigational safety by being:
 - (a) suitably marked for navigational purposes
 - (b) sited to allow an adequate distance between farms for safe navigation
 - (c) located at least 250 metres from a commercial shipping lane
 - (d) comprised of structures that are secured and/or weighted to prevent drifting
 - (e) able to be rehabilitated when no longer operational.

Medium and High Rise Development (3 or more storeys)

OBJECTIVES

- 1 Medium and high rise development that provides housing choice and employment opportunities.
- 2 Residential development that provides a high standard of amenity and adaptability for a variety of accommodation and living needs.
- 3 Commercial, office and retail development that is designed to create a strong visual connection to the public realm and that contributes to the vitality of the locality.
- 4 Buildings designed and sited to be energy and water efficient.

PRINCIPLES OF DEVELOPMENT CONTROL

Design and Appearance

- 1 Buildings should:
 - (a) achieve a human scale at ground level through the use of elements such as canopies, verandas or building projections
 - (b) provide shelter over the footpath where minimal setbacks are desirable
 - (c) ensure walls on the boundary that are visible from public land include visually interesting treatments to break up large blank façades.
- 2 Non-residential land uses within the ground floor level areas of buildings and foyers within residential buildings should be designed to enable surveillance from public land to the inside of the building at night.
- 3 Entrances to multi-storey buildings should:
 - (a) be oriented towards the street
 - (b) be clearly identifiable
 - (c) provide shelter, a sense of personal address and transitional space around the entry
 - (d) provide separate access for residential and non-residential land uses.

Visual Privacy

- 4 The visual privacy of ground floor dwellings within multi-storey buildings should be protected through the use of design features such as the elevation of ground floors above street level, setbacks from the street and the location of verandas, windows, porticos or the like.

Building Separation and Outlook

- 5 Residential buildings (or the residential floors of mixed use buildings) should:
 - (a) have adequate separation between habitable room windows and balconies from other buildings to provide visual and acoustic privacy for dwelling occupants and allow the infiltration of daylight to interior and outdoor spaces

- (b) ensure living rooms have, at a minimum, a satisfactory short range visual outlook to public or communal space.

Dwelling Configuration

- 6 Buildings comprising more than 20 dwellings should provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling.
- 7 Dwellings with 3 or more bedrooms located on the ground floor of medium and high rise buildings should, where possible, have the windows of habitable rooms overlooking internal courtyard space or other public space.

Adaptability

- 8 Multi-storey buildings should enable flexibility in their internal design and layout to support adaptive reuse, including accommodation for larger tenancies or the conversion of student accommodation to self contained apartments.

Environmental

- 9 Multi-storey buildings should:
 - (a) minimise detrimental micro-climatic and solar access impacts on adjacent land or buildings, including effects of patterns of wind, temperature, daylight, sunlight, glare and shadow
 - (b) incorporate roof designs that enable the provision of rain water tanks (where they are not provided elsewhere), photovoltaic cells and other features that enhance sustainability.
- 10 Green roofs (which can be a substitute for private or communal open space provided they can be accessed by occupants of the building) are encouraged on all new residential, commercial or mixed use buildings.
- 11 Development of 5 or more storeys, or 21 metres or more in building height (excluding the rooftop location of mechanical plant and equipment), should be designed to minimise the risk of wind tunnelling effects on adjacent streets by adopting one or more of the following:
 - (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street
 - (b) substantial verandas around a building to deflect downward travelling wind flows over pedestrian areas
 - (c) the placement of buildings and use of setbacks to deflect the wind at ground level.

Site Facilities and Storage

- 12 Dwellings should provide a covered storage area, specifically for the storage of sundry items which would not normally be stored within kitchen cupboards or wardrobes, which is not less than 8 cubic metres and located within one or more of the following areas:
 - (a) in the dwelling (but not including a habitable room)
 - (b) in a garage, carport or outbuilding
 - (c) within an on-site communal facility.
- 13 The design of driveway crossovers, parking areas, accessways and elements that interact with the public realm should safely and efficiently accommodate the collection of waste and recycling materials.
- 14 Development should provide a dedicated area for the on-site storage, collection and sorting of recyclable materials and waste that is consistent with the following:

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- (a) easily and safely accessible to the collection point
 - (b) easily and safely accessible to residents and collection service providers
 - (c) well screened and secure to prevent vandalism and theft
 - (d) designed to reduce odour and discourage vermin.
- 15 Development with a gross floor area of 2000 square metres or more should provide for the communal storage and management of waste.

Metropolitan Open Space System

OBJECTIVES

- 1 A Metropolitan Open Space System (MOSS), in and around metropolitan Adelaide, that:
 - (a) is comprised of public and private land
 - (b) is clearly defined and linked
 - (c) has an open or natural character
 - (d) provides a visual and scenic contrast to the built urban environment
 - (e) separates different parts of the metropolitan area.
- 2 Conservation and restoration of existing and modified habitats.
- 3 Conservation of sites of scientific or heritage interest.
- 4 The provision of corridors for the movement of wildlife.
- 5 A range of recreation and leisure areas including a network of cycle and walking paths integrating MOSS and adjoining land uses.
- 6 A range of active recreation and sporting facilities of regional or State significance, including facilities that may be used for national and international events.
- 7 Stormwater management in association with recreation, aquifer recharge and water quality management.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve and enhance the natural and open character and amenity of land located within the MOSS.
- 2 Development within the MOSS should contribute to the rehabilitation and restoration of aquatic and terrestrial ecosystems and water catchments.
- 3 Landscaping within MOSS locations should:
 - (a) incorporate remnant vegetation
 - (b) use locally indigenous plant species wherever possible
 - (c) provide shade and windbreaks along cyclist and pedestrian routes and around picnic and barbecue areas, seating and car parking areas
 - (d) maximise opportunities for passive surveillance along the park.
- 4 Development on public land within the MOSS should incorporate:
 - (a) easily identified access points
 - (b) pedestrian and cycle linkages within open spaces and between other open space networks
 - (c) facilities to provide and support a range of recreation and leisure activities.

Natural Resources

OBJECTIVES

- 1 Retention, protection and restoration of the natural resources and environment.
- 2 Protection of the quality and quantity of South Australia's surface waters, including inland, *marine and estuarine* and underground waters.
- 3 The ecologically sustainable use of natural resources including water resources, including *marine waters*, ground water, surface water and watercourses.
- 4 Natural hydrological systems and environmental flows reinstated, and maintained and enhanced.
- 5 Development consistent with the principles of water sensitive design.
- 6 Development sited and designed to:
 - (a) protect natural ecological systems
 - (b) achieve the sustainable use of water
 - (c) protect water quality, including receiving waters
 - (d) reduce runoff and peak flows and prevent the risk of downstream flooding
 - (e) minimise demand on reticulated water supplies
 - (f) maximise the harvest and use of stormwater
 - (g) protect stormwater from pollution sources
- 7 Storage and use of stormwater which avoids adverse impact on public health and safety.
- 8 Native flora, fauna and ecosystems protected, retained, conserved and restored.
- 9 Restoration, expansion and linking of existing native vegetation to facilitate habitat corridors for ease of movement of fauna.
- 10 Minimal disturbance and modification of the natural landform.
- 11 Protection of the physical, chemical and biological quality of soil resources.
- 12 Protection of areas prone to erosion or other land degradation processes from inappropriate development.
- 13 Protection of the scenic qualities of natural and rural landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be undertaken with minimum impact on the natural environment, including air and water quality, land, soil, biodiversity, and scenically attractive areas.
- 2 Development should ensure that South Australia's natural assets, such as biodiversity, water and soil, are protected and enhanced.

- 3 Development should not significantly obstruct or adversely affect sensitive ecological areas such as creeks, wetlands, estuaries and significant seagrass and mangrove communities.
- 4 Development should be appropriate to land capability and the protection and conservation of water resources and biodiversity.

Water Sensitive Design

- 5 Development should be designed to maximise conservation, minimise consumption and encourage reuse of water resources.
- 6 Development should not take place if it results in unsustainable use of surface or underground water resources.
- 7 Development should be sited and designed to:
 - (a) capture and re-use stormwater, where practical
 - (b) minimise surface water runoff
 - (c) prevent soil erosion and water pollution
 - (d) protect and enhance natural water flows
 - (e) protect water quality by providing adequate separation distances from watercourses and other water bodies
 - (f) not contribute to an increase in salinity levels
 - (g) avoid the water logging of soil or the release of toxic elements
 - (h) maintain natural hydrological systems and not adversely affect:
 - (i) the quantity and quality of groundwater
 - (ii) the depth and directional flow of groundwater
 - (iii) the quality and function of natural springs.
- 8 Water discharged from a development site should:
 - (a) be of a physical, chemical and biological condition equivalent to or better than its pre-developed state
 - (b) not exceed the rate of discharge from the site as it existed in pre-development conditions.
- 9 Development should include stormwater management systems to protect it from damage during a minimum of a 1 in 100 year average return interval flood.
- 10 Development should have adequate provision to control any stormwater over-flow run-off from the site and should be sited and designed to improve the quality of stormwater and minimise pollutant transfer to receiving waters.
- 11 Development should include stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure the carrying capacities of downstream systems are not overloaded.
- 12 Development should include stormwater management systems to minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system.

- 13 Stormwater management systems should preserve natural drainage systems, including the associated environmental flows.
- 14 Stormwater management systems should
 - (a) maximise the potential for stormwater harvesting and reuse either on-site or as close as practicable to the source
 - (b) utilise, but not be limited to, one or more of the following harvesting methods:
 - (i) the collection of roof water in tanks
 - (ii) the discharge to open space, landscaping or garden areas, including strips adjacent to car parks
 - (iii) the incorporation of detention and retention facilities
 - (iv) aquifer recharge.
- 15 Where it is not practicable to detain or disposed of stormwater on site, only clean stormwater runoff should enter the public stormwater drainage system.
- 16 Artificial wetland systems, including detention and retention basins, should be sited and designed to:
 - (a) ensure public health and safety is protected
 - (b) minimise potential health risks arising from the breeding of mosquitoes.
- 17 Site drainage should not discharge into or onto a service lane unless adequately designed and engineered for such function.

Water Catchment Areas

- 18 Development should ensure watercourses and their beds, banks, wetlands and floodplains are not damaged or modified and are retained in their natural state, except where modification is required for essential access or maintenance purposes.
- 19 No development should occur where its proximity to a swamp or wetland will damage or interfere with the hydrology or water regime of the swamp or wetland.
- 20 A wetland or low-lying area providing habitat for native flora and fauna should not be drained, except temporarily for essential management purposes to enhance environmental values.
- 21 Along watercourses, areas of remnant native vegetation, or areas prone to erosion, that are capable of natural regeneration should be fenced off to limit stock access.
- 22 Development such as cropping, intensive animal keeping, residential, tourism, industry and horticulture, that increases the amount of surface run-off should include a strip of land at least 20 metres wide measured from the top of existing banks on each side of a watercourse that is:
 - (a) fenced to exclude livestock
 - (b) kept free of development, including structures, formal roadways or access ways for machinery or any other activity causing soil compaction or significant modification of the natural surface of the land
 - (c) revegetated with indigenous vegetation comprising trees, shrubs and other groundcover plants to filter run-off so as to reduce the impacts on native aquatic ecosystems and to minimise soil loss eroding into the watercourse.

- 23 Development resulting in the depositing of an object or solid material in a watercourse or floodplain or the removal of bank and bed material should not:
- (a) adversely affect the migration of aquatic biota
 - (b) adversely affect the natural flow regime
 - (c) cause or contribute to water pollution
 - (d) result in watercourse or bank erosion
 - (e) adversely affect native vegetation upstream or downstream that is growing in or adjacent to a watercourse.
- 24 The location and construction of dams, water tanks and diversion drains should:
- (a) occur off watercourse
 - (b) not take place in ecologically sensitive areas or on erosion-prone sites
 - (c) provide for low flow by-pass mechanisms to allow for migration of aquatic biota
 - (d) protect the needs of downstream users
 - (e) minimise in-stream or riparian vegetation loss
 - (f) incorporate features to improve water quality (e.g. wetlands and floodplain ecological communities)
 - (g) protect ecosystems dependent on water resources.
- 25 Irrigated horticulture and pasture should not increase groundwater-induced salinity.
- 26 Development should comply with the current *Environment Protection (Water Quality) Policy*.

Biodiversity and Native Vegetation

- 27 Development should retain existing areas of native vegetation and where possible contribute to revegetation using locally indigenous plant species.
- 28 Development should be designed and sited to minimise the loss and disturbance of native flora and fauna, *including marine animals and plants, and their breeding grounds and habitats*.
- 29 Native vegetation should be conserved and its conservation value and function not compromised by development if the native vegetation does any of the following:
- (a) provides an important habitat for wildlife or shade and shelter for livestock
 - (b) has a high plant species diversity or includes rare, vulnerable or endangered plant species or plant associations and communities
 - (c) provides an important seed bank for indigenous vegetation
 - (d) has high amenity value and/or significantly contributes to the landscape quality of an area, including the screening of buildings and unsightly views
 - (e) has high value as a remnant of vegetation associations characteristic of a district or region prior to extensive clearance for agriculture
 - (f) is growing in, or is characteristically associated with a wetland environment.

- 30 Native vegetation should not be cleared if such clearing is likely to lead to, cause or exacerbate any of the following:
- (a) erosion or sediment within water catchments
 - (b) decreased soil stability
 - (c) soil or land slip
 - (d) deterioration in the quality of water in a watercourse or surface water runoff
 - (e) a local or regional salinity problem
 - (f) the occurrence or intensity of local or regional flooding.
- 31 Development that proposes the clearance of native vegetation should address or consider the implications that removing the native vegetation will have on the following:
- (a) provision for linkages and wildlife corridors between significant areas of native vegetation
 - (b) erosion along watercourses and the filtering of suspended solids and nutrients from run-off
 - (c) the amenity of the locality
 - (d) bushfire safety
 - (e) the net loss of native vegetation and other biodiversity.
- 32 Where native vegetation is to be removed, it should be replaced in a suitable location on the site with locally indigenous vegetation to ensure that there is not a net loss of native vegetation and biodiversity.
- 33 Development should be located and occur in a manner which:
- (a) does not increase the potential for, or result in, the spread of pest plants, or the spread of any non-indigenous plants into areas of native vegetation or a conservation zone
 - (b) avoids the degradation of remnant native vegetation by any other means including as a result of spray drift, compaction of soil, modification of surface water flows, pollution to groundwater or surface water or change to groundwater levels
 - (c) incorporates a separation distance and/or buffer area to protect wildlife habitats and other features of nature conservation significance.
- 34 Development should promote the long-term conservation of vegetation by:
- (a) avoiding substantial structures, excavations, and filling of land in close proximity to the trunk of trees and beneath their canopies
 - (b) minimising impervious surfaces beneath the canopies of trees
 - (c) taking other effective and reasonable precautions to protect both vegetation and the integrity of structures and essential services.
- 35 Horticulture involving the growing of olives should be located at least:
- (a) 500 metres from:
 - (i) a national park
 - (ii) a conservation park

- (iii) a wilderness protection area
 - (iv) the edge of a substantially intact stratum of native vegetation greater than 5 hectares in area
- (b) 50 metres from the edge of stands of native vegetation 5 hectares or less in area.
- 36 Horticulture involving the growing of olives should have at least one locally indigenous tree that will grow to a height of at least 7 metres sited at least every 100 metres around the perimeter of the orchard.

Soil Conservation

- 37 Development should not have an adverse impact on the natural, physical, chemical or biological quality and characteristics of soil resources.
- 38 Development should be designed and sited to prevent erosion.
- 39 Development should take place in a manner that will minimise alteration to the existing landform.
- 40 Development should minimise the loss of soil from a site through soil erosion or siltation during the construction phase of any development and following the commencement of an activity.

Open Space and Recreation

OBJECTIVES

- 1 The creation of a network of linked parks, reserves, recreational trails and recreation areas at regional and local levels.
- 2 Pleasant, functional and accessible open spaces providing a range of physical environments.
- 3 A wide range of settings for active and passive recreational opportunities.
- 4 The provision of open space in the following hierarchy:
 - State
 - Regional
 - District
 - Neighbourhood
 - Local.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Urban development should include public open space and recreation areas.
- 2 Public open space and recreation areas should be of a size, dimension and location that:
 - (a) facilitate a range of formal and informal recreation activities to meet the needs of the community
 - (b) provide for the movement of pedestrians and cyclists
 - (c) incorporate existing vegetation and natural features, watercourses, wildlife habitat and other sites of natural or cultural value
 - (d) link habitats, wildlife corridors, public open spaces and existing recreation facilities
 - (e) enable effective stormwater management
 - (f) provides for the planting and retention of large trees and vegetation
 - (g) ensure such areas are usable and suitable for their purpose and, in any event, have a depth to width ratio that does not exceed 2:1.
- 3 Open space should be designed to incorporate:
 - (a) pedestrian, cycle linkages to other open spaces, centres, schools and public transport nodes
 - (b) park furniture, shaded areas and resting places to enhance pedestrian comfort
 - (c) safe crossing points where pedestrian routes intersect the road network
 - (d) easily identified access points
 - (e) frontage to abutting public roads to optimise pedestrian access and visibility
 - (f) re-use of stormwater for irrigation purposes

- (g) recreational trails where appropriate.
- 4 Where practical, access points to regional parks should be located close to public transport.
- 5 District level parks should be at least 3 hectares in size, and provided within 2 kilometres of all households that they serve.
- 6 Neighbourhood parks should be at least 0.5 hectares and generally closer to 1 hectare in size, and provided within 500 metres of households that they serve.
- 7 Local parks should generally be a minimum of 0.2 hectares in size, and should be centrally located within a residential area, close to schools, shops and generally within 300 metres of households that they serve.
- 8 No more than 20 per cent of land allocated as public open space should:
 - (a) have a slope in excess of 1 in 4
 - (b) comprise creeks or other drainage areas.
- 9 Signage should be provided at entrances to and within public open space to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes and park activities.
- 10 Buildings in open space, including structures and associated car parking areas, should be designed, located and of a scale that is unobtrusive and does not detract from the desired open space character.
- 11 Development in open space should:
 - (a) be clustered where practical to ensure that the majority of the site remains open
 - (b) where practical, be developed for multi-purpose use
 - (c) be constructed to minimise the extent of hard paved areas.
- 12 Open spaces and recreation areas should be located and designed to maximise safety and security by:
 - (a) ensuring that within urban areas, their edges are overlooked by housing, commercial or other development that can provide effective informal surveillance
 - (b) ensuring fenced parks and playgrounds have more than one entrance or exit when fenced
 - (c) locating play equipment where it can be informally observed by nearby residents and users during times of use
 - (d) clearly defining the perimeters of play areas
 - (e) providing lighting around facilities such as toilets, telephones, seating, litter bins, bike storage and car parks
 - (f) focusing pedestrian and bicycle movement after dark along clearly defined, adequately lit routes with observable entries and exits.
- 13 Landscaping associated with open space and recreation areas should:
 - (a) not compromise the drainage function of any drainage channel
 - (b) provide shade and windbreaks along cyclist and pedestrian routes, around picnic and barbecue areas and seating, and in car parking areas
 - (c) maximise opportunities for informal surveillance throughout the park

- (d) enhance the visual amenity of the area and complement existing buildings
 - (e) be designed and selected to minimise maintenance costs
 - (f) provide habitat for local fauna.
- 14 Development of recreational activities in areas not zoned for that purpose should be compatible with surrounding activities.
- 15 Recreation facilities development should be sited and designed to minimise negative impacts on the amenity of the locality.

Orderly and Sustainable Development

OBJECTIVES

- 1 Orderly and economical development that creates a safe, convenient and pleasant environment in which to live.
- 2 Development occurring in an orderly sequence and in a compact form to enable the efficient provision of public services and facilities.
- 3 Development that does not jeopardise the continuance of adjoining authorised land uses.
- 4 Development that does not prejudice the achievement of the provisions of the Development Plan.
- 5 Development abutting adjoining Council areas having regard to the policies of that Council's Development Plan.
- 6 Urban development located only in zones designated for such development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not prejudice the development of a zone for its intended purpose.
- 2 Land outside of townships and settlements should primarily be used for primary production and conservation purposes.
- 3 Urban development should form a compact extension to an existing built-up area.
- 4 Ribbon development should not occur along the coast, water frontages or arterial roads shown in *Location Maps and Overlay Map(s) - Transport*.
- 5 Development should be located and staged to achieve the economical provision of public services and infrastructure, and to maximise the use of existing services and infrastructure.
- 6 Where development is expected to impact upon the existing infrastructure network (including the transport network), development should demonstrate how the undue effect will be addressed.
- 7 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to not prejudice the orderly development of adjacent land.
- 8 Development should be undertaken in accordance with the *Concept Plans Maps ChSt/1 to 27*.
- 9 Development in localities having a dysfunctional or unsatisfactory layout, or unhealthy or obsolete development should improve or rectify those conditions.
- 10 Development should avoid significant changes to natural ground that may adversely affect surrounding properties.

Regulated Trees

OBJECTIVES

- 1 The conservation of regulated trees that provide important aesthetic and/or environmental benefit.
- 2 Development in balance with preserving regulated trees that demonstrate one or more of the following attributes:
 - (a) significantly contributes to the character or visual amenity of the locality
 - (b) indigenous to the locality
 - (c) a rare or endangered species
 - (d) an important habitat for native fauna.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should have minimum adverse effects on regulated trees.
- 2 A regulated tree should not be removed or damaged other than where it can be demonstrated that one or more of the following apply:
 - (a) the tree is diseased and its life expectancy is short
 - (b) the tree represents a material risk to public or private safety
 - (c) the tree is causing damage to a building
 - (d) development that is reasonable and expected would not otherwise be possible
 - (e) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree.
- 3 Tree damaging activity other than removal should seek to maintain the health, aesthetic appearance and structural integrity of the tree.

Renewable Energy Facilities

OBJECTIVES

- 1 The development of renewable energy facilities, such as wind and biomass energy facilities, in appropriate locations.
- 2 Location, siting, design and operation of renewable energy facilities to avoid or minimise adverse impacts and maximise positive impacts on the environment, the local community and the State.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Renewable energy facilities, including wind farms and ancillary developments, should be located in areas that maximise efficient generation and supply of electricity.
- 2 Wind farms and ancillary development such as substations, maintenance sheds, access roads and connecting power-lines, should be sited, designed and operated in a manner that:
 - (a) avoids or minimises negative impacts on the character, landscape quality, visual significance or amenity of the area
 - (b) uses elements of the landscape and appropriate materials and finishes to minimise visual impact
 - (c) avoids or minimises the potential for adverse impact on areas of native vegetation, conservation, environmental, geological, tourism or built or natural heritage significance
 - (d) does not impact on the safety of water or air transport and the operation of ports, airfields and designated landing strips
 - (e) avoids or minimises nuisance or hazard to nearby property owners and/or occupiers, road users and wildlife by not:
 - (i) causing shadowing, flickering, reflection or blade glint impacts
 - (ii) creating excessive noise
 - (iii) interfering with television and radio signals
 - (iv) modifying vegetation, soils and habitats
 - (v) striking birds or bats.

Residential Development

OBJECTIVES

- 1 Safe, convenient, pleasant and healthy-living environments that meet the full range of needs and preferences of a diverse community.
- 2 A diverse range of dwelling types and sizes available to cater for changing demographics, particularly smaller household sizes and supported accommodation.
- 3 Medium and high density residential development in areas close to activity centres, public and community transport and public open spaces.
- 4 The revitalisation of residential areas to support the viability of community services and infrastructure.
- 5 Affordable housing, student housing and housing for aged persons provided in appropriate locations.
- 6 Increased affordable housing opportunities through land division and the conversion of buildings to a residential use.
- 7 Student housing located within close proximity to public transport, public open space and other support facilities and services.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Residential allotments and sites should maximise solar orientation and have the area and dimensions to accommodate:
 - (a) the siting and construction of a dwelling and associated ancillary outbuildings
 - (b) the provision of landscaping and private open space
 - (c) convenient and safe vehicle, pedestrian and cycling access and parking
 - (d) water sensitive design systems that enable the storage, treatment and reuse of stormwater.
- 2 Residential allotments should be of varying sizes to encourage housing diversity.

Design and Appearance

- 3 Dwellings and accommodation at ground floor level should contribute to the character of the locality and create active, safe streets by incorporating one or more of the following:
 - (a) front landscaping or terraces that contribute to the spatial and visual structure of the street while maintaining adequate privacy for occupants
 - (b) individual entries for ground floor accommodation
 - (c) opportunities to overlook adjacent public space.
- 4 Residential development should be designed to ensure living rooms have an external outlook.
- 5 Entries to dwellings or foyer areas should be clearly visible from the street, or access ways that they face to enable visitors to easily identify individual dwellings.

Garages, Carports and Outbuildings

- 6 Garages, carports and residential outbuildings should have a roof form and pitch, building materials and detailing that complement the associated dwelling.
- 7 Residential outbuildings, including garages and sheds, should not be constructed unless in association with an existing dwelling.
- 8 Garages, carports and residential outbuildings should not dominate the streetscape and be designed within the following parameters:

Parameter	Value
Maximum floor area	60 square metres for sites up to 500 square metres in area. 12 per cent of the site area for sites greater than 500 square metres in area.
Maximum wall height	3 metres
Maximum building height	5 metres
Minimum setback from a primary road frontage where located within the Residential Character Zone	5.5 metres or 0.5 metres behind the main face of the associated dwelling, whichever is the greater distance from the primary road frontage.
Minimum setback from a primary road frontage where located outside of the Residential Character Zone	5.5 metres from the primary road frontage for garages and carports and only forward of the associated dwelling where it has been designed and sited to minimise its visual and other impacts. Outbuildings should not protrude forward of any part of its associated dwelling.
Minimum setback from a secondary road frontage	0.9 metres or in-line with the existing dwelling.
Minimum setback from a rear or side vehicle access way	0 metres
Maximum length along the boundary	12 metres
Maximum frontage width of garage or carport with an opening facing a rear access lane	No maximum
Maximum frontage width of garage or carport with an opening facing the street	Less than 50 per cent of the allotment frontage provided the width does not exceed 6 metres within the Residential Zone or Residential Character Zone.

- 9 Garages and carports should only be sited forward of the associated dwelling where it can be demonstrated that all of the following can be achieved:
 - (a) the siting of the garage or carport does not compromise the ability to provide any required off street car parking on the site
 - (b) the width of the primary road frontage is at least 15 metres
 - (c) the width of the garage or carport is no greater than 6 metres or does not constitute more than 40 per cent of the primary road frontage, whichever is less
 - (d) the built form of the garage or carport is designed to complement the associated dwelling

- (e) the garage or carport does not form a visually dominant element in that part of the streetscape
 - (f) the siting of the garage or carport does not compromise driver sight lines.
- 10 Rear vehicular access to garages or carports on sites that front the coast or Esplanade, or from a road that provides primary frontage for other sites, should:
- (a) have a maximum width of 6 metres or 80 per cent of the width of the site, whichever is the lesser
 - (b) landscape the balance of the frontage for a depth of at least 1 metre.
- 11 Except where otherwise specified in a particular zone, policy area or precinct, where there is no building set back pattern already established in the locality, development should be setback from the street boundaries in accordance with the following table:

Estimated traffic volume of the street	Minimum primary frontage	Minimum secondary frontage
300 - 2000 vehicles per day	4 metres from the main face of the dwelling, excluding any veranda, porch etc.	1 metre from any part of the dwelling
2001 - 6000 vehicles per day	4 metres from the main face of the dwelling, excluding any veranda, porch etc.	2 metres from any part of the dwelling

Site Coverage

- 12 Site coverage should be limited to ensure sufficient space is provided for:
- (a) pedestrian and vehicle access and vehicle parking
 - (b) domestic storage
 - (c) outdoor clothes drying
 - (d) a rainwater tanks
 - (e) private open space and landscaping
 - (f) convenient storage of household garbage and recycling receptacles.

Private Open Space

- 13 Private open space (available for exclusive use by residents of each dwelling) should be provided for each dwelling and should be sited and designed:
- (a) to be accessed directly from the internal living areas of the dwelling
 - (b) to be generally at ground level (other than for residential flat buildings) and to the side or rear of a dwelling and screened for privacy
 - (c) to take advantage of, but not adversely affect, natural features of the site
 - (d) to minimise overlooking from adjacent buildings
 - (e) to achieve separation from bedroom windows on adjoining sites
 - (f) to have a northerly aspect to provide for comfortable year round use

- (g) not to be significantly shaded during winter by the associated dwelling or adjacent development
- (h) to be partly shaded in summer
- (i) to minimise noise or air quality impacts that may arise from traffic, industry or other business activities within the locality
- (j) to have sufficient area and shape to be functional, taking into consideration the location of the dwelling, and the dimension and gradient of the site.

14 Dwellings should provide private open space in accordance with the following table:

Site area per dwelling (square metres)	Minimum area excluding any area at ground level at the front of the dwelling (square metres)	Minimum dimension (metres)	Minimum area to be provided at the rear or side of the dwelling, and be directly accessible from a habitable room (square metres)
<200 or within Integrated Medium Density Policy Area 20	8 where the dwelling has one bedroom or comprises a Studio (where there is no separate bedroom)	2	8
	11 where the dwelling has two bedrooms	2	11
	15 where the dwelling has 3 or more bedrooms	2	15
200<300	24	4	16
300-500	60, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres.	4	16
>500	80, of which 10 may comprise balconies, roof patios and the like, provided they have a minimum dimension of 2 metres.	4	24

- 15 Private open space should not include driveways, effluent drainage areas, rubbish bin storage areas, sites for rainwater tanks and other utility areas, and common areas such as parking areas and communal open space.
- 16 Private open space at ground level should be designed to provide a consolidated area of deep soil (an area of natural ground which excludes areas where there is a structure underneath, pools, and non-permeable paved areas) to:
- (a) assist with ease of drainage
 - (b) allow for effective deep planting
 - (c) reduce urban heat loading and improve micro-climatic conditions around sites and buildings.

- 17 Dwellings located above ground level should provide private open space in accordance with the following table:

Dwelling type	Minimum area of private open space
Studio (where there is no separate bedroom)	No minimum requirement
One bedroom dwelling	8 square metres
Two bedroom dwelling	11 square metres
Three + bedroom dwelling	15 square metres

- 18 Private open space located above ground level should have a minimum dimension of 2 metres and be directly accessible from a habitable room.
- 19 Private open space may be substituted for the equivalent area of communal open space where:
- (a) at least 50 per cent of the communal open space is visually screened from public areas of the development
 - (b) ground floor communal space is overlooked by habitable rooms to facilitate passive surveillance
 - (c) it contains landscaping and facilities that are functional, attractive and encourage recreational use
 - (d) each dwelling is still provided with an area of useable private open space of at least 8 square metres with a minimum dimension of 2 metres directly accessible from a habitable room.

Communal Open Space

- 20 Communal open space should be shared with more than one dwelling, not be publicly accessible and exclude:
- (a) private open space
 - (b) public right of way
 - (c) private streets
 - (d) parking areas and driveways
 - (e) service and storage areas
 - (f) narrow or inaccessible strips of land.
- 21 Communal open space should only be located on elevated gardens or roof tops where the area and overall design is useful for the recreation and amenity needs of residents and where it is designed to:
- (a) address acoustic, safety, security, and wind effects
 - (b) minimise overlooking into habitable room windows or onto the useable private open space of other dwellings
 - (c) facilitate landscaping and food production
 - (d) be integrated into the overall façade and composition of buildings.

Visual Privacy

- 22 Except for buildings of 3 or more storeys, upper level windows, balconies, terraces and decks that overlook habitable room windows or private open space of dwellings should maximise visual privacy through the use of measures such as sill heights of not less than 1.5 metres or permanent screens having a height of 1.5 metres above finished floor level.

Noise

- 23 Other than within an area designated for the purposes of the *Noise and Air Emissions Overlay*, residential development close to high noise sources (eg major roads, railway lines, tram lines and industry) should be designed to locate bedrooms, living rooms and private open spaces away from those noise sources, or protect these areas with appropriate noise attenuation measures.
- 24 Noise generated by fixed noise sources such as air conditioning units and pool pumps should be located, designed and attenuated to avoid nuisance to adjoining landowners and occupiers.
- 25 External noise and artificial light intrusion into bedrooms should be minimised by separating or shielding these rooms from:
- (a) active communal recreation areas, other common access areas, parking areas and vehicle access ways
 - (b) service equipment areas and fixed noise sources on the same or adjacent sites.
- 26 The number of dwellings sharing a common internal pedestrian entry within a residential flat building should be minimised to limit noise generation in internal access ways.

Site Facilities and Storage

- 27 Site facilities for group dwellings, multiple dwellings and residential flat buildings should include:
- (a) mail box facilities sited close to the major pedestrian entrance to the site
 - (b) bicycle parking for residents and visitors (for developments containing more than 6 dwellings)
 - (c) household waste and recyclable material storage areas of a size suitable for the expected volume, located away from dwellings, screened from public view and in an area easily accessible for waste collection.

Affordable Housing

- 28 Affordable housing should be well integrated and complementary in design and appearance to other dwellings within the development.

Dependent Accommodation

- 29 Dependent accommodation (i.e. accommodation where the living unit is connected to the same services of the main dwelling) should be developed on the same allotment as the existing dwelling only where:
- (a) the site is of adequate size and configuration
 - (b) the accommodation has a small floor area relative to the associated main dwelling(s)
 - (c) adequate outdoor space of a minimum of 20 square metres is provided for the use of all occupants
 - (d) adequate on-site car parking is provided
 - (e) the building is designed to, and comprises colours and materials that will, complement the associated dwelling.

Swimming Pools and Outdoor Spas

- 30 Swimming pools, outdoor spas and associated ancillary equipment and structures should be sited so as to protect the privacy and amenity of adjoining residential land.

Student Housing

- 31 Student housing should ensure:
- (a) the internal layout and facilities are designed to provide sufficient space and amenity for the requirements of student life and promote social interaction
 - (b) a range of living options are provided to meet a variety of accommodation needs, such as one bedroom, two bedroom and disability accessible units
 - (c) common or shared facilities are provided to enable a more efficient use of space, including:
 - (i) shared cooking, laundry and external drying facilities
 - (ii) internal and external communal open space
 - (iii) common storage facilities at the rate of 8 cubic metres for every 2 dwelling units or students
 - (iv) common on-site car parking to meet anticipated demand in accordance with [Table ChSt/2 – Off Street Vehicle Parking Requirements](#)
 - (v) secure and sheltered bicycle parking at the rate of one space for every 2 students
 - (d) and be designed and sited to allow easy adaptation to accommodate an alternative use of the building in the event it is no longer required for student housing.
- 32 Student housing should be provided with both private and communal open space, excluding areas for clothes drying, bin storage, car parking and car manoeuvring areas, in accordance with the following:

Type of open space	Minimum open space/communal space requirements
Private open space (can be located at ground level or above ground e.g. balcony)	8 square metres per dwelling unit / student with a minimum dimension of 2 metres
Communal open space (can be located at ground level or above ground e.g. roof garden)	4 square metres per student with a minimum dimension of 3 metres and direct access from indoor communal space

Significant Trees

OBJECTIVES

- 1 The conservation of significant trees, in Metropolitan Adelaide, that provide important aesthetic and environmental benefit.
- 2 The conservation of significant trees in balance with achieving appropriate development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should preserve the following attributes where a significant tree demonstrates at least one of the following attributes:
 - (a) makes an important contribution to the character or amenity of the local area; or
 - (b) is indigenous to the local area and its species is listed under the *National Parks and Wildlife Act 1972* as a rare or endangered native species
 - (c) represents an important habitat for native fauna
 - (d) is part of a wildlife corridor of a remnant area of native vegetation
 - (e) is important to the maintenance of biodiversity in the local environment
 - (f) forms a notable visual element to the landscape of the local area.
- 2 Development should be undertaken so that it has a minimum adverse effect on the health of a significant tree.
- 3 Significant trees should be preserved, and tree-damaging activity should not be undertaken, unless:
 - (a) in the case of tree removal:
 - (i) the tree is diseased and its life expectancy is short; or
 - (ii) the tree represents an unacceptable risk to public or private safety; or
 - (iii) the tree is within 20 metres of a residential, tourist accommodation or habitable building and is a bushfire hazard within a Bushfire Prone Area; or
 - (iv) the tree is shown to be causing or threatening to cause substantial damage to a substantial building or structure of value; and
 - (v) all other reasonable remedial treatments and measures have been determined to be ineffective; and
 - (vi) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.
 - (b) in any other case:
 - (i) the work is required for the removal of dead wood, treatment of disease, or is in the general interests of the health of the tree; or

- (ii) the work is required due to unacceptable risk to public or private safety; or
 - (iii) the tree is shown to be causing or threatening to cause damage to a substantial building or structure of value; or
 - (iv) the aesthetic appearance and structural integrity of the tree is maintained; or
 - (v) it is demonstrated that all reasonable alternative development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.
- 4 Development involving ground work activities such as excavation, filling, and sealing of surrounding surfaces (whether such work takes place on the site of a significant tree or otherwise) should only be undertaken where the aesthetic appearance, health and integrity of a significant tree, including its root system, will not be adversely affected.
- 5 Land should not be divided or developed where the division or development would be likely to result in a substantial tree-damaging activity occurring to a significant tree.

Siting and Visibility

OBJECTIVES

- 1 Protection of scenically attractive areas, particularly natural, *and coastal* landscapes.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to minimise its visual impact on:
 - (a) the natural, rural or heritage character of the area
 - (b) areas of high visual or scenic value, particularly rural and coastal areas
 - (c) views from the coast, near-shore waters, public reserves, tourist routes and walking trails
 - (d) the amenity of public beaches.
- 2 Buildings should be sited in unobtrusive locations and, in particular, should:
 - (a) be grouped together
 - (b) where possible be located in such a way as to be screened by existing vegetation when viewed from public roads.
- 3 Buildings and structures should be designed to minimise their visual impact in the landscape, in particular:
 - (a) the profile of buildings should be low and the rooflines should complement the natural form of the land
 - (b) the mass of buildings should be minimised by variations in wall and roof lines and by floor plans which complement the contours of the land
 - (c) large eaves, verandas and pergolas should be incorporated into designs so as to create shadowed areas that reduce the bulky appearance of buildings.
- 4 The nature of external surface materials of buildings should not detract from the visual character and amenity of the landscape.
- 5 Development should be screened through the establishment of landscaping using locally indigenous plant species:
 - (a) around buildings and earthworks to provide a visual screen as well as shade in summer, and protection from prevailing winds
 - (b) along allotment boundaries to provide permanent screening of buildings and structures when viewed from adjoining properties and public roads
 - (c) along the verges of new roads and access tracks to provide screening and minimise erosion.

Sloping Land

OBJECTIVES

- 1 Development on sloping land designed to minimise environmental and visual impacts and protect soil stability and water quality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development and associated driveways and access tracks should be sited and designed to integrate with the natural topography of the land and minimise the need for earthworks.
- 2 Development and associated driveways and access tracks, including related earthworks, should be sited, designed and undertaken in a manner that:
 - (a) minimises their visual impact
 - (b) reduces the bulk of the buildings and structures
 - (c) minimises the extent of excavation and fill
 - (d) minimises the need for, and the height of, retaining walls
 - (e) does not cause or contribute to instability of any embankment or cutting
 - (f) avoids the silting of watercourses
 - (g) protects development and its surrounds from erosion caused by water run-off.
- 3 Driveways and access tracks across sloping land should be accessible and have a safe, all-weather trafficable surface.
- 4 Development sites should not be at risk of landslip.
- 5 Development on steep land should include site drainage systems to minimise erosion and avoid adverse impacts on slope stability.
- 6 Steep sloping sites in unsewered areas should not be developed unless the physical characteristics of the allotments enable the proper siting and operation of an effluent drainage field suitable for the development intended.
- 7 The excavation and/or filling of land outside townships and urban areas should:
 - (a) be kept to a minimum and be limited to a maximum depth or height no greater than 1.5 metres so as to preserve the natural form of the land and the native vegetation
 - (b) only be undertaken in order to reduce the visual impact of buildings, including structures, or in order to construct water storage facilities for use on the allotment
 - (c) only be undertaken if the resultant slope can be stabilised to prevent erosion
 - (d) result in stable scree slopes which are covered with top soil and landscaped so as to preserve and enhance the natural character or assist in the re-establishment of the natural character of the area.

Supported Accommodation, Housing for Aged Persons and People with Disabilities

OBJECTIVES

- 1 Provision of well designed supported accommodation for community groups with special needs in appropriate locations.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Supported accommodation (including nursing homes, hostels, retirement homes, retirement villages, residential care facilities and special accommodation houses) and housing for aged persons and people with disabilities should be:
 - (a) located within walking distance of essential facilities such as convenience shops, health and community services and public and community transport
 - (b) located where on-site movement of residents is not unduly restricted by the slope of the land
 - (c) sited and designed to promote interaction with other sections of the community, without compromising privacy
 - (d) of a scale and appearance that reflects the residential style and character of the locality
 - (e) provided with public and private open space and landscaping to meet the needs of residents.
- 2 Supported accommodation and housing for aged persons and people with disabilities should be designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents that include:
 - (a) ground-level access or lifted access to all units
 - (b) internal communal areas and private spaces
 - (c) an interesting and attractive outlook from units and communal areas for all residents, including those in wheelchairs
 - (d) useable recreation areas for residents and visitors, including visiting children
 - (e) adequate living space allowing for the use of wheelchairs with an attendant
 - (f) spaces to accommodate social needs and activities, including social gatherings, internet use, gardening, keeping pets, preparing meals and doing personal laundry
 - (g) storage areas for items such as boats, trailers and caravans
 - (h) storage for items such as small electric powered vehicles and other personal items, including facilities for recharging small electric powered vehicles
 - (i) mail boxes and waste disposal areas within easy walking distance of all units.
- 3 Access roads within supported accommodation and development incorporating housing for aged persons and people with disabilities should:
 - (a) not have steep gradients

- (b) provide convenient access for emergency vehicles, visitors and residents
 - (c) provide space for manoeuvring cars and community buses
 - (d) include kerb ramps at pedestrian crossing points
 - (e) have level-surface passenger loading areas.
- 4 Car parking associated with supported accommodation and housing for aged persons and people with disabilities should:
- (a) be conveniently located on site within easy walking distance of resident units
 - (b) be adequate for residents, staff, service providers and visitors
 - (c) include covered and secure parking for residents' vehicles
 - (d) have slip-resistant surfaces with gradients not steeper than 1 in 40
 - (e) allow ease of vehicle manoeuvrability
 - (f) be designed to allow the full opening of all vehicle doors
 - (g) minimise the impact of car parking on adjacent residences owing to visual intrusion and noise
 - (h) be appropriately lit to enable safe and easy movement to and from vehicles.
- 5 Supported accommodation should include:
- (a) ground-level access or lifted access to all units
 - (b) an interesting and attractive outlook from units and communal areas for all residents including those in wheelchairs
 - (c) adequate living space allowing for the use of wheelchairs with an attendant
 - (d) storage for items such as small electric powered vehicles and other personal items, including facilities for recharging small electric powered vehicles.'
- 6 Car parking associated with supported accommodation should:
- (a) have adequate identifiable provisions for staff
 - (b) include private parking spaces for independent living units
 - (c) include separate and appropriately marked places for people with disabilities and spaces for small electrically powered vehicles.

Telecommunications Facilities

OBJECTIVES

- 1 Telecommunications facilities provided to meet the needs of the community.
- 2 Telecommunications facilities sited and designed to minimise visual impact on the amenity of the local environment.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Telecommunications facilities should:
 - (a) be located to meet the communication needs of the community
 - (b) use materials and finishes that minimise visual impact
 - (c) have antennae located as close as practical to the support structure
 - (d) be located primarily in industrial, commercial, business, office, centre and rural zones
 - (e) where technically feasible, be co-located with other telecommunications facilities
 - (f) incorporate landscaping to screen the development, particularly equipment shelters and huts
 - (g) be designed and sited to minimise the visual impact on the character and amenity of the local environment, in particular visually prominent areas, main focal points and significant vistas.
- 2 Telecommunications facilities in areas of high visitation and community use should use innovative design techniques (eg sculpture and other artworks) where possible and where the resulting design would positively contribute to the character of the area.
- 3 Telecommunications facilities should be located in residential zones only if sited and designed to minimise visual impact by:
 - (a) using existing buildings and vegetation for screening
 - (b) where possible, incorporating the facility within an existing structures that may serve another purpose maintaining that structure's character
 - (c) taking into account the size, scale, context and characteristics of existing structures, landforms and vegetation so as to complement the local environment.
- 4 Telecommunications facilities should not have a direct or significant effect on the amenity, character and settings of Historic Conservation Areas, Local heritage places, State heritage places or State Heritage Areas.

Tourism Development

OBJECTIVES

- 1 Environmentally sustainable and innovative tourism development.
- 2 Tourism development that assists in the conservation, interpretation and public appreciation of significant natural and cultural features including State or local heritage places.
- 3 Tourism development that sustains or enhances the local character, visual amenity and appeal of the area.
- 4 Tourism development that protects areas of exceptional natural value, allows for appropriate levels of visitation, and demonstrates a high quality environmental analysis and design response which enhances environmental values.
- 5 Ensure new development, together with associated bushfire management minimise the threat and impact of bushfires on life and property while protecting the environment.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Tourism development should have a functional or locational link with its natural, cultural or historical setting.
- 2 Tourism development and any associated activities should not damage or degrade any significant natural and cultural features.
- 3 Tourism development should ensure that its scale, form and location will not overwhelm, over commercialise or detract from the intrinsic natural values of the land on which it is sited or the character of its locality.

Tourism Development in Association with Dwelling(s)

- 4 Tourist facilities developed on the site of a dwelling should not detrimentally affect residential amenity.
- 5 Car parking for tourist accommodation associated with a dwelling should be provided at the rate of one space for each guest room or suite of rooms, and ensure that:
 - (a) parking areas are attractively developed and landscaped, or screen fenced, and do not dominate the street frontage
 - (b) the bedrooms of residential neighbours are suitably shielded from noise and headlight glare associated with guest vehicle movements
 - (c) a domestic character is retained through the scale and appearance of landscaping and paving materials that provide a suitable all-weather surface.

Transportation and Access

OBJECTIVES

- 1 A comprehensive, integrated, affordable and efficient air, rail, sea, road, cycle and pedestrian transport system that will:
 - (a) provide equitable access to a range of public, community and private transport services for all people
 - (b) ensure a high level of safety
 - (c) effectively support the economic development of the State
 - (d) have minimal negative environmental and social impacts
 - (e) maintain options for the introduction of suitable new transport technologies.
- 2 Development that:
 - (a) provides safe and efficient movement for all transport modes
 - (b) ensures access for vehicles including emergency services, public infrastructure maintenance and commercial vehicles
 - (c) provides off street parking
 - (d) is appropriately located so that it supports and makes best use of existing transport facilities and networks.
 - (e) provides convenient and safe access to public transport stops.
- 3 A road hierarchy that promotes safe and efficient transportation in an integrated manner throughout the State.
- 4 Provision of safe, pleasant, accessible, integrated and permeable pedestrian and cycling networks that are connected to the public transport network.
- 5 Safe and convenient freight and people movement throughout the State.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Land uses arranged to support the efficient provision of sustainable transport networks and encourage their use.

Movement Systems

- 2 Development should be integrated with existing transport networks, particularly major rail, road and public transport corridors as shown on *Location Map* and *Overlay Maps - Transport*, and designed to minimise its potential impact on the functional performance of the transport network.
- 3 Transport corridors should be sited and designed so as to not unreasonably interfere with the health and amenity of adjacent sensitive land uses.

- 4 Roads should be sited and designed to blend with the landscape and be in sympathy with the terrain.
- 5 Land uses that generate large numbers of visitors such as shopping centres, places of employment, schools, hospitals and medium to high density residential uses should be located so that they can be serviced by the public transport network and encourage walking and cycling.
- 6 Development generating high levels of traffic, such as schools, shopping centres and other retail areas, and entertainment and sporting facilities, should incorporate passenger pick-up and set down areas. The design of such areas should minimise interference to existing traffic and give priority to pedestrians, cyclists and public and community transport users.
- 7 The location and design of public and community transport set-down and pick-up points should maximise safety and minimise the isolation and vulnerability of users.
- 8 Development should provide safe and convenient access for all anticipated modes of transport.
- 9 Development at intersections, pedestrian and cycle crossings, and crossovers to allotments should maintain or enhance sightlines for motorists, cyclists and pedestrians to ensure safety for all road users and pedestrians.
- 10 Driveway crossovers affecting pedestrian footpaths should maintain the level of the footpath.
- 11 Driveway crossovers should be **single vehicle width**, separated and the number minimised to optimise the provision of on-street visitor parking (where on-street parking is appropriate).
- 12 Development should be designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive land uses.
- 13 Industrial/commercial vehicle movements should be separated from passenger vehicle car parking areas.
- 14 Development should provide for the on-site loading, unloading and turning of all traffic likely to be generated.
- 15 **Shared pedestrian and vehicular areas should be designed to ensure each area is distinguishable through the use of paving, street furniture, landscape treatment and suitable signage.**

Cycling and Walking

- 16 Development should ensure that a permeable street and path network is established that encourages walking and cycling through the provision of safe, convenient and attractive routes with connections to adjoining streets, paths, open spaces, schools, pedestrian crossing points on arterial roads, public and community transport stops and activity centres.
- 17 Development should provide access, and accommodate multiple route options, for pedestrians and cyclists by enhancing and integrating with:
 - (a) open space networks, recreational trails, parks, reserves and sport and recreation areas
 - (b) Adelaide's principal cycling network (Bikedirect), which includes arterial roads, local roads and off road paths as depicted in *Overlay Maps - Transport*.
- 18 New developments should give priority to and not compromise existing designated bicycle routes.
- 19 Where development coincides with, intersects or divides a proposed bicycle route or corridor, development should incorporate through-access for cyclists.
- 20 Developments should encourage and facilitate cycling as a mode of transport by incorporating end-of-journey facilities including:

- (a) showers, changing facilities, and secure lockers
 - (b) signage indicating the location of bicycle facilities
 - (c) secure bicycle parking facilities provided at the rate of 3 spaces per 50 employees.
- 21 On-site secure bicycle parking facilities should be:
- (a) located in a prominent place
 - (b) located at ground floor level
 - (c) located undercover
 - (d) located where surveillance is possible
 - (e) well lit and well signed
 - (f) close to well used entrances
 - (g) accessible by cycling along a safe, well lit route.
- 22 Pedestrian and cycling facilities and networks should be designed and provided in accordance with relevant provisions of the *Australian Standards and Austroads Guides*.

Access

- 23 Development should have direct access from an all weather public road.
- 24 Development should be provided with safe and convenient access which:
- (a) avoids unreasonable interference with the flow of traffic on adjoining roads
 - (b) provides appropriate separation distances from existing roads or level crossings
 - (c) accommodates the type and volume of traffic likely to be generated by the development or land use and minimises induced traffic through over-provision
 - (d) is sited and designed to minimise any adverse impacts on the occupants of and visitors to neighbouring properties.
- 25 Development should not restrict access to publicly owned land such as recreation areas.
- 26 The number of vehicle access points onto arterial roads as shown on *Overlay Maps - Transport* should be minimised, and where possible access points should be:
- (a) limited to local roads (including rear lane access)
 - (b) shared between developments.
- 27 Development with access from *Primary and Secondary Arterial Roads shown on Overlay Map ChSt/1 – Transport* should be sited to avoid the need for vehicles to reverse onto or from the road.
- 28 Vehicle access points servicing a corner allotment should be located at least 6 metres from the road intersection or junction in accordance with Australian Standard AS 2890.
- 29 Where a site has a frontage to both the Esplanade and another road, no vehicular access should be provided off the Esplanade.

- 30 Access points onto public roads should be limited to two with only one access point allowed where a site is less than 20 metres in width.
- 31 Development with access from arterial roads or roads as shown on *Overlay Maps - Transport* should be sited to avoid the need for vehicles to reverse onto or from the road.
- 32 Structures such as canopies and balconies that encroach onto the footpath of a road should not cause visual or physical obstruction to:
- (a) signalised intersections
 - (b) heavy vehicles
 - (c) street lighting
 - (d) overhead electricity lines
 - (e) street trees
 - (f) bus stops.
- 33 Except within the **Urban Core Zone**, access provided to the rear of a site should provide space for landscaping, paving and footpaths particularly when operating as a primary frontage for a dwelling or dwellings.
- 34 Driveways, access tracks and parking areas should be designed and constructed to:
- (a) follow the natural contours of the land
 - (b) minimise excavation and/or fill
 - (c) minimise the potential for erosion from surface run-off
 - (d) avoid the removal of existing vegetation
 - (e) be consistent with *Australian Standard AS 2890 Parking facilities*.
- 35 Development serviced by heavy vehicles should:
- (a) be located where the access roads have a carriageway of at least 9.0 metres
 - (b) provide a turning circle of at least 30 metres in diameter to ensure the safe and convenient manoeuvring of vehicles on site.
- 36 Development serviced by heavy vehicles including 'B' doubles should be located and the access provided so that direct vehicle access from authorised heavy vehicle routes can be provided without traversing residential or similar environmentally sensitive zones.

Access for People with Disabilities

- 37 Development should be sited and designed to provide convenient access for people with a disability.
- 38 Where appropriate and practical, development should provide for safe and convenient access to the coast and beaches for disabled persons.

Vehicle Parking

- 39 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand in accordance with [Table ChSt/2 – Off Street Vehicle Parking Requirements](#).

- 40 Development should be consistent with *Australian Standard AS 2890 Parking facilities*.
- 41 Vehicle parking areas should be sited and designed to:
- (a) facilitate safe and convenient pedestrian linkages to the development and areas of significant activity or interest in the vicinity of the development
 - (b) include safe pedestrian and bicycle linkages that complement the overall pedestrian and cycling network
 - (c) not inhibit safe and convenient traffic circulation
 - (d) result in minimal conflict between customer and service vehicles
 - (e) avoid the necessity to use public roads when moving from one part of a parking area to another
 - (f) minimise the number of vehicle access points onto public roads
 - (g) avoid the necessity for vehicles to reverse onto public roads
 - (h) where practical, provide the opportunity for shared use of car parking and integration of car parking areas with adjoining development to reduce the total extent of vehicle parking areas and the requirement for access points
 - (i) not dominate the character and appearance of a site when viewed from public roads and spaces
 - (j) provide landscaping that will shade and enhance the appearance of the vehicle parking areas
 - (k) include infrastructure such as underground cabling and connections to power infrastructure that will enable the recharging of electric vehicles.
- 42 Where vehicle parking areas are not obviously visible or navigated, signs indicating the location and availability of vehicle parking spaces associated with businesses should be displayed at locations readily visible to users.
- 43 Vehicle parking areas that are likely to be used during non-daylight hours should provide floodlit entry and exit points and site lighting directed and shaded in a manner that will not cause nuisance to adjacent properties or users of the parking area.
- 44 Vehicle parking areas should be sealed or paved in order to minimise dust and mud nuisance.
- 45 To assist with stormwater detention and reduce heat loads in summer, outdoor vehicle parking areas should include landscaping.
- 46 Vehicle parking areas should be line-marked to delineate parking bays, movement aisles and direction of traffic flow.
- 47 On-site visitor parking spaces should be sited and designed to:
- (a) not dominate internal site layout
 - (b) be clearly defined as visitor spaces not specifically associated with any particular dwelling
 - (c) be accessible to visitors at all times.

Vehicle Parking for Residential Development

- 48 On-site parking should be provided having regard to:
- (a) the number, nature and size of proposed dwellings

- (b) proximity to centre facilities, public and community transport within walking distance of the dwellings
 - (c) the anticipated mobility and transport requirements of the likely occupants, particularly groups such as aged persons
 - (d) availability of on-street car parking
 - (e) any loss of on-street parking arising from the development (eg an increase in number of driveway crossovers).
- 49 Vehicle parking areas servicing more than one dwelling should be of a size and location to:
- (a) serve users, including pedestrians, cyclists and motorists, efficiently, conveniently and safely
 - (b) provide adequate space for vehicles, including emergency service vehicles, to manoeuvre between the street and the parking area
 - (c) reinforce or contribute to attractive streetscapes.

Vehicle Parking for Mixed Use and Corridor Zones

- 50 The provision of ground level vehicle parking areas, including garages and carports (other than where located along a rear lane access way), should:
- (a) not face the primary street frontage
 - (b) be located to the rear of buildings with access from a shared internal laneway
 - (c) ensure vehicle park entries are recessed at least 0.5 metres behind the main face of the building.
- 51 Development should provide off-street vehicle parking and specifically marked accessible car parking places to meet anticipated demand.
- 52 Loading areas and designated parking spaces for service vehicles should:
- (a) be provided within the boundary of the site
 - (b) not be located in areas where there is parking provided for any other purpose.
- 53 Vehicle parking spaces and multi-level vehicle parking structures within buildings should:
- (a) enhance active street frontages by providing land uses such as commercial, retail or other non-car park uses along ground floor street frontages
 - (b) complement the surrounding built form in terms of height, massing and scale
 - (c) incorporate facade treatments along major street frontages that are sufficiently enclosed and detailed to complement neighbouring buildings consistent with the desired character of the locality.
- 54 In mixed use buildings, the provision of vehicle parking may be reduced in number and shared where the operating hours of commercial activities complement the residential use of the site.

Undercroft and Below Ground Garaging and Parking of Vehicles

- 55 Undercroft and below ground garaging of vehicles should only occur where envisaged in the relevant zone or policy area or precinct, and ensure:
- (a) the overall height and bulk of the undercroft structure does not adversely impact on streetscape character of the locality or the amenity of adjacent properties

- (b) vehicles can safely enter and exit from the site without compromising pedestrian safety or causing conflict with other vehicles
 - (c) driveway gradients provide for safe and functional entry and exit
 - (d) driveways and adjacent walls, fencing and landscaping are designed to provide adequate sightlines from vehicles to pedestrians using the adjacent footpath
 - (e) openings to undercroft areas are integrated with the main building so as to minimise visual impact
 - (f) landscaping, mounding and/or fencing is incorporated to improve its presentation to the street and to adjacent properties
 - (g) the overall streetscape character of the locality is not adversely impaired (e.g. visual impact, building bulk, front setbacks relative to adjacent development)
 - (h) the height of the car park ceiling does not exceed 1 metre above the finished ground level.
- 56 In the case of undercroft and below ground car parks where cars are visible from public areas, adequate screening and landscaping should be provided.

Waste

OBJECTIVES

- 1 Development that, in order of priority, avoids the production of waste, minimises the production of waste, reuses waste, recycles waste for reuse, treats waste and disposes of waste in an environmentally sound manner.
- 2 Development that includes the treatment and management of solid and liquid waste to prevent undesired impacts on the environment including, soil, plant and animal biodiversity, human health and the amenity of the locality.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should be sited and designed to prevent or minimise the generation of waste (including wastewater) by applying the following waste management hierarchy in the order of priority as shown below:
 - (a) avoiding the production of waste
 - (b) minimising waste production
 - (c) reusing waste
 - (d) recycling waste
 - (e) recovering part of the waste for re-use
 - (f) treating waste to reduce the potentially degrading impacts
 - (g) disposing of waste in an environmentally sound manner.
- 2 The storage, treatment and disposal of waste materials from any development should be achieved without risk to health or impairment of the environment.
- 3 Development should avoid or minimise as far as practical, the discharge or deposit of waste (including wastewater) onto land or into any waters (including processes such as seepage, infiltration or carriage by wind, rain, sea spray, stormwater or by the rising of the water table).
- 4 Untreated waste should not be discharged to the environment, and in particular to any water body.
- 5 Development should include appropriately sized area to facilitate the storage of receptacles that will enable the efficient recycling of waste.
- 6 Development that involves the production and/or collection of waste and/or recyclable material should include designated collection and storage area(s) that are:
 - (a) screened and separated from adjoining areas
 - (b) located to avoid impacting on adjoining sensitive environments or land uses
 - (c) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system
 - (d) located on an impervious sealed area graded to a collection point in order to minimise the movement of any solids or contamination of water

- (e) protected from wind and stormwater and sealed to prevent leakage and minimise the emission of odours
- (f) stored in such a manner that ensures that all waste is contained within the boundaries of the site until disposed of in an appropriate manner.

Wastewater

- 7 The disposal of wastewater to land should only occur where methods of wastewater reduction and reuse are unable to remove the need for its disposal, and where its application to the land is environmentally sustainable.
- 8 Wastewater storage lagoons should not be sited in any of the following areas:
 - (a) within land subject to a 1 in 100 year average return interval flood event
 - (b) within 50 metres of the top of the bank of a watercourse
 - (c) within 500 metres of the coastal high water mark
 - (d) where the base of the lagoon would be below any seasonal water table.
- 9 Wastewater storage lagoons should be avoided within a water protection area within the meaning of Part 8 of the *Environment Protection Act 1993*.
- 10 Wastewater storage lagoons should be sufficiently separated from adjacent land uses that may be sensitive to adverse odours.
- 11 Wastewater storage lagoons should be designed and constructed in accordance with the current *Environment Protection (Water Quality) Policy*.

Waste Treatment Systems

- 12 Development that produces any effluent should be connected to an approved waste treatment system which may include sewage, community wastewater management systems, or on-site wastewater treatment and disposal methods.
- 13 The methods for, and siting of, effluent and waste storage, treatment and disposal systems should minimise the potential for environmental harm and adverse impacts on:
 - (a) the quality of surface and groundwater resources
 - (b) public health
 - (c) the amenity of a locality
 - (d) sensitive land uses.
- 14 Waste treatment should only occur where the capacity of the treatment facility is sufficient to accommodate likely maximum daily demands including a contingency for unexpected high flows and breakdowns.
- 15 Any domestic waste treatment system or effluent drainage field should be located within the allotment of the development that it will service.
- 16 A dedicated on-site effluent disposal area should not include any areas to be used for, or could be reasonably foreseen to be used for, private outdoor open space, driveways, car parking or outbuildings.

- 17 The spreading or discharging of treated liquid or solid waste onto the ground should only occur where the disposal area consists of soil and vegetation that has the capacity to store and use the waste without contaminating soil or surface or ground water resources or damaging crops.
- 18 Stock slaughter works, poultry processors, saleyards, piggeries, cattle feedlots, milking sheds, milk processing works, fish processing works, wineries, distilleries, tanneries and fellmongeries, composting works and concrete batching works should have a wastewater management system that is designed so as not to discharge wastes generated by the premises:
 - (a) into any waters
 - (b) onto land in a place where it is reasonably likely to enter any waters by processes such as:
 - (i) seepage
 - (ii) infiltration
 - (iii) carriage by wind, rain, sea spray, or stormwater
 - (iv) the rising of the watertable.

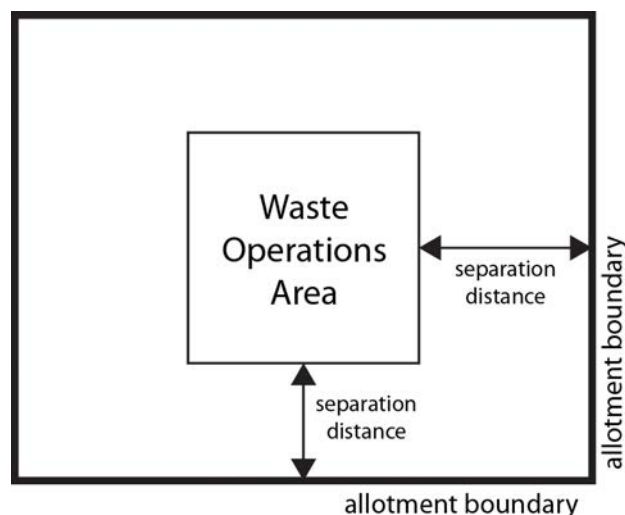
Waste Management Facilities

OBJECTIVES

- 1 The orderly and economic development of waste management facilities in appropriate locations.
- 2 Minimisation of human and environmental health impacts from the location and operation of waste management facilities.
- 3 Protection of waste management facilities from incompatible development.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Waste management facilities should be located and designed to minimise adverse impacts on both the site and surrounding areas from the generation of surface water and groundwater pollution, traffic, noise, odours, dust, vermin, weeds, litter, gas and visual impact.
- 2 Waste management facilities in the form of land fill and organic processing facilities should not be located in existing or future township, living, residential, centre, office, business, institutional or environmental protection, conservation, landscape, water protection and open space areas.
- 3 Waste management facilities should not be located where access to the facility requires, or is likely to involve, the use of non-arterial roads in adjacent residential areas.
- 4 Waste management facilities should be appropriately separated from sensitive land uses and environmentally-sensitive areas. The separation distance between the waste operations area and sensitive uses should be incorporated within the development site as illustrated in the figure below. The waste operations area includes all closed, operating and future cells.



- 5 Only land uses and activities that are compatible with both a waste management facility and any adjacent land uses may be located within the separation distance.
- 6 Separation and/or noise attenuation should be used to ensure noise generation associated with the waste management operation does not unreasonably interfere with the amenity of sensitive land uses.
- 7 Sufficient area should be provided within the waste operations area for the:
 - (a) maximum expected volume of material on the site at any one time

- (b) containment of potential groundwater and surface water contaminants
 - (c) diversion of clean stormwater away from the waste and potentially-contaminated areas.
- 8 Landscaping should be provided to screen views of the processing facilities and operational areas.
- 9 Waste management sites should be accessed by appropriately constructed and maintained roads.
- 10 Traffic circulation movements within any waste management site should:
- (a) be of a dimension and constructed to support all vehicles transporting waste
 - (b) enable all vehicles to enter and exit the site in a forward direction.
- 11 Suitable access for emergency vehicles should be provided to and within waste management site.
- 12 Chain wire mesh or pre-coated painted metal fencing to a minimum height of 2 metres should be erected on the perimeter of a waste management facility site to prevent access other than at entry points.
- 13 Plant, equipment or activities that could cause a potential hazard to the public should be enclosed by a security fence.
- 14 Litter control measures that minimise the incidence of wind blown litter should be provided.
- 15 The waste operations area of a landfill or organic waste processing facility should be sited:
- (a) at least 3 kilometres from an airport used by commercial aircraft to minimise the risk of bird strikes to aircraft
 - (b) at least 1.5 kilometres from an airport used by piston aircraft
 - (c) at least 500 metres from:
 - (i) the boundaries of the allotment
 - (ii) the nearest dwelling, shop, office, public institution or other building designed primarily for human occupation in the case of an organic waste processing facility for the composting of waste
 - (d) at least 250 metres from a public open space reserve, forest reserve, national park, conservation zone or policy area
 - (e) at least 100 metres from:
 - (i) the nearest surface water (whether permanent or intermittent)
 - (ii) a 1 in 100 year average return interval flood event area.
- 16 The waste operations area of a landfill should not be located on land:
- (a) that is subject to land slipping
 - (b) with ground slopes greater than 10 per cent, except where the site incorporates a disused quarry.
- 17 The waste operations area of an organic waste processing facility should not be located on land:
- (a) that is subject to land slipping
 - (b) with ground slopes greater than 6 per cent

- (c) where the interface of the engineered landfill liner and natural soils would be within any of the following:
 - (i) 15 metres of unconfined aquifers bearing groundwater with less than 3000 mg/L total dissolved salts
 - (ii) 5 metres of groundwater with a water quality of 3000 to 12 000 mg/L total dissolved salts
 - (iii) 2 metres of groundwater with a water quality of greater than 12 000 mg/L total dissolved salts.
- 18 Where required, a leachate barrier should be provided between the operational areas and underlying soil and groundwater.
- 19 Landfill activities that have a total storage capacity exceeding 230 000 cubic metres should make sustainable use of landfill gas emissions. For smaller landfill activities, if the sustainable use of the landfill gas emissions is not practical or feasible, flaring should be used to avoid gases being vented directly to the air.

Overlay Section

Affordable Housing Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) - Affordable Housing*.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this Overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the Overlay will prevail.

OBJECTIVES

- 1 Affordable housing that is integrated into residential and mixed use development.
- 2 Development that comprises a range of affordable dwelling types that cater for a variety of household structures.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development comprising 20 or more dwellings should include a minimum of 15 per cent affordable housing unless the development is to occur in stages and it can be demonstrated that any shortfall in affordable housing from any stage of development will be accommodated in another stage or stages.

Noise and Air Emissions Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) – Noise and Air Emissions*.

INTERPRETATION

Where the Objectives and/or Principle of Development Control that apply in relation to this Overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the General Section of the Development Plan, the Overlay will prevail.

OBJECTIVES

- 1 Protect community health and amenity from adverse impacts of noise and air emissions.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Noise and air quality sensitive development located adjacent to high noise and/or air pollution sources should:
 - (a) shield sensitive uses and areas through one or more of the following measures:
 - (i) placing buildings containing less sensitive uses between the emission source and sensitive land uses and areas
 - (ii) within individual buildings, place rooms more sensitive to air quality and noise impacts (e.g. bedrooms) further away from the emission source
 - (iii) erecting noise attenuation barriers provided the requirements for safety, urban design and access can be met
 - (b) use building design elements such as varying building heights, widths, articulation, setbacks and shapes to increase wind turbulence and the dispersion of air pollutants provided wind impacts on pedestrian amenity are acceptable
 - (c) locate ground level private open space, communal open space and outdoor play areas within educational establishments (including childcare centres) away from the emission source.

Strategic Transport Routes Overlay

Refer to the [Map Reference Tables](#) for a list of maps that relate to this overlay.

The following policies apply to the 'designated area' marked on the relevant *Overlay Map(s) - Strategic Transport Routes*.

INTERPRETATION

Where the Objectives and/or Principles of Development Control that apply in relation to this overlay are in conflict with the relevant General Objectives and/or Principles of Development Control in the Development Plan, the overlay will prevail.

OBJECTIVES

- 1 Development that recognises the importance of strategic transport routes and does not impede traffic flow or create hazardous conditions for pedestrians, cyclists or drivers of vehicles, including emergency services vehicles.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development adjacent to a strategic transport route should:
 - (a) avoid the provision of parking on the main carriageway
 - (b) be accessible via service roads, where possible, that provide:
 - (i) parking off the main carriageway
 - (ii) a buffer from the main carriageway for pedestrian and cycle activity
 - (c) not impede the potential for overhead cabling and associated infrastructure to be established in an existing or proposed tram corridor.
- 2 Vehicular site access should not be provided on to a strategic transport route where an alternative access is available.
- 3 Development adjacent kerbside bus stops should be set back to provide sufficient space for indented bus bays with associated hard stand area, shelter and a **minimum** 1.2 metre wide continuous accessible path behind the bus shelter.

Zone Section

Adelaide Shores Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating a range of short-term tourist accommodation and associated facilities, sporting, entertainment, cultural and recreational activities including associated spectator and administrative facilities.
- 2 Development of commercial and retail uses that are associated with and in support of recreation and tourist development.
- 3 Short-term tourist accommodation that is predominantly in the form of caravan and camping sites, cabins and serviced apartments surrounded by open landscaped areas.
- 4 Development of integrated recreational areas and facilities that accommodate a range of activities accessible to the community.
- 5 Buildings and facilities designed to high architectural standards and established within a landscaped setting, including tree plantings around buildings and car parking areas.
- 6 Development including car parking and facilities that are located and designed to blend in with existing vegetation and landscaping and designed to enhance the natural features of the local environment, including visual amenity, landforms, fauna and flora.
- 7 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone includes land in the care and control of the West Beach Trust, which will be developed and maintained as a distinct and vibrant recreation, sporting and tourism destination of State significance, in accordance with [Concept Plan Map ChSt/27 – Adelaide Shores](#). The zone will include a range of tourist activities and tourist development, including tourist accommodation, such as camping sites, caravans and cabins. Land within the zone also comprises a range of recreation and sporting uses including public reserves, golf courses, driving ranges, tennis courts, sporting clubs and grounds, indoor sporting facilities, skate and BMX facilities. It is desirable for recreation uses to be integrated further with tourism related land uses in the area, aiming to generate year-round activities. The zone presents as a key location for tourist accommodation opportunities to be established adjacent the West Beach dune, beaches and coastal areas, which includes opportunity for boating activities.

Commercial and retail activities in the zone will be associated with recreation, sporting or tourism uses. Development should be of a size and scale required to primarily serve the needs of tourists, visitors to the area and users of nearby recreational facilities. Development in the order of 500 to 1500 square metres of total retail floor space, that may include smaller tenancies that cater for a mix of uses, is envisaged. Development should be consistent with the open landscape character of the site through the application of appropriate setbacks and landscaping, building scale and materials that are compatible with this character.

Land with frontage to Tapleys Hill Road may accommodate commercial development, where designed to complement other forms of retail and commercial development within the immediate locality. Tapleys Hill Road, however, is classified as a major traffic and freight route that caters for the safe and efficient movement of all types of road users. As this road provides optimal travel efficiency and reliability of travel times throughout the day for heavy vehicles, particularly when freight and commuter peaks coincide, it is envisaged that new development within the vicinity of Tapleys Hill Road will be designed and sited to minimise any impact or interference on the flow of traffic along Tapleys Hill Road. In achieving this, no additional access points are to be created to Tapleys Hill Road.

SARDI (SA Research and Development Institute-a research division of Primary Industries and Resources SA) is a purpose built marine and freshwater research facility located off Hamra Road. There may be an opportunity to provide improved linkages to the facility or incorporate an educational component, such as an interpretive centre.

Development including buildings and facilities will be sited so as not to cause an unreasonable nuisance or disturbance to occupants of adjacent residential buildings or tourist accommodation within the locality. Furthermore, development will have regard to possible noise impacts from the operation of the adjacent Adelaide Airport and must ensure that the safe operation of the airport is not compromised. Development will have also regard to potential odour impacts of the adjacent Glenelg Wastewater Treatment Plant and must ensure that the future operation of the plant is not compromised.

There may be areas within the zone that are contaminated because of previous activities. Further investigations to determine the extent of site contamination and necessary remediation measures may therefore be required to ensure that sites are suitable and safe for their intended use.

Public realm improvements along Military Road will enhance the appearance and street presentation, while promoting low speed vehicle movements and providing for a safer environment for pedestrians and cyclists. Convenient and safe pedestrian access along with pathways for cycling and movement throughout the zone are encouraged, including improved pedestrian and cycling connections beyond the zone.

The Patawalonga Creek which traverses the zone may in the long term accommodate part of a proposed airport runway extension. The Patawalonga Creek and surrounding land will continue to be protected and enhanced as a location of environment and habitat value.

The zone will be developed as a major tourism and sporting hub including the improvement to existing sporting and recreational facilities as well as the establishment of new sporting and recreational facilities. It is envisaged that tourism uses will be primarily located to the west of Military Road with recreation and sporting uses, occurring mostly to the east of Military Road.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the zone:

- amenity block, including shower, toilet and laundry facilities
- amusement machine centre
- cabin
- car parking
- caravan permanently fixed to land
- clubroom or meeting room in association with a recreation, community or tourist development
- community centre
- consulting room in association with a recreation, community or tourist development
- dwelling in association with and ancillary to tourist accommodation (ie manager's residence) or a dwelling and/or accommodation that provide accommodation for participants and/or staff of sports related activities
- educational establishments where they relate to marine, sport, recreation or tourism activities
- emergency services facility
- entertainment, cultural or exhibition facility in association with a recreation, community or tourist development
- fencing in association with recreation, community or tourist development
- golf course or golf driving range
- hall or theatre
- hotel or licensed premises in association with a recreation, community or tourist development
- indoor and outdoor recreation facility
- lighting for night use of facilities
- marine-related research activities
- office in association with recreation, community or tourist development

- outbuilding or store in association with recreation, community or tourist development
 - personal service establishment
 - recreation area/sports ground and associated facilities including a tennis court, basketball court or playground
 - restaurant
 - shops or groups of shops in association with recreation, community or tourist development
 - showground
 - special event
 - spectator and administrative facilities ancillary to recreation, community or tourist development
 - swimming pool/spa
 - tourist park and other forms of tourist accommodation including a motel, backpacker's hostel, caravan park and/or camping ground.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Tourist accommodation and other uses associated with tourist development including laundries and ablution facilities, shops, restaurants, entertainment, cultural and recreational facilities should be designed and sited to serve holiday makers visiting the area, users of nearby recreational facilities and tourists.
- 4 Development within the zone should be sited and designed in an integrated and co-ordinated manner to facilitate connection with tourist, recreation, community and boating land uses and activities within the zone and adjacent coastal areas.
- 5 A community, sport and recreation hub comprising indoor and outdoor sport, community, entertainment and function facilities should be developed on the southern side of Hamra Avenue and be integrated and coordinated with the range of land uses and activities within the locality, including the adjacent marine-related research activities, where possible.
- 6 The community, sport and recreation hub should be designed to include a hotel or tavern that is able to provide dining facilities for at least twice the number of patrons as bar facilities, to primarily serve the needs of tourists, visitors, local residents and people associated with recreation activities in the immediate locality.
- 7 Intensive tourist and golfing related facilities including function and conference facilities, restaurants, entertainment and recreation facilities should be located primarily on allotments that front onto Military Road.
- 8 Retail development is to be associated with recreation, sporting or tourism uses and should be of a size and scale required to serve the needs of tourists, visitors to the area and users of nearby recreational facilities.
- 9 Administrative, service, storage and maintenance facilities should be unobtrusively sited and screened.

Form and Character

- 10 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 11 Buildings, caravans fixed to land, annexes and recreational facilities within a caravan park or tourist development should be setback a minimum of:
- (a) 1 metre from an internal road
 - (b) 6 metres from a public road
 - (c) 2 metres from the boundary of the caravan park or camping ground.
- 12 Recreation facilities associated with tourist accommodation should be of a size and scale that will maintain the open natural character of the area and be ancillary to the primary role and function of the caravan park or tourist development.

- 13 Strong thematic landscaping incorporating locally indigenous plant species should be established to improve the landscape, provide shade and shelter, create interest and provide attractive landscaped civic spaces that are well connected with other activities and encourage public interaction.
- 14 Landscaping should form an integral part of the design and be used to define spaces and activity areas, reinforce internal networks, screen utility areas and enhance the visual amenity of the area.
- 15 Development adjacent to the Patawalonga Creek should enhance the character and appearance of the area and retain the creek as a water resource and stormwater channel.
- 16 Internal roads should be surfaced to prevent dust becoming a nuisance.
- 17 Safe and convenient pedestrian and bicycle pathways should be developed to encourage access between land uses and activities within the area, such as tourist accommodation, recreation and tourist facilities, boating and marine facilities and adjacent coastal areas.
- 18 Priority should be given to pedestrian movement within the area and provision for vehicular movement should be limited to that required for access to car parking areas, accommodation units and servicing requirements.
- 19 Every caravan, cabin or dwelling site within a caravan park or tourist development should provide parking for at least one vehicle, either located on the site or grouped within the park.
- 20 Car parking areas should be consolidated and co-ordinated where possible, shaded and screened with vegetation and setback to both improve the amenity of the zone and minimise the impact on adjacent residential areas
- 21 The shared use of car parking facilities between different activities and venues should be encouraged.
- 22 In areas prone to flooding or other natural hazards, buildings and structures (including annexes attached to caravans or caravans fixed to land) should be designed and constructed so that they can be removed in the event of a hazard.
- 23 Development adjacent to the facilities within the Glenelg Wastewater Treatment Plant should satisfy (a) and (b):
 - (a) not prejudice the plant's current or future operations
 - (b) be located, designed and developed having regard to the potential environmental impacts associated with odour from the operation of the facility and the potential expanded operation of the facility.
- 24 Advertisements and/or advertising hoardings should be associated with recreation, sporting and tourism activities within Adelaide Shores and measure no greater than 9 metres in height and up to 18 square metres in sign area.

Traffic and Access

- 25 There should be no additional access points to West Beach Road unless it can be demonstrated that there are movement and safety benefits that reduce impact on adjacent residential areas. Intersection treatments should also be designed to minimise impact on adjacent land uses.

Land Division

- 26 Other than a lease plan, land division can only occur where a rearrangement of existing allotment boundaries takes place as part of a co-ordinated development scheme or as a rationalisation of land holdings, that is designed to allow more efficient and economic use of land consistent with the objectives for the zone.
- 27 No additional allotments should be created within the zone.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of development	Exceptions
Dwelling	Except for any of the following: (a) a manager's residence in association with and ancillary to tourist accommodation (b) a dwelling and/or accommodation for participants and/or staff of sports related activities.
Fuel depot	
Horse keeping	
Industry	
Land division	Except where no additional allotments are created either partly or wholly within the zone or the division is the subject of a lease plan in association with and ancillary to sporting, tourist or recreational uses.
Motor repair station	
Nursing home	
Service trade premises	
Supported accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development (except where the development is non-complying) are designated:

Category 1	Category 2
All forms of development which are envisaged in the zone and located 60 metres or more from a Residential Zone .	All forms of development which are envisaged in the zone, excluding development of a minor nature, where they are located less than 60 metres from a Residential Zone .

Airfield Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone primarily accommodating aircraft operations, passenger terminals, airport and aviation-related light industrial, service industrial, warehouse and storage purposes
- 2 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This zone accommodates a range of services and facilities necessary for the safe, convenient and efficient operation of aviation activities at the Adelaide Airport. This zone will continue to accommodate a range of airport and export related industrial and commercial uses, and recreational activities of an open character on land reserved for long term aviation needs. Development will be compatible with the principle aviation function of the airport and maintain the long term operational and safety needs of the airport.

Development will promote the economic improvement of the State by maintaining the airport as the international, national and regional gateway to South Australia, enhancing the airport as a major element of public infrastructure and facilitating the movement of time sensitive freight and passengers by infrastructure improvements.

Adequate separation distances between non-aviation and aviation development on airport land, and between development on airport land and off airport uses will be adopted. The Adelaide Airport has also been committed as one of the sites for stormwater harvesting. The Adelaide Airport stormwater scheme will source water from the Brownhill/Keswick Creek System. A storage basin will capture flows, which will then be treated using biofiltration and media filtration. It is anticipated to initially harvest about 400 megalitres a year.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - air passenger or air freight terminal
 - aircraft related facility
 - airport
 - fuel depot
 - light industry ancillary to and in association with aviation activities
 - recreation area
 - service industry ancillary to and in association with aviation activities
 - shop within the terminal building
 - warehouse ancillary to and in association with aviation activities.
- 2 Development listed as non-complying is generally inappropriate.
- 3 Development that would be adversely affected by noise and other hazards caused by airport activities should not be undertaken in the zone.
- 4 Development should not impede the use of the zone for aviation purposes.
- 5 Development west of Tapleys Hill Road should primarily be for recreational activities of an open character until such time as the area is required for aviation purposes.

Form and Character

- 6 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 7 Commercial and light industrial development located on the airport site should:
- (a) facilitate the more efficient operation of the airport
 - (b) be sited in defined clusters
 - (c) not adversely affect the amenity of surrounding land uses.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Community centre	
Consulting room	
Dwelling	
Educational establishment	Except where ancillary to and in association with aviation activities.
Hospital	
Horticulture	
Hotel	
Intensive animal keeping	
Motel	
Nursing home	
Place of worship	
Pre-school	
Residential flat building	
Shop	Except where located within the terminal building.
Tourist accommodation	
Waste reception, storage, treatment or disposal	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Coastal Open Space Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 Coastal land protected from development other than that necessary for conservation, recreational activity and public facilities.
- 2 Preservation and upgrading of the scenic character of the coastal landscape and foreshore areas fronting urban areas, townships or settlements.
- 3 Development of foreshore areas for recreational use with essential conveniences and facilities for the public.
- 4 Land subject to inundation or susceptible to erosion kept free of development.
- 5 Maintenance of safe and convenient pedestrian access between the beach and adjoining development.
- 6 Maintenance of open space links with local and regional open space networks and corridors.
- 7 Within **Precinct 1 Fort Glanville**, short-term tourist accommodation and associated facilities.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are considered appropriate in the zone:
 - barbecue, picnic table, shelter
 - caravan park and associated facilities in **Precinct 1 Fort Glanville**
 - coastal protection works
 - community recreation facility directly related to water activities (such as sailing clubs, boat ramps)
 - jetty
 - play ground, play equipment
 - public car parking
 - shop with a gross leasable area of less than 250 square metres
 - tourist-related facilities and tourist accommodation in **Precinct 1 Fort Glanville**.
- 2 The provision of facilities should be related to the demand for such facilities so as to prevent oversupply and inappropriate siting.
- 3 Development should be for public purposes and use.
- 4 Development that does not require a coastal location should not be located in the zone.

Form and Character

- 5 Development should not diminish the ability of the public to use and enjoy the coast or to gain access to the foreshore.
- 6 Community facilities including shelters, boat ramps, public conveniences and kiosks, should be sited in convenient and accessible locations linked to the surrounding vehicular and pedestrian movement networks.

- 7 Development should be designed and sited to be compatible with conservation and enhancement of the coastal environment and scenic beauty of the zone.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precinct.

Precinct 1 Fort Glanville

- 8 Development within the precinct should primarily be for a range of tourist-related facilities and short-term tourist accommodation in the form of caravan and camping sites and cabins.
- 9 Development accessory to tourist accommodation, such as kiosks, laundries, ablutions and storerooms, should be provided where appropriate.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Boarding house	
Bus depot	
Bus station	
Cemetery	
Commercial forestry	
Community centre	Except where located in Precinct 1 Fort Glanville and in association with and ancillary to tourist accommodation.
Consulting room	
Crematorium	
Dairy	
Dam	
Dwelling	
Educational establishment	
Farming	
Fuel depot	
Hall	
Horse keeping	

Form of Development	Exceptions
Horticulture	
Hospital	
Hotel	
Indoor recreation centre	Except where located in Precinct 1 Fort Glanville and in association with and ancillary to tourist accommodation.
Industry	
Intensive animal keeping	
Land division	Except where: <ul style="list-style-type: none"> (a) no additional allotments are created wholly or partly within the zone (b) there is no increase in the number of allotments with frontage or direct access to the coast.
Mining	
Motel	
Motor repair station	
Nursing home	
Office	Except where located in Precinct 1 Fort Glanville and in association with and ancillary to tourist accommodation or where used for the purposes of administering the <i>National Parks and Wildlife Act 1972</i> .
Petrol filling station	
Place of worship	
Pre-school	
Public service depot	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where the gross leasable area is less than 250 square metres.
Stadium	
Stock sales yard	
Stock slaughter works	
Store	
Theatre	
Tourist accommodation	Except where located in Precinct 1 Fort Glanville .
Warehouse	
Waste reception, storage, treatment or disposal	

Form of Development	Exceptions
Water tank	
Welfare institution	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .

District Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre that accommodates a range of retail facilities, offices, consulting rooms, and cultural, community, public administration, entertainment, educational, religious and residential facilities to serve the community and visitors within the surrounding district **distributed across five distinct centres within the following suburbs:**
 - Fulham Gardens
 - Hindmarsh
 - Kilkenny
 - West Lakes
 - Woodville.
- 2 Development of a visually and functionally cohesive and integrated district centre.
- 3 A centre accommodating medium to high-density residential development in conjunction with non-residential development.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - affordable housing in **Precinct 21 Railway Station**
 - aged care accommodation
 - bank
 - child care centre
 - civic centre
 - consulting room
 - discount department store
 - dwelling in conjunction with non-residential development
 - educational establishment
 - emergency services facility
 - entertainment facility
 - health facility
 - hospital
 - hotel
 - indoor games centre
 - library
 - mixed use development in **Precinct 21 Railway Station**
 - motor repair station
 - office
 - place of worship
 - playing field
 - pre-school
 - residential flat building in **Precinct 21 Railway Station**
 - restaurant
 - row dwelling in **Precinct 21 Railway Station**
 - shop
 - supermarket
 - swimming pool
 - tourist accommodation in **Precinct 21 Railway Station.**

- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 Medium to high-density residential development and development comprising a variety of residential and non-residential uses may be undertaken provided such development does not prejudice the operation of existing or future retail activity within the zone.

Form and Character

- 4 Development should be sited and designed to promote linkages between the various developments within the centre and adjoining main roads.
- 5 Facilities within the centre should be sited and designed with a view to promoting after-hours use to reinforce the centre as the focus of social activity in the district.
- 6 Undercroft or semi-basement car parking areas should not project above natural or finished ground level by more than 0.75 metres.
- 7 Except in Precinct 21, dwellings should be located only behind or above non-residential uses on the same allotment.
- 8 Medium density development that achieves gross densities of between 23 and 45 dwellings per hectare (which translates to net densities of between 40 and 67 dwellings per hectare) should typically be in the form of 2 to 4 storey buildings.
- 9 High density development that achieves gross densities of more than 45 dwellings per hectare (which translates to net densities of more than 67 dwellings per hectare) should typically be in the form of over 4 storey buildings.

Vehicle Parking

- 10 Vehicle parking should be provided in accordance with the rates set out in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies)

Land Division

- 11 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Fulham Gardens Policy Area 1

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The planned and orderly expansion of the Fulham Gardens Centre through the redevelopment of existing residences to centre related activities.
- 2 The controlled expansion of existing facilities and the staged development of new facilities to increase the range of services provided in the Fulham Gardens Centre.
- 3 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The **Fulham Gardens Policy Area 1** should be a central place for that part of north western Adelaide stretching from Burbridge Road in the south, Findon Road in the east and the Grange Railway line in the north.

Precinct 2 Retail Core Fulham Gardens

This precinct provides the focus for shopping, business, community and leisure activities within the policy area. The area will be developed to provide additional specialty shopping and a wider range of goods and services to meet weekly shopping needs.

Pedestrian and vehicular conflict across Valetta Road should be minimised. The appearance of the centre will be improved by additional landscaping. It is envisaged that the service area adjacent to The Parkway will be upgraded and its appearance improved.

Extension of the centre complex will take advantage of location and open onto the adjacent public open space.

Additional landscaping comprising shade trees and raised paved pedestrian walkways will be developed within the car park.

Precinct 3 Commercial Fringe Fulham Gardens

This precinct primarily accommodates banking, large space retailing, service trade premises, bulky goods outlet, office, restaurant and take away food facilities. The area will be well screened from housing at the rear and access and parking areas co-ordinated and integrated so that it develops in a visually and functionally cohesive manner.

It is envisaged that building development will be limited in bulk and scale and sited a sufficient distance from Irene Avenue to allow adequate landscaping and to provide for a central parking and servicing area separated from the residential frontage.

New development is expected to provide lighting to the walkway on the southern boundary of the precinct and pedestrian access will be integrated with that walkway.

Precinct 4 Recreation Fulham Gardens

This precinct will primarily accommodate leisure, recreational, institutional and community facilities in the Centre.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/1 - Fulham Gardens Policy Area 1](#).
- 3 Building development should be a maximum of 12 metres in height.
- 4 Vehicle access from Valetta Road and adjacent car parking areas north and south should be rationalised.
- 5 No additional vehicle access should be provided from Irene Avenue.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 2 Retail Core Fulham Gardens

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 7 The following forms of development are envisaged in the precinct:
 - community facility
 - leisure facility
 - office
 - shop.

Precinct 3 Commercial Fringe Fulham Gardens

- 8 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 9 The following forms of development are envisaged in the precinct:
 - bank
 - bulky goods outlet
 - office
 - restaurant
 - service trade premises
 - takeaway food outlet.
- 10 The maximum gross leasable floor area of a shop or group of shops, other than a restaurant or bulky goods outlet, should be in the order of 250 square metres.

Precinct 4 Recreation Fulham Gardens

- 11 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 12 The following forms of development are envisaged in the precinct:
 - community facilities
 - institutional facilities
 - leisure facilities
 - recreational facilities.

Hindmarsh Policy Area 2

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

This policy area represents the heart of the original Town of Hindmarsh comprising the former Council Chambers, former Town Hall, Police Station, Post Office, commercial centre, banks, churches, former schools and many older houses. The area will be reinvigorated through establishment of a wide variety of social, economic, cultural and residential uses, sensitively designed to reinforce the desired character, to become a vibrant local community hub for social, economic and cultural development.

While new development in recent years comprises a significant component of the area, the prevailing urban character of the central part of the Centre is still predominantly 1870-1910 encompassing the Victorian, Edwardian and Early Federation periods, overlaid by Art Deco elements from the Inter-War period. The urban character of this area is established by the original layout, the fine civic and commercial premises along Port Road and by the other older buildings found in Orsmond, Milner and Mary Streets. The area is notable for its concentration of State and Local Heritage Places. The historic heart extends to the Hindmarsh Soccer Stadium and adjoining historic structures.

Buildings along Port Road are mostly two-storey and built in typical Victorian fashion. These and other older buildings in the area are characterised by bluestone, sandstone, red brick and rendered masonry walls and parapets, with hipped and gabled corrugated iron roofs, and verandas over footpaths. Windows are typically timber with shop display windows and double-sash windows predominating. The streets are lined with mature trees which contribute to the established older character of the area. The area is focused onto Port Road and Milner Street. New development will maintain and strengthen this urban built form and character.

Most recent development has occurred towards the edges of the policy area including office development on the northern side of Port Road, the Adelaide Entertainment Centre at the eastern gateway and commercial, wholesaling and service industry activities towards the south-western end of the zone. These areas will be regenerated and reinforced by a more intensive mix of business and commercial activities complemented by appropriately designed urban housing and tourist accommodation.

An impressive, active and attractive frontage to Port Road and maximization of pedestrian access and amenity is encouraged through new development.

Pedestrian-oriented frontages, particularly to Port Road, including the provision of verandas, pergolas or other shelter, and outdoor dining facilities are encouraged, whilst ensuring that vehicular sight distances along footpaths are not impaired.

Pedestrian spaces will be developed with an open character, be surrounded by buildings and include structures which achieve a human scale, and have a high standard of landscaping.

Retail development, including a supermarket, comparison goods and specialty shops, is envisaged primarily along the southern side of Port Road only, with the exception of bulky goods outlets and restaurants, which are appropriate on both sides of Port Road.

Artist's and craftspeople's studios and workshops that are small scale, low impact, and which may occur in conjunction with residential use, are appropriate.

The modelling of facades, fenestration and architectural detailing which contribute to a rich, textural character is desired in the development of the area.

It is envisaged that development fronting Port Road will contribute to active street frontages at ground level, create a cohesive townscape and provide appropriate definition to Port Road as the major thoroughfare linking Adelaide with Port Adelaide, with buildings sited on or close to the front boundary.

On streets other than Port Road small street set backs are appropriate for development unless there is a predominant set back pattern already established in the street. Landscaping will be located between the building and the street. The street will be defined and the adjacent buildings complemented through the appropriate scale and siting of development. Buildings up to two storeys are appropriate on streets other than Port Road. Where buildings exceed this height their upper levels will be set back from adjoining sites by stepping the third storey or roof back further than the first two storeys.

Buildings or walling will screen car parking, including undercroft parking, from view from Port Road.

Port Road vehicle egress and access will be restricted, with vehicle movements limited to other roads adjacent to the development.

High quality landscaping and the retention of existing mature trees will be achieved through the appropriate design and siting of development.

Non-residential uses which generate noise, including live entertainment, will continue to operate within the centre. Noise sensitive development such as residential development in the immediate vicinity of such uses, will be designed, orientated and constructed to minimise conflicts and not unreasonably interfere with the continued operation of these uses.

To improve appearance and reduce crime, landscaping will be provided along allotment boundaries that abut the railway line. Development, including land division, adjoining the railway line will facilitate the provision of a bikeway running adjacent to the railway.

Precinct 5 Historic Hub

This precinct comprises the historic heart of the Hindmarsh Policy Area 2 and is the principal area for a range of retail, business and office uses. Uses of a leisure, institutional, community or educational nature are appropriate, as well as development that contributes to evening and weekend activity. The precinct will comprise a diversity of housing types and densities for all age groups and to meet a range of housing needs, as well as tourist accommodation. Shop top housing is appropriate. In particular, the opportunity to convert existing buildings to residential use at upper levels is envisaged along Port Road, Manton and Orsmund Streets.

South of Port Road between Milner and Mary Streets, the opportunity exists for the development of a supermarket, specialty shops and facilities for large space retail and comparison goods. South of Port Road, in-between Milner and George Streets and east of Mary Street, will predominantly comprise a range of retail, office and community uses. Fronting Port Road, opportunities exist for further office, residential, commercial and community activities. Existing retail, office and leisure use activities fronting Port Road west of George Street will be supported.

Traditional patterns of development will be maintained and incorporate verandas or balconies and appropriate parapets and roof forms.

Precinct 6 Port Road Office

South of Port Road, the Port Road frontage comprises a range of retail, office and leisure uses, with commercial and residential uses comprising the remainder of the precinct to the south. These uses should be reinforced, whilst the addition of shop top housing is also appropriate.

North of Port Road and east of Anne Street, this precinct comprises primarily office uses. Residential accommodation is also appropriate provided it is designed to minimize external impacts from adjoining or nearby noisy activities.

North of Port Road and west of Anne Street, this precinct comprises commercial and service industry uses. Residential development is inappropriate where it would unreasonably interfere with the operation of existing industrial premises in the precinct.

The status of the junctions of South Road with Port Road and Manton Street as corner sites and gateways to the precinct will be reinforced with development of appropriate scale and built form and with a minimal setback.

Precinct 7 Manton Street

This precinct comprises a range of office, commercial and community uses with opportunities for further office, residential, shop top housing, commercial and community uses in the remainder of the precinct. In particular, the opportunity to convert existing buildings and sites to residential uses exists along Manton and Orsmund Streets.

Precinct 8 Gateway

This precinct contains a major entertainment facility, and the remainder of the precinct should comprise office, leisure and tourist accommodation uses, as well as uses of a community or institutional nature. The opportunity exists to develop office, restaurant and other business activities in association with the major entertainment facility. However, residential development is inappropriate where it would unreasonably interfere with the operation of the Adelaide Entertainment Centre or other entertainment facilities in the precinct.

Development on the corner of Port Road and Adam Street will reflect its status as a major gateway to Adelaide's north western suburbs and address this major intersection. Development in this location will approach the maximum building height, be of substantial scale and of good quality finishes and external appearance.

Development fronting Manton Street or Adam Street will be of appropriate siting and scale to define and provide a hard edge to each street.

Precinct 9 Recreation Hindmarsh

The opportunity exists to reinforce and improve this precinct's sporting facility and related activities.

Development will be encouraged to define streets and corners through the use of appropriate scale, setback and well articulated built form elements. Indented landscaping along street frontages will also be encouraged.

Precinct 10 Port Road Gardens

This precinct comprises the Port Road and Milner Street medians which have a distinctive character established by the street trees and adjacent heritage buildings. The formal open landscape and Art Deco character of structures on the Port Road median should be retained and enhanced through careful design, use of art and planting.

The entrances to Port Road at Park Terrace and South Road should be improved to reflect their status as part of the gateway to Port Road and opportunities should be taken to enhance the existing setting and features through the provision of major art works, sculptures, landscaping and the like.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/2 – Hindmarsh Policy Area 2](#).
- 3 Residential buildings should:
 - (a) have a maximum height of 6 metres on side and rear boundaries

- (b) be stepped back 1 metre for every additional metre in height above 6 metres to reduce bulk, scale and overshadowing.
- 4 Development should be a maximum height of 12 metres (or 3 storeys) above natural ground level at any point.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 5 Historic Hub

- 5 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 6 Residential development should not displace retail and commercial frontages at street level facing Port Road.
- 7 Development should be two to three storeys in height and be built to the full width of any front site boundary to Port Road.

Precinct 6 Port Road Office

- 8 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 7 Manton Street

- 9 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 8 Gateway

- 10 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 11 Development fronting Manton Street or Adam Street and those sections of Port Road identified as 'Major Townscape Focus' in [Concept Plan Map ChSt/2 – Hindmarsh Policy Area 2](#) should not exceed 15 metres in height.
- 12 Development along those sections of Port Road not identified as 'Major Townscape Focus' in [Concept Plan Map ChSt/2 – Hindmarsh Policy Area 2](#) within the precinct should not exceed 12 metres in height.

Precinct 9 Recreation Hindmarsh

- 13 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 10 Port Road Gardens

- 14 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Kilkenny Policy Area 3

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

Land Use and Density of Development

The State Government's Planning Strategy for Metropolitan Adelaide envisages that the policy area will be developed as a large District Centre serving the inner north-western suburbs of metropolitan Adelaide where substantial urban renewal is occurring. To meet this objective and to provide a key community focal point for the surrounding district, a transformation in development through the intensification and diversification of activities within the policy area will be encouraged.

Desirable land use activities will focus on serving the district shopping needs of the area. Complementary land uses that assist in extending the usage of the Centre beyond normal working hours will also be encouraged to enhance its vibrancy and safety. A wide range of shopping, entertainment, office, community, cultural, religious and recreational activities are appropriate, together with medium to medium/high density housing in some parts of the policy area. Residential development will recognise that a range of impacts and disturbances may occur as a result of commercial and community activities operating within the policy area.

To accommodate new facilities, development in the existing District Centre will intensify within already developed areas. It is envisaged that significant expansion of the existing shopping complex will occur in the portion of the policy area bounded by Torrens Road, Hanson Road, First Avenue, Gray Street and Regency Road.

Building Design

Buildings that appear as isolated monoliths with austere facades setback too far from main roads and surrounded by open, poorly landscaped car parking, are inappropriate. Improvements in the appearance of development will be achieved by providing buildings with an outward orientation in areas of high pedestrian activity, such as entrances to buildings/malls and along highly visible facades such as those directly facing roads, ground level car parking areas and pedestrian thoroughfares. Design features such as pedestrian shelter/verandas against building facades, variation in the texture of building materials and articulation that creates light and shade on facades are envisaged. The setback of built form should maintain the predominate pattern of buildings.

Public Realm and Landscaping

Visitor facilities and amenities will be provided in the form of designated footpaths that promote safety, covered walkways that provide shade and shelter, and outdoor eating/meeting spaces that promote social interaction. Child and parent friendly facilities will also be provided, including public toilets, parenting rooms and child minding facilities.

A variety of open and enclosed spaces will be provided throughout the District Centre to promote diversity in the form and function of gathering areas within the Centre. The incorporation of large plaza spaces that promote interaction at all hours, thereby maximising safety, are also envisaged.

Landscaping will be carefully integrated with built form, ensuring that vegetation is sustainable, drought tolerant, locally indigenous species and matched to the scale of development while also providing a comfortable, pleasant and attractive environment at pedestrian level. Extensive landscaping of ground level car parking areas will also be a prominent feature of the Centre. Pedestrian and/or shared pedestrian, bicycle and vehicular paths will be well landscaped and visually prominent. Development will also incorporate artwork features as an integral element of buildings and public spaces.

Movement and Parking

The policy area will develop an integrated, safe and convenient movement system for vehicles, cyclists and pedestrians with as little reliance as possible on the use of the surrounding arterial roads for intra-centre movement of vehicles. Main vehicle access points from the surrounding arterial roads will be limited. The use of basement and rooftop level car parking will be maximised ahead of the provision of new ground level parking areas, together with adequate and convenient provisions of parking for people with disabilities or prams. Emphasis will be given to a comprehensive and convenient pedestrian circulation network. Cyclists will also be provided with appropriate bicycle parking and storage facilities.

Environment

Development will incorporate energy efficient building design and materials, and maximise the use of renewable energy sources. Development will seek to minimise noise, visual impacts and other amenity impacts on residential land uses particularly along First Avenue. Stormwater will be carefully managed so as to minimise its impact off-site and maximise its potential for reuse on-site.

Public Transport

A bus interchange is an essential facility that will be retained within the policy area. It is anticipated that any future redevelopment of the Centre may require the relocation of the existing interchange within the policy area. However, in order to maintain safe and convenient pedestrian access between the interchange and the main shopping centre as the major destination for users of public transport, the interchange will remain centrally located. Such a facility will be large enough to cater for short, medium and long term anticipated public transport demands.

Precinct 11 Retail Core Kilkenny

The precinct will maintain its place as the focus of the Centre, allowing for the future development of retail activities, including bulky goods outlets and restaurants, as well as a range of office, institutional, community, entertainment and leisure activities. It is envisaged that this retail core area will be consolidated and integrated further.

The design of additional floor space to the major complex will be the focus of future community facilities in the centre, which may include library and leisure facilities

The integral public transport interchange facilities centrally located within the centre will continue to provide convenient and accessible service. Future centre alterations and additions will enable the continuation of this important facility.

Precinct 12 Regency Road

It is envisaged that the precinct will be developed primarily for bulky goods outlets and service trade premises as well as restaurant activities. The precinct to remain highly visible from the retail core and has excellent exposure to Regency Road with integrated parking facilities. Activities not compatible with the **Precinct 11 Retail Core Kilkenny** may be suitable in this precinct.

Precinct 13 Hanson Road

This precinct has a good quality business character which will be reinforced through further development primarily of offices, banks and consulting rooms.

Precinct 14 Commercial Fringe Kilkenny

This precinct will be developed primarily for bulky goods outlets and service trade premises. Offices and other uses to complement the **Precinct 11 Retail Core Kilkenny** are also appropriate within this precinct.

This precinct should be developed around shared and integrated parking for uses that have little impact external to their site.

Precinct 15 Residential

This precinct will be developed as an integrated, self contained and environmentally sustainable medium to high density residential area. Development is encouraged that comprises a variety of housing types including aged accommodation.

The orientation of development will be designed to encourage links with retail uses in **Precinct 11 Retail Core Kilkenny**.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/3 – Kilkenny Policy Area 3](#).
- 3 Except where located in Precinct 15 Residential, no additional vehicular access points should be created to Regency Road, Torrens Road or Hanson Road.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 11 Retail Core Kilkenny

- 4 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 5 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - community facilities
 - entertainment
 - institution
 - leisure facilities
 - office
 - restaurant
 - retail.
- 6 No parking should be provided between building setbacks and First Avenue.

Precinct 12 Regency Road

- 7 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 8 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - restaurant
 - service trade premises.
- 9 The maximum gross leasable floor area of a shop or group of shops, other than a restaurant or bulky goods outlet, should be in the order of 100 square metres.

Precinct 13 Hanson Road

- 10 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 11 The following forms of development are envisaged in the precinct:
 - bank
 - consulting room
 - office.
- 12 Development should be a maximum of 9 metres in height.
- 13 Car parking and service areas should be located at the rear of the building or behind the building line.

Precinct 14 Commercial Fringe Kilkenny

- 14 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 15 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - office
 - service trade premises.
- 16 Buildings should be a maximum of 12 metres in height.
- 17 Car parking and service areas should be located at the rear of the building or behind the building line.

Precinct 15 Residential

- 18 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 19 The following forms of development are envisaged in the precinct:
 - medium to high density dwellings.
- 20 Development should not exceed a floor area ratio of 0.6.
- 21 Building development should be a maximum height of 13.5 metres.
- 22 The maximum gross leasable floor area for office uses should be in the order of 5000 square metres.
- 23 Residential development should be oriented towards the centre of the policy area to minimise noise impacts from arterial roads.
- 24 Development should ensure car parking facilities are designed to provide a buffer between any adjacent housing or noise sensitive uses and higher intensity uses.
- 25 Communal open space with dimensions at least 50 metres by 50 metres should be provided in the centre of the policy area.
- 26 Development for housing for the aging should provide useable open space at the rate of 35 square metres per dwelling, part of which is able to accommodate an area having a dimension at least 5 metres by 4 metres with the balance of the open space having a minimum width of 2.5 metres.
- 27 No more than two access points off Torrens Road and two off Regency Road should be provided.

West Lakes Policy Area 4

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The policy area should be the central place for that part of north western Adelaide stretching from Burbridge Road in the south, East Avenue in the east and Port Road/Grand Junction Road/Bower Road in the north. It should serve the shopping needs of the area together with the District Centre at Kilkenny and Port Adelaide Regional Centre.

The policy area will function as a public transport interchange and be the focus of the public and community transport network.

The range of activities provided in the policy area should be expanded to include additional leisure, community, entertainment and cultural facilities.

It will be important to maintain and enhance the existing urban design character and townscape in new development.

Buildings set back from roadways with landscaped car parking areas between the building and road alignment are appropriate.

Parking areas will be landscaped with attractive tall shade trees and large areas broken into smaller ones by planting between areas of parking and along the main pedestrian refuge pathways. The landscaping treatment will be used to enhance the character and reinforce the identity of the Centre.

It will be important to develop pedestrian spaces with an open character, human scale and high standard of landscaping.

The Lake will be protected by ensuring development is located, designed and managed so that it does not detract from the character and amenity of the Lake or discharge waste or stormwater run off so as to pollute the Lake.

The addition of access points off West Lakes Boulevard is not appropriate.

Precinct 16 Retail Core West Lakes

This precinct provides the focus for retailing, personal service, leisure and community activities within the West Lakes Policy Area. It operates within a closed mall environment surrounded by ground level car parking.

The development of after hours activities and institutional and community facilities is encouraged. The northern end of the precinct will continue to provide the focus for large space retail including bulky goods outlets, fast food, restaurant and commercial activities in the West Lakes Centre. Residential development is not envisaged within this precinct.

Any extension of floor space will be designed to ensure centrally located and convenient public and community transport access, linkages to retail areas and shelter, lighting and comfort for public transport users.

The establishment of public transport interchange facilities will be important in new development as an integral part of extension of floor space so that they are central to major centre activities and conveniently accessible for public transport vehicles from the surrounding road systems. Alterations and additions that similarly enable the establishment of such facilities as part of the development are desirable.

Precinct 17 Lakefront

Existing Character / Pattern of Development

This precinct forms the interface between the major retail complex of West Lakes Mall and the artificial lake of West Lakes. Existing development along the lakefront comprises primarily low scale commercial development taking advantage of lakefront views. Two exceptions to the built form are the 6 storey Lakes Hotel and, just outside the area, the Island Point apartment building.

Land Use / Function

The precinct will continue to be developed as a mixed use area providing a range of office, leisure, restaurant, residential, tourist accommodation, open space and community activities and utilising the lake front for views and public spaces.

It is envisaged that residential development at medium to high densities will only occur where located above non-residential land uses.

Streetscape / Built Form

New development in this precinct that takes advantage of its proximity to a range of services, public transport and the location on the lakefront through an increase in the scale of development is desirable. Development is not appropriate within the precinct that exceeds 4 storeys (15 metres) in height, as indicated on [Concept Plan Map ChSt/4 – West Lakes Policy Area 4](#).

Building Design

New development of a contemporary architectural style which complements and contributes positively to the existing built form is desirable within the precinct. To ensure the mass and bulk of new buildings is not greater than that of adjacent buildings, it will be appropriate to articulate new buildings by stepping in higher floors commensurate with existing landmark buildings in the locality.

The provision of articulated and varied facades to buildings is important in order to make the scale of development more human. High quality structured landscaping is also desirable to mitigate large scale building facades, provide visual amenity and shade, and help establish a clear hierarchy of vehicle and pedestrian movement patterns across the precinct.

It is important that new buildings and additions to existing buildings complement the established character. Characteristics which could be incorporated into new building design include:

- (a) variation in walling setbacks with angled roof planes
- (b) rendered or brick walls, and primarily light textures and colours
- (c) primarily strong horizontal elements with clearly defined and segmented vertical elements
- (d) an asymmetrical appearance for higher buildings as viewed from the lakefront and Brebner Drive.

Allowing for spaces between buildings will enable development to provide clear and unobstructed sightlines from Brebner Drive through to the lake.

Public Realm

It will be important to improve and strengthen the interface between buildings and the public realm by establishing clearly defined breaks between buildings to provide landscaping and sightlines to the lake from Brebner Drive and through the provision of pedestrian linkages between Brebner Drive and the lakefront walkway.

The provision and/or enhancement of pedestrian linkages between Precinct 15 Retail Core West Lakes and this precinct and between Brebner Drive and the lakefront walkway is desirable.

The creation of vibrant frontages and public spaces adjacent to the lakefront is desirable. High levels of human activity and interaction will be achieved by integrating these public spaces with the buildings.

It will be important to provide consistent high quality landscaping along both the Brebner Drive and lake frontages of the precinct and within the car parking areas that is integrated with the built form and pedestrian linkages.

Fencing to the Brebner Drive or lakefront frontages is not appropriate.

Public participation within the West Lakes Policy Area will be encouraged through prominent buildings that facilitate the creation of public spaces adjacent to the lakefront. It will be important to clearly define these public spaces by landscaping, pavement treatment, lighting and street furniture.

Environment

It will be important to minimise noise and other amenity impacts on residential land uses.

Key Elements of Desired Character

The following design elements will be given greater weight when determining whether or not a development proposal is in accordance with the desired character:

- (a) reducing bulk and mass as the height of buildings increases
- (b) creation of public spaces adjacent to the lakefront, and where possible in-between buildings
- (c) creation of a pedestrian friendly and human scale built form adjacent to the lakefront and Brebner Drive, including pedestrian linkages between Brebner Drive and the lakefront and in accordance with [Concept Plan Map ChSt/4 – West Lakes Policy Area 4](#).

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development are envisaged in the policy area:

- community facilities
- cultural facilities
- department store
- entertainment
- office
- supermarket
- specialty shop.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 3 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/4 - West Lakes Policy Area 4](#).
- 4 Car parking should be available for shared use between activities within the policy area and the adjoining Stadium Zone outside retail trading hours.

Land Division

- 5 Land division should not create additional allotments which would require access points to be created off West Lakes Boulevard.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 16 Retail Core West Lakes

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 17 Lakefront

Land Use

- 7 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 8 Medium to high-density residential development should only be undertaken in conjunction with a primary envisaged use in the precinct and provided such development does not prejudice the operation of existing or future commercial activities within the precinct.
- 9 The facade of new development should include all of the following:
 - (a) a clearly defined base, middle and top
 - (b) architectural features which give human scale at street level and the lake-front, such as entrance porches
 - (c) the breaking up of large, horizontal facades into smaller, articulated sections, where the proportion of solid to void is at a human scale
 - (d) a well defined entry point
 - (e) the use of subdued colours and textures
 - (f) the use of shading devices.

Form and Character

- 10 Development to the north of 141- 143 Brebner Drive, as defined in [Concept Plan Map ChSt/4 - West Lakes Policy Area 4](#) should not exceed 4 storeys nor 15 metres in height.
- 11 Development to the south of 141-143 Brebner Drive (inclusive), as defined in [Concept Plan Map ChSt/4 - West Lakes Policy Area 4](#) should not exceed 6 storeys nor 21 metres in height.
- 12 The mass and bulk of new buildings and/or additions to existing buildings should be reduced:
 - (a) as the height of buildings increases
 - (b) by restricting the total floor area of buildings so as to generally not exceed the following prescriptive requirements:
 - (i) 55 per cent of the site area for 1st to 3rd levels
 - (ii) 45 per cent of the site area for 4th level or greater.

- 13 Undercroft or semi-basement car parking areas should not project above natural or finished ground level by more than 750 millimetres.
- 14 The façade of new development should include the use of shading devices.
- 15 Dwellings should only be located above non-residential land uses.
- 16 Medium density residential development should typically be in the form of 3 to 4 storey buildings.
- 17 High density residential development should typically be in the form of 4 to 6 storey buildings.
- 18 Development should optimise safety and security by maximising activity on lakefront and street frontages.
- 19 All plant, air conditioning and lift well structures should be screened from public view.

Woodville Policy Area 5

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The concentration of activities of a hospital and related medical and consulting room nature with appropriate residential development around the Queen Elizabeth Hospital.
- 2 A large mix of activities including medium and high density housing supported by a mix of compatible land uses to make use of the public transport facilities serving the centre.
- 3 The concentration of activities of office, retail, restaurant and residential activities between Port Road and the rail station around the Civic Centre and in the Core Area within Precinct 21 Railway Station as shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#).
- 4 Development of St Clair Reserve as a major indoor and outdoor district level facility.
- 5 Well designed and functional mixed use areas with a walkable urban form, pedestrian and cyclist friendly streetscapes, and active street frontages that facilitate personal interaction and promote public transport use.
- 6 A strong urban design focus for the Woodville Centre, particularly at the intersection of Woodville and Port Roads, and the adjacent public spaces and within [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#).
- 7 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area will be developed with a distinct 'village high street' character and should function within each Precinct to accommodate a diverse range of activities and facilities. The Woodville Centre should only serve sections of its adjoining residential areas with retail services.

The policy area's significance as a District Centre lies in its accommodating the Queen Elizabeth Hospital, Civic Centre and Town Hall, major transport interchanges and the St Clair Recreation centre and open space. The policy area is located around the remnants of the historic Woodville village and has strong urban design elements and an intimate 'high street' atmosphere not found in more conventional suburban centres.

The conservation and enhancement of the historic townscape and buildings along Woodville Road and the reinforcement of the village high street character is important. The improved appearance of buildings with Port Road frontages is desirable. Except within Precinct 21 Railway Station where a landscaped open space frontage is appropriate, pedestrian oriented frontages will be reinforced to Woodville Road through development, including the siting of buildings at the street alignment and the provision of verandas, pergolas and other shelter for pedestrians.

Development consistent with controlled expansion of existing facilities and the staged development of new facilities to increase the range of services in the policy area is desired.

The policy area will utilise and reinforce these major distinguishing characteristics to provide an alternative pedestrian oriented, high quality environment containing a wide range of activities and facilities.

Precinct 18 Woodville Road Medical

This precinct contains The Queen Elizabeth Hospital, ancillary health facilities, consulting rooms, aged care accommodation and community facilities. Development related to the hospital is appropriate. It is envisaged that the heritage and streetscape significance of the historic church complex at the junction of Woodville Road and Church Street will be retained, and complemented by new development to the south.

It is desirable to minimise the visual and operational impact of the hospital and associated carparking and traffic impact on adjacent residential areas. This can be achieved through the careful siting and design of new development in the hospital grounds.

It is envisaged that development will be scaled down in intensity towards the boundaries of the precinct. To improve amenity in the precinct it is desired that buildings are of a high standard of design with car parking areas consolidated and landscaped at the rear of properties facing Woodville Road. A plaza setting will be created by siting buildings back from the road with landscaped set back areas. The amenity of Woodville Road will be improved through high quality building designs with articulated facades.

A mixture of residential, office, community and hospital uses is envisaged in the western section of the precinct of a lower scale and intensity to be compatible with a residential environment.

Precinct 19 Woodville Road Gardens

This precinct is a formal landscaped open space complementing the Port Road frontage, historic buildings and major townscape elements. The public space will provide a sense of arrival from all directions.

Precinct 20 Civic

This precinct is the core of the policy area containing a range of retail, business, office, community and civic activities. The precinct has the major townscape element of the Port Road / Woodville Road intersection and St Margaret's Church at one end and the Civic Centre, Town Hall and rail station at the other. The two are linked by a lower scale established strip shopping development and activities straddling Woodville Road.

The traditional pattern of development is a valued feature and should be maintained. It is desired that new buildings be of a contemporary architectural character which is sympathetic and reflective of existing traditional building scale, form and style.

The major townscape element of the Port Road / Woodville Road intersection will be reinforced by development of appropriately large scale to complement St Margaret's Church and the wide expanse of Port Road. It will be important to reinforce the hard edged continuous and varied Woodville Road frontage and enhance the pedestrian environment with verandas, pergolas and awnings to provide pedestrian protection and to achieve an appropriate scale.

The Woodville Road frontage has a historic and intimate 'high street' ambience and a more pedestrian dominant environment is desired. The use of widened footpaths for outdoor eating and after hours restaurants and leisure activities are encouraged. Medium to high density residential development and shop top housing is appropriate.

The Port Road frontage contains a mix of office and service uses and has a lower visual amenity. Its current disparate commercial and service character should be improved by good quality new development, preferably two storey in height with minimal set back and with all parking at the rear. It is envisaged that the rail station will be linked by a safe pedestrian network to other parts of the policy area and surrounding residential areas. In order to achieve this pedestrian access and safety across Woodville Road will need to be improved by new crossing facilities.

Precinct 21 Railway Station

The Railway Station precinct will feature an attractive, high-quality mix of medium to high density housing integrated with a range of cafes, shops and offices and quality public open spaces focused around a bustling and revitalised Woodville Railway Station. An attractive, well-landscaped plaza may be established near the Woodville Railway Station and fronting Woodville Road to provide a focus for community activities such as open air markets, outdoor dining and events.

Within the precinct, the land to the north east of the proposed new collector road (St Clair Avenue), surrounding the St Clair Recreation Centre and the land to the north of Brocas Avenue will be further developed as a major district recreational area with a range of educational, institutional and community facilities. Additional development of the open space for both passive and active recreation is desirable.

Residential development will be restricted to the Core Area and Transition Area as shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#) and will include a variety of adaptable housing types that cater for a range of household types, ages and life cycle stages, including residential flat buildings and row dwellings, aged and student accommodation and serviced apartments. Residential development will aim to achieve a minimum net density of 70 dwellings per hectare in the Core Area and provide a minimum of 15 per cent of residential dwellings for affordable housing. Detached and semi-detached dwellings are not appropriate in the precinct.

Development will be connected to Woodville Road and the “St Clair” residential development to the north via a new collector road (St Clair Avenue). Other roads within the zone will be local in function and nature.

The circulation of cars will be restricted to ensure that pedestrians and cyclists are able to move safely and freely. Off-road paths will be provided which are safe, accessible, well signed and connect to shops, community facilities, the railway station, surrounding residential development, the high school and St Clair Recreation Centre. In particular, a shared-use recreational trail will be established alongside the railway line which, ultimately, will connect the precinct with the Central Business District to the east and Port Adelaide to the west.

Parking will typically be in either basement or decked structures, with at grade parking limited to lower density housing forms such as row dwellings, parking associated with the St Clair Recreation Centre and educational area to the north of Brocas Avenue, visitor parking in small car parks or on-street parking. Parking rates for residential development will be less than traditional suburban areas in order to take full advantage of the excellent location of the precinct to services, facilities and public transport.

Open space and landscaping forms an integral part of the precinct. New open space areas will soften the built form and create quality public spaces. These will include 22 per cent of the land to the south west of the St Clair Avenue extension being developed as open space along the Woodville Road frontage and an additional area of land in this locality to be developed as a playing field. Other open space areas will include a shared-use recreational trail along the former Glenys Nunn Drive and an internal pocket park to provide further recreational opportunities for residents. The St Clair Reserve open space will be further developed for both passive and active recreation.

The intent is that the majority of existing Significant and Regulated Trees will be retained as landscape features within areas of open space. In particular, the existing Red Gums alongside the railway line will be retained within the shared-use recreational trail.

To minimise the impacts on development in adjacent zones, the land to the south west of the new collector road (St Clair Avenue) within the precinct will be developed as a more intensive core separated from nearby lower density residential zones by a less intensely developed area as illustrated in [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#).

The form, scale and mix of development will be at its greatest intensity in the Core Area where buildings of up to five storeys in height are envisaged. Within the Core Area, land uses located at street level may include a mix of residential, shop, office and tourist accommodation. Office uses may be appropriate on upper floors especially where they provide a buffer between more active ground floor and residential activity. The mix of uses will cater for the day to day needs of local residents and workers and will be at its greatest intensity closest to the Woodville Railway Station. Features and activities that attract people and add vitality to the street, such as display windows, retail shopfronts and outdoor dining areas are desirable.

Land developed predominantly for residential purposes, but which may include other low impact uses such as offices, will act as a buffer between the higher intensity area of the precinct and Actil Avenue, as indicated by the Transition Area shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#). The Transition Area will be developed with building heights of up to five storeys with three storey row dwellings fronting Actil Avenue.

Development in the Core and Transition Areas will be within defined building envelopes that manage the location and scale of buildings to achieve high quality urban design. Buildings at the interface of the precinct will create an appropriate transition of development scale and massing.

General Building Design

The design of buildings throughout the precinct will feature contemporary designs which also complement the existing character of Woodville. To achieve this, large expanses of glass above ground level will be avoided through the use of a mixture of solid wall surfaces and recessed balcony and window voids, reflective of existing built forms. Building materials will reflect the existing materials within the locality such as brick, timber and corrugated iron.

A diversity of materials, roofs and fenestration will be utilised to create visual interest.

Building design will maximise access to solar energy and minimise overshadowing and overlooking within the constraints of a multi storey development area, through appropriate building envelopes, transition of building heights, design and siting of windows and balconies, and use of landscaping. Building design will provide options for energy generation, water conservation and food production or rooftop gardens.

Core Area/Mixed Use Building Design

The design of larger and mixed use buildings will feature:

- buildings that frame the street spaces by being aligned close to the street
- buildings that create a strong sense of verticality
- buildings that incorporate verandas or awnings to provide pedestrian shelter and balconies, either projecting or recessed, to create light and shade
- buildings that have active frontages where the primary entrance is via the street rather than internal arcades
- buildings that have strong variation in elevations
- buildings with interesting roof shapes and lines.

Within the precinct, front fences will be minimised to ensure visual permeability and avoid large blank walls to encourage passive surveillance, active streetscapes and a visually interesting public realm.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan MapChSt/5 - Woodville Policy Area 5](#).
- 3 Vehicle access points should be coordinated onto Woodville Road and Port Road and shared between adjoining development.
- 4 Large open lot, undercroft or decked vehicle parking areas should be screened from public view by use of well designed building materials, walls, landscaping or other means which enhance the external appearance of the land and buildings.

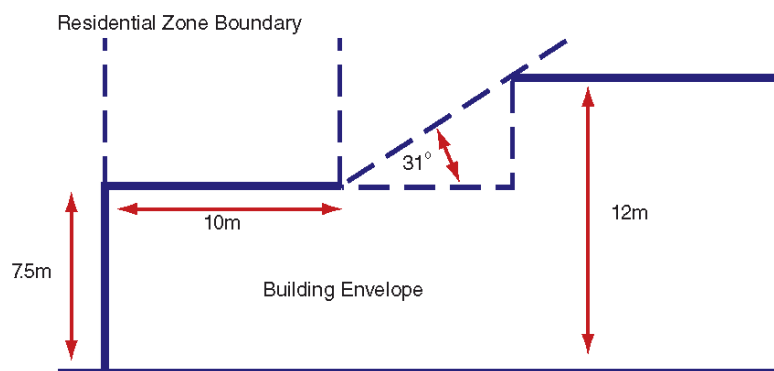
PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 18 Woodville Road Medical

- 1 Development should not be undertaken unless it is consistent with the desired character for the precinct.

- 2 The following forms of development are envisaged in the precinct:
 - aged care accommodation
 - community facilities
 - consulting room
 - hospital
 - office
 - residential.
- 3 Access and egress to car park facilities west of Woodville Road should be only via Woodville Road.
- 4 Development, including a car parking structure, west of Woodville Road, should not exceed 7.5 metres in height within 10 metres of the nearest residential zone boundary. Where building development is more than 10 metres from the nearest residential zone boundary, it should not exceed 12 metres in height and should be contained within a plane angled 31 degrees above the horizontal, measured from any point 7.5 metres above a line located 10 metres from the nearest residential zone boundary, as shown on figure below:



- 5 Building development involving a car parking structure west of Woodville Road should not consist of more than a ground floor and two further levels of parking.
- 6 Building development east of Woodville Road should not exceed 18 metres in height.
- 7 Development east of Woodville Road should be set back 8 metres behind a landscaped plaza to Woodville Road.
- 8 Additional vehicular access to development off Woodville Road and Port Road is undesirable and to minimise these number of access points, land division should consolidate allotments.

Precinct 19 Woodville Road Gardens

- 9 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 10 The following forms of development are envisaged in the precinct:
 - public open space.

Precinct 20 Civic

- 11 Development should not be undertaken unless it is consistent with the desired character for the precinct.

- 12 The following forms of development are envisaged in the precinct:
- civic
 - community facilities
 - office
 - retail.
- 13 Building development should be a maximum of 12 metres in height.
- 14 Buildings on the corner of Port Road and Woodville Road should be of a substantial size and bulk and complement the historic St Margaret's Church. Buildings should have only small setbacks from these corners and be two storeys in height or provide a high parapet wall to the Port Road frontage with an overall height of not less than 5.5 metres.
- 15 Additional vehicular access to development off Woodville Road and Port Road is undesirable and to minimise these number of access points, land division should consolidate allotments.

Precinct 21 Railway Station

- 16 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Land Use

- 17 The following types of development, or a combination thereof, are envisaged in the precinct as identified on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#):

Core Area	Transition Area	Other Areas
Affordable housing	Affordable housing	Community centre
Aged persons accommodation	Aged persons accommodation	Educational establishment
Community centre	Office	Institutional facilities
Consulting room	Public open space	Pre-school
Mixed use development	Residential flat building	Primary school
Office	Retirement village	Public open space
Parking facility	Row dwelling	Recreational facility
Place of worship	Shop or group of shops, excluding a bulky goods outlet or a retail showroom	
Public open space	Supported accommodation	
Residential flat building	Tourist accommodation	
Retirement village		
Row dwelling		
Shop or group of shops, excluding a bulky goods outlet		
Supported accommodation		
Tourist accommodation		

- 18 Development listed as non complying is generally inappropriate.

- 19 With the exception of the St Clair Recreation Centre and Woodville High School where a district focus is appropriate, non-residential land uses should:
- (a) have a local or neighbourhood focus to their scale of activity and intended market catchment.
 - (b) encourage walking to local shopping, community services and other activities
 - (c) not detrimentally impact on the amenity of nearby residents.
- 20 The Core Area, Transition Area and other identified features should be developed in accordance with [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#).
- 21 The Core Area should be developed to include a range of land uses that are high pedestrian generators, directly promote public transport use and provide opportunities for multi-purpose trips.
- 22 Development should primarily take the form of:
- (a) in the Core Area - residential flat buildings, row dwellings, non-residential buildings and buildings comprising two or more land uses with non-residential land uses on the ground floor
 - (b) in the Transition Area - residential flat buildings and row dwellings, with small scale shops and offices.
- 23 Retail and office land uses should be concentrated near the Woodville Railway Station as well as in buildings that provide a visible frontage to Woodville Road.
- 24 The Transition Area should be developed to provide a transition between an intense core of development and neighbouring lower intensity residential development.
- 25 Shops or groups of shops and offices should have a maximum gross leasable area generally in accordance with the following:

Designated area	Office (square metres total floor area)	Shop or groups of shops (square metres gross leasable)
Core Area	1000	1000
Transition Area	100	80

Affordable Housing

- 26 Development should include a minimum of 15 per cent of residential dwellings for affordable housing.

Form and Character

- 27 Development should graduate from medium-rise in the Core Area to low-rise in the Transition Area, especially where the development site abuts the zone boundary.
- 28 Residential development should aim to achieve a minimum net residential density of 70 dwellings per hectare in the Core Area and 60 dwellings per hectare in the Transition Area.
- 29 In the Transition Area, development should provide a built form that provides the transition between the Core Area and neighbouring lower intensity residential development.

Design and Appearance

- 30 Garage top apartments that share the allotment and services of the main dwelling should:
- (a) be no more than 2 storeys in height above the garage (a total of 3 storeys)

- (b) front a street that provides rear access for vehicles
 - (c) complement the existing dwelling or mixed use building.
- 31 Development should be located and designed to:
- (a) maintain the cultural significance and architectural integrity of the Brocas State Heritage place (situated on 111 Woodville Road, Woodville)
 - (b) protect the historical and visual character of the Brocas State Heritage Place and its setting.
- 32 In the Core Area the ground and first floor of buildings of 4 or more storeys should be built:
- (a) to dimensions to allow for adaptation to a range of land uses, including retail, office and residential, without the need for significant change to the building
 - (b) so that the ground floor primary frontage of buildings is visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.
- 33 Except in Core Areas, development of 3 or more storeys in height should ensure that:
- (a) north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 3.00 pm on 21 June
 - (b) ground level open space of existing buildings receives direct sunlight for a minimum of 2 hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
 - (i) half of the existing ground level open space
 - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres).
- 34 Buildings should address public open space and defined pedestrian and cycle routes as illustrated in [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#).
- 35 Masonry fences should be no more than 1.2 metres in height to maintain sight lines between buildings and the street, and to improve safety through passive surveillance.
- 36 Buildings should feature contemporary designs that are reflective of the existing character of Woodville by:
- (a) including walls above ground level that achieve a high 'solid to void' ratio
 - (b) using building materials such as brick, timber and corrugated iron.
- 37 Street facades should have a strong sense of verticality and visual interest with buildings sited close to the street and incorporating both (a) and (b):
- (a) balconies and verandas or awnings to provide pedestrian shelter
 - (b) a diversity of materials, roof styles and fenestration.
- 38 The public realm should be designed and constructed to a high quality and amenity, create more appealing public spaces that include public art, feature lighting, the creative use of high quality recycled materials, street furniture and landscaping (including understorey plantings).

- 39 Side streets and rear lane access ways should be designed to:
- (a) provide space between buildings that reduces building mass and creates a more interesting public realm
 - (b) achieve active frontages at a lower intensity than the primary street frontage.

Building Envelope

Building Height

- 40 Except where airport building height restrictions prevail or PDC 41 or 42 apply, building heights (excluding any rooftop located mechanical plant or equipment) should be consistent with the following parameters:

Designated area	Minimum building height	Maximum building height
Core Area	No minimum	5 storeys and up to 20.5 metres
Transition Area	No minimum	5 storeys and up to 20.5 metres

- 41 Building development north of the new collector road as indicated on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#) should be a maximum of 12 metres in height.
- 42 Building development fronting Actil Avenue should be a maximum of 3 storeys in height.

Setbacks from the Primary Road Frontage

- 43 Buildings (excluding garages, verandas, porticos and the like) should be set back from the primary road frontage in accordance with the following parameters:

Designated area	Minimum setback from the primary road frontage (metres)
Core Area	No minimum
Transition Area	3

- 44 Where associated with row dwellings, garages should be set back 5.5metres from the primary road frontage.

Setbacks from Side Boundaries

- 45 Within the Core and Transition Areas shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#), buildings (excluding verandas, porticos and the like) should be set back from side boundaries in accordance with the following parameters:

Designated area	Minimum setback from side boundaries (metres)
Within the Core Area	No minimum
Within the Transition Area	No minimum

Other Setbacks

- 46 Within the Core and Transition Areas shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#), buildings (excluding verandas, porticos and the like) should be set back in accordance with the following parameters:

Setback parameter	Value (metres)
Minimum setback from secondary road frontage	0.9
Minimum setback from a rear lane access way	No minimum where the access way is 6.5 metres or more. OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles.
Minimum setback from the rear allotment boundary	5 metres except where: <ul style="list-style-type: none"> (a) development abuts the wall of an existing or simultaneously constructed building on the adjoining land (b) a building envelope plan exists for the adjoining land that proposes a wall on the rear boundary be constructed to the same or lesser length and height as provided for by that plan.

Off Street Vehicle Parking

- 47 Vehicle parking should be provided at the following rates:
- (a) for residential development, 0.75 car parking spaces per dwelling
 - (b) for a shop or group of shops, 3 car parking spaces per 100 square metres of gross leasable floor area
 - (c) for tourist accommodation, 1 car parking space for every 4 bedrooms up to 100 bedrooms and 1 car parking space for every 5 bedrooms over 100 bedrooms
 - (d) all other non-residential uses, 3 car parking spaces per 100 square metres of gross leasable floor area at ground floor level and 1.5 car parking spaces per 100 square metres of gross leasable floor area above ground floor level.
- 48 A lesser car parking rate may be applied where justified based on local circumstances, for example where:
- (a) the proposed development is adjacent to a designated pedestrian and/or cycling path
 - (b) the proposed development is in convenient walking distance to readily accessible and frequent public transport
 - (c) convenient on-street car parking is readily available (other than where on-street parking may be removed as a result of road works/widening)
 - (d) the proposed development is on or adjacent to the site of a heritage place which hinders the provision of on-site parking
 - (e) there is the opportunity to exploit shared car parking areas between uses based upon compatible hours of peak operation
 - (f) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means

- (g) for studio apartments, student accommodation, affordable housing, retirement villages or aged persons' accommodation.
- 49 Except for in association with row dwellings, car parking at basement level, undercroft parking or multi level parking (or a combination of) should be provided rather than 'at grade' parking.

Land Division

- 50 Land division should be undertaken in accordance with [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#) and should result in new allotments of a size and configuration that ensures the desired character and principles of development control for the precinct can be achieved, including a large area of open space provided along the Woodville Road frontage.
- 51 Land division should result in an allotment pattern that includes a well planned and connected network of walking and cycling routes.
- 52 Land division should result in a limited number of new vehicular access points to Actil Avenue and St Clair Avenue and no new access points in addition to St Clair Avenue to Woodville Road.
- 53 Streets that provide rear access for vehicles are encouraged.
- 54 A traditional street grid pattern should be reinforced in any comprehensive development of areas for mixed use activity to maintain clear sightlines and ensure maximum connectivity.
- 55 Within the Core and Transition Areas as shown on [Concept Plan Map ChSt/24 - Precinct 21 Railway Station](#), wherever practicable, land division and site amalgamation should:
- (a) create allotments that vary in size and are suitable for a variety of residential and commercial uses
 - (b) improve the level of integration associated with the design and layout of buildings, vehicle parking areas, access points and landscaping treatments
 - (c) create allotments that enable the provision of a minimum number of 15 per cent of residential dwellings for affordable housing.
- 56 Road reserves should be designed to provide on-street parking spaces that vary in size to accommodate the parking of cars and commercial vehicles.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

- 1 Subject to the conditions contained in [Table ChSt/1 - Conditions for Complying Development](#):
 - (a) demolition of a building, outbuilding or fence on a site which is not listed as a State or Local heritage place or designated as a contributory item in **Hindmarsh Policy Area 2**.
 - (b) work undertaken within a building or outbuilding on a site which is not listed as a State heritage place that does not increase the total floor area of the building or outbuilding and does not alter its external appearance in **Hindmarsh Policy Area 2**.
- 2 A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:
 - (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
 - (b) the development is located inside any of the following area(s):
 - **Precinct 2 Retail Core Fulham Gardens**
 - **Precinct 5 Historic Hub**
 - **Precinct 6 Port Road Office**
 - **Precinct 7 Manton Street**
 - **Precinct 11 Retail Core Kilkenny**
 - **Precinct 16 Retail Core West Lakes**
 - (c) the building is not a State heritage place
 - (d) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
 - (e) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
 - (f) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):

- (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (g) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
- (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Fulham Gardens Policy Area 1

Within Precinct 2 Retail Core Fulham Gardens

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry.
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 3 Commercial Fringe Fulham Gardens

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding and greater than 6 metres in height. 	

Form of Development	Exceptions
Industry	Except where it is for a service industry.
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 4 Recreation Fulham Gardens

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding and greater than 6 metres in height. 	
Dwelling	
Fuel depot	
Industry	Except where it is for a service industry.
Motor repair station	
Service trade premises	
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Hindmarsh Policy Area 2

Within Precinct 5 Historic Hub

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry.
Motor repair station	
Road transport terminal	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 6 Port Road Office

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry that is located north of Port Road and east of Anne Street or fronting the southern side of Port Road.
Motor repair station	Except where it is located south of Port Road.
Road transport terminal	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 7 Manton Street

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry.
Motor repair station	
Road transport terminal	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 8 Gateway

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry that is located south of Port Road.
Motor repair station	

Form of Development	Exceptions
Road transport terminal	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 9 Recreation Hindmarsh

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	Except where it is for a service industry.
Motor repair station	
Road transport terminal	
Waste reception, storage, treatment or disposal	Except where it involves the remediation of land.

Within Precinct 10 Port Road Gardens

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing but excluding illuminated signs (d) is freestanding and greater than 6 metres in height. 	
Industry	
Motor repair station	
Road transport terminal	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Kilkenny Policy Area 3

Within Precinct 11 Retail Core Kilkenny

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Industry	Except where it is for a service industry.

Form of Development	Exceptions
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 12 Regency Road

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Dwelling	
Industry	Except where it is for a service industry.
Tourist accommodation	
Supported accommodation	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 13 Hanson Road

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Amusement machine centre	
Fuel depot	
Hotel	
Indoor recreation centre	
Industry	
Motor repair station	
Petrol filling station	
Service trade premises	
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 14 Commercial Fringe Kilkenny

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Industry	Except where it is for a service industry.
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Within Precinct 15 Residential

Form of Development	Exceptions
Advertising and/or advertising hoardings	Except where it is for only one identification hoarding per street frontage.
Amusement machine centre	
Fuel depot	
Hotel	
Indoor recreation centre	
Industry	
Motor repair station	
Petrol filling station	
Service trade premises	
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

West Lakes Policy Area 4

Within Precinct 16 Retail Core West Lakes

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Dwelling	
Industry	Except where it is for a service industry.
Warehouse	

Within Precinct 17 Lakefront

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Industry	Except where it is for a service industry.
Warehouse	

Woodville Policy Area 5

Within Precinct 18 Woodville Road Medical

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
Industry	Except where it is for a service industry.
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land

Within Precinct 19 Woodville Road Gardens

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing. 	
All uses	Except where it the involves the remediation of land or for any of the following purposes: <ul style="list-style-type: none"> (a) recreation area (b) shop (c) office (d) consulting room.

Within Precinct 20 Civic

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding on Woodville Road, north of Port Road. 	
Industry	Except where it is for a service industry.

Form of Development	Exceptions
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land

Within Precinct 21 Railway Station

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding on Woodville Road. 	
Fuel depot	
Industry	
Motor repair station	
Service trade premises	
Warehouse	
Waste reception, storage, treatment or disposal.	Except where it involves the remediation of land.

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 or Local Heritage Place that is described in Table ChSt/6 or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 .	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 . Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area.

Education Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone primarily accommodating educational facilities and associated recreational facilities.
- 2 Development of a scale compatible with existing buildings and surrounding residential areas.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - education facility
 - recreation facility.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.

Form and Character

- 3 Development should be compatible with the height and scale of existing buildings.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table Chst/1 - Conditions for Complying Development](#):

- education establishment
- pre-school
- primary school.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Amusement machine centre	
Bus depot	
Caravan park	
Consulting room	
Dwelling	

Form of Development	Exceptions
Fire station	
Hall	
Hotel	
Indoor recreation centre	
Industry	
Motor repair station	
Office	
Petrol filling station	
Road transport terminal	
Service trade premises	
Shop	
Showground	
Stadium	
Store	
Telecommunications facility	
Theatre	
Warehouse	
Waste reception, storage, treatment or disposal	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .

Home Industry Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating small-scale service and light industries where people live and work on the same site.
- 2 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone comprises mainly well established residential properties interspersed with existing smaller scale commercial or industrial premises. Areas, such as along Ledger Road, Beverley and Carlton Crescent, Woodville North, provide a transition and a separation between areas dedicated to full industrial activity and areas dedicated to housing. Other areas, such as north of Athol Street, Athol Park, and Adele Avenue, Kidman Park, are located within industrial areas and provide continued opportunity to work from home with a balance between residential amenity and industrial activity.

The zone will be developed as a home industry area comprising housing fronting roads with small scale, low impact industrial uses such as warehousing or similar industries, located behind housing in rear yards. Existing commercial or industrial uses, as well as home industries, should minimise impacts on more sensitive zones. Residential development should recognise the industrial location and implement sound attenuation, screening and other measures to protect their own residential amenity. Large scale manufacturing and high impact industries are not appropriate.

The area will be enhanced through development with a high quality of appearance and design and extensive landscaping. It is desirable to improve amenity and provide pedestrian shelter through the planting of street trees of appropriate scale.

A residential appearance and function to the street frontage will be maintained. Signs will be of a small-scale nature and located and designed to reflect the residential character and amenity of the zone.

The amalgamation of sites in Beverley (north of the BASA Stadium) is encouraged to provide a frontage of at least 15 metres.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development in association with a detached dwelling are envisaged in the zone:
 - catering and hospitality supplies
 - collectables and repairs
 - cottage industry
 - equipment rental, hire and repairs
 - furniture and cabinet making
 - manufacture and repair of textile, clothing and footwear
 - minor engineering workshop
 - minor expansion of existing industrial and commercial uses
 - photographic or graphics studio
 - pottery and crafts
 - printing or screening

- store
 - studio workshops (music, craft, dance, gym)
 - vehicle service and repairs.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 A home industry should operate on allotments on which there is an occupied detached dwelling.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 5 Home industries should comply with the following:
- (a) an additional on-site car parking space should be provided for every non-resident employee
 - (b) off-street parking should be provided for all private business and customer vehicles
 - (c) there should be provision for all loading and unloading of vehicles to take place on the site of the development
 - (d) no building or activity associated with the home industry should be sited closer to any street alignment than the existing or approved dwelling
 - (e) other than persons living on the site, no more than three persons should be employed on the site
 - (f) no vehicle used in association with any industry or business should exceed 5 tonnes tare weight
 - (g) the total area used in association with the home industry (whether within a building or external space) should not be greater than 200 square metres or 25 per cent of the allotment
 - (h) no building or structure should exceed 7 metres in height.
- 6 A home industry should only be undertaken on an allotment which has an area of at least 750 square metres and a frontage to a public road not less than 20 metres.
- 7 No direct access points should be created to:
- (a) William Street, Beverley
 - (b) Dixon Street, Royal Park (west of Wattle Street).

Land Division

- 8 Land division should create allotments with an area of at least 750 square metres and a frontage to a public road not less than 20 metres.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding that:	
(a) is roof mounted and projects above the roof line	
(b) is parapet mounted and protrudes above the top of the parapet	
(c) is animated or flashing.	
Amusement machine centre	
Dairy	
Dwelling	Except a detached dwelling where it involves any of the following: <ul style="list-style-type: none"> (a) minor residential additions or alterations to an existing residence (b) a caretaker's residence (c) short term workers' accommodation (d) it is in association with a home industry (e) it does not preclude the development of the site as a home industry.
Educational establishment	
Fuel depot	
Horticulture	
Hospital	
Hotel	
Industry	Except where it involves either of the following: <ul style="list-style-type: none"> (a) it is for a home industry (b) the expansion of an existing industry use by no more than 30 per cent .
Intensive animal keeping	
Mining	
Motor repair station	Except where it is: <ul style="list-style-type: none"> (a) in association with a dwelling (b) located on the same allotment.
Nursing home	
Petrol filling station	
Place of worship	
Road transport terminal	
Service trade premises	Except where it is: <ul style="list-style-type: none"> (a) in association with a dwelling (b) located on the same allotment.
Shop or group of shops (including any expansion or addition)	Except where the gross leasable area is less than 250 square metres.
Stock sales yard	

Form of Development	Exceptions
Stock slaughter works	
Store	Except where it involves the expansion of an existing use by no more than 30 per cent.
Supported accommodation	
Warehouse	Except where it involves the expansion of an existing use by no more than 30 per cent.
Waste reception, storage, treatment or disposal	Except where it involves the remediation of land.
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .

Local Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre accommodating small-scale convenience shopping, office, **service**, medical and community facilities to serve the day-to-day needs of the local community.
- 2 A centre characterised by traditional corner stores or small groups of shops located within easy walking distance of the population they serve.
- 3 A centre accommodating residential development in conjunction with non-residential development.
- 4 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

All centres in this zone should provide a local centre function where the maximum gross leasable floor area of a shop or group of shops is in the order of 450 square metres.

Bartley Terrace, West Lakes Shore

The centre will be encouraged to retain its range of local services and maintain its character and good amenity. The car park areas require upgrading with additional landscaping comprising taller species and groundcovers.

Burbridge Road, West Beach

With a small supermarket and a wide range of specialty shops, the centre will be encouraged to retain its range of local services and maintain its character and good amenity. The car park areas require upgrading with additional landscaping comprising taller species and groundcovers.

Cheltenham Parade / Torrens Road, Pennington

It will be necessary to upgrade the centre in terms of presentation and range of services and to rationalise and landscape its car parking areas. Pedestrian access across Addison Road should be maintained.

Crittenden Road / Grange Road, Flinders Park

The upgrade of the centre to improve its appearance and facilitate better integration is encouraged. Pedestrian access across Grange Road should be maintained.

David Terrace, Kilkenny

It is desirable that the centre be consolidated and its environment upgraded.

Frederick Road / Lochside Drive, West Lakes

The maintenance of the character and amenity of the centre is encouraged. The use of trees and groundcovers to improve landscaping in the car parking area is supported.

Sportsmans Drive, West Lakes

The maintenance of the character and amenity of the centre is encouraged. The use of trees and groundcovers to improve landscaping in the car parking area is supported.

Frederick Road / Adare Court, Grange

Non retail services are discouraged in the centre. The establishment of suitable tenancies to serve the growing local catchment population is encouraged.

Grange Road / Arlington Terrace, Welland

The improvement of the centre is envisaged through the development of an adequate range of local retail services, including a quality seven day mini-mart.

Grange Road, Grange

The Centre, with a small supermarket and a number of shops and services, straddles both sides of Grange Road. The upgrading of the appearance of the centre, coordinated signage and enhanced landscaping is encouraged. Pedestrian access across Grange Road should be maintained.

Grange Road, Kidman Park

The Centre straddles both sides of Grange Road. The upgrading of the appearance of the centre and coordinated signage and enhanced landscaping is encouraged. Pedestrian access across Grange Road should be maintained.

Hanson Road / Millicent Street, Athol Park

The upgraded appearance of the centre through the provision of landscaping and an extended veranda along the front of the shops to improve pedestrian amenity is encouraged. Development to the south will incorporate access to the rear carpark. Development on this site of a similar setback to existing buildings, that reflects its corner location and provides informal surveillance of the adjoining reserve is appropriate.

Henley Beach Road, Henley Beach / Henley Beach South

The centre, with a small supermarket and a number of shops and services, is encouraged to maintain its good quality environment and enhance this with additional tree planting in car park areas.

Mirani Court, West Lakes Shore

The centre contains public toilets and a large car park area to the rear used by beachgoers. The use of taller species and groundcovers to improve landscaping in the car parking area is supported. The appearance and amenity of the shops requires improving and the range of tenancies should include a butcher and fruit and vegetable shop to meet local needs. High quality shop-top residential development is encouraged where this can make a positive contribution to revitalising the centre. Pedestrian access across Military Road should be improved. It is desirable that new development is setback four metres from the coastal reserve or zone or is in line with adjacent development where this is setback further.

Port Road / Hawke Street, Albert Park

The consolidation and strengthening of the centre is supported. The use of trees and groundcovers to improve landscaping is encouraged.

Tapleys Hill Road / First Avenue, Seaton

To enable the centre to provide a local centre function for each of its communities either side of Tapleys Hill Road it will be necessary to improve pedestrian access across Tapleys Hill Road. The use of trees and groundcovers to improve landscaping in carparks and along the street frontage is encouraged.

Trimmer Parade / Hallville Street, Woodville West

The improvement of the centre through the upgraded appearance of its buildings and landscaping of its rear car parking area is encouraged.

Torrens Road / Mais Street, Brompton

It is envisaged that the centre will be enlarged up to a floor area of 900 square metres to provide a quality mini-mart and range of associated local centre functions to serve the growing local population. It will be important to integrate the extensions to the centre with the existing buildings and car parking area and to upgrade the environment and appearance of the centre through tree planting and provision of improved pathways and pedestrian shelter.

South Road, Ridleyton

The centre is encouraged to maintain its good quality environment and enhance this with additional tree planting in car park areas.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - advertisement
 - community facility
 - consulting room
 - mini-mart
 - office
 - office and dwelling
 - shop
 - shop and dwelling.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 Residential development and development comprising a variety of residential and non-residential uses may be undertaken provided such development does not prejudice the operation of existing or future retail activity within the zone.
- 4 Development should not be of a high traffic generating nature and should not add significantly to traffic volumes in residential streets outside the zone.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 6 Dwellings should be located only behind or above non-residential uses on the same allotment.
- 7 Residential development should include a diversity of housing types and densities while not detracting from the primary function of the centre.
- 8 The maximum gross leasable floor area of a restaurant or takeaway food premises should be in the order of 250 square metres.
- 9 Development should provide pedestrian shelter along street frontages.
- 10 Development should be a maximum of 9 metres in height, except within parts of Bartley Terrace Local Centre which are adjacent to Council reserves, where development may be up to 12 metres in height.
- 11 Building work except fences, should be set back at least 4 metres or in line with adjacent building work, where this is a greater setback, on land abutting the **Coastal Open Space Zone**.
- 12 Development abutting residential areas should incorporate an intensive landscape buffer to screen centre activities.

Vehicle Parking

- 13 Vehicle parking should be provided in accordance with the rates set out in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies)

Land Division

- 14 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Local Shopping Policy Area 7

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precinct.

DESIRED CHARACTER

This area will retain existing contributory buildings, with complimentary new buildings, structures and additions, sited and designed to maintain the traditional shopping and streetscape character. The hard edged frontage with shops directly abutting the footpath will be maintained and enlivened by an attractive street environment, outdoor eating areas, street furniture and plantings. It will be important for development to respect the design features of existing streetscapes. Above street level, it is desirable that windows compliment the proportions of existing fenestration, and roofing materials and colour are compatible with those used traditionally. The continuity of street facades, parapet lines and verandas and the solid masonry character of the existing traditional buildings will be reinforced through the design of new buildings. The replacement of unsympathetic recent replacements and additions, including steel posted flat verandas, is encouraged.

Precinct 46 Stations

Belmore Terrace Location

This area will retain its group of Art Deco style single storey shops, characterised by painted rendered shopfronts with banded parapet and a full width cantilever awning along the frontages to Belmore Terrace and Park Street.

Rosetta Street Location

This area will retain the group of four attached shops with recessed entrances and tiled shopfronts, gabled corrugated galvanised iron roofs and metal parapet to the front elevation, and verandas. A Bungalow style dwelling attached to the shops and an Austerity style house to the north facing Rosetta Street are also valued features.

Elizabeth Street Location

This area comprises four groups of small shops with three attached dwellings. All of the shops abut the footpath, have gabled galvanised iron roofs and parapets to the front elevation. It is desirable that the unsympathetic replacement steel posted flat veranda along the Elizabeth Street and Day Terrace frontages of shops 1-5 Elizabeth Street are replaced. It will be important to retain the cantilever awnings or verandas which are integral to the design of the other shop buildings over the full width of the footpath. Two of the dwellings are integrated with the shops, while the third abuts the shops on the western side of Elizabeth Street, and faces Day Terrace. The buildings within the area date from the Edwardian to the early Bungalow era and are valued features.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - community facilities
 - consulting room with a maximum gross leasable floor space in the order of 450 square metres
 - office with a maximum gross leasable floor space in the order of 450 square metres
 - shop with a maximum gross leasable floor space in the order of 450 square metres.

Form and Character

- 2 Development should not be undertaken unless it is consistent with the desired character for the policy area.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precinct.

Precinct 46 Stations

- 3 Development should not be undertaken unless it is consistent with the desired character for the precinct.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved

- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding that: <ul style="list-style-type: none"> (a) is roof mounted and projects above the roof line (b) is parapet mounted and protrudes above the top of the parapet (c) is animated or flashing (d) is freestanding and greater than 4 metres in height. 	
Building work set back less than 4 metres from a Coastal Open Space Zone boundary or a public road that is adjacent to a Coastal Open Space Zone .	Except for either of the following: <ul style="list-style-type: none"> (a) the construction of a fence (b) building work within Local Shopping Policy Area 7.
Dwelling	Except where in conjunction with a non-residential development
Fuel depot	
Horticulture	
Industry	Except a service industry.
Public service depot	
Road transport terminal	
Service trade premises	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 or Local Heritage Place that is described in Table ChSt/6 or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 .	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 . Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area.

Metropolitan Open Space System Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone comprising open space that accommodates a range of public and private activities, including passive and active recreation land uses, habitat conservation and restoration, in an open and natural setting.
- 2 Development of recreational activities that do not detrimentally affect fauna, flora or other features of the natural environment.
- 3 Private land within the Metropolitan Open Space System (MOSS) contributing to regional open space networks and providing an open, natural and rural character accommodating low-scale uses such as non-intensive agriculture.
- 4 Public land within the MOSS that provides for recreation areas and facilities, sporting facilities and conservation of the open, natural character.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - conservation
 - farming
 - office in association with recreation and sport facilities
 - outbuildings associated with open space maintenance
 - playground
 - public toilets block and barbeque facilities
 - recreation areas and facilities
 - shop in association with recreation and sport facilities
 - sport facilities
 - structures associated with public facilities such as car parking, picnic/barbeque areas and shelters and toilets.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 Publicly owned land within the MOSS should be used for any of the following:
 - (a) to provide natural or landscaped open space using locally indigenous plant species
 - (b) to accommodate a range of public recreation, sporting and institutional facilities and uses
 - (c) to accommodate stormwater retention and management
 - (d) to conserve and restore areas of native natural vegetation and wildlife habitats and corridors
 - (e) to conserve sites of scientific, cultural or heritage interest
 - (f) for revegetation purposes using locally indigenous plant species

(g) to provide a buffer to adjoining areas of conservation significance.

4 Privately owned land within the MOSS should be used for any of the following:

(a) rural activities and agriculture (but not intensive animal keeping)

(b) low-impact sporting facilities

(c) conservation purposes.

Form and Character

5 Landscaped buffers should be provided around the perimeter of recreation or sporting facilities.

Linear Park (River Torrens / Karrawirra Pari) Policy Area 8

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A linear park primarily comprising open space providing a contrast with the built urban environment.
- 2 A linear park providing an open space corridor across the metropolitan Adelaide.
- 3 A linear park that accommodates a range of passive and unstructured recreation and leisure areas which do not detrimentally affect the natural and modified environments of the park, or nearby urban development.
- 4 A river system which provides for the maintenance of stormwater capacity and flood mitigation measures for adjoining areas.
- 5 A linear park that promotes the use, awareness and preservation of
 - (a) the riverine environment and enhancement of natural or semi natural habitats for the movement of wildlife and conservation of biodiversity
 - (b) areas of scientific, archaeological or cultural significance.
- 6 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area covers the River Torrens Linear Park, known as Karrawirra Pari. The park follows the River Torrens from the Adelaide Hills to the coast.

The policy area is primarily an open space with aesthetic, stormwater management and recreation functions. Development within the policy area is appropriate where it is integral to any of these functions provided any structures are not a dominant feature.

The policy area will provide for the establishment and maintenance of a linear open space and biological corridor based on the existing watercourse channel, associated floodplains and vegetation.

Water quality of the river and the recreational opportunities will be retained and enhanced.

Pedestrian, cycle and recreation facilities, such as drinking fountains and barbecues, should be incorporated into the Linear Park; however, facilities for sports competitions or events are inappropriate.

Landscaping should ensure that any remnant native vegetation is incorporated into the landscape design and that indigenous vegetation is utilised wherever possible.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - interpretive signage and facilities
 - lighting
 - passive outdoor recreation facilities

Charles Sturt Council
Zone Section
Metropolitan Open Space System Zone
Linear Park (River Torrens/Karrawirra Pari) Policy Area 8

- playground
 - recreation area
 - structures associated with public facilities such as car parking, picnic/barbeque areas and shelters and toilets.
- 2 Development should only occur where it is integral to the aesthetic, drainage or recreation function of the park.
- 3 Development should allow for unstructured passive and active recreation, with organised sporting activities restricted to existing sites and buildings.

Form and Character

- 4 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 5 Development should ensure that public access to the River Torrens Linear Park is retained and enhanced for cyclists and pedestrians.
- 6 All development including pedestrian and cycle paths and landscaping should take into account the changing flow regime and width of waterways and be constructed of permeable material where practical to reduce stormwater runoff.
- 7 Vehicular access or crossings should not occur through the River Torrens Linear Park.
- 8 Buildings should be:
- (a) restricted in size and number
 - (b) sited so as not to detract from the open natural character of the policy area
 - (c) constructed of materials which blend with the riverine landscape.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to all of the following criteria:

- (a) the conditions contained in [Table Chst/1 - Conditions for Complying Development](#)
- (b) no development being undertaken within 60 metres of the centreline of the River Torrens or within the River Torrens:
 - recreation area.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement	Except in association with conservation works or tourist information purposes.
Advertising hoarding	Except in association with conservation works or tourist information purposes.
Amusement machine centre	
Bus depot	
Bus station	
Caravan park	
Cemetery	
Community centre	
Consulting room	
Crematorium	
Dairy	
Dwelling	
Educational establishment	
Emergency services facility	
Fuel depot	
Hall	
Helipad	
Horticulture	
Hospital	
Hotel	

Form of Development	Exceptions
Industry	
Indoor recreation centre	
Intensive animal keeping	
Land division	Except where no additional allotments are created partly or wholly within the zone.
Major public service depot	
Minor public service depot	
Motel	
Motor repair station	
Nursing home	
Office	Except where directly related to the administration and management of recreation and sport facilities.
Petrol filling station	
Place of worship	
Pre-school	
Prescribed mining operations	
Primary school	
Restaurant	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where the gross leasable area is less than 80 square metres
Showground	
Stadium	
Stock sales yard	
Stock slaughter works	
Store	
Telecommunications facility	
Theatre	
Tourist accommodation	
Warehouse	
Waste reception, storage, treatment or disposal	
Welfare institution	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .

Mixed Use Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A functional and diverse zone accommodating a mix of commercial, community, light industrial, medium density residential, office, and small-scale shop land uses.
- 2 Development that minimises any adverse impacts upon the amenity of the locality within the zone.
- 3 Development that does not impact on the amenity of adjacent land in residential or similarly environmentally sensitive zones.
- 4 Development which provides for the future re-alignment of Holbrooks Road, Kilkenny Road, David Terrace and Torrens Road.
- 5 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

A range of land uses are envisaged in the zone including service uses, offices, commercial, motor trade, wholesaling, showrooms and medium density residential development. It is envisaged that the area will also accommodate a variety of low impact industrial activities that do not create any appreciable nuisance and that have minimal off-site impacts, such as noise, air, waste and waste emissions, traffic generation and movement. Activities with minimal detrimental impacts on the amenity of properties in residential or similar environmentally sensitive zones are appropriate.

External walls of a building, structure or any addition or alteration principally composed of masonry, rendered masonry, brick, stone, concrete block, timber, painted/patterned cement fibre sheeting or glass or similar materials with high quality finishes on facades on a primary street frontage are desirable.

It is appropriate for offices to be located to the street frontage of a site and constructed principally of masonry, rendered materials, painted concrete tilt slabs and glass or other similar materials with high quality finishes.

Arterial road corners will be defined with the primary orientation of development provided to the arterial road. Development on arterial corners located close to or on the arterial road boundary and following the alignment of the arterial road boundaries around the corner is desirable.

Buildings of large mass and bulk with long lengths of solid walling are not appropriate on the zone boundary and will be avoided through articulation and innovative design.

It is envisaged that a landscaped area 3 metres wide along boundaries with any road or reserve will be provided in non-residential developments, subject to the required setback for any building being in accordance with the setback principles or as required in the relevant policy area. Where a dwelling is converted to non-residential use, the area between the dwelling and the road will require landscaping.

In order to protect the amenity of adjoining zones a landscaped area two metres wide at the rear and/or side of the site where it adjoins a zone will be established as part of non-residential development. Where a building is not located on the zone boundary, it is envisaged that the appearance of the development from the adjoining zone in the intervening area will be enhanced through appropriate tree planting.

Where buildings are developed with minimal or no setback, the use of indented landscaping or intensive landscape strips is envisaged.

In order to enhance the appearance of development as seen from the street and public places landscaping will incorporate the planting of trees.

The erection of open ornamental or decorative fences, other than wire mesh, at the street frontage of a site is appropriate as part of development. Fences to public roads, including black or dark green plastic coated wire mesh, as long as set back to the building line or behind landscaped areas, are also appropriate.

Where buildings are built within three metres of the street frontage, visual definition and a hard edge to the street boundary will be achieved through the appropriate design and materials of fences to street frontages.

Precinct 47 Mixed Use

This precinct is characterised by a commercial character with a large number of business, motor trade and showroom premises and a number of industrial activities. Single houses remain interspersed throughout the area. The precinct is a focus for commercial, vehicle and business activities.

Much of the precinct fronts arterial roads, such as Grange, Port and Torrens Roads. Consequently, it is important to establish a well designed character and form that enhances the attractiveness of these roads. Some parts of the area are of a high quality and well landscaped nature whilst other areas are of a disparate character, requiring more attention to built form, design, siting and landscaping to improve the areas amenity. Interface issues, such as noise, arise due to the proximity of housing.

This precinct will be further developed as a mixed use area of commercial and medium density residential activities. Two storey housing is desirable and should be designed for arterial road conditions. Mixed use housing developments is encouraged. Higher impact uses such as crash repairers and manufacturing should be restricted. High traffic generating uses are inappropriate.

The enhancement of the area through high quality buildings with extensive landscaping and upgrading of existing lower quality buildings is supported. Conversion of existing houses to commercial use will be required to maintain landscaped front gardens. Car parking at the side or rear of the premises and buffered by screen planting from adjacent housing is generally appropriate. It is desirable to integrate rear parking areas between adjoining sites. Consolidation of former housing allotments is desirable and the rationalisation of signage is encouraged.

Proximity of the area to adjoining residential areas necessitates careful design to minimise impact on residential amenity and careful consideration of appropriate uses. It will be necessary to buffer adjacent housing by screen plantings or masonry walls on rear boundaries. Site amalgamations are very desirable and it will be inappropriate to allow any vehicles to back onto arterial roads.

Land fronting Fourth Avenue Cheltenham, excluding existing lawfully operating commercial land uses, will comprise residential development with a frontage to Fourth Avenue, with small scale, low impact commercial land uses located to the rear of dwellings. Residential buildings designed to reinforce the established residential character of Fourth Avenue are supported.

Precinct 48 Hindmarsh Place

This precinct comprises a very old part of Hindmarsh, which forms part of the Historic Conservation Area shown on *Overlay Maps – Heritage*, and which contains a number of important older buildings of high integrity, including two Victorian church complexes. The precinct also accommodates attached and detached cottages. The precinct adjoins the Hindmarsh Stadium, an important present day cultural facility, as well as a number of historic premises, such as the former fire station, former Christian Chapel and former municipal hall. An early hotel, cottages and brewery adjoin in Chapel Street and add to the historic character of this area.

The predominant character is that of the 1870-1910 period characterised by walls constructed of bluestone and red brick, hipped and gable roofs and corrugated iron roofing. The area is adjoined by more recent development.

This precinct should retain heritage listed buildings with complementary new buildings or structures sited and designed to maintain the strong urban character, reflective of the character of contributory and heritage places. Substantial masonry and iron fences and gates which screen car parking areas are encouraged to

re-establish a sense of enclosure to the streets. Two storey buildings with high solid to void content set off by extensive landscaping are encouraged. Car parking is appropriate if properly designed into an integrated streetscape.

This policy area will be further developed as a mixed use area of commercial and medium density residential activities. Ancillary uses related to the Hindmarsh Stadium are appropriate.

Visual definition to the street boundary will be achieved through the appropriate design and materials of fences to street frontages that are sympathetic to the historic character.

Precinct 49 South Road

This precinct, which forms part of the Historic Conservation Area shown on *Overlay Maps – Heritage*, comprises a number of older buildings of high integrity, including a former church, a hall and a nursing home, as well as a number of character dwellings. The policy area adjoins an area of character housing dating from the 1890's to late 1910's in West Hindmarsh to the east.

This precinct should retain existing contributory buildings with complementary new buildings or structures sited and designed to maintain the strong urban character. Car parking areas designed behind substantial masonry and iron fences and gates will establish a sense of enclosure to South Road. Two storey buildings with high solid to void content set off by extensive landscaping are encouraged. Car parking is appropriate if properly designed into an integrated streetscape. Proximity of the area to adjoining residential areas necessitates careful design to minimise impact on residential amenity and careful consideration of appropriate uses.

This precinct should be further developed as a mixed use area of commercial and medium density residential activities. Two storey housing is desirable and should be designed for arterial road conditions. Mixed use housing developments is encouraged. Higher impact uses such as crash repairers and manufacturing and high traffic generating uses are inappropriate.

Visual definition to the street boundary will be achieved through the appropriate design and materials of fences to street frontages that are sympathetic to the historic character.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - community facility
 - consulting room
 - detached dwelling
 - institutional facility
 - light industry
 - low traffic generating uses
 - medium density housing
 - motor repair station
 - office
 - service trade premises
 - semi-detached dwelling
 - shop or group of shops where the gross leasable area is less than 250 square metres
 - warehouse.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone

- 4 The bulk and scale of development should be compatible with adjoining land uses.
- 5 Car parking areas should be provided at the rear or side of premises.
- 6 Direct pedestrian access to public buildings should be provided from rear or side parking areas.
- 7 Advertisements and advertising hoardings should not include any of the following:
 - (a) flashing or animated signs
 - (b) bunting, streamers, flags, or wind vanes
 - (c) roof-mounted advertisements projected above the roofline
 - (d) parapet-mounted advertisements projecting above the top of the parapet
 - (e) freestanding sign/sign trees greater than 6 metres in height.
- 8 Buildings and structures should not exceed 60 per cent site coverage with flexibility in the distribution of site coverage where multiple sites are involved.
- 9 Buildings should be setback from a primary road frontage the same distance as one or the other of the adjoining buildings or the average of the setbacks of the adjoining buildings, unless otherwise specified in the relevant policy area.
- 10 Except where otherwise specified in a policy area, buildings should be setback at least 3 metres to a secondary road frontage; where adjoining buildings are closer than 3 metres, buildings should have a similar setback.
- 11 Where car park areas are between a building and a street, they should be setback a minimum of 3 metres from the street boundary with a landscape strip planted with trees and groundcovers incorporated within that setback, unless otherwise specified in the relevant policy area.
- 12 Development except housing should provide landscaping over 10 per cent of the site.
- 13 Landscaping should shade car parking areas with trees interspersed in between every third car park.
- 14 External walls constructed of metal clad pre-coloured or painted materials should comprise less than 50 per cent of the facade of the primary street frontage of buildings and structures.
- 15 Where the site of a non-residential building abuts the boundary of a site in a zone where residential development or sensitive land uses are envisaged or abuts land used for a school, hospital, or the like, building development should not intrude into a plane angled 31 degrees above the horizontal into the site, as measured from a point 3 metres above the boundary.

Land Division

- 16 Land division should ensure that new allotments do not have direct vehicular access to an arterial road, as shown on *Overlay Maps - Transport*, unless the allotment has a frontage of at least 25 metres.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 47 Mixed Use

- 17 Development should not be undertaken unless it is consistent with the desired character for the precinct.

- 18 No further vehicular access points should be provided for non-residential development off the following roads:
- (a) Albert Park - Lawton Street
 - (b) Allenby Gardens- Charles Street
 - (c) Cheltenham - Fourth Avenue
 - (d) Croydon - Princes Street
 - (e) Findon - Alice Street
 - (f) Fulham Gardens - Webb Street
 - (g) Kidman Park - Robe Street
 - (h) Woodville – Bower Street.

Precinct 48 Hindmarsh Place

- 19 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 20 Buildings and structures should not exceed 8 metres in height.

Precinct 49 South Road

- 21 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 22 Buildings and structures should not exceed 8 metres in height.

Urban Village Findon Policy Area 9

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An urban village offering an attractive urban environment and an inner metropolitan lifestyle.
- 2 A diverse, vibrant, robust and adaptable community, including resident and visitor populations sharing the urban village.
- 3 Medium to high density, compact, affordable and adaptable housing choices.
- 4 Activities that generate employment and economic viability for the community.
- 5 Safe and efficient access for sustainable modes of transport where walking, cycling and public transport are given priority.
- 6 Development that respects the core retail function of the Findon Shopping Centre.
- 7 Development that respects the amenity of traditional residential areas.
- 8 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

This policy area has been designated as a location suitable for the establishment of an Urban Village. The Village core is adjacent the **Neighbourhood Centre Zone** at Findon, the retail activity node for the community, and provides existing physical and social infrastructure needed for the Urban Village to grow sustainably.

The purpose of the policy area is to ensure that the area around the Village Core provides for a variety of mixed uses needed to support the Core as an activity node, and to provide the flexibility in site planning and development control needed to ensure the intensity of development and the vitality of activity that is desired for the **Urban Village Findon Policy Area 9**.

Urban Place

The policy area will be developed as a vibrant inner metropolitan neighbourhood, offering diversity in housing choice, local employment, and an interactive public environment that provides long term sustainability and social inclusion for the community. It will be characterised by quality architecture and adaptable built forms at a human scale suitable for a variety of mixed uses, diversity of people, walkability, and a desirable urban environment that makes for people friendly experiences, and a sense of community.

Development within that portion of **Precinct 50 Village Employment and Living**, located south of Adele Avenue, Kidman Park, will be sited with minimal front setbacks to support the desire for residential development at higher density.

Land Uses / Function

Housing choice will be created by the provision of both medium and higher density living opportunities within a modern compact architecture that can achieve the vibrancy of lifestyle desired for the Village atmosphere, and required to support a mix of land uses. A portion of housing opportunities will be affordable to the community, and integrated within the broader housing opportunities.

Local employment opportunities for the community will be generated within the Village from a variety of uses that are required to support the local and surrounding population. These will include a range of community services, minor retailing, office/commercial, storage and warehouse activities, bulky goods outlets and a limited range of service trade premises, and light industrial activities likely to be demanded in the future.

A number of existing commercial/industrial activities are located within that portion of **Precinct 50 Village Employment and Living**, located south of Adele Avenue, Kidman Park. Such activities can be a source of nuisance including noise, lighting and general activity. These activities fulfil an important employment generating function. While some of these activities will, overtime, be replaced, other employment generating activities may remain and expand reinforcing the mixed-use function of the zone.

Development proposed in proximity to these existing uses should include appropriate design measures such as noise attenuation, acoustic screens, building materials, doubled glazed windows and wall and roof insulation and be sited to reduce the potential for adverse impacts on and from existing uses.

Topography within that portion of **Precinct 50 Village Employment and Living**, located south of Adele Avenue, Kidman Park, is varied in some areas as a result of existing fill. Development will be sited and designed to minimise interface impacts on adjoining residential allotments in the adjacent Residential Zone or precinct 52 Village Living Low by utilising design solutions such as increased setbacks from interface boundaries to reduce the requirement for earthworks and retaining walls as well as ensuring that the potential for overlooking and additional overshadowing is minimised. The creation of new allotments will be undertaken in a manner that considers allotment gradients, size and dimensions required to achieve this outcome.

There may be areas within that portion of **Precinct 50 Village Employment and Living**, located south of Adele Avenue at Kidman Park, that are contaminated because of previous activities. Further investigations to determine the extent of site contamination and necessary remediation measures may therefore be required to ensure that sites are suitable and safe for their intended use, particularly for residential development.

Public Realm

Public environments will become integrated with the spaces in the private realm in a manner that creates interconnecting streets and paths, with architectural features and outdoor activities that will encourage pedestrian activity. The compact pattern of the area will reduce dependency on the car, improve the environment, and promote interactive neighbourhoods.

New public streets will create a sense of identity through streetscaping consistent with Urban Village themes.

Development Structure

It is desired that the structure for mixed use developments is consistent with the [Concept Plan Map ChSt/6 – Urban Village Findon Policy Area 9](#). The primary aim of the concept plan and for development generally is to ensure that streetscapes, open space, residential accommodation, and business premises readily co-exist and form a compatible and cohesive Urban Village, promote a sense of vitality, viability, and identity for the Village, and to ensure the sympathetic integration of this neighbourhood with adjacent neighbourhoods.

Responsible use of resources including land utilisation, building re-use, and adaptation of vertical and horizontal spaces for multiple uses is encouraged.

Environmental sustainability is important in this policy area and new development should focus on design that is sustainable.

Sustainable development outcomes through innovation in stormwater management, water conservation, energy efficiency, waste minimisation and urban biodiversity are supported.

A range of housing sizes and types, including medium to high density mixed use and residential developments, aged care and cluster style housing is to be promoted to reflect the living role of the Village.

It is expected that building design, including roof style, will respond in a contextual manner to the desired future character, recognising the need for employment and residential built forms to co-exist, and the desire to create a compact metropolitan image. This will include useable roof spaces wherever possible.

Material and colour selection for new buildings, and landscape treatments of private spaces, including car parking areas, will seek to respond to the potential for employment and living environments to co-exist, reinforce the sense of vitality and modern built forms, and assist in the reduction of environmental impacts from roads (noise and air quality) to living areas.

Building design on land designated as a Village Marker on the [Concept Plan Map ChSt/6 – Urban Village Findon Policy Area 9](#) is expected to respond in a distinctive manner through site layout and architectural design to define and signify the Village Core and emphasise links to the Village Core.

Development will contribute to the Urban Village character, the quality of the public environment, and the street life through respecting the central Village Green, emphasising links from the Village Green to the Village Core, quality architecture and the integration of private and public open spaces to achieve connectivity.

It is envisaged that pedestrian connectivity will be provided by, amongst others, the allocation of public open space, shared private and public open spaces, or through a colonnade style development. Convenient It will be important to provide pedestrian access to the Village Core, through or between buildings fronting Grange Road and Findon Roads.

The location of any local public open space provision within that portion of **Precinct 50 Village Employment and Living** located south of Adele Avenue at Kidman Park, will be further considered by Council at the development proposal stage to ensure, as much as possible, a coordinated approach to the provision and design of open space for the local area.

A number of mature trees are located along the Findon Road frontage of that portion of **Precinct 50 Village Employment and Living** located south of Adele Avenue at Kidman Park. The retention of these trees is considered desirable and, where practical, they will be incorporated into the design of future developments.

Any new vehicle access to that portion of **Precinct 50 Village Employment and Living** located south of Adele Avenue at Kidman Park will be focussed on the key vehicle access points indicated on [Concept Plan Map ChSt/6 – Urban Village Findon Policy Area 9](#). An internal road network will be provided that not only services the land immediately adjacent to an access point, but also provides safe and convenient linkages with neighbouring land facilitating satisfactory future division of that land and inter-communication.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - affordable housing within that portion of **Precinct 50 Village Employment and Living** that is located south of Adele Avenue, Kidman Park
 - bulky goods outlet
 - community services
 - dwelling
 - home industry
 - light industry
 - office
 - service industry within that portion of **Precinct 50 Village Employment and Living** that is located south of Adele Avenue, Kidman Park
 - service trade premises that only comprise indoor displays or are primarily for the sale, rental or display of building materials, landscaping materials, sheds, domestic garages or outbuildings
 - shop or group of shops with a gross leasable floor area of 250 square metres or less
 - store
 - supported accommodation
 - warehouse.
- 2 Land should be used for a diversity of land uses, services and facilities and should attract cultural, educational and community uses that will strengthen the role of the village, with a predominant focus on employment, living and community activities.

- 3 Land uses that generate high levels of pedestrian activity and interact with the public should be located at ground levels fronting Grange Road and Findon Road.
- 4 Land uses that do not generate high levels of pedestrian activities should be located to the rear of active uses or on above ground levels.

Form and Character

- 5 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 6 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/6 - Urban Village Findon Policy Area 9](#).
- 7 Site layout and building design should protect sun light access to public spaces and not produce adverse wind impacts to private or public areas.
- 8 Where a building cannot form a consistent building alignment with Grange Road or Findon Road, or where part of a building is set back from the front boundary to either of these roads, the area between the front property boundary and the set back building wall should be landscaped.
- 9 The front walls of new buildings fronting Grange Road or Findon Road should:
 - (a) incorporate shade and shelter devices for pedestrians
 - (b) present as predominantly transparent walls, with a dominant void to solid ratio; and be interesting visually through the use of light and shade, particularly through a combination of small indentations and overhangs
 - (c) incorporate interesting roof forms and extend beyond the roof line to create a parapet where necessary to obscure any roof fixtures (such as solar panels, aerials or dishes, and air conditioning units), from ground level public view
 - (d) have indented pedestrian entrance areas accessible and visible from the road.
- 10 Where land adjoins a **Residential Zone**, development should be designed to be sympathetic to the desired character for that zone.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 50 Village Employment and Living

- 11 Development should be designed within the following parameters:

Parameter	Value
Dwelling densities	Medium to high (greater than 35 dwellings per hectare net)
Building heights	15 metres (4 storeys)

- 12 Residential development should comprise one or two storey buildings which do not exceed 8.5 metres building height within 25 metres of the northern, southern and western boundaries of that portion of the precinct south of Adele Avenue, Kidman Park.

- 13 Retail development should:
- (a) be ancillary to and reinforce the adjacent Findon Shopping Centre
 - (b) be limited to primarily bulky goods outlets and a limited range of service trade premises along the Grange Road frontage of the precinct
 - (c) be limited to primarily bulky goods outlets and service trade premises and a limited range of other shops along the Findon Road frontage and internal to that portion of the precinct located south of Adele Avenue, Kidman Park.
- 14 Dwellings are appropriate:
- (a) above or at the rear of commercial ground level uses within that portion of the precinct north of Adele Avenue, Kidman Park
 - (b) irrespective if they are associated with commercial uses or not in that portion of the precinct south of Adele Avenue, Kidman Park.
- 15 The majority of the frontage of any building in the precinct facing Grange Road or Findon Road should be sited on the front property boundary.
- 16 Dwelling setbacks in that portion of the precinct south of Adele Avenue, Kidman Park should be in accordance with the following parameters:

Parameters	Value
Minimum rear boundary setback for single storey	3 metres
Minimum rear boundary setback for two storeys	6 metres
Minimum side boundary setback for single storey	0.9 metres
Minimum side boundary setback for two storeys	2 metres

- 17 The visual impact of walls abutting **Precinct 51 Village Living Medium** should be softened through the following methods:
- (a) provide a 5.0 metre landscaped strip for screen planting
 - (b) render and architectural detailing of rear walls.
- 18 On-site car parking should be provided at street level or below ground level in the precinct.
- 19 An internal road network should be provided in that portion of **Precinct 50 Village Employment and Living** located south of Adele Avenue at Kidman Park that:
- (a) connects to a key vehicle access point on Findon Road as indicated on [Concept Plan Map ChSt/6 – Urban Village Findon Policy Area 9](#)
 - (b) services the land immediately adjacent to the access point
 - (c) provides safe and convenient linkages with neighbouring land facilitating satisfactory future division of that land and inter-communication.
- 20 Land division adjacent to the western and southern boundaries within that portion of **Precinct 50 Village Employment and Living** located south of Adele Avenue, Kidman Park should create allotments which have sufficient depth to minimise the extent of fill, reduce the scale of boundary structures and provide usable private open space.

Precinct 51 Village Living Medium

21 Development should be designed within the following parameters:

Parameter	Value
Dwelling densities	Medium (35 to 70 dwellings per hectare net)
Building heights	12 metres (3 storeys)

22 Development should comprise one or two storey buildings which do not exceed 8.2 metres in height within 30 metres of the residential zone boundary to the south of the precinct.

Precinct 52 Village Living Low

23 Development should be designed within the following parameters:

Parameter	Value
Dwelling densities	Low (less than 35 dwellings per hectare net)
Building heights	8.5 metres (2 storeys)

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table Chst/1 - Conditions for Complying Development](#):

Within **Precinct 48 Hindmarsh Place** and **Precinct 49 South Road**:

- (a) the demolition of a building, outbuilding or fence on a site which is not listed as a State or Local heritage place or a contributory item
- (b) work undertaken within a building or outbuilding on a site, which is not listed as a State heritage place, and does not increase the total floor area of the building or outbuilding and does not alter its external appearance.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding that:	
(a) is roof mounted and projects above the roof line	
(b) is parapet mounted and protrudes above the top of the parapet	
(c) is animated or flashing	
(d) is freestanding and greater than 6 metres in height.	
Amusement machine centre	
Fuel depot	
General industry	
Horse keeping	
Horticulture	
Intensive animal keeping	
Prescribed Mining Operations	
Road transport terminal	
Shop or group of shops (including any expansion or addition).	Except where either of the following applies: <ul style="list-style-type: none"> (a) the gross leasable area is less than 250 square metres (b) it is a bulky goods outlet located in Precinct 50 Village Employment and Living of the Urban Village Findon Policy Area 9.
Special industry	
Stock sales yard	

Form of Development	Exceptions
Stock slaughter works	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Bank	Bulky goods outlet
Child care facility	Club
Consulting room	Community centre
Dwelling within Precinct 51 Village Living Medium and Precinct 52 Village Living Low	Dwelling not located within Precinct 51 Village Living Medium and Precinct 52 Village Precinct Low .
Office with a total floor area less than 450 square metres	Funeral parlour
Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 or Local Heritage Place that is described in Table ChSt/6 or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 .	Hospital
Shops with a total floor area less than 250 square metres	Indoor recreation centre
Supported accommodation	Light industry
	Motel
	Motor repair station
	Office with a total floor area greater than 450 square metres
	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .
	Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area.
	Petrol filling station
	Service industry
	Service trade premises
	Store
	Warehouse

Neighbourhood Centre Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A centre providing a range of facilities to meet the shopping, community, business, and recreational needs of the surrounding neighbourhood **distributed across five distinct centres within the following areas:**
 - Findon
 - Findon Road
 - Henley Square
 - Royal Park
 - Welland.
- 2 A centre that provides the main focus of business and community life outside a district centre, and provides for the more frequent and regularly recurring needs of a community.
- 3 A centre accommodating residential development in conjunction with non-residential development.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - bank
 - child care facility
 - consulting room
 - dwelling in conjunction with non-residential land uses
 - library
 - health centre
 - office
 - petrol filling station
 - place of worship
 - playing field
 - pre-school
 - primary school
 - recreation area
 - restaurant
 - shop
 - supermarket.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 Residential development and development comprising a variety of residential and non-residential uses may be undertaken provided such development does not prejudice the operation of existing or future retail activity within the zone.

Form and Character

- 4 Dwellings should be located only behind or above non-residential uses on the same allotment.

Vehicle Parking

- 5 Vehicle parking should be provided in accordance with the rates set out in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever applies).¹

Land Division

- 6 Land division in the Neighbourhood Centre Zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

Findon Policy Area 10

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The policy area will serve as the centre for the daily to weekly shopping and business needs for the surrounding areas. It is envisaged that the Findon Centre will accommodate an enlarged and upgraded supermarkets and a greater range of specialty shops, office, commercial, leisure, entertainment, recreation and community facilities. It is important that development within the centre results in better integration between the centre facilities within and across all of the precincts and a high standard of design providing a wide range of tenancies, facilities and activities.

The improvement of the appearance of the Centre from adjacent residential areas is supported.

Noisy or night time activities are unsuitable close to Findon Road, Winona Street, Ray Street or adjoining residential zones. Where such activities are undertaken, they need to be designed and managed to minimise any impact on residential amenity.

Precinct 53 Retail Core Findon

This precinct provides the focus for shopping, business and community activities. Medium and high density residential development may be appropriate. It is envisaged that the precinct will be developed by enlarging and upgrading supermarket choice and through a greater range of specialty shops, office, commercial, leisure and community facilities. It will be important to integrate the expanded centre with the existing centre and development to the north and east and for development to be of a high standard of design.

It is desirable that development by way of extension to the existing centre complex will have a built form and character based upon a pedestrian mall on a north south axis with supporting pedestrian access ways which are free of vehicular traffic. It will also be important for development to create a human, village like scale form and character, and incorporate places and spaces suitable for undertaking social and cultural activities.

Precinct 54 Winona Street

It is envisaged that this precinct will accommodate medium density, small scale residential, offices, leisure and restaurant uses provided they are compatible with a residential environment. It will be important for an integrated basis to take advantage of shared parking and open space arrangements with the land to the west.

Precinct 84 Findon Centre East

The precinct will accommodate a small-scale supermarket of up to 2000 square metres of gross leasable floor area, along with limited specialty retail and bulky goods facilities. Residential development is encouraged above non-residential uses.

Development will be sited and designed to provide active facades and avoid the presentation of blank walls to the Grange Road and Findon Road frontages while also balancing the need to provide appropriate loading facilities and parking areas to support envisaged forms of development.

Pedestrian movement will be improved through the consolidation of vehicular access points and the improvement of connections with the retail core through safe crossing points to Findon Road. Development on a site with existing non-conforming vehicle access arrangements will be rectified through relocation and upgrade to comply with relevant standards. Development of the precinct involving vehicle access via the Noblet Street / Grange Road intersection should ensure safe and convenient movements, which may involve an upgrade of the intersection.

The amenity of the precinct will be improved through the provision of landscaping comprising trees, shrubs and groundcovers along the road frontages and within the car parking areas.

Development will be located and designed to minimise impacts on the amenity of residential properties abutting the precinct, particularly with regard to building bulk and scale, as well as noise and odour from servicing arrangements and plant equipment. This will be achieved through the provision of appropriate interface treatments along the residential interface which may include acoustic walls, landscaping or other similar measures.

There are areas within the Precinct known to be affected by potentially contaminating activities. Offsite contamination may also exist on adjacent or nearby land, which may impact the use of the land within the zone. Development of these areas affected will not proceed unless appropriate investigations and remediation (where required) has been undertaken.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/7 - Findon Policy Area 10](#).
- 3 Development should be a maximum of 12 metres in height.
- 4 One freestanding advertising sign tree may be provided for any additional retail development adjoining Findon Road.
- 5 Vehicular access points to Grange Road or Findon Road should be provided in accordance with [Concept Plan Map ChSt/7 - Findon Policy Area 10](#).

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 53 Retail Core Findon

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 54 Winona Street

- 7 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 8 Development should not generate high volumes of traffic and parking during morning and afternoon peaks to minimise disruption to traffic flows along Findon Road.

Precinct 84 Findon Centre East

- 9 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 10 Development should include appropriate interface treatment measures along the boundary of the Residential Zone to minimise impacts associated with noise, odour and building bulk & scale.
- 11 Development should result in the rationalisation of access points onto Grange Road and Findon Road.

Findon Road Policy Area 11

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 The provision of a safe pedestrian environment that gives priority to pedestrians and public transport over through traffic.
- 2 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

It will be important to consolidate the policy area towards its southern end and maintain and improve its environment and functioning. The appearance, surroundings, parking areas and accessibility of the Centre will be upgraded by new development. The policy area contains a number of individual groups of shops which require improved integration. It is intended that further intensification of development on the eastern side of Findon Road will be concentrated toward the southern end of the policy area.

It will be important to include verandas or awnings in any additions or conversion of existing buildings to new centre uses or new buildings fronting Findon Road to provide pedestrian protection.

The upgrade of the general appearance of the policy area, including where adjacent to residential areas, is desirable. It will be important to improve pedestrian access and protection and provide verandas or canopies adjacent to bus stops. Outdoor furniture and structures with an integrated design theme are encouraged. Lighting should be improved throughout public areas and provided to the walkway joining Findon Road and Esk Street.

It is envisaged that landscaping will be improved throughout the policy area. The development of a formal landscaped node incorporating upgraded paving, seating and street furniture between Trimmer Parade and Crittenden Road is supported. It is desirable to provide additional landscaping throughout existing and new car parking areas with appropriate shade trees and shrubs. An avenue of large growing trees along Findon Road will provide shade and amenity.

The location of noisy or night time activities close to housing adjoining the policy area is not appropriate. Such activities will be located, with associated car parking, where they can be separated by existing buildings from adjacent housing.

Precinct 54 Retail Core Findon Road

This precinct provides the focus for shopping, business and office activities. Institutional and community uses are appropriate.

Retail development will be consolidated on the western side of Findon Road with banking, office, restaurant, community and lower traffic generating uses concentrated on the eastern side of Findon Road.

The conversion of older shops to low impact commercial uses or residential development should be considered.

Precinct 55 Commercial Fringe Findon Road

This precinct primarily accommodates large space retailing, office, commercial, service and residential development. Non-residential development that is well screened from housing at the rear with access and parking co-ordinated and integrated where possible is appropriate.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/8 - Findon Road Policy Area 11](#).
- 3 Development should be a maximum of 9 metres in height.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 55 Retail Core Findon Road

- 4 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 56 Commercial Fringe Findon Road

- 5 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Henley Square Policy Area 12

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An increase in the range of services and facilities provided in the policy area, including shop top intensive medium density housing.
- 2 A policy area reinforced as a meeting place, venue for open air entertainment and functions and uses such as restaurants and outdoor cafes.
- 3 Conservation and enhancement of identified historic buildings.
- 4 The provision of a safe pedestrian environment which gives priority to pedestrians and public transport over through traffic.
- 5 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The policy area provides the focus of community, shopping and service needs for residents, beach users and visitors and will continue to develop as a vibrant precinct renowned for its cosmopolitan restaurants and range of specialty and convenience shops. The policy area will continue as a meeting place for visitors and the local community. It is desirable to enhance the seaside village atmosphere through new activities. Night time and weekend activities but not late night entertainment activities, are appropriate.

Buildings will enhance the seaside, village character and provide appropriate enclosure to streets and the Square. It is important that new development is sympathetic to the heritage characteristics of the buildings of heritage value.

Development of a contemporary architectural character which is sympathetic and reflective of the existing heritage buildings in terms of scale, form and style is appropriate.

Development that incorporates modelling of facades, fenestration and architectural detailing which contributes to a rich, textural quality is desirable. Use of materials and finishes that respect State heritage places and Local heritage places and contribute to the character of the policy area are supported.

Enhancement of the pedestrian environment and pedestrian protection and shelter will be improved with verandas, pergolas and awnings.

It is desirable to provide additional landscaping throughout existing and new car parking areas with appropriate shade trees and shrubs.

Precinct 57 Henley Square

This precinct is predominantly commercial, with a variety of buildings providing several types of service ranging from retail shops to banking and restaurants. These buildings form the visual boundaries of Henley Square. The Square is a landscaped open space area which, with the adjoining beach, provides a natural focus for the policy area and setting for the adjoining buildings. The Square is part of the Metropolitan Open Space System.

The south and east sides of the Square are dominated by two-storey buildings, with balconies and verandas, displaying a range of detail from cast iron lace to timber brackets and stucco ornamentation. On the north side of the Square the buildings are generally single-storey and of simple detailing and construction. The built integrity of the policy area is good, with the majority of the original buildings remaining.

Buildings facing the Square are set-back relative to the buildings facing Main Street, therefore establishing the Square as a spatial entity. Footpaths are generally narrow, with no planted verges, and are paved with concrete pavers. The commercial nature of the policy area has precluded substantial plantings.

The precinct should remain a commercial area with a unique character due to its distinctive layout which creates a community focus area. The Square will retain its open, landscaped character and be further reinforced as a meeting place venue for open air entertainment and functions and uses such as restaurants and outdoor cafes. The openness of the Square is emphasized by the view to the west of the Henley Jetty and the sea. It is important that this vista is retained and enhanced and not detrimentally affected by inappropriate development.

Development will respect and enhance the existing pattern of development and the nineteenth and early twentieth century streetscapes of the policy area. It is envisaged that development will create a continuous edge of one and two-storey scale buildings around the Square ensuring buildings on the northern side don't overshadow the square. It is important that buildings and additions are consistent with the traditional character and design of the heritage buildings of the precinct including the siting, setback, scale, bulk, fenestration and building materials without necessarily replicating historical elements and decorative detailing. Buildings and additions are encouraged to have a roof-shape and pitch consistent with heritage buildings.

It is desirable that development adjoining the Square and along Seaview Road in proximity to the Square is built to the street frontage consistent with established development in the locality to achieve a more imposing and less domestic scale than elsewhere in the locality.

Precinct 58 Seaview Road

This precinct surrounds the historic centre of the policy area and comprises a mix of uses, such as a small supermarket and a wide range of shops, offices, restaurants and other uses. Additional activities which reinforce existing uses are appropriate, as well as shop top housing.

Development built to the street frontage and of contemporary modern design is supported. It is desirable that car parking be located to the rear or side of premises and where possible, shared. Enhancement of the pedestrian environment and pedestrian protection and shelter will be improved with verandas, pergolas and awnings.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/9 - Henley Square Policy Area 12](#).
- 3 Development should not exceed a height of 10.2 metres from natural ground level at any point.
- 4 Development should be built to the street frontage and of contemporary modern design.
- 5 Car parking should be to the rear or side of premises and, where possible, shared.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 57 Henley Square

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 58 Seaview Road

- 7 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Royal Park Policy Area 13

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The policy area will serve as a centre for the daily to weekly shopping and business needs of the surrounding areas, including the suburbs of Queenstown and Alberton. The Royal Park Centre contains a nominally large floor area, much of which is not used for primary retailing or Neighbourhood Centre activities. The policy area contains two retail nodes, the major being based around a small supermarket at the northern end. The expansion of the supermarket enlargement of the node to provide a more adequate range of shopping and centre facilities and off street parking is supported. It will be important to maintain the residential amenity in Cedar Avenue.

A smaller node which is of a local centre scale is located between Myrtle and Pine Streets with the Hendon Hotel at its southern end.

It will be important to include verandas or awnings in any additions or conversion of existing buildings to centre uses or new buildings fronting Tapleys Hill Road to provide pedestrian protection, particularly adjacent to bus stops.

The appearance of new development and the upgrading of existing buildings must be of a high standard.

It is desirable to provide additional landscaping throughout existing and new car parking areas with appropriate shade trees and shrubs.

An avenue of large growing trees along Tapleys Hill Road will provide shade and amenity on the street frontage.

The landscaping of areas adjoining Cedar Avenue with grass, trees and shrubs is supported.

The location of noisy or night time activities close to housing adjoining the policy area is not appropriate. Such activities will be located, with associated car parking, where they can be separated by existing buildings from adjacent housing.

Buildings will be designed to ensure privacy of adjacent residences in Cedar Avenue.

Precinct 59 Retail Core Royal Park

This precinct provides the main focus for shopping, business and office activities. The expansion of the small supermarket is encouraged as part of an integrated redevelopment providing additional specialty and convenience shopping. Uses of an institutional and community nature are appropriate.

It is envisaged that buildings fronting Cedar Avenue north of Poplar Street should be setback to the existing building alignment of the supermarket adjacent to Cedar Avenue and the setback landscaped to provide a pocket park for residents.

It will be important to provide parking in any expansion of the supermarket or new retail complex at the northern end of the Centre as an extension to the existing car park.

Precinct 60 Commercial Fringe Royal Park

This precinct accommodates a wide range of retail, large space retail, office, commercial and service uses of a local or neighbourhood nature together with medium to high density housing.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/10 - Royal Park Policy Area 13](#).
- 3 No vehicular access should be provided off Cedar Avenue to centre development, including extensions to the supermarket, unless they access development of a residential nature.
- 4 Buildings should be a maximum of 9 metres in height.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 59 Retail Core Royal Park

- 5 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 60 Commercial Fringe Royal Park

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Welland Policy Area 14

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Improvement and co ordination of access points, parking layout and landscaping of parking areas and the minimising of access points off Port Road.
- 2 Development that contributes to the desired character of the policy area and precincts.

DESIRED CHARACTER

The policy area shall serve as the centre for the weekly shopping and business needs for the surrounding areas. The policy area provides two supermarkets and a range of convenience and specialty shops and offices. Development will consolidate the existing Welland Centre and provide additional shopping, services and community facilities which are lacking in the existing policy area.

The appearance, surroundings, parking areas and accessibility of the Welland Centre by pedestrians and vehicles require improvement. Loading and service areas require screening and greening of the policy area is desirable to improve its image.

Any conversion of existing buildings to centre uses or new buildings fronting Port Road will provide verandas or awnings to provide pedestrian protection.

Storage and loading areas adjacent to Way Terrace and Brand Avenue will be improved and screened.

Additional landscaping of shade trees and shrubs will be provided throughout existing and new car parking areas.

Development abutting Brand Avenue will provide a dense screen of landscaping between buildings and the road and be orientated northwards and integrated with the remainder of the policy area.

Outdoor structures and furniture should have an integrated design theme.

Precinct 61 Retail Core Welland

This area is the focus for shopping, business and community activities. Development will consolidate the existing Centre and provide additional shopping, institutional and community facilities which are lacking in the existing Centre.

The upgrading of the appearance of the area and establishment of landscaping and pedestrians links through the car park to Port Road is supported.

Precinct 62 Commercial Fringe Welland

This area primarily accommodates banking, large space retailing, office, service trade and restaurant facilities. Uses of an institutional and community nature are appropriate.

Development will be well screened from housing at the rear and access and parking co-ordinated and integrated with existing parking areas in **Precinct 61 Retail Core Welland** so that it develops in a visually and functionally cohesive manner.

PRINCIPLES OF DEVELOPMENT CONTROL

Form and Character

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/11 - Wellend Policy Area 14](#).
- 3 Vehicular access points off Port Road should be limited to only those shown on [Concept Plan Map ChSt/11 - Wellend Policy Area 14](#).
- 4 Noisy or night time development should not be located close to Way Terrace or Brand Avenue.
- 5 Development should be a maximum of 12 metres in height.
- 6 Access to development adjacent to Brand Avenue should be provided from existing car parking areas or driveways wherever possible and avoid additional traffic generation in Brand Avenue.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 61 Retail Core Welland

- 7 Development should not be undertaken unless it is consistent with the desired character for the precinct.

Precinct 62 Commercial Fringe Welland

- 8 Development should not be undertaken unless it is consistent with the desired character for the precinct.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:

- (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
- (b) the building is not a State heritage place
- (c) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
- (d) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
- (e) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road
 - (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (f) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table ChSt/2 - Off Street Vehicle Parking Requirements](#) or the desired minimum rate in [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) (whichever table applies) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved

- (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Form of Development	Exceptions
Advertisement and/or advertising hoarding that:	
(a) is roof mounted and projects above the roof line	
(b) is parapet mounted and protrudes above the top of the parapet	
(c) is animated or flashing	
(d) is freestanding and greater than 6 metres in height within Findon Policy Area 10 and within Findon Road Policy Area 11	
(e) is freestanding and greater than 4 metres in height within Henley Square Policy Area 12	
(f) is freestanding signs or sign trees on Tapleys Hill Road, south of Poplar Street within Royal Park Policy Area 13 .	
Fuel depot	
Horticulture	
Industry	Except where it is for a service industry.
Major public service depot	
Motor repair station	
Road transport terminal	
Store	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 or Local Heritage Place that is described in Table ChSt/6 or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 .	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 . Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area.

Residential Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A residential zone comprising a range of dwelling types.
- 2 Increased dwelling densities in close proximity to centres, public transport routes and public open spaces.
- 3 Medium density residential housing on amalgamated sites or land adjoining the coast, River Torrens Linear Park, large public open spaces and roads of high traffic volumes.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling
 - dwelling addition
 - small scale non-residential uses that serve the local community, for example:
 - child care facilities
 - health and welfare services
 - open space
 - primary and secondary schools
 - recreation areas
 - shops, offices or consulting rooms
 - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.
- 3 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings with densities higher than, but compatible in form, scale and design with adjoining residential development.
- 4 Non-residential development should be of a nature and scale that:
 - (a) serves the needs of the local community
 - (b) is consistent with the character of the locality
 - (c) does not detrimentally impact on the amenity of nearby residents.
- 5 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.

Form and Character

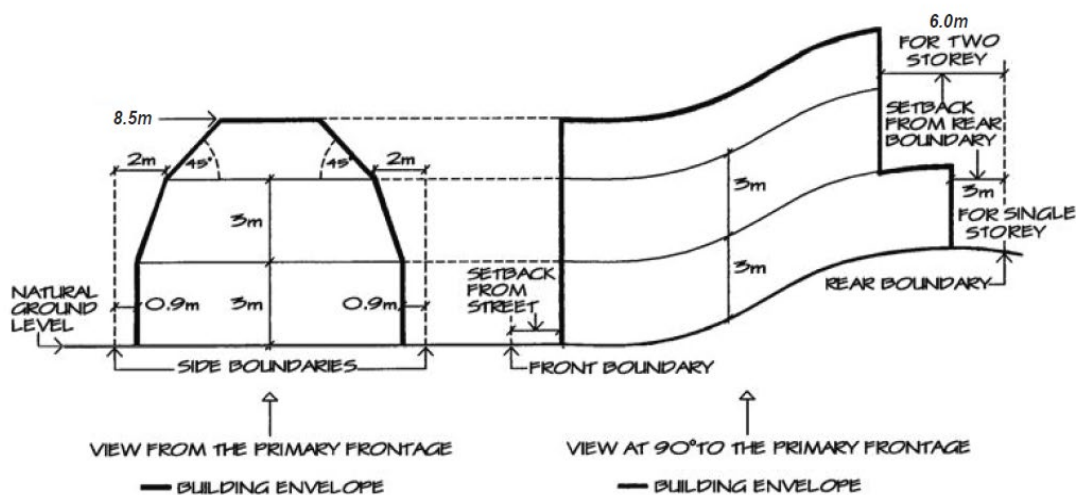
- 6 Development should not exceed two storeys in height except where specified in the relevant policy area.

- 7 The roof pitch of a dwelling should be less than 5 degrees or greater than 25 degrees.
- 8 Front fences should be incorporated as part of a dwelling development where the established character of a locality is identified with this element.
- 9 A new dwelling should not be developed to the rear of an existing dwelling where its access relies on a driveway which is located adjacent to or alongside an existing dwelling unless it can satisfy all of the following:
 - (a) the proposed access is at least 4 metres in width and includes a suitable driveway, landscaping and any communal area
 - (b) a minimum 2.5 metre setback is provided between the main face of the rear dwelling and the internal access road
 - (c) the dwelling located to the rear of the site is single storey or single storey in appearance where a second storey is incorporated into the roof space.
- 10 A lesser frontage than that specified in the relevant policy area may be considered in the following circumstances:
 - (a) within **Policy Areas 15 to 19**, for a dwelling where it relies on rear vehicular access to service parking areas
 - (b) within **Policy Areas 15 to 19**, for a group dwelling or a residential flat building where the site is greater than 1200 square metres in area provided the frontage is not less than 5 metres.
- 11 The minimum site area can be reduced by 15 per cent for a detached, semi-detached or row dwelling:
 - (a) within **Policy Areas 15 and 16**, where the site adjoins either of the following:
 - (i) the River Torrens Linear Park
 - (ii) a public open space reserve at least 2500 square metres in area or a public road which is adjacent to such a reserve
 - (b) within **Policy Areas 15 to 17**, where one or more of the following applies:
 - (i) it replaces a development listed as non-complying in the zone
 - (ii) the development involves a site greater than 1200 square metres in area.
- 12 Within **Policy Area 18**, the minimum site area can be reduced by 15 per cent, where one or more of the following applies:
 - (a) where it involves the construction of a dwelling that will replace an existing non-complying use in the zone
 - (b) where it involves the construction of a dwelling other than a residential flat building or group dwelling on a site greater than 1200 square metres in area.
- 13 In **Policy Areas 15, 16 and 17**, all dwelling types and densities may be considered where all of the following can be achieved:
 - (a) sites have a width greater than 25 metres
 - (b) there is not more than one vehicular access per 20 metres of frontage
 - (c) the site adjoins or has direct vehicular access to an arterial road or distributor road, as shown on *Overlay Maps – Transport*, and the road is currently serviced by public transport.

- 14 Row dwellings within **Policy Areas 15, 16, 17, 18, 19 and 20** should provide vehicular access to off-street car parking facilities from a rear public or private laneway, unless the site width of each dwelling exceeds 8 metres.
- 15 Side boundary walls should comply with all of the following (except where otherwise specified in the relevant policy area):
- have a maximum wall height of no more than 3 metres
 - have a maximum wall length of no more than 8 metres
 - be constructed along one side of the allotment only.
- 16 Dwellings which have direct frontage to a street should be designed to ensure that at least one habitable room window faces onto the primary street frontage.
- 17 Roof decks and green roofs may exceed building height limits where they have been designed and sited to achieve all of the following:
- to minimise overlooking into adjacent and nearby private open space and habitable room windows
 - to minimise overshadowing of adjacent property
 - to ensure all associated structures (eg stair well enclosure) are centrally located, screened from public view and designed to form an integral part of the building in relation to external finishes, form and colours.

Building Envelope Requirements

- 18 Development should be consistent with the building envelope requirements as shown in the figure below, except where a variation to the building envelope is specified in a particular policy area:



Inner Suburban Policy Area 15

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area encompasses parts of Bowden, Brompton, Renown Park and Ridleyton. It will continue to accommodate a variety of dwelling types up to two storeys (8.5 metres) in height. The net residential density of new dwellings will be in the order of 35 to 40 dwellings per hectare or lower (ie the lower end of the medium density range of 35 to 70 dwellings per hectare). This equates to a minimum average site area per dwelling of around 250 square metres.

Buildings will face the street. Low and semi-transparent front fencing will allow views to and from front yards and the adjacent street. Narrow frontage dwelling allotments (<8 metres) will require special attention to ensure that off-street car parking facilities do not dominate the streetscape. Row dwellings are discouraged unless 'rear loaded' or site frontages are sufficiently wide to minimise the visual and other impacts of off-street car parking.

The height, scale, bulk and density of housing will transition down to and be consistent with that of housing in adjacent Historic Conservation Areas, without the need to replicate 'character' housing styles.

New development will be characterised by contemporary housing that integrates well with the area's cosmopolitan and historic character of the late 19th century and early 20th century housing.

The policy area will experience a gradual transition in character over time to accommodate more intensive and contemporary development to capitalise on its proximity to public transport and the Adelaide Central Business District.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Existing industrial or commercial uses should be replaced with housing.
- 3 Residential development should achieve maximum net residential densities of up to 40 dwellings per hectare.
- 4 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), frontage to a public road and depth of not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Detached	250 minimum	8 - frontage
Semi-detached	250 minimum	8 - frontage
Group dwelling Residential flat building	220 average	16 - frontage for regular site 8 - frontage for irregular site

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Row dwelling	220 minimum	7 - frontage 25 - depth
All other dwelling types	250 minimum	9 - frontage

Mid Suburban Policy Area 16

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area covers the vast majority of the **Residential Zone**, extending from Renown Park and West Hindmarsh in the east to Semaphore Park and Henley Beach in the west. It will continue to accommodate predominantly detached and semi-detached dwellings up to two storeys (8.5 metres) in height. Residential flat buildings and group dwellings will occur on larger (>1200 square metres) and wider (>20 metres) allotments, as well as on sites adjacent to arterial roads and sites facing larger areas (>2500 square metres) of public open space. Row dwellings will only be developed on sites adjacent to arterial roads or a large (>2500 square metres) area of public open space.

The net residential density of new dwellings will be <35 dwellings per hectare (ie low density). This equates to a minimum average site area per dwelling of around 300 square metres.

Front property boundary setbacks of new dwellings will not be onstrained by prevailing adjacent setbacks, which can be particularly large and unduly limit development potential. Instead the default will be a 5 metre front setback for the dwelling façade and 5.5 metres for garages/carports.

The consolidation of existing land holdings to form larger development sites adjacent to arterial roads will be encouraged. This will facilitate and improve waste collection, the reduction of driveway crossovers, pedestrian safety, and the safety and efficiency of arterial road traffic movements.

Buildings will face the street. Front yards will be primarily landscaped areas open to public views, with private open space provided to the rear and side of a dwelling site. Low and semi-transparent front fencing will allow views to and from front yards and the adjacent street.

On-site car parking facilities, and particularly double garages, will be designed and sited to minimise their impact on the streetscape.

The policy area will experience a gradual transition in character to accommodate sometimes more intensive and contemporary development to capitalise on its proximity to the Adelaide Central Business District and the coastline.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Row dwellings should only occur where one of the following applies:
 - (a) the site adjoins an arterial road, as shown on *Overlay Maps – Transport*
 - (b) the primary frontage of the dwellings will face the River Torrens Linear Park or a public reserve greater than 2500 square metres in area, and should not be located in other parts of the policy area.
- 3 Residential development should achieve maximum net residential densities of up to 35 dwellings per hectare.

- 4 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), frontage to a public road and depth of not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Detached	300 minimum	8 - frontage
Semi-detached	300 minimum	8 - frontage
Group dwelling Residential flat building	300 average	16 - frontage for regular site 8 - frontage for irregular site
Row dwelling	250 minimum	8 - frontage 25 - depth
All other dwelling types	300 minimum	9 - frontage

- 5 Residential development located on land affected by airport or foundry noise shown on [Concept Plan Map ChSt/12 - Allotments Affected by Aircraft Noise](#) and [Concept Plan Map ChSt/13 - Allotments Affected by Foundry Noise](#), should be designed to locate bedrooms, living rooms and private open spaces away from those noise sources, or protect these areas with appropriate noise attenuation measures.

Western Edge Policy Area 17

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area encompasses most of the coastal strip of land generally west of Military Road. It will continue to accommodate predominantly detached and semi-detached dwellings up to two storeys (8.5 metres) in height. Three storey (12 metre) buildings will be appropriate in some limited circumstances adjacent to the **Neighbourhood Centre Zone – Henley Square Policy Area 12**.

The net residential density of new dwellings will be <35 dwellings per hectare (ie low density). This equates to a minimum average site area per dwelling of around 300 square metres.

Front property boundary setbacks of new dwellings will not be constrained by prevailing adjacent setbacks, which can be particularly large and unduly limit development potential. Instead the default will be a 5 metre front setback for the dwelling façade and 5.5 metres for garages/carports.

The consolidation of existing land holdings to form larger development sites adjacent to arterial roads will be encouraged. This will facilitate and improve waste collection, the reduction of driveway crossovers, pedestrian safety, and the safety and efficiency of arterial road traffic movements.

Buildings will face the street. Front yards will be primarily landscaped areas open to public views, with private open space provided to the rear and side of a dwelling site. Low and semi-transparent front fencing will allow views to and from front yards and the adjacent street.

On-site car parking facilities, and particularly double garages, will be designed and sited to minimise their impact on the streetscape. Under-croft parking may be appropriate, subject to design measures to minimise their visual impact on the streetscape.

Development will be designed to minimise its visual impact on the seaside character by adopting design techniques to reduce the bulk and scale of buildings. This will include the use of 'flat' roof forms and parapets, façade articulation and modelling, use of light colours and coloured materials for external cladding, and careful siting and design of stair wells, lifts and roof decks. The maintenance of appropriate building setbacks from the **Coastal Open Space Zone** boundary, and the establishment of a landscaped setting within this setback area, will assist in mitigating flood and erosion risks to development, providing space for any future coast protection works, and maintaining a more open seaside character.

The building of habitable rooms above garages along Seaview and Military Roads is appropriate subject to a high standard of design, which minimises visual impacts on the streetscape, increases the potential for passive surveillance of and addresses the street.

Vehicle access to sites will not interfere with the growth and retention of existing Norfolk Island Pines within the adjacent verge of the public road reserve.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.

- 2 Residential development should achieve maximum net residential densities of up to 35 dwellings per hectare.
- 3 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), frontage to a public road and depth of not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Detached	300 minimum	8 - frontage
Semi-detached	300 minimum	8 - frontage
Group dwelling Residential flat building	300 average	16 - frontage for regular site 8 - frontage for irregular site
Row dwelling	300 minimum	8 - frontage 25 - depth
All other dwelling types	300 minimum	9 - frontage

- 4 Dwellings on allotments that have a primary road frontage along the western side of Seaview Road, should be setback from this road a minimum of 5 metres.
- 5 Development should be carried out in accordance with 'Building Envelope Requirements' of the zone except where the following variations may apply:
- (a) the rear setback for single storey buildings may be reduced in the following circumstances:
- (i) in the case of a carport - nil metres
 - (ii) in the case of a garage/outbuilding - 0.9 metres
 - (iii) where the rear boundary adjoins a service lane - nil metres
- (b) the rear setback for 2 storey buildings may be reduced in the following circumstances:
- (i) where the rear boundary adjoins a service lane - nil metres
 - (ii) where a site has a frontage to the coast and adjoins that part of Seaview Road shown on *Overlay Maps ChSt/2, ChSt/6, ChSt/11, ChSt/17, ChSt/22 and ChSt/23 – Transport*, no closer than an associated garage
 - (iii) where a site has a frontage to the coast elsewhere - 5 metres
- (c) 3 storey (12 metres) buildings may be appropriate provided the building height is no greater than 10.2 metres and only where located within either of the following areas:
- (i) bounded by Henley Beach and Seaview Roads, Esplanade and the **Neighbourhood Centre Zone - Henley Square Policy Area 12**
 - (ii) bounded by **Neighbourhood Centre Zone - Henley Square Policy Area 12**, Military Road to North Street, North Street, Seaview Road, Marlborough Street and Esplanade.
- 6 Building work on allotments that have frontage to a coastal reserve or zone should only exceed the setbacks identified on [Concept Plans ChSt/14, 15, 16 and 17 – Building Restriction Areas](#), where it complies with the following:
- (a) it is not integral to the structural or functional requirements of an associated dwelling

- (b) it is readily removable or sacrificial in the event of coastal inundation, coastal erosion or the construction of coast protection works
- (c) it is designed and sited to minimise their visual impact on the seaside character.

7 Sheds, garages, carports and similar outbuildings, whether freestanding or not, should be setback within the following parameters:

Parameter	Value
Minimum setback from primary road frontage	Western side of Seaview Road (non-arterial part): (a) 2 metres where the building width is greater than 50 per cent of the allotment frontage (b) 1 metre where the building width is 50 per cent or less than the allotment frontage (c) at least 5.5 metres for a single garage/carport. Eastern side of Seaview Road (non-arterial part) and other roads: (a) where the finished ground floor level of the associated dwelling is 1.5 metres or less above the level of the adjacent kerb - 5.5 metres or behind the main face of the dwelling, whichever is the greater (b) where the finished ground floor level of the associated dwelling is greater than 1.5 metres above the level of the adjacent kerb - 5.5 metres.
Minimum setback from secondary road frontage (excluding Seaview Road - western side)	(a) if located on the boundary with the adjoining allotment - not less than the average of the setbacks of the adjoining buildings (b) where the development is for a carport, or a garage setback a distance of 0.6 metres or more from the boundary with the adjoining allotment - in line with the associated dwelling.

8 Tourist accommodation facilities may be appropriate on land south of Estcourt Road to and including the site of Estcourt House.

9 Development south of Estcourt Road to and including the site of Estcourt House should not be higher than the existing Estcourt House building and generally not exceed 2 storeys provided the existing landform of the coastal dune that contributes to the landscape significance of this portion of the coast is maintained.

West Lakes General Policy Area 18

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area includes the majority of the suburbs of West Lakes and West Lakes Shore. It will continue to accommodate predominantly detached and semi-detached dwellings up to two storeys (8.5 metres) in height. The net residential density of new dwellings will be <35 dwellings per hectare (ie low density). This equates to a minimum average site area per dwelling of around 350 square metres.

Sites with direct lake frontage will achieve higher net densities up to 40 dwellings per hectare, which equates to a maximum average site area per dwelling of around 250 square metres.

Front property boundary setbacks of new dwellings will not be constrained by prevailing adjacent setbacks, which can be particularly large and unduly limit development potential. Instead the default will be a 5 metre front setback for the dwelling façade and 5.5 metres for garages/carports.

The consolidation of existing land holdings to form larger development sites adjacent to arterial roads will be encouraged. This will facilitate and improve waste collection, the reduction of driveway crossovers, pedestrian safety, and the safety and efficiency of arterial road traffic movements.

Variability in housing styles and design is acceptable.

Development will be designed to minimise its visual impact on the seaside character by adopting design techniques to reduce the bulk and scale of buildings. This will include the use of 'flat' roof forms and parapets, façade articulation and modelling, use of light colours and coloured materials for external cladding, and careful siting and design of stair wells, lifts and roof decks. The maintenance of appropriate building setbacks from the **Coastal Open Space Zone** boundary, and the establishment of a landscaped setting within this setback area, will assist in mitigating flood and erosion risks to development, providing space for any future coast protection works, and maintaining a more open seaside character.

Dwellings will face the street, and on properties that have frontages to both a public street and the lake, dwellings will be designed to address both.

Low and open style fencing will allow views to and from front yards and the adjacent lake or coast.

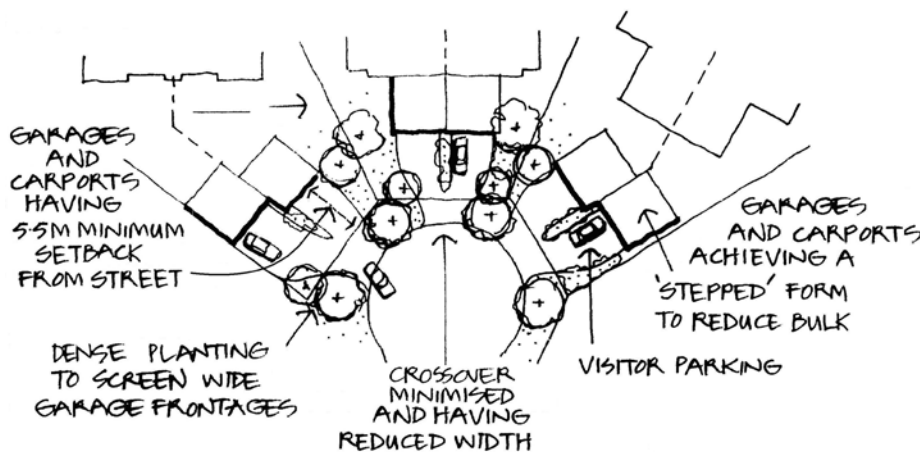
PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Residential development should achieve maximum net residential densities of up to 35 dwellings per hectare.
- 3 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), frontage to a public road and depth of not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
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Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Detached	350 minimum	10 - frontage
Semi-detached	350 minimum	10 - frontage
Group dwelling Residential flat building	350 average	18 - frontage for regular site 10 - frontage for irregular site
Row dwelling	350 minimum	10 - frontage 25 - depth
All other dwelling types	350 minimum	10 - frontage

- 4 The two storey component of a building should be setback no closer than 5 metres from the rear boundary.
- 5 Building work on allotments that have frontage to a coastal reserve or zone should only exceed the setbacks identified on [Concept Plans ChSt/14, 15, 16 and 17 – Building Restriction Areas](#), where it complies with the following:
 - (a) it is not integral to the structural or functional requirements of an associated dwelling
 - (b) it is readily removable or sacrificial in the event of coastal inundation, coastal erosion or the construction of coast protection works
 - (c) it is designed and sited to minimise their visual impact on the seaside character.
- 6 Development on sites with a frontage to a coastal reserve or zone should be consistent with the requirements as shown in the figure below:



Fencing

- 7 Fencing of boundaries forward of the main façade of a dwelling, and that abut lake or coastal frontage should:
 - (a) be open in style
 - (b) have a maximum height of 1.2 metres
 - (c) incorporate materials that match the associated dwelling.

- 8 Courtyard walls should:
- (a) be setback at least 2 metres from the Lake or coastal boundary (unless an existing easement requires a greater setback)
 - (b) constitute less than 30 per cent of the site frontage width
 - (c) incorporate materials that match the associated dwelling.

West Lakes Medium Density Policy Area 19

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area comprises smaller pockets of land distributed throughout West Lakes and West Lakes Shore. It will continue to accommodate a variety of housing types up to two storeys (8.5 metres) in height. Net residential density of new dwellings will be in the order of 35 to 40 dwellings per hectare or lower (ie the lower end of the medium density range of 35 to 70 dwellings per hectare). This equates to a minimum average site area per dwelling of around 250 square metres.

Sites with direct lake frontage may contain development up to three storeys (12 metres) in height provided the site is at least 30 metres from housing in other **Residential Zone** policy areas.

Front property boundary setbacks of new dwellings will not be constrained by prevailing adjacent setbacks, which can be particularly large and unduly limit development potential. Instead the default will be a 5 metre front setback for the dwelling façade and 5.5 metres for garages/carports.

The consolidation of existing land holdings to form larger development sites adjacent to arterial roads will be encouraged. This will facilitate and improve waste collection, the reduction of driveway crossovers, pedestrian safety, and the safety and efficiency of arterial road traffic movements.

Variability in housing styles and design is acceptable. There will not be a need to replicate the style and design of the original buildings designed and built in the 1970's and 1980's.

Development will be designed to minimise its visual impact on the seaside character by adopting design techniques to reduce the bulk and scale of buildings. This will include the use of 'flat' roof forms and parapets, façade articulation and modelling, use of light colours and coloured materials for external cladding, and careful siting and design of stair wells, lifts and roof decks. The maintenance of appropriate building setbacks from the **Coastal Open Space Zone** boundary, and the establishment of a landscaped setting within this setback area, will assist in mitigating flood and erosion risks to development, providing space for any future coast protection works, and maintaining a more open seaside character.

Dwellings will face the street, and on properties that have frontages to both a public street and the lake, dwellings will be designed to address both.

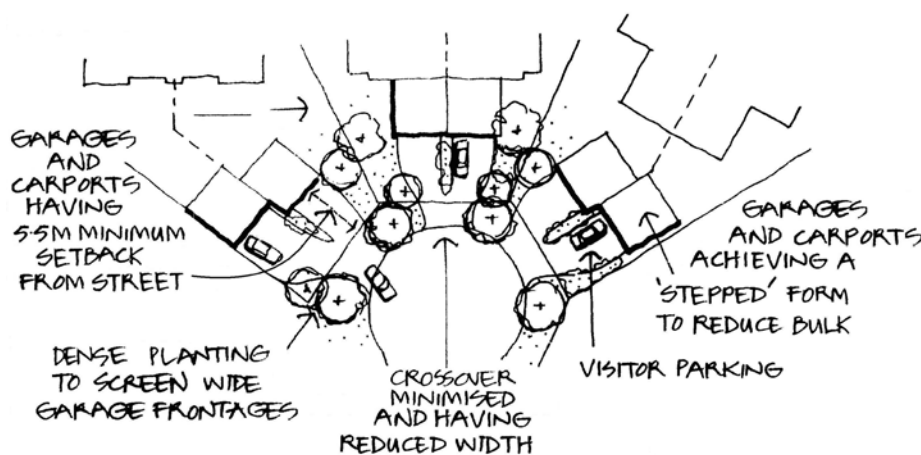
Low and open style fencing will allow views to and from front yards and the adjacent lake or coast.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Residential development should achieve maximum net residential densities of up to 40 dwellings per hectare.
- 3 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling), frontage to a public road and depth of not less than that shown in the following table:

Dwelling type	Site area (square metres)	Minimum dimensions (metres)
Detached	250 minimum	8 - frontage
Semi-detached	250 minimum	8 - frontage
Group dwelling Residential flat building	200 average	16 - frontage for regular site 10 - frontage for irregular site
Row dwelling	200 minimum	7 - frontage 25 - depth
All other dwelling types	250 minimum	9 - frontage

- 4 The two storey component of a building should be setback no closer than 5 metres from the rear boundary.
- 5 Building work on allotments that have frontage to a coastal reserve or zone should only exceed the setbacks identified on [Concept Plans ChSt/14, 15, 16 and 17 – Building Restriction Areas](#), where it complies with the following:
 - (a) it is not integral to the structural or functional requirements of an associated dwelling
 - (b) it is readily removable or sacrificial in the event of coastal inundation, coastal erosion or the construction of coast protection works
 - (c) it is designed and sited to minimise their visual impact on the seaside character.
- 6 Development on sites with a frontage to a coastal reserve or zone should be consistent with the requirements as shown in the figure below:



Development of former Council Depot land

- 7 Development of the land referred to as CT Reference No. 5486/297 should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/21 – Former West Lakes Council Depot Land](#).

Fencing

- 8 Fencing of boundaries forward of the main façade of a dwelling, and that abut lake or coastal frontage should:
 - (a) be open in style
 - (b) have a maximum height of 1.2 metres'

(c) incorporate materials that match the associated dwelling.

9 Courtyard walls should:

(a) be setback at least 2 metres from the Lake or coastal boundary (unless an existing easement requires a greater setback)

(b) constitute less than 30 per cent of the site frontage width

(c) incorporate materials that match the associated dwelling.

Integrated Medium Density Policy Area 20

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

No housing density limitations apply in the policy area and it is expected that its character will be represented by integrated subdivisions accommodating a wide range of dwelling types at medium to high densities, such as detached, semi-detached, row and group dwellings, residential flat buildings and aged person's housing, which demonstrate best practice urban development. Development will contribute to a cohesive housing form and style. Buildings of up to two storeys in height are appropriate, with three storey buildings appropriate in the middle of the policy area, adjacent the **Neighbourhood Centre Zone – Findon Policy Area 10** and, in any case, at least 30 metres away from housing in other policy areas of the **Residential Zone** or **Residential Character Zone**, and in accordance with the [Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site](#). It is envisaged that sites previously used for commercial purposes or land that has been previously used for waste disposal will be replaced with housing after, where necessary, decontamination has occurred.

Development that will achieve cohesive streetscapes whilst allowing for variety in housing form and style is desirable. Because of the higher density and integrated nature of housing development within these areas, it is not expected that infill development will satisfy all General Section provisions relating to such matters as visual impact, overshadowing and access to the rear of sites within the policy area itself. The need for water sensitive urban design (WSUD) will be increased as a result of the potential for greater site-cover, however, the ability to address WSUD on individual allotments may be constrained. Therefore, it is expected that stormwater management techniques may be integrated into the public realm, including within road reserves and public open space, within or in close proximity to the new development. This will not occur at the expense of well-designed and functioning roads and public open space.

Parts of the policy area contain clusters of South Australia Housing Trust (SAHT) public housing, some of which was developed prior to 1968. This housing is approaching the end of its economic life. Opportunities exist to make more efficient use of the land for higher density housing initiatives. Such areas will be redeveloped to accommodate a range of dwelling types to meet a variety of housing needs, including social, community, market and affordable housing. The planned redevelopment of these areas will improve housing diversity and affordability, encourage social mix and address site planning matters, such as WSUD, traffic management and upgrading the public realm, in an integrated manner.

Development within Bowden and Brompton shall reinforce the established rectilinear street and allotment pattern. It is envisaged that additional public open space will be provided in one substantial area south of Third Street between West and Brown Streets that would enable convenient pedestrian access from these streets and have proportions and a shape that ensures the reserve is usable and suitable for its purpose.

In Findon, west of Findon Road, housing development needs to make efficient use of sites and integrate effectively with surrounding land uses, especially the Findon Shopping Centre facility, as well as being sited and designed to contribute to and enhance attractive existing or expected streetscape character.

In Devon Park, within the area identified in [Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site](#), redevelopment shall be undertaken in a coordinated manner in accordance with the Concept Plan. The site, formerly used for industrial purposes, is proposed to be replaced with medium density housing after, where necessary, the proper remediation (in accordance with an appropriate remediation management plan) of potentially contaminated land is carried out to ensure land is suitable for the intended use.

In Devon Park, stormwater management and water sensitive urban design initiatives would need to consider potential contamination at the site to avoid mobilisation of contaminants and to protect receiving waters from water quality impacts associated with urban stormwater runoff.

In Devon Park, dwellings and residential flat buildings of up to three storeys will be developed where an appropriate transition is provided at the interface with adjoining residential land and industrial areas. The interface between the redevelopment site and adjoining areas should be well managed, with particular regard to the visual impact of new buildings, residential character, overlooking and overshadowing.

Overlooking of the adjoining industrial area will be minimised by the appropriate siting of windows and ground level private open space. The potential noise and air emission impacts from the nearby rail corridor, sportsground and industrial land uses should be managed through appropriate fencing, orientation of buildings and further suitable design treatments.

In Devon Park, development will result in a number of smaller buildings rather than fewer larger buildings to provide appropriate landscaped open space and to allow for pedestrian and vehicle access between buildings. Redevelopment of the Devon Park redevelopment site should be outward-facing, with housing fronting open spaces and roads, minimising side and rear fences to public spaces, except where the development is opposite the existing industrial land on Alexander Avenue (north of the site), where development should be designed inwards, with an articulated rear façade, ground level private open space and tall landscaping.

In Devon Park, well-designed pedestrian links through the site, connecting the Sportsground to the rail corridor and to existing residential areas, should be provided for the benefit of future residents as well as existing residents adjoining the site.

These links should be of a size and dimension to encourage passive recreation, allow for street furniture and ensure pedestrian safety.

In Devon Park, the site should be serviced by an internal road through the redevelopment site, with vehicle access points limited to those identified by the Concept Plan. Direct driveway access should be encouraged to the rear of housing, minimising the number of driveway access points to Bolingbroke Avenue, Alexander Avenue and Exeter Terrace. The design of the internal driveway access points identified by the Concept Plan should ensure safe and convenient movement. The internal driveway access onto Alexander Avenue should be designed in a manner to maximise separation distances from the existing crossovers associated with the existing industrial land on Alexander Avenue (north of the site).

In Devon Park, localised public open space should be provided for the purposes of protecting a majority of significant trees on the site as well as for local residential amenity, with solid fences to the boundary of open space minimised. The areas of public open space will be in addition to the private open space areas of each dwelling. Development should be designed to protect significant trees located on the site not located within local public open space. Additional landscaping will take place along the northern edge of the site to reduce overlooking into the existing industrial area north of the site.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be setback from the rear boundary in accordance with the following, except where otherwise stated in [Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site](#):

Parameter	Value
Minimum setback from the rear property boundary	1 metre for single storey components
	2 metres for two storey components
	3 metres for three storey components
	3 metres for the entire building where the site abuts land in another residential zone or policy area

- 3 Except in relation to a secondary street frontage, side boundary walls should be designed and sited to minimise impacts on the amenity of adjoining land, not include openings and achieve at least one of the following:
- (a) where it immediately abuts an existing or simultaneously constructed boundary wall on an adjoining allotment, the wall should not exceed the length and height of the adjoining wall
 - (b) where a building envelope plan exists for the adjoining land that allows for a wall on the side boundary, the wall should not exceed the length and height as provided for by that plan.
- 4 Except in Devon Park, dwellings should be setback from street boundaries in accordance with the following table:

Road type	Minimum setback (excluding any veranda, porch etc)
Primary road frontage	2 metres for single and two storey components
	3 metres for three storey components
	4 metres for the entire building where the site abuts land in another residential zone or policy area
Secondary road frontage	1 metre for single and two storey components
	2 metres for three storey components

- 5 In Devon Park, in the area shown on [Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site](#), the following applies:
- (a) Development should be carried out in accordance with [Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site](#).
 - (b) Residential development should not be developed until the site has been assessed and remediated as necessary to ensure that it is suitable for the proposed use.
 - (c) The site should be serviced by an internal roadway with a primary access located opposite the industrial area on Alexander Avenue and designed to minimise conflict with the entry and exit of industrial vehicles from the nearby industrial area on the north side of Alexander Avenue. A secondary access should be provided from Exeter Terrace.
 - (d) Additional driveway access off of Bolingbroke Avenue, Alexander Avenue and Exeter Terrace should be avoided, with vehicle access provided from an internal roadway.
 - (e) Dwellings shall provide a high level of design, with innovative and articulated facades where visible from the adjoining road network.
 - (f) For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than 6 metres or 50 per cent of the frontage width, whichever is the lesser. Garage or carport openings wider than these standards are inappropriate, except where any of the following apply:
 - (i) a site has rear vehicular access and from which vehicular access is obtained, in which case no maximum width applies
 - (ii) a site has a frontage of less than 12 metres and the dwelling is 2 or more storeys in height and incorporates protrusions such as verandahs, projecting windows, porches, balconies etc which provide articulation in the building as it presents to the street, in which case garages or carports should have a maximum width of 6 metres or 80 per cent of the frontage width, whichever is the lesser.

- (g) For 2 and 3 storey components of buildings, the rear setback distance should only be reduced to not less than 3 metres where it can be demonstrated that all of the following can be achieved:
- (i) solar access for adjoining properties will be maintained
 - (ii) there will be minimal loss of visual and acoustic privacy for future residents
 - (iii) direct overlooking of private open space areas of any adjoining sites from upper level living rooms will be avoided
 - (iv) adequate private open space will be available.
- (h) Where allotments have direct frontage to a public reserve:
- (i) rear vehicular access should be available to the allotments
 - (ii) dwellings should be at least 2 storeys in height
 - (iii) buildings should be orientated to provide passive surveillance from habitable rooms towards the reserve.
- (i) Site layout and building design should protect sunlight access to public spaces.
- (j) Dwelling setbacks should be in accordance with the following parameters:

Parameter	Value
Minimum setback from Alexander and Bolingbroke Avenues for up to two storeys	5 metres
Minimum setback from Alexander and Bolingbroke Avenues for three storeys	6 metres
Minimum setback from Exeter Terrace for up to two storeys. An additional setback may be required for clearances to electricity infrastructure	3 metres
Minimum setback from Exeter Terrace for three storeys	5 metres
Minimum setback from the southern boundary identified on Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site for up to two storeys, except where adjoining the boundary of a dwelling on the southern boundary	3 metres
Minimum setback from the southern boundary identified on Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site for three storeys, except where adjoining the boundary of a dwelling on the southern boundary. A lesser setback may be appropriate where an allotment is designed with public reserve frontage, rear vehicular access and protection of sunlight access to public spaces	8 metres
Minimum setback from the boundary of a dwelling on the southern boundary identified on Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site for single storey	3 metres
Minimum setback from the boundary of a dwelling on the southern boundary identified on Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site for two storeys	8 metres
Minimum setback from the boundary of a dwelling on the southern boundary identified on Concept Plan Map ChSt/26 - Devon Park Medium Density Redevelopment Site for three storeys	16.5 metres

Woodville Medium Density Policy Area 21

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 Development comprising primarily medium-density housing providing compact, affordable and adaptable housing choices designed to complement surrounding areas of open space, neighbouring centres and public transport nodes.
- 2 Development that supports best practice in sustainable urban development and design and in community service and infrastructure provision.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area is an infill site, formerly occupied by the Sheridan/Actil Factory and Trident Plastics Factory, located within close proximity of the Woodville District Centre and adjoining the former Cheltenham Racecourse, Woodville High School, St Clair Oval and Recreation Centre and the Woodville Railway Station.

The site, formerly used for industrial purposes, is proposed to be replaced with housing after, where necessary, the proper remediation of potentially contaminated land is carried out to ensure land is suitable for the intended use.

The policy area will be characterised by an integrated residential subdivision which demonstrates best practice sustainable urban development, and design offering a wide range of predominately medium density allotment sizes and dwelling types, including the provision of affordable and adaptable housing.

Increased densities will generally be located adjacent to areas of high public amenity including public open space areas and public transport nodes, and within the southern part of the site that is closest to the railway station, shopping precinct and other facilities. Smaller allotments are encouraged to accommodate medium density housing, the delivery of affordable and social housing products and smaller household formations.

The subdivision layout and subsequent development will:

- (a) create attractive, well landscaped, legible and liveable environments
- (b) convey a sense of place to residents and visitors
- (c) establish movement networks for vehicular, pedestrian and bicycle traffic
- (d) adopt passive energy and water conservation principles
- (e) be based on a modified grid pattern.

The subdivision layout also needs to underpin the movement networks for vehicular, pedestrian and bicycle traffic by:

- (a) connecting key areas of open space
- (b) establishing links with open space, pedestrian and bicycle paths, and to services and facilities such as schools, shopping precincts, community establishments and health services in neighbouring suburbs
- (c) providing easy access to public transport routes

- (d) ensuring bicycle/pedestrian pathways are designed and landscaped so as to be easily identifiable, accessible and safe
- (e) minimising potential conflicts between vehicular traffic and bicycle/pedestrian pathways
- (f) reducing potential vehicular traffic intrusion into adjoining residential neighbourhoods.

The road access to Actil and Brocas Avenues must give consideration to drop off and pick up zones adjacent to Woodville High School.

It is desirable that development contributes to a cohesive housing form and style and is of a high architectural standard and scale appropriate to the size of allotments and to the streetscape character so as not to appear dominant with respect to neighbouring buildings and to complement the interface relationship between the public and private realms. This can be established in part through appropriate design that includes interesting roof forms such as high pitches and parapets, high ceilings, building articulation, recessed vehicle garaging, use of light colours and materials and through suitable attractive landscaping. The inclusion of front verandas will encourage street interaction and activity providing for passive surveillance and safer streets.

It is anticipated that the policy area will be developed at a gross density of approximately 20 dwellings per hectare. There may be sites within the policy area where dwellings will be developed between a range of 15 dwellings per hectare for lower density and up to 35 to 40 dwellings per hectare for higher density. Housing forms will primarily be up to two storeys. However, higher buildings may be appropriate in certain locations such as adjoining public open space areas, adjacent Torrens Road and within the southern portion of the site where higher density development is envisaged close to public transport and local facilities. The use of iconic built form and scale will be considered to emphasise major arrival focus points, accentuate key corners and terminate vistas. Development should create an appropriate height transition between higher level development within the policy area and established low rise residences adjacent to it.

The policy area should have a leafy garden village feel created by a seamless landscape between private gardens, street planting and integration with adjacent public open space. A primary central open space area will provide the main community focus, as well as providing a link between St Clair Oval and the future development of Cheltenham Park Policy Area 22 and Trident Plastics. This space could also be used for stormwater management. New streets shall create a sense of identity and cohesion through landscaping and, where possible, be developed with pedestrian movements focussed on one side, particularly where adjacent to public open spaces. In such locations the adjoining housing may be developed with reduced front setbacks.

Development of the policy area shall strongly emphasise both visual and physical connectivity with the surrounding area and facilities. Priority is given to establishing cohesive pedestrian and bicycle movement corridors and visual links between new development and adjoining areas including public recreation areas, public transport nodes and local shopping and community facilities. Development to facilitate connection with Woodville Road through the reopening of Brocas Avenue is desirable. The creation of a major arrival focus where Actil Avenue intersects with Brocas Avenue, through appropriate built form, landscaping and public open space, is envisaged.

New development will need to take into consideration the future development of the Cheltenham/Woodville locality by providing opportunities for integration, movement and open space corridors.

Areas adjacent to the policy area, on the northern side of Torrens Road, include non-residential uses such as industry. It will be important for development within the policy area to address reducing potential impacts on existing uses and vice versa. Development will acknowledge and respond to the activity and function of existing and adjoining land uses through solutions that mitigate adverse impacts without affecting the long term viability of those uses whilst allowing for future changes in use.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should comprise a wide range of dwelling types covering a range of densities including both large and small housing allotments for medium density housing.
- 3 Development should be carried out in accordance with the [Concept Plan Map ChSt/18 - Cheltenham Park and Woodville Medium Density](#).
- 4 Buildings should generally be of one to two storeys in scale and, in any event, should not have a maximum height exceeding 3 storeys (12 metres) above natural ground level at any point. Three storey buildings may be appropriate in areas adjacent to public open space, open space connections, adjacent Torrens Road and within proximity of the Woodville Railway Station and local facilities.
- 5 Dwellings should be designed within the following setback parameters:

Parameter	Value
Minimum setback from primary road frontage	4.5 metres - along Actil Avenue between Torrens Road and Brocas Avenue. 1.5 metres - where the allotment adjoins a public reserve and rear vehicular access is available. 3.5 metres - all other allotments.
Minimum setback from secondary road frontage	1.5 metres
Minimum setback from rear boundary	3 metres - single storey. 6 metres - 2 or more storeys. Nil - where adjoining a service or access lane.

- 6 For two or more storey components of buildings, the rear setback distance may be reduced to not less than 3 metres where it can be demonstrated that all of the following can be achieved:
 - (a) solar access for adjoining properties will be maintained
 - (b) there will be minimal loss of visual and acoustic privacy for future residents
 - (c) direct overlooking of private open space areas of any adjoining sites from upper level living rooms will be avoided
 - (d) adequate private open space will be available.
- 7 Garages and carports whether freestanding or not should be designed within the following setback parameters, whichever provides the greatest distance:

Parameter	Value
Minimum setback from primary road frontage	5.5 metres
Minimum setback from secondary road frontage	1.5 metres
Minimum setback from other road frontages	0.5 metres behind the main face of the dwelling or in line with the main face of the dwelling where the dwelling incorporates building elements and projections that provide articulation in the building.

- 8 Allotments may have direct frontage to a public reserve provided that the public park has an area of at least 2000 square metres and that rear vehicular access is available to the allotments.
- 9 Except for the Trident Plastics Factory site, no new road access should be provided from Torrens Road.
- 10 No vehicular access to individual allotments should be provided from Torrens Road.
- 11 An internal road network should be provided to support residential development on the Trident Plastics Factory site that:
 - (a) provides connection to a key vehicle access point on Torrens Road
 - (b) services land within the Trident Plastics site
 - (c) provides safe and convenient secondary vehicle access to neighbouring land to the east and south
 - (d) provides safe and convenient pedestrian linkages with the surrounding locality.
- 12 In the case of multiple dwellings on one site, access to parking and garaging areas from public streets should be via a common driveway.
- 13 Garage or carport openings greater than 6 metres or 50 per cent of the frontage width are not appropriate, except where either of the following apply:
 - (a) a site having solely a vehicular access from the rear - no maximum width applies
 - (b) a site having a frontage of less than 12 metres and a dwelling of two or more storeys incorporating building elements and projections that provide articulation in the building - a maximum width of 6 metres or 80 per cent of the width of the site, whichever is the lesser distance.

Cheltenham Park Policy Area 22

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A range of housing forms, including medium to high density dwellings and affordable housing.
- 2 Development that supports best practice in sustainable urban development and design and in community service and infrastructure provision.
- 3 Development that provides safe and convenient access to passenger rail services, Woodville District Centre, Woodville High School, St Clair Oval and Recreation Centre, public transport routes and other local facilities.
- 4 Development and the pattern of development that acknowledges the potential for future redevelopment of adjoining sites and ensures that urban design solutions do not restrict current or future use, or preclude future integration with adjoining sites.
- 5 Public open space areas developed for a range of passive and active outdoor recreation activities, and the enhancement of conservation, revegetation and biodiversity.
- 6 Creation of a network of linked paths, reserves and recreation areas at regional and local levels.
- 7 Development that supports the viability of community services and infrastructure and reflects good residential design principles.
- 8 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area is a residential infill site, formerly occupied by the Cheltenham Racecourse, located within close proximity of the Woodville District Centre, Woodville High School, St Clair Oval and Recreation Centre and the Woodville Railway Station.

The policy area will be developed in accordance with the [Concept Plan Map ChSt/18 - Cheltenham Park and Woodville Medium Density](#) and comprise a broad range of dwelling types, including medium to high density housing, and areas of open space. It is also envisaged that the policy area will include a mixed use development precinct integrated with a passenger rail transit station.

A total of 35 per cent of the policy area (excluding the railway corridor), which is in excess of 17 hectares, will be retained for open space, with at least 11 hectares located in one or two parcels. The area of open space will incorporate:

- (a) a range of recreation and sporting activities of both a passive and active nature
- (b) stormwater management areas in the form of wetlands, creek lines and permanent water bodies, including an aquifer storage and recovery system
- (c) an east-west linear open space corridor, linking the policy area to Woodville Road
- (d) north-south open space corridors linking Torrens Road to the Railway reserve
- (e) smaller parks providing local open space needs, and an attractive setting for higher density housing forms and any buffer requirements
- (f) water sensitive urban design features and sustainable landscapes to enhance biodiversity.

Open space provided in the form of reserves (excluding roads, road verges, buffer areas, and entrances and similar components) will not only help provide an attractive focal point and setting for high density dwellings in the policy area, particularly taller buildings, but also encourage a sense of place for residents and visitors. All open space areas will support a network of pedestrian and bicycle paths located throughout the site to facilitate access and linkages with adjacent facilities, public transport services and areas. It is important that the amount of useable space available for the public is maximised, the amount of land used for ornamental purposes such as landscape and entrance statements is minimised and that it is accessible to all ages and those with disabilities, and are located and designed to encourage new and existing residential communities to gather.

Land division must be planned to support open space and movement networks. The movement networks and development configuration are to be based on a modified grid pattern to achieve good interconnectivity and will:

- (a) connect key areas of open space within the policy area
- (b) establish links with open space, pedestrian and bicycle paths, and to services and facilities such as schools, shopping precincts, community establishments and health services in neighbouring suburbs
- (c) provide easy access to public transport routes
- (d) ensure bicycle/pedestrian pathways are designed and landscaped so as to be easily identifiable, accessible and safe
- (e) minimise potential conflicts between vehicular traffic and bicycle/pedestrian pathways
- (f) reduce potential vehicular traffic intrusion into adjoining residential neighbourhoods
- (g) facilitate major neighbourhood entry and exit points at Torrens Road and Cheltenham Parade.

It is expected that the public road system will mostly encircle reserves and open space areas. Allotments with a direct frontage to a public reserve and open space is envisaged provided rear vehicular access is available to the allotments. In addition, where an allotment immediately adjoins public open space, clear, safe and efficient pedestrian and bicycle access will be provided on the open space land, along with adequate visitor parking.

The policy area will be characterised by an integrated residential subdivision offering a wide range of housing types, with multi-storey, medium to high density housing forms comprising semi-detached and row dwellings and residential flat buildings, with an overall density of at least 20 dwellings per hectare achieved. Medium to high density housing will dominate areas of high public amenity including locations adjacent to major open spaces, public transport routes, local shops and public facilities, and within the southern part of the site in order to:

- (a) provide an aesthetically pleasant setting for those dwellings
- (b) ensure residents with reduced private open space areas have easy access to public reserves
- (c) facilitate access to services and facilities for a greater number of people
- (d) provide a sense of space for residents
- (e) improve opportunities for passive energy conservation in buildings
- (f) facilitate passive surveillance of adjacent reserves and corridors
- (g) reduce potential for overshadowing and overlooking of any adjacent lower level residential development and private open space areas
- (h) encourage public transport use, cycling and walking.

Areas of medium to high density development will be supported by a subdivision layout that demonstrates a capacity to accommodate multi-storey buildings to maximise the number of households taking advantage of the site's attributes, the delivery of affordable and social housing products, and access to smaller household formations. At least 15 per cent of the housing developed within the policy area will be for affordable and high needs housing.

The built form character will be established through appropriate design, including interesting roof forms such as high pitches and parapets, high ceilings, building articulation, recessed vehicle garaging, and appropriate landscaping. The siting and design of buildings needs to also promote cohesive streetscapes whilst allowing for variety in housing form and style. This can be achieved through attention to scale and bulk of buildings relative to one another and to the allotment size and to the way in which buildings address the street frontage (both street frontages in the case of corner allotments).

While front fencing and landscaping (or a combination of these elements) will provide delineation between public and private spaces, the inclusion of front verandahs and low front fencing will also be encouraged to promote street interaction and activity, to provide for a safe environment through passive surveillance and to emphasise thermal efficiency.

The use of iconic built form and scale will be considered to emphasise major arrival focus points, accentuate key corners and terminate vistas. The development of the policy area will also recognise the former uses of the land, which could include the reuse of the racecourse entry gates off Cheltenham Parade in the design of open space or as an entry statement to residential areas.

Non-residential land uses such as a small supermarket and specialty shops, health and community facilities and other service facilities will form part of a mixed use precinct that is integrated with a new rail transit station located in the policy area, and complements higher residential densities. This mixed use precinct will cater primarily to the needs of rail commuters and residents within the policy area. Car parking areas will be shared between facilities and be provided beneath buildings.

The policy area will be extensively landscaped, with high quality street planting (with species selected to be in scale with associated development) integrated with open space areas. Smaller pocket parks and landscaped buffer areas will be located within the residential areas, with adjoining housing developed with reduced setbacks. Paved areas within public view are to be softened with the use of landscaping.

Development is to contribute to a cohesive extension of the urban form, emphasising both visual and physical connectivity through road and pedestrian/cycle networks and a high quality public environment that is designed to facilitate:

- (a) access to a new railway station developed in the policy area or to nearby Woodville Railway Station
- (b) pedestrian and bicycle movements between the policy area and nearby residential neighbourhoods
- (c) access to public recreation areas, public transport nodes and local shopping and community facilities.

Areas adjacent to the policy area include residential and non-residential activities. Development within the policy area will:

- (a) create an appropriate transition between established low rise residences adjacent to the Policy Area and higher level development internal to it
- (b) acknowledge and respond to the activity and function of existing and adjoining non residential land uses through solutions that mitigate adverse impacts without affecting the long term viability of those uses, whilst allowing for future changes in use
- (c) emphasise links and connectivity with existing local facilities such as St Clair Oval and Recreation Centre, Woodville High School, Woodville District Centre, library and civic centre services, and potential future development of adjoining sites.

Housing and other sensitive uses need to be sited and designed to mitigate potential noise impacts from nearby non-residential activities and from major traffic routes and the rail line. This may be achieved through the adoption of solid, freestanding acoustic barriers, vegetated mounds and the orientation of dwellings. However, freestanding acoustic barriers will not be located where they would compromise connectivity with nearby residential areas.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should be carried out in accordance with the [Concept Plan Map ChSt/18 - Cheltenham Park and Woodville Medium Density](#).
- 3 Development should comprise:
 - (a) a wide range of dwelling types at a range of densities, with a minimum 15 per cent of residential dwellings being for affordable housing including 5 per cent for high needs housing
 - (b) open space and associated facilities comprising at least 35 per cent (17.1 hectares) of the entire policy area (excluding the rail corridor), with a minimum of 11 hectares being accommodated in no more than two parcels one of which is to be located at the corner of Torrens Road and Cheltenham Parade
 - (c) areas for the management of stormwater in the form of wetlands and stormwater reuse (including an aquifer storage and recovery system)
 - (d) a licensed community sport and recreation club adjacent Torrens Road or Cheltenham Parade
 - (e) an area dedicated for mixed use development located adjacent to, and integrated, with a passenger rail transit station which should incorporate:
 - (i) retail development including a supermarket and specialty shops provided the gross leasable floor area does not exceed 5000 square metres
 - (ii) health care and community facilities, offices and other employment generating activities
 - (iii) higher density residential development consisting of apartments, multi-storey aged persons accommodation and 'shop-top' housing
 - (iv) shared vehicle parking opportunities provided primarily beneath the buildings with access that minimises the potential for conflicts with pedestrians and cyclists.
- 4 Medium to high density housing should be located close to areas of open space, public transport routes, local shops and public facilities.
- 5 Medium to high density housing in the form of multi storey buildings should predominate:
 - (a) on allotments fronting major reserves and open spaces as indicated on [Concept Plan Map ChSt/18 - Cheltenham Park and Woodville Medium Density](#)
 - (b) within 400 metres of a passenger rail transit station located in the policy area or within 600 metres of the Woodville Railway Station.
- 6 Affordable housing should be distributed throughout the policy area and located to optimise access to shops, social services and facilities and public transport.

7 Buildings should be of 2 storeys (8.5 metres) to 4 storeys (15 metres) in height. Taller buildings should only occur in areas adjacent to public open space, open space connections or within close proximity to a railway station and/or local facilities.

8 Dwellings should be designed within the following setback parameters:

Parameter	Value
Minimum setback from primary road frontage	1 metre - where the allotment is adjacent to a public reserve and rear vehicular access is available. 2.5 metres - all other allotments.
Minimum setback from secondary road frontage	1.5 metres
Minimum setback from rear boundary	3 metres for single storey. 5 metres for 2 and 3 storeys. Variable for 4 or more storeys. Nil where adjoining a service or access lane.

9 For 2 and 3 storey components of buildings, the rear setback distance should only be reduced to not less than 3 metres where it can be demonstrated that all of the following can be achieved:

- (a) solar access for adjoining properties will be maintained
- (b) there will be minimal loss of visual and acoustic privacy for future residents
- (c) direct overlooking of private open space areas of any adjoining sites from upper level living rooms will be avoided
- (d) adequate private open space will be available.

10 Four storey or more components of buildings should be setback from the rear boundary a distance that would minimise adverse impacts on neighbouring development, such as overshadowing and overlooking.

11 Dwellings sited on side boundaries should be designed to minimise impacts on the amenity of adjoining land. When located on a side boundary, the dwellings' walls should have no openings and achieve at least one of the following:

- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height.
- (b) where a building envelope plan exists for the adjoining land that proposes a wall on the side boundary, be constructed to the same or lesser length and height as provided for by that plan
- (c) be located to maintain pedestrian access to the rear of the site along one side of the dwelling, unless one of the following is achieved:
 - (i) access is provided directly via a carport or garage and avoids the need to enter other parts of a dwelling
 - (ii) an acceptable alternative method of gaining access to the rear of a dwelling is provided, including a rear access or service lane.

12 Built form should be appropriate to the size of the allotment on which it is to be constructed, the width of the street on which it is located and the streetscape character while not appearing dominant with respect to neighbouring buildings and structures.

- 13 Garages and carports whether freestanding or not should be designed within the following setback parameters, whichever provides the greatest distance:
 - (a) at least 0.5 metres behind the main face of the associated dwelling
 - (b) in line with the main face of the associated dwelling if the dwelling incorporates protrusions such as verandahs, projecting windows, porches, balconies etc which provide articulation in the building as it presents to the street
 - (c) at least 5.5 metres from the primary frontage.
- 14 Where allotments have direct frontage to a public reserve:
 - (a) rear vehicular access should be available to the allotments
 - (b) dwellings should be at least 2 storeys in height
 - (c) buildings should be orientated to provide passive surveillance from habitable rooms towards the reserve.
- 15 New vehicular access to individual allotments should not be provided from Torrens Road or Cheltenham Parade.
- 16 In the case of a site containing more than one dwelling, access to parking and garaging areas from public streets should be via a common driveway.
- 17 Buildings having 4 storeys or more should make provision for undercroft or basement parking.
- 18 For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than six metres or 50 per cent of the frontage width, whichever is the lesser. Garage or carport openings wider than these standards are inappropriate, except where any of the following apply:
 - (a) a site has rear vehicular access and from which vehicular access is obtained, in which case no maximum width applies
 - (b) a site has a frontage of less than 12 metres and the dwelling:
 - (i) is 2 or more storeys in height
 - (ii) incorporates protrusions such as verandahs, projecting windows, porches, balconies etc which provide articulation in the building as it presents to the street,

in which case garages or carports should have a maximum width of 6 metres or 80 per cent of the frontage width, whichever is the lesser.
- 19 Site layout and building design should protect sunlight access to public spaces.

Land Division

- 20 Land division should support the provision of a broad range of housing options.
- 21 Allotments fronting major open space and public reserves as shown on [Concept Plan Map ChSt/18 - Cheltenham Park and Woodville Medium Density](#) or within 400 metres of a passenger rail transit station that may be developed within the policy area or 600 metres of the Woodville Railway Station should be capable of accommodating multi-storey medium to high density housing.

- 22 Land division should accommodate open space and movement networks that provide for the following:
- (a) strong connections and safe and convenient access to public facilities, public transport, adjacent future development sites and the surrounding existing established neighbourhoods
 - (b) areas for the management of stormwater in the form of wetlands and stormwater reuse (including an aquifer storage and recovery system).

Woodville West Policy Area 23

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 A range of housing forms at medium to high densities, including a minimum 15 per cent affordable housing.
- 2 Development that supports best practice in sustainable urban development and design and in community service and infrastructure provision.
- 3 Development that provides safe and convenient access to passenger rail services, Frank Mitchell Park, public transport routes and other local facilities.
- 4 Development and the pattern of development that acknowledges the potential for future redevelopment of adjoining sites and ensures that urban design solutions do not restrict current or future use, or preclude future integration with adjoining sites.
- 5 Creation of a network of linked paths, reserves and recreation areas at regional and local levels.
- 6 Development that supports the viability of community services and infrastructure.
- 7 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

This policy area comprises a significant regeneration site that will result in the replacement of high concentrations of original public housing stock with a new urban form that capitalises on the area's proximity to the Albert Park Railway Station and potential improvements to the mass rail transit system.

The policy area will be developed in accordance with [Concept Plan Map ChSt/22 - Woodville West](#), and create a distinctive identity where low to medium rise buildings and land use activities successfully integrate with open spaces and streetscapes to stimulate community interaction, interest and participation. This will include the delivery of a broad range of medium to high density and affordable housing products catering for a wide variety of household needs, and a range of complementary non-residential uses such as small scale shops, cafes, offices, and consulting rooms, including home based employment.

The overall urban form will:

- (a) take cues from a land division pattern based on a modified grid that achieves good interconnectivity and permeability within and through the Policy Area
- (b) establish links between areas of open space, pedestrian and bicycle paths
- (c) provide access to public transport routes, including the nearby Albert Park Railway Station
- (d) ensure bicycle/pedestrian pathways are designed and landscaped so as to be easily identifiable and safe
- (e) include laneways and local streets where pedestrian and bicycle movements take priority over vehicular traffic
- (f) create an appropriate transition between established low rise residences adjacent to the policy area and higher level development internal to it.

Character will be established through:

- (a) built form that includes interesting and varied roof forms, solid to void ratios, comparably high wall/storey heights (derived from ceiling heights between 2.7-3.0 metres), building articulation, recessed vehicle garaging, and appropriate landscaping
- (b) cohesive streetscapes through the use of setbacks and attention to scale between individual buildings and the street(s) that they face
- (c) front fencing and landscaping (or a combination of these elements) to delineate between public and private spaces, in a manner that allows visual permeability and avoids large blank surfaces
- (d) the provision of front verandahs, balconies and low front fencing that promote street interaction and activity and maximise opportunities for passive surveillance of public spaces
- (e) extensive landscaping with high quality street planting and public realm integrated with open space areas
- (f) pocket parks and landscaped buffer areas located within the residential areas, with adjoining housing developed with minimal setbacks.

The new built form and pattern of development is expected to drive the streetscape character along roads or sections of roads that are subject to comprehensive redevelopment. Where relevant, development will take cues from the primary road setback of existing dwellings adjacent to and/or opposite the development site to promote cohesive streetscapes.

A Core Precinct will be established as the focus for community interaction and activity for residents, workers and visitors to the policy area. The Core Precinct will be characterised by:

- (a) the use of multi-storey buildings that are attractive and functional and clearly identify it as a community hub
- (b) minimal to zero setbacks for buildings to define streets, street corners and public spaces
- (c) well defined connections with surrounding pedestrian and bicycle networks and public transit facilities
- (d) a significant neighbourhood open space that is legible and accessible, incorporating high quality treatments to create an inviting place for people to congregate
- (e) activated public spaces through:
 - (i) the inclusion of non-residential uses at ground level along pedestrian thoroughfares linked to transit stops, and the edges of public reserves
 - (ii) parking areas being located other than at ground level and/or designed to contribute to vibrant place making
- (f) opportunities for a high degree of passive surveillance of public spaces and pedestrian thoroughfares through the positioning of non-residential uses and building entrances, and the orientation of buildings
- (g) the tallest buildings in the policy area, up to a maximum five storeys (17.5 metres) from natural ground level.

The design of street networks will make visual distinctions between places for cars and places for pedestrians. Local streets (including rear access and service lanes) will be designed as 'urban walks' that promote pedestrian priority, the street as a place for interaction and pedestrian permeability particularly to the Core Precinct and transit stops.

PRINCIPLES OF DEVELOPMENT CONTROL

- 1 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 2 Development should comprise:
 - (a) a wide range of dwelling types at medium and high densities, with a minimum 15 per cent of residential dwellings being for affordable housing including 5 per cent for high needs housing
 - (b) open space areas for amenity and recreation
 - (c) areas for the management of stormwater, which may be integrated as part of an open space area provided it does not unreasonably compromise the recreation or amenity value and use of the open space
 - (d) small scale non-residential uses (e.g. shops and offices) within the Core Precinct shown in [Concept Plan Map ChSt/22 - Woodville West](#)
 - (e) other than in the Core Precinct, consulting room or office attached to and integrated with a dwelling, where the floor area used as a consulting room or office does not exceed 50 square metres.
- 3 Residential development should achieve a net site density in accordance with the following:
 - (a) outside the Core Precinct – between 35 and 70 dwellings per hectare
 - (b) in the Core Precinct – a minimum of 70 dwellings per hectare
- 4 Affordable housing should be distributed throughout the policy area and located to optimise access to shops, social services and public transport.
- 5 The height of development should not exceed (from natural ground level) those indicated on [Concept Plan Map ChSt/22 - Woodville West](#).
- 6 Other than the Core Precinct shown on [Concept Plan Map ChSt/22 - Woodville West](#), buildings should be designed within the following setback parameters:

Parameter	Value
Minimum setback from a primary road frontage	1 metre – where the allotment adjoins a public reserve and rear vehicular access is available. 2.5 metres – all other allotments
Minimum setback from a secondary road frontage	1.5 metres
Minimum setback from a rear boundary	3 metres for single storey 5 metres for 2 and 3 storeys Nil where adjoining a service or access lane.

- 7 The minimum building setback parameters detailed above may be reduced where it can be demonstrated that all of the following can be achieved:
 - (a) solar access for adjoining properties will be maintained
 - (b) minimal loss of visual and acoustic privacy for future residents
 - (c) minimal direct overlooking of private open space areas of any adjoining sites from upper level living rooms and balconies

- (d) adequate private open space will be available
 - (e) adequate sight-lines are maintained for motorists
 - (f) blank walls facing public boundaries are avoided
 - (g) adequate area for landscaping along street frontages is available.
- 8 For 2 and 3 storey components of buildings, the rear setback distance should only be reduced below 3 metres where it can be demonstrated that all of the following can be achieved:
- (a) solar access for adjoining properties will be maintained
 - (b) there will be minimal loss of visual and acoustic privacy for future residents
 - (c) direct overlooking of private open space areas of any adjoining residential sites from upper level living rooms will be avoided
 - (d) adequate private open space will be available.
- 9 Four storey or more components of buildings should be sited and designed to minimise adverse impacts on neighbouring development.
- 10 Development should provide a primary road setback to:
- (a) complement the established streetscape character when fronting any portion of a road that forms part of the boundary to this Policy Area, other than where opposite a public reserve or fronting Alma Terrace
 - (b) create a cohesive streetscape character along other roads or sections of roads subject to comprehensive redevelopment.
- 11 In the Core Precinct shown on [Concept Plan Map ChSt/22 - Woodville West](#), building setbacks from primary road frontages should be minimal to zero to define streets, street corners and public spaces unless a larger setback is required to meet noise and/or air quality standards.
- 12 Residential development close to high noise and/or air pollution sources (e.g. major roads, railway lines, tram lines and industry) should be designed or measures taken to minimise adverse impacts from those sources.
- 13 Except in relation to a secondary street frontage, buildings sited on side boundaries should be designed to minimise impacts on the amenity of adjoining land. When located on a side boundary, the buildings' walls should have no openings and achieve at least one of the following:
- (a) be located immediately abutting the wall of an existing or simultaneously constructed building on the adjoining land to the same or lesser length and height.
 - (b) where a building envelope plan exists for the adjoining land that proposes a wall on the side boundary, be constructed to the same or lesser length and height as provided for by that plan
 - (c) be located to maintain pedestrian access to the rear of the site along one side of the dwelling, unless one of the following is achieved:
 - (i) access is provided directly via a carport or garage and avoids the need to enter other parts of a dwelling
 - (ii) an acceptable alternative method of gaining access to the rear of a dwelling is provided, including a rear access or service lane.

- 14 Built form should be appropriate to the size of the allotment on which it is to be constructed, the width of the street on which it is located and the streetscape character while not appearing dominant with respect to neighbouring buildings and structures.
- 15 Garages and carports, whether freestanding or not, should be designed within the following setback parameters, whichever provides the greatest distance:
 - (a) at least 0.5 metres behind the main face of the associated dwelling
 - (b) in line with the main face of the associated dwelling if the dwelling incorporates protrusions such as verandahs, projecting windows, porches, balconies etc which provide articulation in the building as it presents to the street
 - (c) in any other case, at least 5.5 metres from the primary frontage
 - (d) nil where adjoining a service or access laneway.
- 16 Where allotments have direct frontage to a public reserve:
 - (a) rear vehicular access should be available to the allotments
 - (b) dwellings should be at least 2 storeys in height
 - (c) buildings should be orientated to provide passive surveillance from habitable rooms towards the reserve.
- 17 For each dwelling, the maximum width (including the width of any support structure) of any garage or carport opening that faces a street, should be no greater than six metres or 50 per cent of the frontage width, whichever is the lesser. Garage or carport openings wider than these standards are inappropriate, except where any of the following apply:
 - (a) a site has rear vehicular access and from which vehicular access is obtained, in which case no maximum width applies
 - (b) a site has a frontage of less than 12 metres and the dwelling:
 - (i) is 2 or more storeys in height
 - (ii) incorporates protrusions such as verandahs, projecting windows, porches, balconies etc which provide articulation in the building as it presents to the street,in which case garages or carports should have a maximum width of 6 metres or 80 per cent of the frontage width, whichever is the lesser.
- 18 Site layout and building design should protect sunlight access to public spaces.
- 19 All plant, air conditioning and lift well structures should be screened from public view.

Land Division

- 20 Land division should support the provision of a broad range of housing options and the accommodation of buildings that address street frontages, laneways and public reserves to enhance opportunities for passive surveillance of public spaces.
- 21 Land division should accommodate open space and movement networks that provide for the following:
 - (a) safe and convenient access to public facilities, public transport, adjacent future development sites and the surrounding residential areas
 - (b) areas for the management of stormwater.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table ChSt/1 - Conditions for Complying Development](#):

- domestic outbuilding.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	Except where any of the following apply: (a) an advertisement area is 2 square metres or less (b) located within Woodville West Policy Area 23 .
Amusement machine centre	
Caravan park	
Consulting room	Except where located within Woodville West Policy Area 23 .
Crematorium	
Dwelling which is setback less than 4 metres from a boundary with a coastal reserve or zone and located on an allotment marked as ' Minimum Coastal Set-back – 4m ' as shown on Concept Plan Map ChSt/15 - Building Restriction Areas .	
Dwelling which is setback less than 8 metres from a boundary with a coastal reserve or zone on allotments marked as ' Minimum Coastal Set-back - 8m ' as shown on Concept Plan Map ChSt/15 - Building Restriction Areas , and Concept Plan Map ChSt/16 - Building Restriction Areas and Concept Plan Map ChSt/17 - Building Restriction Areas .	
Dwelling which is setback less than 15 metres from a boundary with a coastal reserve or zone and located on an allotment marked as ' Minimum Coastal Set-back – 15m ' as shown on Concept Plan Map ChSt/17 - Building Restriction Areas .	
Dwelling which is setback more than 45 metres distant seaward from the Seaview Road frontage on allotments marked as ' Maximum Seaview Road Set-back – 45m ' as shown on Concept Plan Map ChSt/17 - Building Restriction Areas .	

Form of Development	Exceptions
Dwelling which is setback more than 45 metres distant seaward from the northerly prolongation of Seaview Road frontage on allotments affected land shown on Concept Plan Map ChSt/17 - Building Restriction Areas .	
Dwelling on land located within the area marked 'Dwelling Restriction Area' as shown on Concept Plan Map ChSt/14 - Building Restriction Area .	
Fuel depot	
Horse keeping	
Horticulture	Except where it is associated with the City Farm located at Green Street, Brompton existing at 4 September 2003.
Hospital	
Hotel	
Industry	
Intensive animal keeping	
Motor repair station	
Office	Except where located within Cheltenham Park Policy Area 22 or Woodville West Policy Area 23 .
Petrol filling station	
Plant nursery	Except where it is associated with the City Farm located at Green Street, Brompton existing at 4 September 2003.
Prescribed mining operation	
Public service depot	
Restaurant	Except where located within the Cheltenham Park Policy Area 22 or Woodville West Policy Area 23 .
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where any of the following apply: <ul style="list-style-type: none"> (a) the gross leasable area is less than 100 square metres (b) where located within Cheltenham Park Policy Area 22 (c) the gross leasable area is 500 square metres or less where located within Woodville West Policy Area 23.
Stock sales yard	
Stock slaughter works	
Store	

Form of Development	Exceptions
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
	<p>Child care facility</p> <p>Community centre</p> <p>Dwelling and office in Woodville West Policy Area 23 where the floor area used for office does not exceed 50 square metres</p> <p>Dwelling and consulting room in Woodville West Policy Area 23 where the floor area used for consulting room does not exceed 50 square metres</p> <p>Dwelling that is not Category 1 or non-complying</p> <p>Meeting hall (excluding a place of worship)</p> <p>Nursing home</p> <p>Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6</p> <p>Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area</p> <p>Recreation area</p> <p>Residential flat building in Woodville West Policy Area 23 that is not Category 1 or non-complying</p> <p>Supported accommodation within Cheltenham Park Policy Area 22.</p>

Residential Character Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 Preservation of the existing development patterns and built form.
- 2 Infill development that is designed to reflect the traditional character elements of the area, particularly as presented to the streetscape.
- 3 Increased housing density in restricted situations.
- 4 Replacement of existing industrial and commercial uses with housing.
- 5 Development of home businesses in contributory places along South Road and Port Road.
- 6 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone embraces areas of historic residential character which have a high level of amenity that produces safe, convenient and distinctive living environments for all residents, along with local community facilities that complement the living environment. It recognises 15 individual precincts, each having a distinctive historical character formed by the interaction between buildings, spaces, topography, vegetation and landscaping, general street pattern and layout of the area. It is the collective integrity of the built form that shapes the nature of each precinct.

Many areas have streets lined with mature street trees and have attractive landscaped public spaces both of which complement residential development, and collectively form a strong part of the character of the area. New development needs to respond to the special attributes of the respective precinct, including views, vistas, existing vegetation and landmarks.

An increase in the density of housing may take place on corner sites or where dwellings replace a non-complying use or an existing building not listed as a contributory item. Existing industrial or commercial uses in the zone ought to be replaced with residential uses as development opportunities arise.

A home business, being a dwelling where no more than 50 per cent of the total floor area of the dwelling is used for a business purpose, will be encouraged in contributory places along South Road and Port Road.

It will be important to retain the amenity and character of the area by respecting the existing development pattern of the precincts.

Precinct 66 Bowden / Brompton

The Bowden Brompton precinct accommodates some of the City of Charles Sturt's oldest houses and buildings, dating to the earliest years of European Settlement in the region. The subdivision pattern, dating from 1839 in Bowden and 1849 in Brompton, is one of small, narrow allotments designed for typically early inner-city residential living. The existing significant buildings are primarily small, single-storey, detached and attached, single and double fronted workers' cottages dating from the 1850's to 1900 period. They are characterised by red brick and bluestone walls, gable and hipped corrugated iron roofs and small setbacks from the street. The predominant urban character remains that of the Victorian era, with an intimate and small-scale character which has considerable charm and interest.

Ovingham is unique in that it incorporates strongly sloping sites and retains many dominating stone and iron front walls. Some 1930s to 40s Bungalows and Austerity houses, together with more recent SA Housing Trust 1970s to 1980s townhouses are also located in Ovingham.

This precinct also includes a significant proportion of more recent commercial and industrial premises which detract from the older Victorian character of the area.

Precinct 67 Brompton Park

The Brompton Park subdivision precinct accommodates some of the City of Charles Sturt's oldest houses and buildings, dating to the earliest years of European Settlement in the region. The Brompton Park subdivision pattern, dating from 1876, is one of very small, narrow allotments designed for typically early inner-city residential living. The existing significant buildings are primarily small, single-storey, detached and attached, single and double fronted workers' cottages dating from the 1870's to 1890's period. They are characterised by red brick and bluestone walls, gable and hipped corrugated iron roofs and small setbacks from the street. The predominant urban character remains that of the Victorian era, with an intimate and small-scale character which has considerable charm and interest.

This precinct also includes more recent commercial and industrial premises which detract from the older Victorian character of the area.

Precinct 68 Early SA Housing Trust

Blight Street, Ridleyton

This part of the precinct accommodates one of Adelaide's earliest SA Housing Trust developments, dating from 1939. Designed in the 'early modern' style, the buildings comprise attached and detached single-storey houses. These houses are characterised by red brick walls, incorporating protruding courses to give a striped effect and painted sections to contrast with the red brick. Roofs are typically hipped, while a curved flat concrete entrance porch canopy gives a streamlined accent to each house. The housing stock is very intact, and the streetscape and urban character is quite uniform.

Bell Street, Pennington

This part of the precinct accommodates some very early SAHT housing dating from early 1938. The original layout and subdivision reflects 'Garden Suburb' urban design concepts popular at the time. This layout still exists and provides strong urban focal points in Sinclair Square and Tulloch Avenue.

The housing consists of semi-detached single-storey dwellings constructed of red brick with hipped and gable corrugated iron roofs.

The housing in Bell Street has terracotta tile roofing, indicating a refinement of the original design. Some brickwork has protruding courses to give a horizontal-banded effect. The styling is austere, with late-modern overtones, as seen in the occasional horizontal banding and in the concrete entrance porch canopies. Overall, the area has a very uniform and austere character with little planting or street trees.

Precinct 69 Croydon / West Croydon

North-East of the Railway Line

Excepting the area focussed around Rosetta Street, this part of the precinct accommodates housing almost exclusively from the late 1890s to late 1910s, incorporating Edwardian-to-early-Federation and early bungalow housing styles. Typically single-storey, detached dwellings, these houses are characterised by hipped, gable and Dutch-gable roofs, combined with bluestone, sandstone and red brick walls, and corrugated iron roofs with verandas. Windows are generally dormer, double-sash windows. A number of quite substantial houses are located in the area, particularly fronting on to the railway line.

Housing is uniformly set back from the road and is typically set in mature gardens. The streets are lined with mature trees, which contribute significantly to the established character of the area. The area also includes several churches. Elizabeth Street accommodates a number of small scale shops which act as a focus for the neighbourhood and contribute towards the street's 'sense of place'.

The housing along Day Terrace consists mainly of post-Edwardian villas. These have simple rectangular plan forms and similar characteristics to other houses in the area. The north-eastern side of the railway line has a more intimate streetscape character than the south-western side, with the occasional shop located on the front boundary on Rosetta Street.

More recent housing from the 1910 to 1940 period comprises the area west of Brown Street and north of Herbert Road, this area being focused on to Rosetta Street. Detached bungalow houses predominate, including some fine Queen Anne and Tudor style examples. These houses are characterised by red brick, sandstone and rendered walls, corrugated iron and terracotta tiled roofs, with hipped, gable and Dutch-gable roof forms and verandas. Large front gables are a common architectural feature north of the railway line. Art Deco influences are evident in other houses.

South-West of the Railway Line

Euston Terrace provides a major focus, with an impressive stand of mature Plane trees. This particular street is characterised by well maintained villas and symmetrical cottages of mixed scale with some later infill and redevelopment, much of which is intrusive. Building materials consist mostly of sandstone, red brick and bluestone. Roofs have high gables or are hipped, and predominantly use galvanised iron. Some of the dwellings on this street also have the original masonry and cast iron fences intact.

Building styles on King, Queen and Princess Streets are less uniform than along Euston Terrace. Building scale is smaller, but allotment sizes and setbacks are much more variable. Dwelling styles range from 1890 and 1910 period, including some workers cottages from earlier periods.

Precinct 70 Allenby Gardens

This residential precinct accommodates good quality detached housing from the 1920 to 1930s period, encompassing late Edwardian, Federation and Bungalow styles. The area also includes a small number of Queen Anne and Tudor Houses. These houses are characterised by red brick and sandstone walls, hipped, gable and Dutch gable roofs, terracotta tiles and corrugated iron roofing with decorative gable panelling and timber trim. The area is notable for its strong uniformity of layout and buildings, with minimal new housing development in recent years. The precinct has numerous tree-lined streets and an 'established' atmosphere. Coombe Road is the natural focus for the area, with an attractive school complex of the same vintage at its southern end and planted with Cedar street trees, which assist creating a consistent street tree character.

Precinct 71 Pre World War One

Pennington West

This part of the precinct accommodates a mixture of 1900s to 1920s housing, comprising typically detached single storey Edwardian/Federation/early Bungalow houses, together with some 1940s–1950s houses on the eastern side. The housing is characterised by red brick, sandstone and render walling, terracotta tile and corrugated iron roofing and verandas, and hipped, gable and Dutch-gable roof forms. Some of the villas in this area are particularly fine examples of Edwardian houses, with pressed-metal wall cladding on the fronts, corrugated iron or small-fluted iron cladding on the sides and corrugated iron roofs.

The housing is of good quality and this, together with the uniform allotment size and street layout, gives the area an interesting character. Pennington Terrace is the dominant street in the area with the school and church providing key civic focal points.

West Hindmarsh

This large residential area has a strong, 'turn-of-the-century' character accommodating good quality housing dating from the late 1890s to the late 1910s, set in broad tree-lined streets. The precinct is very intact and consistent in character. The existing housing consists of substantial, single-storey, single fronted symmetrical cottages styled in the manner the early Edwardian/Federation/early Bungalow periods.

These houses are characterised by hipped, gable and Dutch-gable roofs, bluestone, sandstone and red brick walls and corrugated iron roofs with verandas. Windows are generally timber double-sash windows. The housing is very uniform, with little new residential development to date. Housing is set back uniformly 3 to 6 metres from the road, often in mature gardens. The mature trees which line the streets contribute significantly to the area's established older character. The precinct as a whole is focussed on Bond Street.

The area along South Road and Grange Road accommodates some fine late Victorian and Edwardian housing along its South Road and Grange Road frontages, which are particularly significant given their use of Hallet Bricks, made at the nearby Hallet Brickworks.

Precinct 72 Welland Character

This small residential precinct accommodates detached houses from the 1910 to 1920 period, comprising mainly late-Edwardian and Federation housing styles. The area includes a small number of early Bungalows and 1940s Austerity houses. These houses are characterised by red brick and sandstone walls, hipped, gable and Dutch-gable roofs, terracotta tiles and corrugated iron roofing with decorative timber trim. This is consistent through the precinct, although there are a few more recent housing developments. The streets are lined with trees in an irregular manner.

Precinct 73 Woodville South Character

This residential precinct has a variety of medium-sized detached housing on substantial allotments in tree lined streets. Bungalow and Tudor are the most commonly found housing types in the area, with a number of Spanish Mission, Dutch Gable and Art Deco houses dispersed throughout. Freestone, red brick, stucco and baked brick are the most common building materials. Many houses are also characterised by exposed, ornate timber work. Roofs are characterised by the steep pitched gables associated with Tudor housing and Dutch Gables, as well as terracotta tiles and some galvanised iron. Very little new housing has been built in this area in the last 50 years.

The precinct is also notable for its traditional, well maintained gardens, abundant with mature vegetation. Fencing is generally low brick, stucco or wire and is mostly integral with the design of the house. Front setbacks are mostly at about 6 metres. The precinct is notable for its strong uniformity of layout and buildings, particularly in the 'Glen' Streets and Angus Street. The precinct's main focus is the attractively landscaped Woodville Oval complex. Buildings that front Woodville Oval on Oval Avenue and Cedar Avenue are of larger scale than others in the area.

Precinct 74 Kilkenny

This precinct accommodates a wide range of single-storey detached houses dating from the 1850s through to the 1950s. However, the predominant era is 1920 to 1930 in the north of the area and 1910 to 1920 in the south. Edwardian and post-Edwardian cottages and houses are found in the south, while Federation and Bungalow houses, together with a number of 1940s and 1950s houses are found in the north. Housing is characterised by freestone front walls, red brick side walls and corrugated iron and terracotta tile roofs. There is a small proportion of more recent residential development in the precinct.

A small group of Victorian commercial premises is located at the southern end of Wilpena Terrace which, together with the older housing at this end, give it a strong Victorian character reflecting the original village of Kilkenny. Of note are a number of large, high-quality 1930s houses on large allotments at the northern end of the area. The precinct as a whole is focused on Wilpena Terrace.

Precinct 75 Woodville Park

This precinct is of slightly later date than the Woodville precinct to the west, established before World War I through to World War II. The housing is of smaller scale with late symmetrical villas, Federation, Queen Anne and some Bungalow styles. Consistency of character is high, with limited recent home unit and replacement cottage infill. Frontages are narrower and fences and front gardens are a strong element in the streetscape. Building materials are generally red brick, freestone, timber windows, galvanised iron and terracotta tile roofs.

Precinct 76 Woodville

This precinct accommodates housing dating from the late 1880s to 1930s and includes a significant proportion of large, often opulent houses. The precinct has many wide, tree-lined streets, large allotments and large single-storey detached houses, particularly at the western end. The dwellings range from substantial bluestone Victorian villas, through to large Federation and Queen Anne houses. The houses are typically set in large established gardens, which contribute significantly to the attractive character of the precinct. The housing south of the railway line, while still of high quality, is of a slightly smaller scale. There is a small proportion of more recent residential development in the precinct. Belmore Terrace is a key focus in the precinct.

Precinct 77 Cheltenham East

The housing in this precinct is similar in style to that in the Cheltenham West precinct, but is of a slightly smaller scale. This reflects the smaller allotment sizes which creates a more intimate character in this precinct. The subdivision here is also slightly earlier than in the Cheltenham West precinct, with part being divided in 1880.

Precinct 78 Cheltenham West

This large residential precinct accommodates a range of housing, which is particularly historic north of the railway line where there are areas of sandstone and brick villas. West of Stroud Street are 1910 to 1930 era detached houses, styled in the Post-Edwardian/Federation/Bungalow manner with an occasional Tudor or 1940s dwelling. Housing close to Torrens Road and south of the railway line tends to be more 'mixed' in character, including a number of non-contributory buildings.

The precinct contains a significant number of dwellings with pressed metal, 'stone' front walls and corrugated iron clad side walls. However, houses are generally characterised by red brick and sandstone walls, corrugated iron and terracotta tiled roofs, hipped, gable, Dutch-gable roof forms and verandas. Overall, the precinct has a fairly uniform character. The main streets in the precinct are Stroud Street (north and south), with its residual shops and community facilities centred around the station, Buller Terrace and Railway Terrace. Street trees are generally limited, with little advantage taken of the wide streets for avenue plantings.

Precinct 79 Henley Beach

The area around Marlborough Street and Crewe Street, south to Clarence Street/Durham Street generally features smaller villas and Bungalows in the narrower streets. This is also the case south of the Bowling Club to Atkin Street, where small setbacks of 2 to 4 metres (with the exception of Main Street), add to the intimate nature of the area. Larger, grand villas, cottages and bungalows characterise Marlborough Street and East Terrace.

South of Atkin Street, bungalows and Tudor homes of more recent vintage feature and allotments tend to be larger, incorporating setbacks of 5 to 8 metres. The presence of mature Norfolk Island pines along most of East Terrace contributes to the streetscape. This also serves to link the East Terrace to other tree lined streets to the west. It is important that vehicular access to land be located and designed to preserve existing street trees.

Precinct 80 Grange

Grange Road

This part of the precinct along Grange Road from Military Road in the west to Surrey Street in the east is comprised of mainly Bungalow and Tudor homes of moderate size, dating from 1920 to 1940. Allotments in this area range from about 600 square metres to 800 square metres. Setbacks of 4 to 6 metres makes development more intimate and dense than surrounding areas.

Beach Street

This part of the precinct extends from Grange Uniting Church and Charles Sturt Avenue, westward to the foreshore at the Esplanade. The area is characterised by single storey Victorian houses and Federation Bungalows. The Grange Institute, Grange Uniting Church and an early corner store form what would have been a focal point for social and community activity in Grange. A group of houses opposite the Uniting Church display consistent styling and use of materials, with gabled roofs clad with corrugated sheeting, use of bricks and similar veranda detailing with masonry piers and balustrades. The two largest houses in the area are located on the Esplanade and represent the development of Grange as a seaside resort.

Setbacks on the south side of Beach Street are consistent, while set-backs on the northern side are more variable. Beach Street rises gradually toward the west and the houses set above the street. Small retaining walls border the footpath, forming the fence line. Elsewhere, fencing is kept simple and low, and is constructed either of masonry or iron sheeting. Norfolk Island Pines comprise part of the streetscape in the area. It is important that vehicular access to land be located and designed to preserve existing street trees.

Jetty Street

This part of the precinct comprises Bungalows, cottages, Tudor and villas, dating from the late 1800s in the west, to the 1920s in the east. Jetty Street itself features Norfolk Island Pines which act as a connecting feature to Grange Esplanade in the west. As with Beach Street, it is important that vehicular access to land be located and designed to preserve existing street trees. Allotments range in size from 600 to 800 square metres. A significant concentration of Tudor homes on Jetty Street (from Charles Sturt Avenue to High Street), forms the most consistent character of the area.

Grange Esplanade

This part of the precinct is bounded by the Esplanade, Jetty Street and the houses on the eastern side of Seaview Road up to and including The Marines. The Marines is an imposing group of eight, three-storeyed attached terrace houses which are an example of some of the few remaining three-storey terraces in South Australia. The dominant building type in the area is multi-storey, characterised by the three storey Marines terrace houses and the two-storey Grange Hotel, both of which are built to the footpath. Detached dwellings of consistent scale and style occupy the east side of Seaview Road. The use of stone and brick is common to the majority of the buildings in the area with corrugated iron and terra cotta tiles as roofing material.

The Esplanade and Seaview Road run parallel through the area giving it a strong linear character. The line of Seaview Road is emphasised by the Norfolk Island Pines, which also appear on other streets in the area. As with the other areas, it is important that vehicular access to land be located and designed to preserve existing street trees. Building set-backs from the Esplanade and Seaview Road are reasonably consistent, being approximately three to four metres. Given that many houses are also set up from the road, Seaview Road is dominated by rendered retaining walls of varying heights between one to two metres, topped by a variety of fencing styles or remaining unfenced. Garages are of a mixed age and construction.

The Grange Esplanade precinct should primarily accommodate residential development, the Grange Hotel and a small group of shops. Development on the Esplanade should have a maximum height of three storeys, be built to the street frontage to reflect The Marines and comprise attached dwellings to create a strong continuous built form element along the frontage. Development to the rear of these properties, on the western frontage to Seaview Road should have a maximum height of two storeys where compatible with adjacent development with regard to scale and siting. To the east of Seaview Road, given the retaining wall in this location, low or no fencing is appropriate. A maximum height of two stories along Seaview Road is appropriate where compatible with adjacent development with regard to scale, siting and character.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - detached dwelling
 - domestic outbuilding in association with a dwelling
 - domestic structure
 - dwelling addition
 - group dwelling/residential flat building/row dwelling (in limited circumstances)
 - semi-detached dwelling
 - small-scale non-residential uses that serve the local community, for example:
 - child care facilities
 - health and welfare services
 - open space
 - primary and secondary schools
 - recreation areas
 - supported accommodation.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.

- 3 Vacant or underutilised land should be developed in an efficient and co-ordinated manner to increase housing choice by providing dwellings at densities higher than, but compatible in form, scale and design with adjoining residential development.
- 4 Non-residential development should be of a nature and scale that:
 - (a) serves the needs of the local community
 - (b) is consistent with the character of the locality
 - (c) does not detrimentally impact on the amenity of nearby residents.
- 5 The use and placement of outbuildings should be ancillary to and in association with a dwelling or dwellings.
- 6 Development involving a home business should satisfy all of the following:
 - (a) occur only on the site of a contributory place with a frontage to Port Road or South Road
 - (b) have at least 50 per cent of the floor area of the dwelling in residential occupation and use
 - (c) maintain a residential appearance and function, including landscaping, to the street frontage
 - (d) provide adequate car parking for all activities on the site, to be located at the rear of the site and designed to enable all vehicles to enter and exit the site in a forward direction.

Form and Character

- 7 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 8 Where a new dwelling is constructed alongside or within a group of older style residential buildings, the new dwelling should be of a similar height, scale and proportions and be constructed of materials that complement and reinforce the character and design elements of existing buildings.
- 9 New buildings and structures, including garages and carports, should be designed to maintain the prominence of existing historic buildings and should not dominate the appearance of the front elevation of the building.
- 10 A new dwelling should not be developed to the rear of another dwelling where its access relies on a driveway adjacent to/alongside the existing dwelling unless it replaces a non-complying use or an existing building not listed as a contributory item or is located in Pennington in Precinct 68 Early SA Housing Trust and satisfies all of the following:
 - (a) the site has a minimum width of 18 metres
 - (b) the dwelling immediately adjoining the street is designed to address the street in a manner sympathetic to the existing character of the precinct
 - (c) a suitable driveway, landscaping and any communal area at least five metres in width is provided that gives access to the rear of the site
 - (d) a setback from boundaries at least 2.5 metres is provided for any dwelling that does not adjoin a public road.
- 11 Vehicle parking should not be in the form of undercroft garaging or like structures.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 66 Bowden / Brompton

- 12 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 13 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	250 (minimum)	10
Semi-detached	250 (minimum)	7
Group dwelling Residential flat building (where not classed as non-complying)	275 (average)	18
Row dwelling	190 (minimum)	6

- 14 A lesser frontage may be considered for a detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 67 Brompton Park

- 15 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 16 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	125 (minimum)	7
Semi-detached	125 (minimum)	6
Group dwelling Residential flat building (where not classed as non-complying)	275 (average)	18
Row dwelling	125 (minimum)	6

Precinct 68 Early SA Housing Trust

- 17 Development should not be undertaken unless it is consistent with the desired character for the precinct.

- 18 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	450 (minimum) 350 (minimum) for corner sites	15
Semi-detached	270 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	250 (average)	18
Row dwelling (where not classed as non-complying)	300 (minimum)	7

- 19 A group dwelling or a residential flat building may be developed where all of the following can be achieved:
- (a) it is located in Pennington
 - (b) the existing housing stock is maintained
 - (c) the rear yards are combined
 - (d) shared vehicular access to the combined rear yards is provided with minimal loss of existing housing stock.

Precinct 69 Croydon / West Croydon

- 20 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 21 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	350 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 22 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the Residential Character Zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 70 Allenby Gardens

- 23 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 24 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	400 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 25 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 71 Pre World War One

- 26 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 27 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	450 minimum 350 for corner sites minimum	15
Semi-detached	300 minimum	10
Group dwelling Residential flat building (where not classed as non-complying)	300 average	18
Row dwelling (where not classed as non-complying)	350 minimum	7

- 28 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 72 Welland Character

- 29 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 30 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	400 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 31 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 73 Woodville South Character

- 32 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 33 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	350 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 34 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 74 Kilkenny

- 35 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 36 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	450 (minimum) 350 (minimum) for corner sites	15
Semi-detached	350 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 37 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- development listed as non-complying in the zone
 - a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 75 Woodville Park

- 38 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 39 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	400 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 40 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- development listed as non-complying in the zone
 - a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 76 Woodville

- 41 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 42 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	16
Semi-detached	400 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 43 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 77 Cheltenham East

- 44 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 45 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	400 (minimum) 350 (minimum) for corner sites	13
Semi-detached	250 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	300 (average)	18
Row dwelling (where not classed as non-complying)	350 (minimum)	7

- 46 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 78 Cheltenham West

- 47 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 48 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	500 (minimum) 350 (minimum) for corner sites	15
Semi-detached	400 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	400 (minimum)	8

- 49 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the Residential Character Zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 79 Henley Beach

- 50 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 51 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling type	Site area (square metres)	Minimum frontage (metres)
Detached	450 (minimum) 350 (minimum) for corner sites	15
Semi-detached	350 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	350 (minimum)	7

- 52 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- (a) development listed as non-complying in the zone
 - (b) a building not included in [Table ChSt/5 – Contributory Items](#).

Precinct 80 Grange

- 53 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 54 A dwelling should have a minimum site area (and in the case of group dwellings and residential flat buildings, an average site area per dwelling) and a frontage to a public road not less than that shown for the respective precinct in the following table:

Dwelling Type	Site Area (square metres)	Minimum frontage (metres)
Detached	450 (minimum) 350 (minimum) for corner sites	14
Semi-detached	350 (minimum)	10
Group dwelling Residential flat building (where not classed as non-complying)	350 (average)	18
Row dwelling (where not classed as non-complying)	350 (minimum)	7

- 55 A lesser site area and frontage may be considered for a semi-detached dwelling where it replaces:
- development listed as non-complying in the zone
 - a building not included in [Table ChSt/5 – Contributory Items](#).

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Advertisement and/or advertising hoarding	Except where an advertisement area is 2 square metres or less
Amusement machine centre	
Caravan park	
Consulting room	Except where it: <ol style="list-style-type: none"> replaces an existing non-residential use is located within Precinct 80 Grange north of Jetty Street and west of Military Road.
Crematorium	
Fuel depot	

Form of Development	Exceptions
Group dwelling	Except where an allotment has a frontage of not less than 18 metres and where any of the following apply: <ul style="list-style-type: none"> (a) it replaces a non-complying development (b) it replaces an existing building not listed as a contributory item (c) it is located in Pennington within Precinct 68 Early SA Housing Trust.
Horse keeping	
Horticulture	
Hospital	
Hotel	
Industry	Except a light or service industry where it is: <ul style="list-style-type: none"> (a) associated with a home business (b) associated with an existing industrial or commercial use and located within Precinct 66 Bowden/Brompton south of Hawker Street and west of Chief Street (c) associated with an existing industrial or commercial use and located within Precinct 67 Brompton Park (d) located within Precinct 80 Grange north of Jetty Street and west of Military Road.
Intensive animal keeping	
Motor repair station	
Office	Except where it is: <ul style="list-style-type: none"> (a) associated with a home business (b) located within Precinct 80 Grange north of Jetty Street and west of Military Road.
Petrol filling station	
Plant Nursery	Except where it is associated with the City Farm located at Green Street, Brompton existing at 4 September 2003
Prescribed mining operations	
Public service depot	
Residential flat building	Except where an allotment has a frontage of not less than 18 metres and where any of the following apply: <ul style="list-style-type: none"> (a) it replaces a non-complying development (b) it replaces an existing building not listed as a contributory item (c) it is located in Pennington within Precinct 68 Early SA Housing Trust.
Restaurant	
Road transport terminal	

Form of Development	Exceptions
Row dwelling	Except where any of the following apply: (a) it replaces a non-complying development (b) it replaces an existing building not listed as a contributory item (c) it is located within Precinct 66 Bowden / Brompton or Precinct 67 Brompton Park.
Service trade premises	
Shop or group of shops	Except where: (a) the gross leasable area is less than 100 square metres (b) it is located within Precinct 80 Grange north of Jetty Street and west of Military Road.
Stock sales yard	
Stock slaughter works	
Store	Except where it is associated with a home business
Warehouse	Except where it is associated with a home business
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 or Local Heritage Place that is described in Table ChSt/6 or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 .	Child care facility Community centre Dwelling that is not Category 1 or non-complying Meeting hall (excluding a place of worship) Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 Partial or total demolition of a Contributory Item that is described in Table ChSt/5 within the Historic Conservation Area Recreation area Supported accommodation

Special Uses Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating special public and private activities of an institutional or open character.

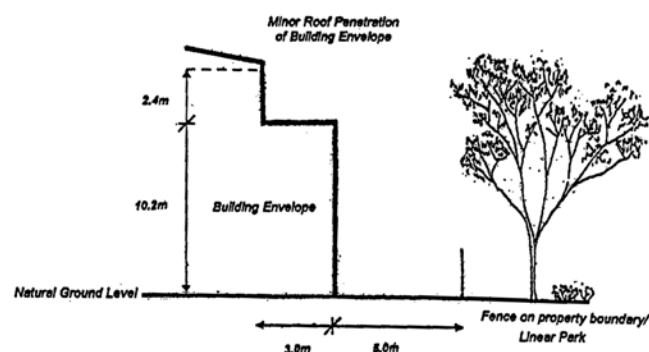
PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - special public or private activities of an institutional or open character
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.

Form and Character

- 3 Development should be carried out in accordance with the concepts shown on [Concept Plan Map ChSt/19 - Former Underdale Campus Site](#).
- 4 Development within the former Underdale Campus should generally not exceed 8.2 metres in height, however buildings not exceeding 12.6 metres in height will be considered where the following requirements are met:
 - (a) being adjacent to, but no closer than 5 metres from the boundary of the **Linear Park (River Torrens / Karrawirra Pari) Policy Area 8** in the **Metropolitan Open Space System Zone**
 - (b) any such building being at least 21 metres from any other building with a building height in excess of 8.2 metres
 - (c) no more than three buildings exceeding 8.2 metres in height
 - (d) where the building does not exceed 10.2 metres in height, being at least 30 metres away from the boundary with the Residential Zone to the west
 - (e) where the building exceeds 10.2 metres in height, being at least 80 metres away from the boundary with the Residential Zone to the west
 - (f) where the uppermost parts of the building exceed 10.2 metres in height, they should be designed to reduce their scale and mass, incorporating appropriate changes in materials and colours, and set back at least 3 metres from the lower parts of the development, as set out in the figure below:



- 5 Car parking within multi-level buildings should be provided at undercroft or basement level.
- 6 Development within the former Underdale Campus shown on [Concept Plan Map ChSt/19 - Former Underdale Campus Site](#) should achieve sound urban design principles and create an excellent design outcome including:
 - (a) the definition and improvement of the River Torrens Linear Park
 - (b) the retention of existing avenues of trees and other existing trees where possible
 - (c) the creation of tree-lined, north-south avenues, utilizing existing trees
 - (d) no vehicular access to Heath Street, Weemala Street or to Hartley Road opposite the Flinders Park Primary School
 - (e) the provision of convenient pedestrian connections from within the former campus site to adjacent areas, public transport and other facilities
 - (f) the improvement of views into the River Torrens Linear Park, including opening up the view line into the Linear Park when travelling south along Holbrooks Road
 - (g) ensuring a high standard of design, especially as viewed from public spaces such as the Linear Park, Holbrooks Road and Hartley Road
 - (h) protection of residential areas from noise associated with Holbrooks Road
 - (i) allowance for road widening of Holbrooks Road
 - (j) boundary structures or fencing adjoining the **Linear Park (River Torrens / Karrawirra Pari) Policy Area 8** in the **Metropolitan Open Space System Zone** not being solid but of an open nature
 - (k) best practice stormwater management, avoiding erosion and stormwater impacts on the River Torrens.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to all of the following criteria:

- (a) the conditions contained in [Table Chst/1 - Conditions for Complying Development](#)
- (b) no development being undertaken within 60 metres of the centreline of the River Torrens or within the River Torrens:
 - cemetery
 - golf course
 - recreation area.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
Amusement machine centre	
Community centre	
Crematorium	
Dwelling	
Electricity generating station	
Hall	
Hospital	
Industry	
Motor repair station	
Office	
Petrol filling station	
Prescribed mining operations	
Road transport terminal	
Service trade premises	
Shop	
Showground	
Store	
Supported accommodation	Except for a supported accommodation within the area affected by Concept Plan Map ChSt/19 - Former Underdale Campus Site
Warehouse	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Cemetery	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .
Golf course	
Recreation area	
The making of any excavation or filling for, or incidental to:	Supported accommodation within the area affected by Concept Plan Map ChSt/19 - Former Underdale Campus Site , providing the height of the development does not exceed 10.2 metres to the eave, and 12.6 metres to the ridgeline (with the roof not containing any habitable space).
(a) the construction, demolition or removal of a building	
(b) provision of underground service infrastructure	
(c) remediation works.	

Stadium Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone accommodating sporting, entertainment, cultural, exhibition and recreational activities and associated spectator and administrative facilities.
- 2 Buildings and facilities designed to high architectural standards and established within a landscaped setting, including tree plantings around buildings and car parking areas.
- 3 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This zone accommodates a stadium facility of metropolitan-wide significance, namely the National Basketball arena and outdoor community recreation grounds at Beverley. This facility provides a range of sporting, recreational and entertainment opportunities, attracting players and spectators from across the metropolitan area.

Precinct 81 Stadium Beverley

The precinct accommodates the national basketball arena together with outdoor community recreation grounds. The stadium is used for a range of sporting, recreational and entertainment events, attracting players and spectators from across the metropolitan area. The precinct provides for a range of indoor and outdoor sporting events, as well as non-sport related events which are restricted in terms of frequency and crowd capacity. The suitability of an event is related to the nature of the event and the intensity of the event.

Outdoor community events such as a local fair or Christmas carol celebration would be limited to the recreation oval. The scale of any event within the precinct would be controlled by the on-site parking capacity.

Management agreements will be in place to ensure the orderly, safe and efficient flow of vehicles and parking. The proposed upgrading of William Street will further improve vehicle distribution to and from the precinct and existing, adjacent industrial areas.

To assist with the interpretation of the objectives and principles of development within the precinct, the following definitions are provided:

- (a) 'Concert' is defined as a 'recital or performance using musical instruments with or without a vocalist'
- (b) 'Event' - means the single occurrence of a performance, including the entering, observing of the performance, and leaving of the stadium, grounds or car park of paying patrons associated with that single performance
- (c) 'Sport Related event' - means an event where the principal purpose is athletic or similar display which may include associated, subordinate sport related activities, but does not include a display which might be organised by a sports organisation where the display is not itself an athletic or similar display/activity.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the zone:
 - car parking
 - clubrooms associated with sports facilities
 - entertainment, cultural and exhibition facilities
 - indoor and outdoor recreation facilities
 - lighting for night use of facilities
 - office associated with sports administration or community services
 - playground
 - shops or groups of shops ancillary to and in association with recreation development
 - showground
 - sports grounds and associated facilities
 - special event
 - spectator and administrative facilities ancillary to recreation development
 - swimming pool.
- 2 Development listed as non-complying is generally inappropriate and not acceptable unless it can be demonstrated that it does not undermine the objectives and principles of the Development Plan.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the zone.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to the following precincts.

Precinct 81 Stadium Beverley

- 4 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 5 Development undertaken in the precinct should comprise a range of sporting, recreational, entertainment, cultural and exhibition events, excluding concerts, which reinforce the purpose of the zone as a district, regional and metropolitan facility/open space and where the scale and intensity of events are tied to the capacity to accommodate, on site, all likely car parking demands generated by those events.
- 6 Sporting, recreational, entertainment, fairs and exhibition events to be in accordance with the conditions detailing management arrangements for all forms of events with respect to noise, parking, traffic and related impacts.
- 7 Development should minimise conflict and should ensure safe and comfortable environments for development in adjoining residential, light industrial and district commercial zones. The nature, intensity, scale, number and frequency of events, and amplification proposed for events should be consistent with the intent of the precinct.
- 8 Development involving indoor non-sport related events should be limited in terms of the number of events, and frequency of events, and having regard to the maximum attendance envisaged for events.
- 9 The recreation oval and adjoining grassed car parking areas should be accessible for public use and enjoyment at all times except when the areas are needed for car parking or other activities associated with events within the precinct.
- 10 No development or activity should involve the keeping of animals within the precinct, whether temporary or permanent.

- 11 Building development (except for temporary structures associated with outdoor events), landscaping, parking areas, access arrangements and outdoor recreation should only be undertaken in accord with [Concept Plan Map ChSt/20 – Precinct 79 Stadium Beverley](#).
- 12 Vehicular access to the car park should be primarily from the main entry off Crittenden Road, with alternate access available from the southern William Street gate and Toogood Avenue.
- 13 Vehicular egress should be primarily via the main entry to Crittenden Road, with secondary egress via Toogood Avenue (right turn only) and the southern William Street gate (right turn only).
- 14 Sufficient provision should be made within the precinct for the parking of vehicles expected to be generated by any event or activity.
- 15 Development should not require the collection of parking fees when patrons enter the grounds to prevent queuing.
- 16 No development to be undertaken where the noise emanating from a sporting recreational, entertainment, cultural or exhibition event is likely to impair the amenity of surrounding residents.
- 17 Development should ensure that amplified music is not played in the stadium when patrons are leaving the building at the end of an event.
- 18 No sound amplification should be conducted outside the stadium other than a public address system associated with an event within the grounds where the sound is managed in accord with the appropriate conditions.
- 19 Development involving indoor events should not exceed the noise level LOCT10 70 dB at 63 Hz measured over at least a five minute period of a musical performance at a point 180 metres distance from the centre of the stadium building.
- 20 Operating hours of the event (other than the use of the existing restaurant), should not extend beyond 11.00 pm Monday to Saturday and 6.00 pm Sunday with car parking areas cleared by 12.00 pm and 7.00 pm respectively.
- 21 Development should not cause a nuisance or disturbance to occupants of nearby residential dwellings in the locality by the generation of excessive noise or the creation of excessive glare from external lighting or firework displays.
- 22 Development should ensure that any car park floodlighting is switched off within fifteen minutes of the clearance of patrons' vehicles from the car park.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*. In addition, the following forms of development are designated as complying subject to the conditions contained in [Table Chst/1 - Conditions for Complying Development](#):

- (a) within **Precinct 81 Stadium Beverley**:
 - (i) indoor sport-related events
 - (ii) indoor non-sport related events, which are not concerts and either of the following apply:
 - (A) where the maximum attendance at any one event does not exceed 750 persons
 - (B) where less than 19 such events have been held in any calendar year and the maximum attendance at any one event is between 750 and 6500 persons; and no more than one event is held within any three day period

- (iii) outdoor sport-related events
- (iv) outdoor non-sport related events attended by no more than 2000 persons
- (v) recreation.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for the following is non-complying:

Form of Development	Exceptions
All events, where conducted within Precinct 81 Stadium Beverley , with a noise level exceeding LOCT10 70 dB at 63 Hz measured over at least a five minute period of a musical performance at any point 180 metres distance from the centre of the stadium building.	
All indoor events, where conducted within Precinct 81 Stadium Beverley , exceeding 750 persons in attendance where a fee is collected from vehicles entering the grounds.	
Cemetery	
Consulting room	
Crematorium	
Dwelling	
Education establishment	
Farming	
Fuel depot	
Hall	
Horse keeping	
Hospital	
Hotel	
Indoor non-sport related events, where conducted within Precinct 81 Stadium Beverley , where the event is a concert exceeding 750 persons in attendance and: <ul style="list-style-type: none"> (a) the event would be the nineteenth or subsequent event held in any calendar year (b) the maximum attendance to any event exceeds 6500 patrons (c) the event would be the second or subsequent event held within any three day period (d) the operating hours of the event (other than the existing restaurant), extends beyond 11.00 pm Monday to Saturday and 6.00 pm Sunday. 	
Industry	
Intensive animal keeping	
Keeping of animals, whether temporary or permanent, within Precinct 81 Stadium Beverley .	

Form of Development	Exceptions
Motel	
Motor repair station	
Nursing home	
Office	Except where associated with sports administration or recreation activities.
Petrol filling station	
Prescribed mining operations	
Public service depot	
Road transport terminal	
Service trade premises	
Shop or group of shops	Except where associated with sports administration or recreation activities.
Stock sales yard	
Stock slaughter works	
Store	
Warehouse	
Waste reception, storage, treatment or disposal	
Wrecking yard	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

Further, the following forms of development are designated:

Category 1	Category 2
Within Precinct 81 Stadium Beverley : (a) indoor sport-related events (b) indoor non-sport related events, which are not concerts and either of the following apply: (i) where the maximum attendance at any one event does not exceed 750 persons (ii) where less than 19 such events have been held in any calendar year and the maximum attendance at any one event is between 750 and 6500 persons; and no more than one event is held within any three day period (c) outdoor sport-related events (d) outdoor non-sport related events attended by no more than 2000 persons (e) recreation.	Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 .

Suburban Activity Node Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A zone that includes a range of medium and high density residential development supported by a mix of compatible land uses.
- 2 Well designed and functional mixed use areas with a walkable urban form, pedestrian and cyclist friendly streetscapes, and active street frontages that incorporate a high level of amenity and facilitate personal interaction and promote public transport use.
- 3 The design and layout of development to encourage walking and cycling and promote public transport use.
- 4 A mixed use area with a variety and concentration of activity close to a key focal point such as a fixed transit stop, activity centre or high quality open space.
- 5 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

This zone will be developed as a medium to high density residential node with integrated mixed use development and quality public open spaces around public transit stops. This node is set within a wider suburban context and offers a focus for local community scale activity. It is designed to foster an area with a sense of identity and uniqueness.

Residential development, primarily in the form of row dwellings and residential flat buildings, is appropriate in the zone. Aged and student accommodation, serviced apartments and diverse housing forms that cater for a range of household types, ages and life cycle stages, which are adaptable, are also encouraged in the zone to take advantage of its proximity to nearby transport services and facilities.

To minimise the impacts on development in adjacent zones, the zone will consist of a Core Area and Transition Area as identified on [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).

The form, scale and mix of development will be at its greatest intensity in the Core Area. Building heights of up to 4 storeys (16.5 metres) maybe appropriate within the Core Area where located more centrally around the existing railway station. Land uses located at street level will include a mix of residential, shop, office and tourist accommodation. Office uses may be appropriate on upper floors especially where they provide a buffer between more active ground floor and residential activity. The mix of uses will cater for the day to day needs of local residents and workers and will be at its greatest intensity closest to the public transit. Features and activities that attract people and add vitality to the street, such as display windows, retail shopfronts and outdoor dining areas are desirable, particularly at ground level adjacent to Trimmer Parade and Tapleys Hill Road.

The Transition Area will act as a buffer between the Core Area and nearby lower density residential zones or lower intensity zones. Land uses in the Transition Area will be predominantly residential but may include other low impact uses such as offices and retail which address the frontage along Trimmer Parade. The density and intensity of development will be less than that of the Core Area but will be greater than neighbouring residential zones. Building heights of up to 3 storeys (12.5 metres) maybe appropriate within the Transition Area where located closest to Trimmer Parade to maximise its location to the railway station.

Trimmer Parade will develop as a pedestrian friendly environment achieved by landscaping, surface treatments, street furniture and building design. Building setbacks along Trimmer Parade and the eastern side of Talpeys Hill Road are subject to road widening requirements and will also consider additional setback requirements to encourage street activation (eg outdoor dining) due to the narrow nature of the existing footpath. Building setbacks adjacent to the railway corridor will take into account the potential electrification of the railway line to address safety distance requirements.

Development is within defined building envelopes that manage the location and scale of buildings to achieve high quality urban design. The building envelopes will contribute to the provision of a coherent public realm by framing the street space and, in particular, the physical and functional character of the road and transit stop. Buildings at the interface of the zone with lower intensity zones will create an appropriate transition of development scale and massing.

The range of setbacks provided in the zone will be critical in providing space for landscaping to soften the hard edge of new built form.

As development intensifies, some overlooking, overshadowing and noise impacts may occur within the zone but this will be moderated through good design and noise attenuation techniques. Solar access may also be reduced. Any adverse impacts for adjoining zones will be minimised through appropriate building envelopes, transition of building heights, design and siting of windows and balconies, and use of landscaping. Buildings will also be designed to maximise solar access.

There are a number of potential sources of air and/or noise emissions within and adjacent to the zone including Tapleys Hill Road, Trimmer Parade, the Grange railway line and a hotel. Development should be sited and designed to mitigate these potential impacts.

To promote a compact pedestrian oriented development, building entrances will be oriented to the street, large parking areas will be minimised and on-site parking areas will be located behind buildings.

The function of Tapleys Hill Road and Trimmer Parade as arterial roads will be protected by minimising access points. Access will be provided from secondary road frontages, rear access ways and serviced by internal roads where possible with provision for on-street car parking, with vehicle access points limited to those identified on [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).

Amalgamation of adjacent allotments or sharing of facilities including communal open space, parking areas and access ways is encouraged.

A high amenity public realm and pedestrian environment will be achieved by landscaping, surface treatments, street furniture and building design. Footpaths will be wide and street trees will shade the footpath and soften the built form. Colonnades, courtyards, awnings and street furniture will create a pedestrian friendly environment. Side and internal streets will incorporate traffic calming and Water Sensitive Urban Design measures.

The street system will be highly connected to surrounding areas and within the zone to maximise walkability and reduce local travel distances. Cycle paths and routes will be provided which are safe, accessible, well signed and connect and link key local destinations (such as the train station, shops, schools and local parks).

A local park will be provided for the purposes of community interaction and for local amenity. The location of public open space is desired to the north of Trimmer Parade with connections to the existing street/footpath network and adjacent to the proposed 'Greenway' along the railway corridor to provide surveillance and interaction with the existing suburb. The desired location of public open space is identified on [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).

Front fences will be designed to ensure visual permeability and avoid large blank walls to encourage passive surveillance, active streetscapes and a visually interesting public realm.

There are areas within the zone known to be affected by potentially contaminating activities. Offsite contamination may also exist on adjacent or nearby land, which may impact the use of the land within the zone. The extent of contamination in some areas is unknown. Development of these areas affected will not proceed unless appropriate investigations and remediation (where required) has been undertaken. This may include in some risk situations, either a Preliminary Site Investigation (PSI), detailed Site Investigation (DSI) or a Site Contamination Audit Report (SCAR) being prepared.

Water Sensitive Urban Design systems, including the harvest, treatment, storage and reuse of storm water, will be integrated throughout the area at the neighbourhood, site and building level. Harvested storm water will improve the aesthetic and functional value of open spaces, including public access ways and greenways. Front fences will be designed to ensure visual permeability and avoid large blank walls to encourage passive surveillance, active streetscapes and a visually interesting public realm.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following types of development, or combination thereof, are envisaged in the zone:
 - affordable housing
 - aged persons accommodation
 - community centre
 - consulting room
 - dwelling
 - educational establishment
 - nursing home
 - office
 - pre-school
 - primary school
 - residential flat building
 - retirement village
 - shop or group of shops, excluding a bulky goods outlet or a retail showroom
 - supported accommodation
 - tourist accommodation.
- 2 Non-residential land uses should have a local or neighbourhood focus to their scale of activity and intended market catchment.
- 3 Development listed as non-complying is generally inappropriate.
- 4 Core Areas, Transition Areas and other identified features should be developed in accordance with the relevant [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).
- 5 Development within walking distance of public transport stops should comprise land uses that directly promote public transport use and provide opportunities for multi-purpose trips.
- 6 Development should primarily take the form of:
 - (a) in Core Areas - residential flat buildings, non-residential buildings and buildings comprising two or more land uses with non-residential land uses on the ground floor
 - (b) in Transition Areas - residential flat buildings, row dwellings, detached and semi-detached dwellings with supporting shops and community facilities.
- 7 Non-residential development should:
 - (a) have a local or neighbourhood focus to their scale of activity and intended market catchment
 - (b) encourage walking to local shopping, community services and other activities

- (c) not detrimentally impact on the amenity of nearby residents.
- 8 Transition Areas should be developed to provide a transition between an intense core of development and neighbouring lower intensity development.
- 9 Shops or groups of shops (other than a bulky goods outlet) and offices should have the following maximum gross leasable areas:

Designated Area	Maximum gross leasable areas (square metres)
Core Area	1000
Transition Area	1000

- 10 Within the Core Area, direct vehicle access from Trimmer Parade and Tapleys Hill Road should be minimised and provided from secondary road frontages, rear access ways and internal roads, with vehicle access points limited to those identified on [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).
- 11 Development incorporating internal roads should be designed to provide on-street car parking.

Form and Character

- 12 Development should be consistent with the desired character for the zone.
- 13 Development should graduate from medium-rise in Core Areas to low-rise in Transition Areas, especially where the development site abuts the zone boundary.
- 14 Garage top apartments that share the allotment and services of the main dwelling should:
 - (a) be no more than 2 storeys in height above the garage (a total of 3 storeys)
 - (b) front a street that provides rear access for vehicles
 - (c) complement the existing dwelling or mixed use building.
- 15 In Core Areas:
 - (a) the ground and first floor of buildings of 4 storeys should be built to dimensions (including a minimum ceiling height of 4.5 metres) to allow for adaptation to a range of land uses, including retail, office and residential, without the need for significant change to the building
 - (b) a minimum of 50 per cent of the ground floor primary frontage of commercial buildings facing Tapleys Hill Road or Trimmer Parade should be visually permeable, transparent or clear glazed.
- 16 Residential development (other than residential development in mixed use buildings), should achieve a minimum net residential site density in accordance with the following:

Designated area	Minimum net residential site density
Core Area	70 dwellings per hectare net
Transition Area	35 dwellings per hectare net

Building Envelopes

Building Height

- 17 Except where airport building height restrictions prevail, building heights (excluding any rooftop located mechanical plant or equipment) should be consistent with the following parameters:

Designated area	Minimum building height	Maximum building height
Core Area	3 storeys (12.5 metres)	4 storeys (16.5 metres)
Transition Area	2 storeys (8.5 metres)	3 storeys and (12.5 metres)

Setbacks from the Primary Road Frontage

- 18 Buildings (excluding verandas, porticos and the like) should be set back from the primary road frontage in accordance with the following parameters:

Designated area	Minimum setback from the primary road frontage (metres)
Core Area and Transition Area	No minimum along Tapleys Hill Road No minimum along Trimmer Parade 3 metres elsewhere

Note: These setbacks are in addition to any setback requirements pursuant to the Metropolitan Adelaide Road Widening Plan.

Setbacks from Side Boundaries

- 19 Buildings (excluding verandas, porticos and the like) should be set back from side boundaries in accordance with the following parameters:

Designated area	Minimum setback from side boundaries (where not on a street or different zone boundary)	Minimum setback from side boundaries on a zone boundary
Core Area and Transition Area	Irrespective of height, no minimum on the boundary within 18 metres from the front property boundary. No minimum for the remaining length for the ground level only. More than 18 metres from the front property boundary, 1 st level and above (ie above ground level) should be setback 2 metres.	0.9 metres for single storey (3 metres in height) 2 metres for two storeys (8.5 metres in height) 3 metres for greater than two storeys

- 20 Unless abutting an existing building, walls (including attached structures) that have a wall height of greater than 4.5 metres, located on or within 2 metres of side allotment boundaries should provide attractive and interesting facades utilising techniques and combinations such as the following:

- (a) including recessed sections of wall
- (b) continuing some façade detailing
- (c) integrating use of different building materials and finishes
- (d) including green landscaped walls / gardens
- (e) including public art, including murals.

Other Setbacks

- 21 Buildings (excluding verandas, porticos and the like) should be set back in accordance with the following parameters:

Setback Parameters	Value (metres)
Minimum setback from secondary road frontage	0.9
Minimum setback from a rear lane access way	No minimum where the access way is 6.5 metres or more OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
Minimum setback from the rear allotment boundary	5

Design and Appearance

- 22 Buildings should address public open space and defined pedestrian and cycle routes as illustrated in [Concept Plan Map ChSt/30 – Suburban Activity Node Zone Seaton](#).
- 23 Masonry fences should be no more than 1.2 metres in height to maintain sight lines between buildings and the street, and to improve safety through passive surveillance.

Land Division

- 24 Land division in the zone is appropriate provided new allotments are of a size and configuration to ensure the objectives of the zone can be achieved.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development are designated as complying subject to the conditions contained in [Table ChSt/2A – Off-street Vehicle Parking Requirements for Designated Areas](#):

- (a) change in the use of land from residential to office on the ground or first floor of a building
- (b) change in the use of land from residential to shop less than 250 square metres on the ground floor of a building.

Non-complying Development

Development (including building work, a change in the use of land or division of an allotment) involving any of the following is non-complying:

Form of development	Exceptions
Bulky goods outlet	
Hotel	Except for alterations and additions to an existing hotel.

Form of development	Exceptions
Industry	
Fuel depot	
Public service depot	
Road transport terminal	
Service trade premises	Except for alterations and additions to an existing service trade premises.
Store	
Transport depot	
Warehouse	
Waste reception storage treatment or disposal	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination thereof (except where the development is non-complying), are designated:

Category 1	Category 2
Advertisement	All forms of development not listed as Category 1
Aged persons accommodation	
All forms of development that are ancillary and in association with residential development.	
Community centre	
Consulting room	
Dwelling	
Dependent accommodation	
Educational establishment	
Hall	
Nursing home	
Office	
Pre-school	
Primary school	
Residential flat building	
Retirement village	
Shop or group of shops	
Supported accommodation	
Tourist accommodation	

Urban Core Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A mixed use zone accommodating a mix of employment generating land uses and medium to high density residential development in close proximity to a high frequency public transport corridor.
- 2 Development within a mixed use environment that is compatible with surrounding development and which does not compromise the amenity of the zone or any adjoining residential zone.
- 3 Smaller dwellings, including innovative housing designs located close to local services and public transit stops.
- 4 Mixed use development integrated with a high quality public realm that promotes walking, cycling, public transport patronage and positive social interaction.
- 5 A zone that provides a spatial separation, or transitions down, in scale and intensity to adjacent lower density residential zones.
- 6 To identify and remediate contaminated land appropriate for its intended use.
- 7 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

The zone applies to the Bowden Urban Village site as defined on [Concept Plan Map ChSt/23 - Bowden Urban Village Urban Core Zone](#), the West Lakes site as defined on [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#) and the Bowden/Brompton site as defined on [Concept Plan Map ChSt/31 – Bowden/Brompton Urban Core Zone](#).

This zone supports housing at medium and high densities and a range of dwelling types, including detached dwellings which are conveniently located in proximity to high frequency public transport services, recreation, commercial, education, shop, office and other mixed use activities. Development within this zone will also result in significant employment generating activity closely aligned to nearby public transport infrastructure and services.

Medium and high density housing, primarily in the form of row dwellings, residential flat buildings and mixed use buildings will be developed in the zone. Development will be encouraged to achieve the desired minimum average site density as shown on [Concept Plan Map ChSt/23 - Bowden Urban Village](#) and [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#). In the case of integrated development areas containing multiple development sites, the aim should be to achieve the desired minimum average site density across all stages of the development where individual building sites may be above or below the average.

Development in the zone will achieve high quality urban design in both the public and private realm. Buildings will contribute to the provision of a coherent public realm by shaping the street space and, in particular, the physical and functional character of development fronting a key arterial road or public transport corridor.

As development intensifies, overlooking, overshadowing and noise impacts will be moderated through good design and noise attenuation techniques. Impacts on adjoining zones will also be addressed through appropriate building envelopes, transition of building heights, design and location of windows and balconies, and use of landscaping. Buildings will also be designed to maximise solar access within the development site.

Development will contribute positively to the quality of the public realm by articulating buildings with canopies, modelled facades and balconies that make use of light and shade and by providing architectural detail. Solid material will be balanced with glazed areas and plant and service equipment will be enclosed and out of view from the street and neighbouring sites.

A proportion of the public open space will be sited away from transit corridors to ensure that residents and workers have a quiet tranquil outdoor place to relax in.

A range of setbacks will be provided in the zone to accommodate development fronting a primary and / or secondary road frontage and will be critical in softening the continuous edge of new built form and provide a higher amenity streetscape and pedestrian environment which is shaded by street trees and other forms of mature vegetation.

Where appropriate, landscaping features including public art, street furniture, bicycle parking rails and higher quality and recycled materials should be used to create high amenity spaces that establish a sense of place, a connection to heritage and promote community cohesion.

Wide footpaths and innovative use of landscaping will help define the street encouraging the development of active land uses at street level along key thoroughfares. Where opportunities exist, consideration should be given to the development of human-scale shared streets without demarcation or kerbing separating users, promoting voluntary behavioural change of all street users.

Cycling routes and pedestrian pathways and high amenity public open space will create an attractive living environment as well as providing convenient and accessible pathways to integrated public transport stops. Public open space will include a range of forms and sizes including smaller intimate spaces and formal plazas. Spaces will be designed as safe and attractive places for a range of community activities as well as water management.

On-site parking areas will be consolidated, shared and, where possible, not visible from the street or public spaces.

Water Sensitive Urban Design systems, including the harvest and reuse of storm water, will be integrated throughout the area at the neighbourhood, street, site and building level. Harvested storm water will improve the aesthetic and functional value of open spaces, including public access ways and greenways. Opportunity to explore connectivity to Council initiatives towards improving stormwater management systems is anticipated. Retention and reuse of all stormwater from the development should be aspired to through the stormwater management system. This would utilise a range of treatment and storage devices to achieve the most productive end use of the water.

Core Areas within the Bowden Urban Village and West Lakes

At Bowden Urban Village and West Lakes, the Core Area will provide the greatest intensity of land use and activity in the zone with a mix of residential, and neighbourhood scale commercial and employment generating activities integrated with adjacent public transit stop(s). Located within the Core Area is the **Main Street Policy Area 24**, which will provide the primary focus for convenience shopping, including supermarkets and cultural and community services in the zone.

Development adjacent to a fixed or other frequent public transport stop should be mixed both vertically and horizontally with non-residential uses encouraged on the ground floor to create visual interest and invite personal interaction on street frontages. The exterior storage or display of goods should not compromise pedestrian movement.

Neighbourhood scale retail activity is envisaged within the Core Area of the zone, in addition to other business and community facilities.

Student and aged accommodation, serviced apartments and affordable housing are also strongly encouraged in the Core Area to assist in delivering an overall mix of residential activity in this area.

Bowden Urban Village

Development will focus around a Core Area with a Transition Area adjoining neighbouring residential zones as identified on [Concept Plan Map ChSt/23 - Bowden Urban Village](#).

Located within the Core Area is the **Main Street Policy Area 24**, which will provide the primary focus for convenience shopping, including supermarkets and cultural and community services in the zone.

The Transition Area will provide a buffer between the Core Area and adjacent residential areas / zones with development taking the form of high quality medium-high density housing where the combination of dwellings and residential flat buildings will provide a range of housing for a diverse community. There will be some provision for mixed use buildings where it does not negatively impact the predominant residential character of the area. Retail floor space in the order of 250-500 square metres will be located within the Transition Area to provide community focus points.

In that portion of the Transition Area adjacent to Chief Street, development closest to the Chief Street alignment should not exceed six storeys in height to complement the streetscape and provide an appropriate transition to lower scale residential areas outside of this zone.

The zone at Bowden Urban Village contains a number of State and Local Heritage listed places and associated Contributory Items. In addition, the former SA Gas Company (SAGASCO) Gas Works are located in an Historic Conservation Area fronting Chief Street. In this Area, buildings of historic value are characterised by bluestone construction, pitched roofs and a variety of scales, from an intimate pedestrian scale near East Street to a large industrial scale along Chief Street and the railway line. It is desirable to conserve and enhance the historic SAGASCO buildings through sympathetic new development. The extent of places of heritage significance within the zone is well documented and their presence will be taken into account in the development of the wider area.

Established industrial uses within and nearby the zone comprising the Thebarton brewery situated near the corner of Adam Street and Port Road in the City of West Torrens and a smallgoods facility on Sixth Street, have the potential to produce odours that could cause nuisance to future users and residents within this zone. Efforts will be made to reduce the potential for land use conflict between these existing uses and new developments likely to be sensitive to odour.

Some parts of the zone are known to be contaminated, including significant contamination within the former Gasworks site located north of the rail line, west of Drayton Street. Other areas within the zone could also be contaminated because of previous activities. Due to these circumstances, development is expected to occur on a precautionary basis where a site contamination audit verifies that a site or sites are suitable and safe for the intended use, particularly where it involves sensitive uses like residential development.

West Lakes

West Lakes will focus around a Core Area with a **Main Street Policy Area 24** along Turner Drive, adjoining the West Lakes Shopping Centre as identified on [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#).

Development will provide a high quality architectural backdrop to the adjacent golf course and provide views over the golf course. West Lakes Boulevard provides a transition of building heights to residential development to the north. Buildings heights within the Core Area directly adjoining West Lakes Boulevard will transition down to a maximum of four storeys. This transition in building heights will provide a complementary built form to that of the mixed-use central component of the Core Area. Development on the eastern side of Frederick Road has a high prospect of redevelopment and should not unduly constrain development of the site.

The zone seeks to realise a compact and vibrant form, promoting integration of differing land uses and accessibility that complements and supports the adjacent District Centre. The zone will capitalise on the separation it enjoys from surrounding lower density areas by providing opportunity for buildings up to eight storeys (within the Core Area). A distinguishable contrast in the urban form to that of the surrounding residential areas is anticipated.

Retail development within the zone (excluding the Core Area and **Main Street Policy Area 24**) will be of small scale convenience stores and tenancies designed to service local community requirements. The amount of retail area within the zone (excluding the Core Area and **Main Street Policy Area 24**) will be in the order of 500 square metres of gross leasable floor area.

Retail development within the Core Area (excluding **Main Street Policy Area 24**) will be of a scale that supports an active, mixed use environment and supplements the function of the adjoining District Centre Zone as a key shopping destination. Shops and commercial uses will be primarily accommodated on the ground floor within mixed use buildings and encouraged to activate both sides of the east-west streets connecting to Turner Drive, particularly the east-west road north of the oval. The amount of retail area within the Core Area (excluding **Main Street Policy Area 24**) will be in the order of 5500 square metres of gross leasable floor area. More intense retail, commercial and community uses are envisaged within the **Main Street Policy Area 24**, to complement and supplement the functioning of the adjoining District Centre.

Pre-schools, early learning centres, primary and secondary schools, along with aged care and retirement living, medical and allied health services are strongly encouraged within the Core Area and **Main Street Policy Area 24** to create a multigenerational approach to community service provision and delivery within the West Lakes area.

Land division should occur in conjunction with the provision of public open space equivalent to 15% of the overall site as depicted in [Concept Plan Map ChSt/25 – West Lakes Urban Core Zone](#). Ideally, opportunities for public use and access to the existing football oval will be explored as the development of West Lakes proceeds. This significant sporting and recreational area, together with incorporation of the Max Basher Reserve (central area of open space), will provide major recreational areas for the local and wider community of the region. The central area of open space (the Max Basheer Reserve) will be aligned with frontage to West Lakes Boulevard and of a size and dimension that allows for future sporting and playing fields. It is envisaged that the ongoing use of the existing football oval as a sporting facility and associated club rooms will continue. Additionally, it is envisaged that east – west, linear parks linking to pedestrian and cycling paths and these key recreational areas will be established, as reflected on [Concept Plan Map ChSt/25 – West Lakes Urban Core Zone](#). These shared pathways will provide the basis for pedestrian and greenway linkages of regional importance that may extend beyond the zone. This will culminate in the order of some 15 per cent open space being provided as part of the total development of West Lakes.

Land directly fronting the Grange Golf Course should primarily be of an open space and linear park nature, unless otherwise indicated on [Concept Plan Map ChSt/25 – West Lakes Urban Core Zone](#).

There may be areas within the zone at West Lakes that are contaminated because of previous activities. Further investigations to determine the extent of site contamination and necessary remediation measures may therefore be required to ensure that sites are suitable and safe for their intended use.

Bowden/Brompton

The Bowden/Brompton site is located immediately to the north of the Bowden Urban Village site. The area is one of transition, with traditional industrial and commercial activities ceasing or relocating, freeing up the land for other activities.

As portion of the adjacent Bowden Urban Village site has already been established as a Core Area providing for the greatest intensity of land use and activity in the zone, including neighbourhood scale retail and commercial and other employment generating activities, the desired future character for the Bowden/Brompton site is to focus on residential activity and low impact mixed uses servicing the local area, contained within a Transition Area.

Development is to occur in accordance with [Concept Plan Map ChSt/31 - Bowden/Brompton Urban Core Zone](#), with new development predominantly taking the form of high quality, medium rise and medium-high density housing where the combination of dwellings and residential flat buildings will provide a range of housing for a diverse community. A minimum of 15 per cent of the housing provision will be affordable housing, integrated into the residential and mixed use development.

There will be some provision for mixed use buildings where it does not negatively impact the predominant residential character of the area. Non-residential uses will be primarily located along Chief and Hawker Streets. Retail floor space in the order of 250 to 500 square metres will be located within the area to provide a community focus point and to service local residents.

A high amenity public realm and pedestrian environment will be achieved by landscaping, surface treatments, street furniture and building design. Footpaths will be wide and street trees will shade the footpath and soften the built form. Colonnades, courtyards, awnings and street furniture will create a pedestrian friendly environment. Side and internal streets will incorporate traffic calming and Water Sensitive Urban Design measures. Existing street tree plantings in Chief Street will form an important element in the streetscape character and will be maintained and enhanced.

Development around the western, northern and eastern periphery of the Bowden/Brompton site will be of lower height (3 and 2 storeys) to complement the streetscape and provide an appropriate transition to lower scale residential areas outside of this zone. Development adjacent to Second and Sixth Streets, which form the southern periphery of the site, will be of a higher form (up to a maximum of 6 and 5 storeys) to complement the potential for higher form development in the adjacent Bowden Urban Village site, and provide for a seamless integration with that development.

Redevelopment of the Bowden/Brompton site will generally retain and improve the existing grid road network. The street system will be highly connected to surrounding areas and within to maximise walkability and reduce local travel distances. Pedestrian and cycle paths and routes will be provided which are safe, accessible, well signed and connect and link key local destinations (such as public transport stops, shops, schools and local open space). Walking and cycling links with a spacing of 50 to 70 metres will provide an optimum circulation network within the larger blocks.

Vehicle access points to Chief Street, Hawker Street, Second Street, Sixth Street and Drayton Street will be minimised, with access to be provided from local streets and laneways. This may involve the establishment of additional internal local roads and laneways to provide improved accessibility and to enable future dwellings to be 'rear loaded'. Rear access to dwellings will reduce the number of crossovers on main roads, improve pedestrian and cyclist movements as well as on-street parking, landscaping and road safety. In addition, some road reserves (Second, Sixth, East, Drayton, Third, Seventh and Eighth Streets) will be widened to provide a better pedestrian friendly environment achieved by wider footpaths, landscaping and street furniture, while encouraging street activation.

Two new key open space squares will be centrally located in the Bowden/Brompton site, providing a focus for new residential development. The open space provision will also include the retention and reinforcement of Ethelbert Square, new corridors along Second Street, Hawker Street and Third Street, and two smaller areas of public open space in the two blocks bound by Eighth, Gibson, Sixth and Drayton Streets. These spaces, together with landscaped street profiles, will improve the overall amenity within the area and interface with surrounding roads and development.

While only two Local Heritage Places are located within the Bowden/Brompton site, there are a number of State Heritage Places, Local Heritage Places and Contributory Items located adjacent to its boundaries.

Their presence will be taken into account in the development of the area.

Established industrial uses within the Bowden/Brompton site, comprising a smallgoods facility on Seventh Street and a packaging facility on Chief Street, have the potential to produce odours and noise that could cause nuisance to future residents or sensitive land uses located in proximity to the activities. Efforts will be made to reduce the potential for land use conflict between these existing uses and new sensitive developments, recognising that the activities may limit or constrain new development until such a time as the activities cease or are relocated. Sensitive land uses will be designed to minimise negative impacts from existing uses.

With a long history of industrial and commercial use, it is expected that a range of soil and groundwater contamination issues will be present in the area. In addition, the presence of two historic pug holes, located in the southern and north-western portions of the area, will need to be considered as there is a risk of contaminants being present to a considerable depth in a non-natural soil profile. Due to these circumstances, development, including associated infrastructure, is expected to occur on a precautionary basis where site

contamination investigations verify that a site or sites are suitable and safe for the intended use, particularly where it involves sensitive uses like residential development.

Stormwater runoff detention is to be managed in several ways, including use of above ground basins, underground storage in Ethelbert Square reserve, on road surfaces and by other Water Sensitive Urban Design measures. Indicative locations for detention basins are shown in association with proposed open space areas on [Concept Plan Map ChSt/31 - Bowden/Brompton Urban Village Zone](#).

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development, or combination thereof, are envisaged in the zone:
 - affordable housing
 - aged persons' accommodation
 - art gallery
 - hall
 - parking facility
 - community centre
 - consulting room
 - dwelling
 - educational establishment
 - licensed premises
 - nursing home
 - office
 - pre-school
 - primary school
 - railway station and associated infrastructure (excluding West Lakes)
 - residential flat building
 - retirement village
 - secondary school
 - shop or group of shops
 - supported accommodation
 - tram station and infrastructure.
- 2 The following additional forms of development, or combination thereof, are envisaged within the Core Area of the zone, identified on [Concept Plan Map ChSt/23 - Bowden Urban Village](#) and [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#):
 - advertisement
 - entertainment venue
 - hotel
 - indoor recreation centre
 - library
 - light industry
 - motel
 - place of worship
 - tourist accommodation.
- 3 Development listed as non-complying is generally inappropriate.
- 4 Core Areas, Transition Areas and other identified features should be developed in accordance with the relevant [Concept Plan Map ChSt/23 - Bowden Urban Village Core Zone](#), [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#) and [Concept Plan Map ChSt/31 - Bowden/Brompton Urban Core Zone](#).
- 5 Core Areas should be developed to include a range of land uses that are high pedestrian generators, directly promote public transport use and provide opportunities for multi-purpose trips

- 6 Light industrial and commercial development (including high technology and research based activity) should be located in Core Areas and be compatible with adjoining uses.
- 7 Core Areas should incorporate integrated high quality public open spaces, sport and recreation facilities, and community areas that act as social hubs for communal activity.
- 8 Except in Core Areas where a higher intensity of development is envisaged, non-residential development should comprise uses that:
 - (a) are of a local or neighbourhood scale
 - (b) encourage walking to local shopping, community services and other activities
 - (c) do not detrimentally impact on the amenity of nearby residents.
- 9 A shop or group of shops should have a maximum gross leasable area generally in accordance with the following:

General Location	Core Area (excluding Main Street Policy Area 24)	Transition Area	Main Street Policy Area 24	Any other area not designated
Bowden Urban Village	500 square metres, except where it is: <ol style="list-style-type: none"> (a) a bulky goods outlet where the maximum is 2000 square metres (b) a restaurant where there is no maximum. 	500 square metres	No maximum	N/A

Form and Character

- 10 Development should be consistent with the desired character for the zone.
- 11 Residential development (other than residential development in mixed use buildings), should achieve a **target** minimum net residential site density in accordance with [Concept Plan Map ChSt/23 - Bowden Urban Village](#) and [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#), recognising that the **target minimum net residential site density may comprise a series of developments or buildings that do not all individually achieve that target minimum.**
- 12 Residential development in a mixed use building should **aim to** achieve a minimum net residential site density of 60 dwellings per hectare.
- 13 In Transition Areas at Bowden Urban Village **and Bowden/Brompton**, development should provide a built form that provides the transition between an intense core of development and neighbouring lower intensity development.
- 14 **At Bowden Urban Village, residential development and other sensitive uses likely to be adversely affected by odour emissions from established industrial activities should:**
 - (a) **be avoided in those parts of the zone closest to the Thebarton brewery (being south of Fourth Street and east of Drayton Street) until those operations cease, or odour reduction control measures have been implemented (or will be implemented within an acceptable period) at the source such that the anticipated odour concentration is within acceptable limits**
 - (b) **only encroach to within 100 metres of the odour sources associated with the smallgoods processing activities on Sixth Street where the odour concentration levels are unlikely to cause nuisance.**

Design and Appearance

- 15 In Core Areas:
- (a) the ground floor of buildings should be built to dimensions (including ceiling heights) to allow for adaptation to a range of land uses including shops, office and residential without the need for significant change to the building, with a target minimum ceiling height of 3.5 metres within the **Main Street Policy Area 24**
 - (b) a minimum of 50 per cent of the ground floor primary frontage of buildings should be visually permeable, transparent or clear glazed to promote active street frontages and maximise passive surveillance.
- 16 Except in Core Areas, development of three or more storeys in height should ensure that:
- (a) north-facing windows to habitable rooms of existing dwelling(s) on the same allotment, and on adjacent allotments, receive at least 3 hours of direct sunlight over a portion of their surface between 9.00 am and 3.00 pm on 21 June
 - (b) ground level open space of existing buildings receives direct sunlight for a minimum of 2 hours between 9.00 am and 3.00 pm on 21 June to at least the smaller of the following:
 - (i) half of the existing ground level open space
 - (ii) 35 square metres of the existing ground level open space (with at least one of the area's dimensions measuring 2.5 metres).
- 17 Buildings should address public open space and defined pedestrian and cycle routes as illustrated in [Concept Plan Map ChSt/23 - Bowden Urban Village Urban Core Zone](#), [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#) and [Concept Plan Map ChSt/31 - Bowden/Brompton Urban Core Zone](#).
- 18 Masonry fences should be no more than 1.2 metres in height to maintain sight lines between buildings and the street and to improve safety through passive surveillance.
- 19 Buildings should predominantly provide vehicle access via a side street or rear lane access way.
- 20 At Bowden Urban Village and the Bowden/Brompton site, the public realm should be designed and constructed to a high quality and amenity, create more appealing public spaces that include public art, feature lighting, the creative use of high quality recycled materials, street furniture and landscaping (including understorey plantings).
- 21 Side streets and rear lane access ways should be designed to:
- (a) provide space between buildings that reduce building mass and creates a more interesting public realm
 - (b) achieve active frontages at a lower intensity than the primary street frontage.
- 22 At West Lakes allotments and buildings should be orientated to maximise passive solar design.
- 23 At West Lakes streetscapes should be designed to incorporate tree plantings and footpaths to both sides of road networks. Street trees should be chosen to maximise solar access in both winter and shading in summer.
- 24 At West Lakes buildings should be designed to provide access to natural lighting, shading and ventilation and minimise windows that face in primarily westerly direction.

Building Envelope

Building Height

25 Except where airport building height restrictions prevail, building heights (excluding any rooftop located mechanical plant or equipment) should be in accordance with one of the following:

(a) the [Concept Plan Map ChSt/23 - Bowden Urban Village](#) where the building is located within the Bowden Urban Village

(b) the parameters in the following table where the building is located within West Lakes:

Designated Area	Minimum building height	Maximum building height
Core Area (except on land adjoining West Lakes Boulevard where a transition of building heights is required as described in the Desired Character for West Lakes)	4 storeys	8 storeys and no more than 32.5 metres
Any area not designated by the above	2 storeys	4 storeys and no more than 16.5 metres

(c) the [Concept Plan Map ChSt/31 - Bowden/Brompton Urban Core Zone](#) where the building is located within the Bowden - Brompton site.

Setbacks from the Primary Road Frontage

26 Buildings (excluding verandas, porticos and the like) should be set back from the primary road frontage in accordance with the following parameters:

General Location	Designated Area	Minimum setback from the primary road frontage	Maximum setback from the primary road frontage
Bowden/Brompton site	Urban Core Zone	No minimum	3 metres
Bowden Urban Village	Urban Core Zone , except where located along Chief Street, south of the rail corridor	No minimum	3 metres
West Lakes	Core Area	No minimum	5.5 metres
West Lakes	Main Street Policy Area 24	No minimum	5.5 metres
West Lakes	Any area not designated by the above	No minimum	5.5 metres

27 At Bowden Urban Village, buildings (including verandas, porticos and the like) fronting Chief Street and within 40 metres of the southern edge of the rail corridor should be sited and designed to enable views of the State heritage place (SAGASCO wall) located north of the rail corridor.

Setbacks from side boundaries

- 28 Buildings (excluding verandas, porticos and the like) should be set back from side boundaries in accordance with the following parameters:

General Location	Designated Area	Minimum setback from side boundaries
Bowden/Brompton site	All of the Urban Core Zone	No minimum
Bowden Urban Village	All of the Urban Core Zone	No minimum
West Lakes	Core Area	No minimum
West Lakes	Main Street Policy Area 24	No minimum
West Lakes	Any area not designated by the above	0.9 metres

Other Setbacks

- 29 Buildings (excluding verandas, porticos and the like) should be set back from the secondary road frontage or rear access way in accordance with the following parameters:

General Location	Minimum set back from secondary road frontage	Minimum set back from a rear access way
Bowden/Brompton site	No minimum	No minimum where the access way is 6.5 metres or more OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
Bowden Urban Village	No minimum	No minimum where the access way is 6.5 metres or more OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles
West Lakes	No minimum	No minimum where the access way is 6.5 metres or more OR Where the access way is less than 6.5 metres in width, the distance equal to the additional width required to make the access way 6.5 metres or more, to provide adequate manoeuvrability for vehicles

- 30 Buildings (excluding verandas, porticos and the like) should be set back from all rear allotment boundaries in accordance with the following parameters:

Designated Area	Minimum set back from a rear allotment boundary
Bowden/Brompton site	5 metres except where: (a) development abuts the wall of an existing or simultaneously constructed building on the adjoining land (b) a building envelope plan exists for the adjoining land that proposes a wall on the rear boundary be constructed to the same or lesser length and height as provided for by that plan.
Bowden Urban Village	5 metres except where: (a) development abuts the wall of an existing or simultaneously constructed building on the adjoining land (b) a building envelope plan exists for the adjoining land that proposes a wall on the rear boundary be constructed to the same or lesser length and height as provided for by that plan.
West Lakes	0 metres where the subject land directly abuts an allotment of a different zone. 0 metres in all other cases, except where development abuts the wall of an existing or simultaneously constructed building on the adjoining land.

Incentives

- 31 Where a minimum of 3 hours sunlight access on 21 June to habitable rooms and open space of dwellings in adjoining zones can be maintained, the following incentives apply to development:

Bowden Urban Village

Form of Development for Bowden Urban Village	Additional building height (except on land abutting an adjoining zone)	Car parking reduction (rounded to the nearest whole number)
The development includes undercroft parking with access from a road located to the side or rear of the site	1 storey	10 per cent
A building including non-residential development on the ground floor (or first two floors) with residential development on the floors above, where the residential component achieves the desired target minimum average site density for the relevant Area as shown on Concept Plan Map ChSt/23 - Bowden Urban Village Urban Core Zone	1 storey	10 per cent except on land shown on <i>Overlay Map(s) - Strategic Transport Routes</i>
A building including a rooftop garden that occupies a minimum 25 per cent of the building footprint area	1 storey	
Sympathetic redevelopment of a local or State heritage place that retains the item and its appearance to the street		30 per cent
Maximum accumulated allowance	For buildings 5 storeys or less – 1 storey (and less than 4) metres additional building height	30 per cent

Form of Development for Bowden Urban Village	Additional building height (except on land abutting an adjoining zone)	Car parking reduction (rounded to the nearest whole number)
	For buildings of 6 storeys or more - 2 storeys (and less than 8 metres) additional building height	

- 32 At Bowden Urban Village, development adjacent to Station Place should ensure that views of the Bowden Railway Station are maintained from the Port Road entrance of the Entertainment Centre.

Off Street Vehicle Parking

- 33 Vehicle parking should be provided in accordance with [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) except where incentives apply, in which case the parking rate should be reduced accordingly.
- 34 A lesser car parking rate may be applied where justified based on local circumstances, for example where:
- (a) the proposed development is adjacent to a designated pedestrian and/or cycling path
 - (b) the proposed development is in convenient walking distance to readily accessible and frequent public transport
 - (c) convenient on-street car parking is readily available (other than where on-street parking may be removed as a result of road works/widening, including along Park Terrace)
 - (d) the proposed development is on or adjacent to the site of a heritage place which hinders the provision of on-site parking
 - (e) there is the opportunity to exploit shared car parking areas between uses based upon compatible hours of peak operation
 - (f) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means
 - (g) for studio apartments, student accommodation, affordable housing, retirement villages or aged persons' accommodation.
- 35 Car parking at basement level, undercroft parking or multi level parking (or a combination of) should be provided rather than 'at grade'.

Land Division

- 36 At Bowden Urban Village and the Bowden/Brompton site, streets that provide rear access for vehicles are encouraged.
- 37 A traditional street grid pattern should be reinforced in any comprehensive development of areas for mixed use activity to maintain clear sightlines and ensure maximum connectivity.
- 38 Wherever practicable, land division and site amalgamation should:
- (a) create allotments that vary in size and are suitable for a variety of residential and commercial uses
 - (b) improve the level of integration associated with the design and layout of buildings, vehicle parking areas, access points and landscaping treatments.
- 39 Road reserves should be designed to provide on-street parking spaces that vary in size to accommodate the parking of cars and commercial vehicles.

- 40 At Bowden Urban Village, vehicular access between First Street / Park Terrace and Second Street / Park Terrace should only be created once the adjacent railway is undergrounded.
- 41 To help maintain operational requirements of the adjacent golf course and for the amenity and safety of residents living in the zone, allotments for residential purposes should only be created adjoining the southern boundary of the zone in a location generally south of and accessed from McCoy Place as indicated on [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#).
- 42 Existing road reserves within the Bowden/Brompton site should be widened to enable the provision of pedestrian footpaths, street trees and other landscaping, street furniture, on-street parking and traffic movement commensurate with its street classification and function.

Main Street Policy Area 24

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An entertainment, shopping and commercial main street supported by medium and high density residential development.
- 2 Development that is largely consistent in height and width, and frames the street at ground and first floor level.
- 3 An interesting and varied skyline as viewed from the street and afar, provided by modulation in roof forms and the use of parapets.
- 4 A visually interesting streetscape with buildings having a high level of fenestration and detail, and balconies oriented towards the street.
- 5 A high quality public realm with active streets created by buildings designed with frequently repeated frontage form and narrow tenancy footprints.
- 6 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area applies to both the Bowden Urban Village site as defined on [Concept Plan Map ChSt/23 - Bowden Urban Village Urban Core Zone](#) and the West Lakes site as defined on [Concept Plan Map ChSt/25 - West Lakes Urban Core Zone](#).

Bowden Urban Village

The **Main Street Policy Area 24** at Bowden Urban Village primarily focuses on land either side of Gibson Street where development will contribute to a main street theme. Gibson Street will be lined with shopfronts and businesses, creating a lively scene that caters for residents and workers, and attracts visitors to the area including those attending events at the nearby Entertainment Centre and Hindmarsh Stadium. Through an appropriate mix of activities, positioning of shady trees and use of deep awnings, the urban form will foster an ambience that encourages long stays for dining and shopping. Above street level, uses will comprise local businesses, commercial offices and apartments.

The main street will provide the primary focus for convenience shopping, including supermarkets, and cultural and community services. These uses will be located towards the centre of the zone (north of the rail corridor) to ensure equitable access for residents and workers. Smaller scale retail development is appropriate south of the rail corridor.

The main street will encourage the development of a destination that attracts people to its combination of and variety of land uses. Development along that portion of Gibson Street to the south of the rail corridor will provide a continuous, attractive and inviting entrance way to the policy area. Ground floor uses will be non-residential and comprise activities that attract people to the location such as shops, offices and consulting rooms.

Buildings will provide visual interest to the pedestrian, contain frequent pedestrian entries and clear windows to the street. Buildings will be built with a zero set back from the main street, with the occasional section of building set back to create outdoor dining areas, visually interesting building entrances and intimate but active spaces. Footpaths will be sheltered with verandas, shelters and the like to enhance the pedestrian experience. Safe, landscaped pedestrian walkways between buildings will encourage permeability to neighbouring streets and enhance the sense of place and identity of the policy area.

Vehicle access will be grouped, where possible, and from the rear of buildings, with the development of lanes for this purpose encouraged.

On the footpath and at the front façade of developments, landscaping will be small scale or vertical, such as through the use of creepers and greenwalls and in planter boxes on the footpath. Street trees will be provided between parking spaces on the street.

Reuse of the existing warehouses between Third and Fourth Streets will provide opportunity for the development of a 'market' place with associated open space. This area will be the focus of a vibrant art, lifestyle, food and culture scene, providing the evolving precinct with an urban 'edge'.

West Lakes

The **Main Street Policy Area 24** at West Lakes will encourage the development of a destination that attracts people to its combination of and variety of land uses. Ground floor uses will be non-residential and the type of uses that attract people to the location such as shops, offices and consulting rooms. The appearance of a fine grain subdivision pattern, where allotments are narrow but deep, will be maintained to encourage development that is varied in façade.

Retail activity is envisaged within the **Main Street Policy Area 24** of the zone that will support and complement the adjacent West Lakes Shopping Centre.

Buildings will provide visual interest to the pedestrian, contain frequent pedestrian entries and clear windows to the street. Buildings will establish a rhythm and pattern of facades that support a variety of tenancies with narrow footprints. Buildings will be built with zero set back from the main street, with the occasional section of building set back to create outdoor dining areas, visually interesting building entrances and intimate but active spaces. The footpath will be sheltered with verandas, shelters and the like to enhance the pedestrian experience. Safe, landscaped pedestrian walkways between buildings will encourage permeability to neighbouring streets and enhance the sense of place and identity of the policy area.

Vehicle access will be grouped, where possible, and from the rear of buildings, with the development of lanes for this purpose encouraged.

On the footpath and at the front façade of developments, landscaping will be small scale or vertical, such as through the use of creepers and green walls and in planter boxes on the footpath. Street trees will be provided between parking spaces on the street.

Development should activate Turner Drive and establish connections to services and facilities located on the western side of this road. This will be achieved through a combination of treatments including wide footpaths (including a shared use pathway along Turner Drive) and the use of roundabouts or signalised intersections at access points into the District Centre Zone as indicated on [Concept Plan ChSt/25 – West Lakes Urban Core Zone](#).

The road network treatments associated with development along Turner Drive should ensure vehicular traffic movements along the road and to / from adjacent development are appropriately maintained and enhanced.

Development that activates both sides of a road perpendicular to Turner Drive will also be encouraged, particularly along the proposed northern road linkage connecting Turner Drive and West Lakes Boulevard. In this case, connections to existing public bus and potential future tram services should be maximised.

The area will be highly permeable for pedestrians.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 Development on the ground floor of buildings should be non-residential.
- 2 There is no maximum floor area for shops or groups of shops in the policy area.

Form and Character

- 3 Development should be consistent with the desired character for the policy area.
- 4 Pedestrian shelter and shade should be provided over footpaths through the use of structures such as awnings, canopies and verandas.
- 5 The ground level street frontages of buildings should contribute to the appearance and retail function of the area by providing at least 5 metres or 60 per cent of the street frontage (whichever is the greater) as an entry / foyer or display window to a shop (including cafe or restaurant) or other community or commercial use which provides pedestrian interest and activation.
- 6 When abutting a footpath, the finished floor level of the ground floor of buildings should be level with the footpath provided the footpath is constructed to an appropriate level to mitigate flood risk at the edge of the development site.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development (except where the development is non-complying) are complying:

- 1 Subject to the conditions contained in [Table ChSt/1 - Conditions for Complying Development](#) and [Table ChSt/2A - Off-Street Vehicle Parking Requirements for Designated Areas](#):
 - (a) change in the use of land, from residential to office on the ground or first floor of a building
 - (b) change in the use of land from residential to shop less than 250 square metres on the ground floor of a building.
- 2 A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:
 - (a) the area to be occupied by the proposed development is located in an existing building and is currently used as a shop, office, consulting room or any combination of these uses
 - (b) the development is located inside any of the following area(s):
 - **Main Street Policy Area 24**
 - (c) the building is not a State heritage place
 - (d) it will not involve any alterations or additions to the external appearance of a local heritage place as viewed from a public road or public space
 - (e) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, it achieves either (i) or (ii):
 - (i) all of the following:
 - (A) areas used for the storage and collection of refuse are sited at least 10 metres from any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop)
 - (B) if the shop involves the heating and cooking of foodstuffs in a commercial kitchen and is within 30 metres of any Residential Zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the development is to be undertaken and operated in accordance with the conditions attached to the previously approved development
 - (f) if the change in use is for a shop with a gross leasable floor area greater than 250 square metres and has direct frontage to an arterial road, it achieves either (i) or (ii):
 - (i) the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road

- (ii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared
- (g) off-street vehicular parking is provided in accordance with the rate(s) specified in [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#) to the nearest whole number, except in any one or more of the following circumstances:
 - (i) the building is a local heritage place
 - (ii) the development is the same or substantially the same as a development, which has previously been granted development approval under the *Development Act 1993* or any subsequent Act and Regulations, and the number and location of parking spaces is the same or substantially the same as that which was previously approved
 - (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) for development involving any of the following is non-complying:

Form of development	Exceptions
Fuel depot	
General industry	
Public service depot	
Road transport terminal	
Service trade premises	
Special industry	
Store	Except where ancillary to an envisaged use.
Transport depot	
Warehouse	
Waste reception, storage, treatment or disposal	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination of, (except where the development is classified as non-complying) are designated:

Category 1	Category 2
Advertisement	All forms of development not listed as Category 1
Aged persons' accommodation	
All forms of development that are ancillary and in association with residential development	
Art gallery	

Category 1

Category 2

Parking facility

Cinema (excluding West Lakes)

Community centre

Consulting room

Dwelling

Educational establishment

Entertainment venue

Hall

Hotel where:

- (a) located in the **Main Street Policy Area 24**
- (b) fronting Park Terrace at Bowden Urban Village.

Indoor recreation facility

Library

Licensed premises

Motel

Nursing home

Office

Pre-school

Primary school

Railway infrastructure (excluding West Lakes)

Railway station (excluding West Lakes)

Recreation area

Residential flat buildings

Retirement village

Shop or group of shops:

- (a) in the **Main Street Policy Area 24**
- (b) outside of the **Main Street Policy Area 24**, at Bowden Urban Village with a gross leasable area of 500 square metres or less (excluding a restaurant)
- (c) outside of the **Main Street Policy Area 24** at West Lakes but within the Core Area, with a gross leasable area of 5500 square metres or less
- (d) outside of the **Main Street Policy Area 24** and Core Area at West Lakes with a gross leasable area of 500 square metres or less
- (e) comprising bulky goods outlet outside of the **Main Street Policy Area 24** at Bowden Urban Village, with a gross leasable area of 2000 square metres or less
- (f) in the Bowden/Brompton site with a gross leasable area of 500 square metres or less (excluding a restaurant).

Supported accommodation

Tram infrastructure

Tram station

Tourist accommodation

Urban Employment Zone

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this zone.

OBJECTIVES

- 1 A mixed use employment zone that accommodates a range of industrial land uses together with other related employment and business activities.
- 2 Development designed and located to provide primary vehicle access via arterial and distributor roads to minimise impacts on residential streets.
- 3 Commercial uses, including shops, offices, consulting rooms, personal service establishments and training centre that provide services for businesses and an expanding workforce, located along the fringes of the zone on arterial roads.
- 4 Provision for large floor plate enterprises, such as major logistics and manufacturing plants, high technology and/or research and development related uses, located to take advantage of existing transport networks.
- 5 The effective location and management of activities at the interface of industrial/commercial activity with land uses that are sensitive to these operations.
- 6 Development that promotes business clusters that provide a range of economic and environmental benefits.
- 7 Development that contributes to the desired character of the zone.

DESIRED CHARACTER

Desirable land uses include a wide range of activities that take advantage of existing transport networks to generate employment, focusing on industry and similar activities, together with offices, industry-related educational establishments, high technology and/or research and development related uses where it is compatible with adjoining uses. Clustering of industrial activities to share resources and reduce waste impacts and energy needs is encouraged, as well as shared use of facilities and services, including training, communication and information technology and car parking areas where practical.

The development of local commercial activities accommodating local shops (including cafes and restaurants), consulting rooms, offices, service trade premises, recreation facilities and training centres are encouraged in certain locations. These uses should primarily occur in the **Main Road Commercial Policy Area 25**.

The existence of a number of well-established industrial activities, which fulfil an important employment function for the surrounding residential areas and region, are protected from the intrusion of inappropriate uses which may reduce their potential operations and the utility of the land resource.

Labour intensive or high traffic generating activities are located within close proximity to public transport, centres, public open space and community facilities.

Bulky goods outlets are to primarily occur in the **Bulky Goods Policy Area 6**. At a lesser scale, bulky goods outlets are also envisaged in the **Main Road Commercial Policy Area 25**.

The character and amenity of the locality will be enhanced through development that provides a high standard of design and external appearance with consideration given to building scale, bulk, articulation, siting and materials. Development will comprise innovative contemporary architecture that is both adaptable

and flexible to accommodate multiple uses or changes in future land uses where practical. Development of large mass and bulk with long lengths of solid walling are not appropriate on the boundary of another zone where more sensitive land uses are anticipated (e.g. residential development) and will be avoided through articulation and innovative design.

Allotments that are along arterial roads or adjoin the boundary of another zone where more sensitive land uses are anticipated (e.g. residential development), will be large enough to accommodate design features and siting arrangements that maintain a high amenity and limit the impact on the adjoining zone.

Development and offices associated with other uses located to the primary street frontage of a site are constructed with high quality finishes. Arterial road corners are defined with the primary orientation of the development. It is envisaged that where appropriate, the amalgamation of smaller allotments will occur to ensure coordinated and efficient site development.

The main focus for entering the zone for industrial traffic, commercial traffic and heavy vehicles will be from arterial and distributor roads and via local roads within the zone and will be restricted from entering residential areas through the use of traffic management measures. Development will consolidate driveway crossovers to optimise the provision for on-street car parking and minimise conflicts with traffic.

High quality landscaping will be carefully integrated with built form as a means of improving amenity, cooling the site through shading and transpiration and providing habitat with climate responsive sustainable vegetation and matched to the scale of development; so providing a comfortable, pleasant and attractive environment. In order to protect the amenity of adjoining zones a landscaped area at the rear and/or sides of the site will be established as part of the development.

It is envisaged that a landscaped area along boundaries with any road or reserve will be provided in developments. Car parking areas will include trees to provide shade and enhance visual amenity. Landscaping will be carefully designed to minimise opportunity for crime by ensuring passive/active surveillance and minimising places of entrapment.

The erection of open ornamental or decorative fences, other than wire mesh, at the street frontage of a site is appropriate as part of a development. Colour coated wire mesh fencing is appropriate where it is sited in line with the building or behind landscaped areas. The appearance of outdoor storage areas will be enhanced through landscaping and also screened with fencing/structures of varied materials that limit potential for vandalism.

All loading and unloading of goods, including temporary storage will occur within the site and preferably to the rear or side of the site.

In Beverley and Findon, the amenity of William Street and Toogood Avenue will be enhanced by screening the depot fronting the streets with intensive landscaping. The pedestrian environment along Port Road will be improved by providing verandas for pedestrian shelter.

In Hendon, it will be important to utilise materials and forms that are reflective of the traditional character of large scale industrial premises south of Farman Avenue, such as 'English Bond' brickwork and traditional sawtooth roofs. In order to protect and improve amenity in Hendon it will be desirable to establish substantial landscaped buffers abutting the Residential Zone along the Gordon, Willowie and Botting Streets residential interfaces and along the rear of the properties on the northern side of Circuit Crescent.

In Hindmarsh, development will be sympathetic in built form with the traditional siting, scale, form, materials and window shapes in order to maintain the character and amenity of River and Adam Streets.

In Kidman Park it will be important to maintain the existing intensive landscaped buffer of 15 metres width on the western boundary of 414 - 450 Findon Road (Allotment 401, DP 19661, CT 6154/507), to protect the amenity of adjoining residential areas.

In Kilkenny, West Croydon and Woodville, it is envisaged that screen landscaping of appropriate scale will be established along the railway line to soften the visual impact of large scale industry. A priority in Woodville will be to make provisions for a landscaped bikeway along the length of the railway line.

Chapel Street, Hindmarsh is an established industrial area that contains a number of historic buildings and a variety of uses. It is compact in size and most premises are correspondingly smaller in operation. It is envisaged that the area will accommodate a variety of uses, such as small offices, consulting rooms, adaptation of historic housing or a range of industrial uses comprising service industrial, warehousing, storage and distribution activities which benefit from the strategic locational advantages of Hindmarsh. Large scale manufacturing and high impact industries are inappropriate. In order to maintain the character of the area it is desirable for development to be reflective of and sympathetic to the traditional siting and built form of development, particularly in proximity to buildings of historic value.

Industry Interface Area

The area provides a transition between the **Core Industry Policy Area 26** and adjoining residential areas. It is envisaged that this area, as shown on *Overlay Maps – Development Constraints* will accommodate a wide range of industry uses (other than special industry), warehousing, motor repair station, office, consulting rooms, retail activities, public service depots, and training centre, which protect the amenity and safety of adjoining residential zoned areas.

Activities which are potentially hazardous or produce negative off-site impacts, such as noise, air, water and waste emissions, significant volumes of industrial traffic or have a detrimental impact on the amenity of properties in residential or similar environmentally sensitive zones are not appropriate. The phasing out of intensive industrial development (such as special industry) is desirable.

Outdoor activities including driveways, truck manoeuvring areas, carparks, waste collection, and loading / un-loading areas should ideally be located away from the interface between surrounding zones which contain more sensitive land uses.

Much of the area has residential interfaces, which experience impact from some activities in terms of visual appearance, building bulk, minimal landscaping, noise, air quality, overshadowing, 24 hour operation, industrial traffic and on-street carparking. This proximity to residential areas acts as a constraint on industrial operations. Other parts of the area have an interface with the River Torrens. Impacts on residential amenity will be minimised through a high standard of design and incorporate extensive landscaping to enhance visual amenity particularly along residential interfaces. Larger scale structures are appropriate within the centre area.

Parts of the area have intermingled residential uses, which experience industrial impacts in terms of visual amenity, noise and traffic. Dwellings retained for residential use should recognise the industrial location and implement sound attenuation, screening and other measures to protect their own residential amenity. The area will be protected from the encroachment of houses which will reduce the land resource for industry.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

1 The following forms of development, or combination thereof, are envisaged in the zone:

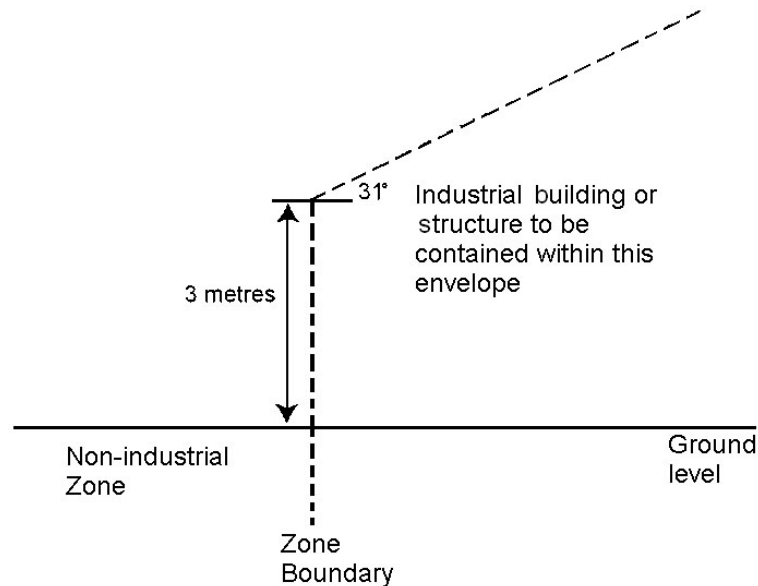
- consulting room
- electricity substation
- fuel depot
- high-tech, bio-tech, research and development
- indoor recreation centre
- industry (other than special industry)
- motor repair station
- office
- petrol filling station
- prescribed mains
- public service depot
- road transport terminal
- service trade premises
- service industry

- shop or group of shops
 - training centre
 - store
 - warehouse.
- 2 Development listed as non-complying is generally inappropriate.
 - 3 Development should not impede the operation of established land uses through encroachment, over development of sites or noise/emissions or any other harmful or nuisance-creating impact.
 - 4 Office uses should be ancillary to an industrial, warehouse, storage or similar activity within the **Core Industry Policy Area 26**.
 - 5 Retail uses should be ancillary to industrial, warehouse, storage or similar activities within the **Core Industry Policy Area 26**.
 - 6 Shops or groups of shops (other than bulky good outlets and service trade premises) should serve the local workforce within the zone and have a gross leasable floor area less than:
 - (a) 500 square metres where located in the **Main Road Commercial Policy Area 25**
 - (b) 250 square metres where located outside of the **Main Road Commercial Policy Area 25**.
 - 7 Bulky goods outlets and service trade premises should only be located in the **Bulky Goods Policy Area 6** or **Main Road Commercial Policy Area 25**.
 - 8 Bulky goods outlets and service trade premises should not have any adverse impacts on heavy vehicle access or freight movements.
 - 9 Restaurants and cafes should only be located in the **Main Road Commercial Policy Area 25** with a gross leasable area of 500 square metres or within bulky goods outlets or service trade premises that are larger than 2000 square metres, and should have a gross leasable area of 250 square metres or less.
 - 10 Short term workers accommodation or other sensitive uses within the zone should be designed and located to ensure the ongoing operation of any existing activity within the zone is not impeded.
 - 11 Dwellings and dwelling additions should be designed in a manner to not restrict operations of adjacent existing or potential non-residential land uses envisaged in the zone by locating noise sensitive rooms and private open spaces away from noise sources, or by the use of appropriate noise attenuation techniques to minimise land use conflicts.
 - 12 The **Core Industry Policy Area 26** should only be developed for land uses that are ancillary and in association with envisaged land uses.
 - 13 Facilities for the handling, storage and dispatch of commodities in bulk should be sited, designed and operated to minimise risks of contamination to the environment and adverse impacts on nearby sensitive land uses and from surrounding land uses.

Form and Character

- 14 Development should not be undertaken unless it is consistent with the desired character for the zone.
- 15 Development should not exceed a maximum height of 12 metres.
- 16 Development on land that directly adjoins the boundary of another zone intended to accommodate residential land uses or a public reserve should be setback 3 metres from the shared boundary or sited on the shared boundary where buildings are constructed of solid masonry or other sound attenuating materials.

- 17 Any building or structure on, or abutting the boundary of **another zone intended to accommodate residential land uses or public open space should be** should be restricted to a height of 3 metres above ground level at the boundary and a plane projected at 31 degrees above the horizontal into the development site from that 3 metre height, as shown in the following diagram:



- 18 Building façades facing **another zone intended to accommodate residential land uses** should not contain openings or entrance ways that would result in the transmission of noise or light spillage that would adversely affect the amenity of nearby residents.
- 19 **Building facades to the primary street frontage should:**
- (a) provide a **minimum 50 per cent** glazing to promote activity and passive surveillance
 - (b) comprise a variety of materials, articulation, design features such as verandas and avoid blank walls.
- 20 **External walls constructed of metal clad pre-coloured or painted materials should comprise less than 50 per cent of the facade of the primary street frontage buildings and structures.**
- 21 Any plant or equipment with potential to cause an environmental nuisance (including a chimney stack or air-conditioning plant) should be sited as far as possible from the adjoining boundary of **another zone intended to accommodate residential land uses** and should be designed to minimise its effect on the amenity of the locality.
- 22 Development should control noise emissions through the use of attenuation devices and sound proofing, particularly activities requiring extended hours of operation.
- 23 Within 50 metres of **the boundary of another zone intended to accommodate residential land uses**, non-residential development (including loading and unloading activities and waste management) should:
- (a) demonstrate appropriate acoustic performance
 - (b) ensure that all noise sources including machinery, loading, unloading and other service areas are located within the building.
- 24 **Non-residential development should provide landscaping comprising at least 10 per cent of the site area.**
- 25 **Landscaping for non-residential development should shade car parking areas with trees interspersed between every third carpark.**

- 26 Car parking areas for non-residential development between a building and a street should be setback a minimum of 3 metres from the street boundary with a landscape strip planted with trees and groundcovers incorporated within that setback, unless otherwise specified in the relevant policy area or precinct.
- 27 Except where stated below, a landscaped buffer of 3 metres should be provided where development abuts the boundary of another zone intended to accommodate residential land uses, a railway corridor or public reserve:

Designated Area	Minimum landscape buffer (Meters)
Glyde Street, Albert Park	8
Tapleys Hill Road and West Lakes Boulevard, Hendon	5
Findon Road and Valetta Road, Kidman Park	5
The western boundary of 414-450 Findon Road, Kidman Park	15
All roads west of Wilson Street, Royal Park (except for Frederick Road and Old Port Road)	5
Frederick Road, Royal Park	8

- 28 Proposed buildings and structures on sites adjacent to the River Torrens should be setback 8 metres from the river front boundary to allow for their screening through appropriate landscaping.
- 29 Advertisements and advertising hoardings should not include any of the following:
- (a) flashing or animated signs
 - (b) bunting, streamers, flags or wind vanes
 - (c) roof-mounted advertisements projected above the roofline
 - (d) parapet-mounted advertisements projecting above the top of the parapet.
- 30 Development involving the reception, storage or transfer of waste should satisfy all of the following:
- (a) be located at least 300 metres from any residential or other sensitive land use
 - (b) not be located in the suburb of Royal Park
 - (c) not involve the processing, treatment or disposal of waste
 - (d) not have an annual throughput or potential to receive, store or transfer greater than 10 000 tonnes per annum
 - (e) involve the unloading, storage or loading of waste inside a fully enclosed and roofed building with an impervious floor
 - (f) minimise adverse impacts to adjoining land uses by intensive landscaping and aesthetic screening of the activities
 - (g) not be located fronting an arterial road, as indicated on Overlay Maps - Transport, or significant public space unless the frontage comprises buildings or structures that present a high quality appearance

- (h) only occur on Certificates of Title 5612/733 (Allotment 10, FP 122257) or 5105/952 (Allotment 19, DP 13164) or 5105/969 (Allotment 51, DP 25157) where it can be demonstrated that it will result in a reduction of any noise, dust, odours or other pollutants, traffic generation
- (i) over-shadowing, signs, light spill, vibration and other impacts which might adversely affect the amenity of the locality.

Land Division

- 31 Land division should create allotments that are of a size and shape suitable for the intended use.
- 32 Land division should ensure that new allotments do not have direct access to the following roads unless they have a frontage of at least 30 metres:
 - (a) Hanson Road and Grand Junction Road, Athol Park
 - (b) William Street (north of the National Basketball arena), Beverley
 - (c) Torrens Road, Brompton
 - (d) Tapleys Hill Road, Royal Park.

Movement Systems

- 33 Development should be designed and located to discourage commercial and industrial vehicle movements through residential streets.
- 34 Development serviced by frequent heavy vehicles or which are high traffic generators should be located in the **Core Industry Policy Area 26**.

Bulky Goods Policy Area 6

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area accommodating a range of bulky goods outlets, industry, offices, service trade premises and warehouse land uses.
- 2 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

Precinct 44 Woodville Bulky Goods

Built form in this part of the policy area is presently characterised by traditional, large-scale, bulky factories and warehouse buildings which contribute to a poor appearance. Opportunities exist to develop under-utilised sites. Parts of the policy area provide a transition to adjoining residential areas. Appropriate land uses include bulky goods outlets, industry, service trade premises and warehousing.

Residential areas do experience impact from some activities in terms of visual appearance, building bulk, minimal landscaping, noise, air quality, overshadowing, 24 hour operation, traffic and on-street carparking. Proximity to residential areas also acts as a constraint on operations.

The area should develop as an intense, high quality, landscaped area. Industry and related operations may operate 24 hours a day. The area should be enhanced through quality development including redevelopment of sites with high quality materials, landscaping and elements to create an attractive and well planned area.

The area should accommodate activities that do not create any appreciable nuisance, that have minimal off-site impacts, such as noise, air, water and waste emissions, traffic generation and movement and which do not have a detrimental impact on the amenity of properties in residential areas. Appropriate activities include bulky goods outlets and a limited range of service trade premises, service, light industry, warehousing and distribution and commercial premises.

Larger scale structures are appropriate within the area. The area should be enhanced through the incorporation of landscaping, particularly along residential interfaces. Landscaping should be established near arterial roads to soften the visual impact of the development. Street trees of appropriate scale should be planted to improve amenity and to provide pedestrian shelter. Screen landscaping of a minimum of 5 metres in depth should be established along the railway line to soften the visual impact of development on the adjacent **Residential Zone** and so as to integrate with screen landscaping in the Urban Employment **Zone**.

Provision should be made for a landscaped bikeway along the length of the railway line.

Development should present an attractive and prominent built form to the Port Road / Cheltenham Parade corner at Woodville as well as public viewpoints by ensuring:

- (a) the massing of buildings provide interest and scale through articulation of form and facades and the use of texture, pattern, graphics and colour
- (b) buildings incorporate glazing to all public frontages to increase the solid to void ratio of external surfaces
- (c) large expanses of carparking are avoided; with car parking predominantly located to the rear and side of any buildings.

Access for heavy, service and delivery vehicles should primarily be from Cheltenham Parade.

Precinct 45 Old Port Road

This part of the policy area is presently characterised by traditional industrial and commercial buildings of poor condition and quality, with unscreened service and storage area, little landscape planting, and a poor level of visual presentation and amenity.

The area should be developed with buildings displaying a high quality of presentation and architectural standard, with car parking adjacent which is complemented by landscape plantings and fencing. While large structures are appropriate within this area, their appearance should be enhanced through the incorporation of planting and fencing, with suitable setbacks to Old Port Road to allow for car parking.

Built form should display a high level of presentation, with elevation treatments that both modulate and articulate facades, so as to provide visual interest and variety in form, scale and appearance. The visual bulk of larger buildings should be minimised.

Appropriate land uses include bulky goods outlet, service industry, light industry, service trade premises, motor repairs, warehousing and commercial premises with associated office and car parking.

Developments comprising multiple tenancies should be undertaken in an integrated manner that assists in achieving a high level of presentation and efficient operation in terms of traffic movement.

Loading and unloading areas should be screened from public view. Landscaping should be established near arterial roads and adjacent to residential areas to soften the visual impact of the development. Street trees of appropriate scale should be planted to improve amenity and to provide pedestrian shelter.

Development should present an attractive and prominent built form to Old Port Road as well as public viewpoints by ensuring:

- (a) the massing of buildings provide interest and scale through articulation of form and facades and the use of texture, pattern, graphics and colour
- (b) buildings incorporate glazing to all public frontages to increase the solid to void ratio of external surfaces
- (c) large expanses of carparking are avoided; with car parking predominantly located to the rear and side of any buildings.

Access along Old Port Road should be consolidated and shared between developments. Access to Dixon Street should serve car parking areas with access for heavy, service and delivery vehicles being via Old Port Road, Clovelly Avenue and Hardy Street. Access to Old Port Road should be in accordance with [Concept Plan Map ChSt/28 - Old Port Road, Royal Park](#).

Precinct 83 Woodville North

Built form in this part of the policy area is presently characterised by traditional, large-scale, bulky factories and warehouse buildings. Opportunities exist to develop under-utilised sites. Parts of the policy area provide a transition to adjoining residential areas.

The area should be developed with buildings displaying a high quality of presentation and architectural standard to Torrens Road, with car parking adjacent which is complemented by landscape plantings and fencing. While large structures are appropriate within this area, their appearance should be enhanced through the incorporation of planting and fencing, with suitable setbacks to Torrens Road to allow for car parking.

Built form should display a high level of presentation, with elevation treatments that both modulate and articulate facades, so as to provide visual interest and variety in form, scale and appearance. The visual bulk of larger buildings should be minimised.

Appropriate land uses include bulky goods outlet, industry, service trade premises, warehousing and distribution and commercial premises.

Developments comprising multiple tenancies should be undertaken in an integrated manner that assists in achieving a high level of presentation and efficient operation in terms of traffic movement.

Loading and unloading areas should be screened from public view. Landscaping should be established near arterial roads and adjacent to residential areas to soften the visual impact of the development. Street trees of appropriate scale should be planted to improve amenity and to provide pedestrian shelter.

Development should present an attractive and prominent built form to Torrens Road as well as public viewpoints by ensuring:

- (a) the massing of buildings provide interest and scale through articulation of form and facades and the use of texture, pattern, graphics and colour
- (b) buildings incorporate glazing to all public frontages to increase the solid to void ratio of external surfaces
- (c) large expanses of carparking are avoided; with car parking predominantly located to the rear and side of any buildings.

Access along Torrens Road should be consolidated and shared between developments. For the portion of the precinct between Carlton Crescent and Audley Street, access for heavy, service and delivery vehicles should generally be via Sheffield Street. For the portion of the precinct located between Audley Street and Burwood Avenue, access is to be via Audley Street and Burwood Avenue, with access permitted to Torrens Road. For the portion of the precinct located between Burwood Avenue and Fletcher Street, access should be via Burwood Avenue and a consolidated single access on Torrens Road.

Access points to Torrens Road should be minimized in number, shared between developments and should be located at least 70 metres from the Carlton Crescent, Hill Smith Boulevard, Audley Street, Burwood Avenue and Fletcher Street junctions. Access to the precinct should be in accordance with [Concept Plan Map ChSt/29 - Torrens Road, Woodville North](#).

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development are envisaged in the policy area:
 - bulky goods outlet
 - industry
 - light industry
 - motor repair station
 - office
 - service industry
 - service trade premises
 - warehouse.
- 2 Bulky goods outlets and service trade premises should have a gross leasable area of 500 square metres or more for each individual tenancy.

Form and Character

- 3 Development should not be undertaken unless it is consistent with the desired character for the policy area.
- 4 Development should not exceed 12 metres in height.

Land Division

- 5 Land division should create allotments that have an area of not less than 5000 square metres unless intended for a specific purpose consistent with the policy area provisions and for which a lesser site area requirement can be demonstrated.

PRECINCT SPECIFIC PROVISIONS

Refer to the [Map Reference Tables](#) for a list of the maps that relate to these precincts.

Precinct 44 Woodville Bulky Goods

- 6 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 7 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - industry
 - service trade premises that comprise only indoor displays or are primarily for the sale, rental or display of building materials, landscaping materials, sheds, domestic garages or outbuildings
 - warehouse.
- 8 Development that will generate medium or high levels of traffic is inappropriate on sites fronting Cheltenham Parade unless the site has a street frontage of at least 50 metres.
- 9 Only one access point should be provided off Port Road east of Cheltenham Parade and should be at least 50 metres east of the intersection of the two roads.

Precinct 45 Old Port Road

- 10 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 11 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - industry
 - motor repair station
 - office
 - service industry
 - service trade premises
 - warehouse.
- 12 Access points to Old Port Road should be minimized in number, shared between developments and should be at least 50 metres west of the junction with Hardy Street and at least 50 metres east of the junction with Clovelly Avenue in accordance with [Concept Plan Map ChSt/28 - Old Port Road, Royal Park](#).
- 13 Development should be in accordance with [Concept Plan Map ChSt/28 - Old Port Road, Royal Park](#).

Precinct 83 Woodville North

- 14 Development should not be undertaken unless it is consistent with the desired character for the precinct.
- 15 The following forms of development are envisaged in the precinct:
 - bulky goods outlet
 - industry
 - office
 - service trade premises
 - warehouse.
- 16 Development should be in accordance with [Concept Plan Map ChSt/29 - Torrens Road, Woodville North](#).

Main Road Commercial Policy Area 25

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area that accommodates a range of retail, commercial and light industrial land uses along arterial road frontages.
- 2 Development that acts as an interface between Core Industry Policy Area 26 and surrounding zones which contain more sensitive land uses.
- 3 A high standard of development which promotes distinctive building, landscape and streetscape design, with high visual and environmental amenity.
- 4 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The policy area will have a strong employment focus, accommodating local shops (including cafes and restaurants), consulting rooms, offices, retail showrooms, service trade premises, recreation facilities and training centre.

These uses will support an expanding workforce, provide support services for business, reflect existing commercial land uses, encourage a wider range of employment generating land uses and improve the interface between surrounding zones which contain more sensitive land uses. A mix of complementary land uses will assist in extending the usage of the policy area beyond normal working hours to enhance its vibrancy and safety.

Development in this policy area will need to be cognisant of the nearby industrial and residential uses and be designed to minimise the impacts to and from these land uses.

Buildings will be varied in form and will incorporate articulated facades, careful detailing and a variety of building materials to create visual interest.

As much of the policy area fronts arterial roads it is important to establish a well-designed character and format that enhances the attractiveness of these roads.

Vehicle access points should be consolidated with access points located off side streets and rear laneways where possible, so that vehicle flows, safety and efficient pedestrian movements are achieved. It is desirable to integrate rear parking areas between adjoining sites. It will be inappropriate to allow any vehicle to back onto arterial roads and should be addressed through design.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development, or combination thereof, are envisaged in the policy area:
 - bulky goods outlet
 - consulting rooms
 - light industry
 - office
 - petrol filling station
 - retail showrooms

- service industry
 - service trade premises
 - shops or groups of shops
 - training centre
 - warehouse.
- 2 Shop or groups of shops should have a gross leasable area of less than 500 square metres.

Form and Character

- 3 Development should be consistent with the desired character of the policy area.
- 4 Vehicle parking should be predominantly located at the rear or underneath buildings.
- 5 The ground floor of buildings should be built to allow for adaptation to a range of land uses without the need for significant change to the building.

Core Industry Policy Area 26

Refer to the [Map Reference Tables](#) for a list of the maps that relate to this policy area.

OBJECTIVES

- 1 An area accommodating a range of industrial land uses together with other related employment and business activities.
- 2 Provision for large floor plate enterprises, such as major logistics and manufacturing plants, high technology and/or research and development related uses, located to take advantage of existing transport networks.
- 3 Development that contributes to the desired character of the policy area.

DESIRED CHARACTER

The area will be developed as an intensively industrialised, high quality, landscaped, 24 hour operation area. High impact industrial uses should locate in this area, which is remote from residential areas. The area is suitable for a wide range of scales and types of industrial activities including modern warehousing and distribution type premises, large well established manufacturing and engineering premises, transportation premises and food processing, as well as a range of smaller industrial and commercial and business activities, including crash repairs. The area will be protected from the intrusion of residential and other inappropriate uses which will reduce the land resource for employment uses or create potential for land use conflicts.

Parts within the area have intermingled residential uses which experience industrial impacts in terms of visual amenity, noise and traffic. Dwellings retained for residential use in these areas should recognise the industrial location and implement sound attenuation, screening and other measures to protect their own residential amenity. In Beverley, conflicts between intermingled housing and industry are recognised and should be minimised by industry land uses through a high standard of design, implementing sound attenuation measures and extensive landscaping to enhance visual amenity particularly along residential interface.

The creation of larger allotments is encouraged for uses such as transport/logistics operations and associated warehousing in location that would benefit from a high level of accessibility to major roads.

To improve the amenity the enhancement of the area will be undertaken through quality development including redevelopment of sites with high quality materials, landscaping and elements to create an attractive and well planned industrial area.

PRINCIPLES OF DEVELOPMENT CONTROL

Land Use

- 1 The following forms of development, or combination thereof, are envisaged in the policy area:
 - electricity substation
 - fuel depot
 - high-tech, bio-tech, research and development
 - indoor recreation centre
 - industry (other than special industry)
 - motor repair station
 - office
 - prescribed mains

- road transport terminal
 - shops or groups of shops
 - training centre
 - store
 - warehouse.
- 2 Shops should have a gross leasable area less than 250 square metres.

Form and Character

- 3 Development should be consistent with the desired character for the policy area.

PROCEDURAL MATTERS

Complying Development

Complying developments are prescribed in Schedule 4 of the *Development Regulations 2008*.

In addition, the following forms of development outside of the 'Industry Interface Area' as shown on *Overlay Maps – Development Constraints*, **Main Road Commercial Policy Area 25**, **Bulky Goods Policy Area 6** and **Historic Conservation Areas** located in Chapel Street, Hindmarsh are designated as complying subject to the condition contained in [Table ChSt/1 – Conditions for Complying Development](#):

- general industry
- light industry
- motor repair station
- road transport terminal
- service industry
- service trade premises
- store
- warehouse.

Non-complying Development

Development (including building work, a change in the use of land, or division of an allotment) involving any of the following is non-complying:

Form of Development	Exceptions
Advertisement or advertising hoarding	Except where the advertisement or advertising hoarding: <ol style="list-style-type: none"> (a) does not move, rotate or incorporate flashing light(s) (b) has no part that projects above the walls or fascia where attached to a building (c) covers less than 10 per cent of the total surface area of a wall oriented to a public road or reserve (d) does not include bunting, streamers, flags or wind vanes.
Amusement machine centre	Except where located in the Main Road Commercial Policy 25 .
Bulky goods outlet	Except where: <ol style="list-style-type: none"> (a) located in the Bulky Goods Policy Area 6 (b) located in the Main Road Commercial Policy Area 25.
Caravan or residential park	
Community centre	Except where: <ol style="list-style-type: none"> (a) located in the Main Road Commercial Policy Area 25 (b) located in the 'Industry Interface Area' as shown on the <i>Overlay Maps - Development Constraints</i>.
Consulting room where located within the Core Industry Policy Area 26	
Dwelling	Except for: <ol style="list-style-type: none"> (a) short term accommodation that is ancillary to and in association with industry (b) minor alterations and additions to existing dwellings (c) a caretaker's residence.

Form of Development	Exceptions
Horticulture	
Hospital	
Intensive animal keeping	
Motel	
Nursing home	
Office where located within the Core Industry Policy Area 26	Except where ancillary to and in association with envisaged land uses.
Place of worship where located within the Core Industry Policy Area 26	Except for alterations and additions to an existing place of worship.
Prescribed mining operations	
Pre-school	
Primary school	Except for alterations and additions to an existing primary school.
Secondary school	
Special industry	
Stadium	
Tourist accommodation	
Waste reception storage treatment or disposal	Except where it involves the remediation of land or where the following applies: <ul style="list-style-type: none"> (a) it is located more than 300 metres from the Residential Zone (b) it is located outside of the suburb of Royal Park (c) it involves the annual throughput or potential to receive, store or transfer of less than 10000 tonnes of waste per annum (d) the waste is to be received, stored, unloaded or loaded or otherwise transferred inside the confines of a fully enclosed roofed building with an impervious floor.
Waste reception storage treatment or disposal on land shown within the ' Industry Interface Area ' as shown on the <i>Overlay Maps - Development Constraints</i> .	
Wrecking yard on land shown within the ' Industry Interface Area ' as shown on the <i>Overlay Maps - Development Constraints</i>	

Public Notification

Categories of public notification are prescribed in Schedule 9 of the *Development Regulations 2008*.

In addition, the following forms of development, or any combination thereof (except where the development is classified as non-complying), are designated:

Category 1	Category 2
<p>All kinds of development except where the site of the proposed development is located within 60 metres of a Residential Zone or another zone intended to accommodate residential land uses.</p>	<p>Development where the site of the proposed development is located within 60 metres of a Residential Zone or another zone intended to accommodate residential land uses.</p>
<p>Partial or total demolition of a building or structure that is not a State Heritage Place that is described in Table ChSt/7 – State Heritage Places or Local Heritage Place that is described in Table ChSt/6-Local Heritage Places or Contributory Item within the Historic Conservation Area that is described in Table ChSt/5 – Contributory Items.</p>	<p>Partial or total demolition of a Contributory Item that is described in Table ChSt/5 – Contributory Items within the Historic Conservation Area.</p>
	<p>Partial or total demolition of a building or structure, alteration, addition or any work that materially affects the heritage value of a Local Heritage Place that is described in Table ChSt/6 – Local Heritage Places.</p>

Table Section

Table ChSt/1 - Conditions for Complying Development

Form of Development	Compliance Criteria / Conditions
All types of development	<ol style="list-style-type: none"> 1 No building being erected added to or altered on any land so that any portion of such building is erected, added to or altered nearer to the existing boundary of a road, or to the boundary of any land shown as being required for road widening on the Plan deposited under the provisions of the <i>Metropolitan Adelaide Road Widening Plan Act, 1972-1976</i>, than six metres (or when the site has more than one boundary to a road in which case the distance to the secondary road is three metres), unless otherwise prescribed in this Table. 2 The finished floor level of any building being erected, added to or altered being a minimum of 300 millimetres above the level of the adjacent street watertable (gutter). 3 The development does not result in the removal or damage to significant tree/s. 4 The development not involving or adjoining a State Heritage Place or Local Heritage Place. 5 The development not involving direct vehicular access to or adjoining an arterial road or a distributor road where there is a current public transport service, as indicated on the <i>Overlay Maps - Transport</i>.
All types of development adjacent to the Coast (except where standards for set-back and elevation of buildings are specified)	<ol style="list-style-type: none"> 1 No part of the site of the development lying within 100 metres of: <ol style="list-style-type: none"> (a) mean high water mark (b) the highest visible tidal debris or seaweed line seaward toe of any sand dunes that abut the beach; and the top of any seacliff or erosion escarpment that abuts the beach or sea, <p>whichever applies at the location and whichever provides the furthest distance from the sea.</p> 2 Any building site being at least 0.3 metres above the 100-year average return interval extreme water level (tide and stormwater combined) and associated wave effects plus an allowance for any land subsidence to the year 2050. 3 The building floor level being at least 0.25 metres above the minimum site level set out in 2 above. 4 The planning authority having given a certificate that it is satisfied that there are practical measures available to the developer and to subsequent owners to protect the development to withstand a further 0.7 metre sea level rise. 5 All effluent is to be disposed of in land based effluent disposal system situated at least 100 metres in from the high water mark.

Form of Development	Compliance Criteria / Conditions
<p>All complying forms of development (within Precinct 81 Beverley of the Stadium Zone)</p>	<ol style="list-style-type: none"> 1 All events at the stadium and surrounding open space areas, except use of the restaurant, not to extend beyond 11.00 pm Monday to Saturday and 6.00 pm Sunday. 2 All events at the stadium and surrounding open space areas, except the use of the restaurant, ensure the car parking areas are cleared by 12.00 pm Monday to Saturday and 7.00 pm Sunday. 3 Access to the car park be from the main entry off Crittenden Road, with secondary access from the southern William Street gate and Toogood Avenue. The northern William Street gate be limited to hospitality suite guests and taxis. 4 Egress from the car park be via the main entry to Crittenden Road with secondary egress to Toogood Avenue (right turn only) and the William Street southern gate (right turn only). 5 Car park floodlighting being switched off within 15 minutes of the clearance of patrons vehicles from the car park after an event at the stadium. 6 No amplified music to be played in the stadium when patrons are leaving the building at the end of an event. 7 No sound amplification to be conducted outside the stadium, within the boundaries of the zone. 8 The recreation oval and adjoining grassed car parking areas being accessible for public use and enjoyment at all times except when the areas are needed for car parking or other activities associated with events at the stadium. 9 There is no collection of parking fees on entering the grounds for indoor non-sporting events exceeding 750 persons.
<p>Non-habitable Outbuilding</p>	<ol style="list-style-type: none"> 1 Being located no closer to the primary frontage than 5.5 metres or not less than 0.5 metres behind the main face of the associated dwelling, whichever provides the greater setback, and no closer than 5.5 metres to a secondary frontage. 2 Having a maximum floor area of: <ol style="list-style-type: none"> (a) 7.5 per cent of site area, where the site area is less than 500 square metres; (b) 10 per cent of site area or 60 square metres, whichever is the less, where the site area is greater than 500 square metres. 3 Having a maximum height to eaves of 2.7 metres. 4 Being 0.9 metres from any side or rear boundary. 5 Roof pitch being no greater than 30 degrees. 6 Does not reduce the available area of private open space to less than that required pursuant to Council Wide principles of development control.

Form of Development	Compliance Criteria / Conditions
General industry Light industry Motor repair station Road transport terminal Service industry Service trade premises Store Warehouse (Outside of the Industry Interface Area, Main Road Commercial Policy Area 25, Bulky Goods Policy Area 6 and Historic Conservation Areas located in Eighth Street, Bowden and Chapel Street, Hindmarsh)	<p>1 The development does not involve an activity of environmental significance or major environmental significance identified in Schedules 21 and 22 of the <i>Development Regulations 2008</i>.</p> <p>2 The development is designed as follows:</p> <ul style="list-style-type: none"> (a) the development site is greater than 60 metres from the nearest residential zone boundary (b) all activity associated with the development can be accommodated within the boundaries of the site/s (c) buildings adjacent public streets are designed to overlook the street and have a maximum unarticulated length of 30 metres (15 metres for offices) (d) the street facade of any building comprises one or more of the following: masonry, brick, stone, concrete block, concrete tilt up slab, rendered masonry, painted-patterned cement fibre sheeting, glass, timber or similar, with pre-coloured metal sheeting located behind the street facade (e) any building is setback from a primary road frontage the same distance of one or the other of the adjoining buildings or the average of the setbacks of adjoining buildings. Buildings should be setback to secondary road frontages not less than 3 metres except where adjoining buildings are closer than 3 metres in which case buildings should have a similar setback (f) building height does not exceed airport building heights shown on <i>Overlay Maps – Development Constraints</i> and is no greater than 12 metres (g) all outside loading and unloading and goods storage areas including waste storage areas should be screened by solid fencing or dense screen landscaping (h) a site coverage does not exceed 60 per cent. <p>3 Landscaping comprises:</p> <ul style="list-style-type: none"> (a) an area not less than 10 per cent of the site (b) a landscaped setback 3 metres wide along any street boundary, except where a building is setback a lesser distance from any street boundary in which case the intervening setback is landscaped (c) a mix of species expected to grow to less than 0.5 metres height and species expected to grow with clear stems to 2 metres height and with the canopy above (d) mulch and automatic irrigation systems (e) trees interspersed in between every third park. <p>4 The development comprises a maximum of two advertising displays, each of which does not exceed 2 square metres, is located below the parapet of a building or if free standing is less than 3 metres in height and where the signage element (when installed) does not encroach upon the public road reserve.</p>

Form of Development	Compliance Criteria / Conditions
5	<p>A maximum of one pylon sign per site that:</p> <ul style="list-style-type: none"> (a) has a maximum height of 6 metres (b) has a maximum area of 8 square metres (c) is located between the building and the front property boundary
6	<p>A maximum of one flush wall sign per site that:</p> <ul style="list-style-type: none"> (a) has a maximum area of 8 square metres (b) is erected on the building façade (c) is located below the parapet of the building.
7	<p>Fencing exceeding 2.1 metres in height (including colour – coated wire mesh fencing) adjacent to public roads should be setback in one of the following ways:</p> <ul style="list-style-type: none"> (a) in-line with the building façade (b) behind the building line (c) behind a landscaped area that softens its visual impact.
8	<p>All buildings, including the associated filling of land, are sited, designed and constructed to prevent the entry of floodwaters in a 1-in-100 year average return interval flood event.</p>
9	<p>All stormwater drainage is retained and treated on-site or connected to an approved stormwater management scheme.</p>
10	<p>Areas used for loading or unloading of materials or for the storage of chemicals and materials used in industrial operations and processes are to incorporate bunding or containment facilities that:</p> <ul style="list-style-type: none"> (a) prevent the entry of external stormwater (b) contain any spilt materials from entering the stormwater system.
11	<p>Carparking should be provided at the following rates whether forming part or all of the development on a site:</p> <ul style="list-style-type: none"> (a) for an industrial, warehouse, storage or similar activity: <ul style="list-style-type: none"> (i) one car park space for each 50 square metres or part thereof for the first 200 square metres of total floor area (ii) one car park space for each 75 square metres or part thereof used where the total floor area exceeds 200 square metres but is less than 2000 square metres (iii) one car park space for every 150 square metres or part thereof where the total floor area exceeds 2000 square metres or 75 per cent of the number of employees where the development is labour intensive (b) for office uses, one carpark space for every 25 square metres total floor area (c) for shop/s, seven carpark spaces for every 100 square metres gross floor area (d) for showroom uses, one carpark space for every 20 square metres total floor area.

Form of Development	Compliance Criteria / Conditions
	12 All vehicles are able to enter and exit the site in a forwards direction.
	13 All loading and/or unloading of vehicles to occur within the boundaries of the site.
	14 Waste collection and storage area provided which are: (a) screened and separated from adjoining areas (b) designed to ensure that wastes do not contaminate stormwater or enter the stormwater collection system.

Table ChSt/2 - Off Street Vehicle Parking Requirements

The following off street vehicle parking requirements apply except where otherwise stated in [Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas](#).

Form of Development	Number of Required Car Parking Spaces		
Bulky goods outlet	3 car parking spaces for every 100 square metres of retail area.		
Carparking for people with disabilities	1 car park for each 100 spaces.		
Community centre	1 car park per 10 square metres of total floor area		
Consulting room	5 car parking spaces for each practitioner		
Dwelling (detached and semi-detached)	2 on site car parking spaces, one of which is covered (the second space can be tandem)		
Dwelling (group, row and residential flat building) and any dwelling within the Integrated Medium Density Policy Area 20	Dwelling size	Average spaces per dwelling where the dwelling is located within 1km of a District Centre, the Integrated Medium Density Policy Area 20, or 200 metres of a railway station, light rail or bus stop	Average spaces per dwelling in any other circumstances
	3 or more bedrooms or a floor area 130 square metres or more	1.25 plus an additional 0.25 visitor parking spaces	2 plus an additional 0.25 visitor parking spaces
	2 bedrooms or a floor area of more than 75 square metres and less than 130 square metres	1 plus an additional 0.25 visitor parking spaces	1.5 plus an additional 0.25 visitor parking spaces
	1 bedroom or a floor area of 75 square metres or less	1 plus an additional 0.25 visitor parking spaces	1 plus an additional 0.25 visitor parking spaces
Hotel	<p>(a) 1 car parking space for every two square metres of total floor area which is available to the public in a bar</p> <p>(b) 1 car parking space for every six square metres of total floor area which is available to the public in a lounge, beer garden or other licensed area</p> <p>(c) 1 per residential unit.</p>		

Form of Development	Number of Required Car Parking Spaces
Industry/warehouse/store or similar activity	<p>(a) 1 car park space for each 50 square metres or part thereof for the first 200 square metres of total floor area</p> <p>(b) 1 car park space for each 75 square metres or part thereof used where the total floor area exceeds 200 square metres but is less than 2000 square metres</p> <p>(c) 1 car park space for every 150 square metres or part thereof where the total floor area exceeds 2000 square metres or 75 per cent of the number of employees where the development is labour intensive.</p>
Meeting hall/place of worship/club/entertainment facility	1 car parking space for every three seats provided or able to be provided.
Motel	1 per room or residential unit. If a restaurant is included, additional parking at the restaurant rate is required.
Nightclub/late night venue	1 car parking space for every two square metres of total floor area which is available to the public in a bar.
Office	1 car park space for every 25 square metres of total floor area.
On site secure bicycle parking racks for non residential development	3 bicycle spaces per 50 employees.
Residential development in a Centre Zone	1 covered space per dwelling separately identified from any shared non-residential use car park
Restaurant	1 per 3 seats provided or capable of being provided.
Retirement Village not located within the Kilkenny Policy Area 3	1 space for each dwelling for independent living units plus 1 space per 2 dwellings for staff, service providers and visitors.
Retirement Village located within the Kilkenny Policy Area 3	Be provided at the rate of 1 per residential unit plus 1 individually accessible car park space per 4 units for visitor car parking. Provision should be made for the parking of boats, caravans etc in a secure area at a rate of not less than 1 such space per 6 independent living units.
Semi-detached dwelling	2 on site parking spaces, one of which his covered (the second space can be tandem).
Service trade premises	3 car parking spaces for every 100 square metres of retail area.
Shop(s)	7 car park spaces for every 100 square metres total floor area.
Student housing	1 space for every 3 students or beds.
Supported accommodation	1 space for every 3 beds.

Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas

Interpretation

- 1 The vehicle parking rates table applies to Designated Areas listed below except where:
 - (a) any applicable condition(s) is/are not met
 - (b) the zone provisions require a lesser amount of on-site vehicular parking spaces than the amount determined using the vehicle parking rates tables below.

Designated Areas

- 2 The following are Designated Areas:

Designated Area	Conditions
Urban Core Zone	None
District Centre Zone Local Centre Zone Neighbourhood Centre Zone Suburban Activity Node Zone	Any part of the development site is located in accordance with at least one of the following: <ol style="list-style-type: none"> (a) within 200 metres of any section of road reserve along which a bus service operates as a high frequency public transit service ⁽²⁾ (b) within 400 metres of a bus interchange⁽¹⁾ that is part of a high frequency public transit service (c) within 400 metres of an O-Bahn interchange⁽¹⁾ (d) within 400 metres of a passenger rail station⁽¹⁾ that is part of a high frequency public transit service⁽²⁾ (e) within 400 metres of a passenger tram station⁽¹⁾ (f) within 400 metres of the Adelaide Parklands.

⁽¹⁾ Measured from an area that contains any platform(s), shelter(s) or stop(s) where people congregate for the purpose waiting to board a bus, tram or train, but does not include areas used for the parking of vehicles

⁽²⁾ A high frequency public transit service is a route serviced every 15 minutes between 7.30am and 6.30pm Monday to Friday and every 30 minutes at night, Saturday, Sunday and public holidays until 10pm.

Applicable off-street vehicular parking requirements

- 3 Development should provide off-street vehicle parking in accordance with the table(s) below. A lesser number of parking spaces may be provided based on the nature of the development and parking conditions in the wider locality including (but not limited to) the following:
 - (a) the development is a mixed use development with integrated (shared) parking where the respective peak parking demands across the range of uses occurs at different times
 - (b) the development is sited in a locality where the respective peak demands for parking for the range of uses (existing and proposed) occurs at different times and suitable arrangements are in place for the sharing of adjoining or nearby parking areas
 - (c) the development involves the retention and reuse of a place of heritage value, where the provision of on-site parking is constrained

Table ChSt/2A - Off Street Vehicle Parking Requirements for Designated Areas

- (d) suitable arrangements are made for any parking shortfall to be met elsewhere or by other means (including a contribution to a car parking fund)
- (e) generous on-street parking and/or public parking areas are available and in convenient proximity, other than where such parking may become limited or removed by future loss of access, restrictions, road modifications or widening.
- (f) the site of the development is located within distances specified in the conditions applicable to Designated Areas for at least two different public transit modes.

TABLES: VEHICLE PARKING RATES**Table 1: Non-residential development excluding tourist accommodation**

Location of development	Desired minimum number of vehicle parking spaces	Maximum number of vehicle parking spaces
All Designated Areas (unless otherwise stated)	3 spaces per 100 square metres of gross leasable floor area	6 spaces per 100 square metres of gross leasable floor area
Urban Core Zone	3 spaces per 100 square metres of gross leasable floor area 1.5 spaces per 100 square metres of gross leasable floor area above ground floor level other than for a shop	3 spaces per 100 square metres of gross leasable floor area

Table 2: Tourist accommodation

Location of development	Desired minimum number of required vehicle parking spaces	Maximum number of vehicle parking spaces
Urban Core Zone	1 space for every 4 bedrooms up to 100 bedrooms and 1 space for every 5 bedrooms over 100 bedrooms	1 space for every 2 bedrooms up to 100 bedrooms and 1 space for every 4 bedrooms over 100 bedrooms

Table 3: Residential development, in the form of residential flat buildings and residential development in multi-storey buildings

Location of development	Rate for each dwelling based on number of bedrooms per dwelling	Plus number of required visitor parking spaces
Urban Core Zone (Bowden Urban Village and Bowden/Brompton)	0.75 per dwelling (with no bedroom limit)	None
Urban Core Zone (West Lakes)	1 per studio or 1 bedroom dwelling, except within 200 metres of a transit stop where the car parking rate should be 0.75 car parking spaces 1 per 2 bedroom dwelling 1.25 per 3 + bedroom dwelling	0.25 per dwelling

TABLE 4: Row, semi-detached and detached dwellings

Location of development	Number of bedrooms, or rooms capable of being used as a bedroom	Number of required vehicle parking spaces
Urban Core Zone (Bowden Urban Village and Bowden/Brompton)	any number of bedrooms	0.75
Urban Core Zone (West Lakes)	1 or 2 bedrooms	1
	3 + bedrooms	2

Table ChSt/3 - Suitable Outdoor Advertising Types

✓ Denotes that the sign type is suitable for the location specified

* Denotes that the sign type is not-suitable for the location specified

District Centre Zone

Zone / Policy Area Location Specific	Below Veranda Level					Veranda or Fascia Level	Above Veranda Level		Freestanding Sign / Sign Trees	
	Flush Wall Signs	Business Plates	Painted Wall Signs	Horizontal Projected Signs	Pole mounted freestanding signs	Fascia Signs	Flush Wall Signs	Flush Wall Signs		
Fulham Gardens Centre Policy Area	✓	✓	✓	✓	✓	✓	*	✓	✓	✓ (not exceeding 6 metres in height)
Hindmarsh Centre Policy Area	✓	✓	✓	*	*	✓	✓	✓ (at parapet level)	✓ (at parapet level)	*
Kilkenny Centre Policy Area	✓	✓	✓	✓	✓	✓	*	✓	✓	✓ (not exceeding 6 metres in height)
West Lakes Centre Policy Area (one of the following signs for each tenancy and one sign tree or building identification sign for each building is appropriate)	✓	✓	✓	*	✓	✓	✓	✓	✓	✓ (not exceeding 6 metres in height)
Woodville Centre Policy Area	✓	✓	✓	✓ (under awning)	✓ (on Port Road frontage and on Woodville Road south of Port Rd)	✓	*	✓	✓	*

Urban Employment Zone

Zone / Policy Area Location Specific	Below Veranda Level				Veranda or Fascia Level	Above Veranda Level	Freestanding Sign / Sign Trees	
	Flush Wall Signs	Business Plates	Painted Wall Signs	Horizontal Projected Signs	Fascia Signs	Flush Wall Signs	Painted Wall Signs	
Within the entire zone	✓	✓	✓	✓ (under canopy only)	✓	✓	✓	✓ (not exceeding 6 metres in height)
Within the Bulky Goods Policy Area	(a) Be integrated into the building design, not extend beyond the top of the walls of the building and not cover more than 15 per cent of a single wall face. (b) Where more than one tenancy, not more than one wall mounted sign per tenancy, provided the sum total of the advertisement area per wall does not exceed 15 per cent.							✓ (One sign on each major road frontage not exceeding 10 metres in height)

Local Centre Zone

Zone / Policy Area Location Specific	Below Veranda Level				Veranda or Fascia Level	Above Veranda Level	Freestanding Sign / Sign Trees	
	Flush Wall Signs	Business Plates	Painted Wall Signs	Horizontal Projected Signs	Fascia Signs	Flush Wall Signs	Painted Wall Signs	
Within the entire zone	✓	✓	✓	✓ (under canopy only)	✓	✓	✓	✓ (not exceeding 4 metres in height)

Mixed Use Zone

Zone / Policy Area Location Specific	Below Veranda Level			Veranda or Fascia Level			Above Veranda Level		Freestanding Sign / Sign Trees	
	Flush Wall Signs	Business Plates	Painted Wall Signs	Horizontal Projected Signs	Pole mounted freestanding signs	Fascia Signs	Flush Wall Signs	Flush Wall Signs	Painted Wall Signs	
Within the entire zone	✓	✓	✓	✓(under canopy only)	✓	✓	✓	✓	✓	✓ (one per site with a frontage of less than 50 metres, not exceeding 6 metres in height)

Neighbourhood Centre Zone

Zone / Policy Area Location Specific	Below Veranda Level				Veranda or Fascia Level	Above Veranda Level		Freestanding Sign / Sign Trees
	Flush Wall Signs	Business Plates	Painted Wall Signs	Horizontal Projected Signs	Fascia Signs	Flush Wall Signs	Painted Wall Signs	
Welland Policy Area	✓	✓	✓	✓ (on buildings with frontages that abut Port Road)	✓	✓	✓	✓ (not exceeding 6 metres in height)
Royal Park Policy Area	✓	✓	✓	x	✓	✓	✓	✓ (except on Tapleys Hills Road south of Poplar Street)
Findon Road Policy Area	✓	✓	✓	x	✓	✓	✓	✓ (not exceeding 6 metres in height)
Findon Policy Area	✓	✓	✓	x	✓	✓	✓	✓ (not exceeding 6 metres in height)
Henley Square Policy Area	✓	✓	✓	✓	✓	✓	✓	✓ (not exceeding 4 metres in height)

Table ChSt/4 - Development Guidelines for Residential Character Zone and Local Heritage Places

1 Restoration and maintenance of a building which contributes significantly to the historic character

Component / Aspect / Issue	Guidelines for Conservation and Development
<p>1.1 General</p> <p>Property owners should understand the period of construction and design and style of the original section of the house and maintain the architectural character of their dwelling.</p>	<p>Original materials and finishes should be retained wherever possible. Repairs or maintenance should be undertaken in materials, colour, shape, profile, fixing and construction techniques which match or are compatible with the original structure.</p> <p>Inappropriate elements from another style or period should not be added to the original section of the house, and any later elements should be replaced with appropriate materials and details whenever possible. Avoid using unsympathetic materials. Do not repoint old masonry with hard cement mortar or different colour mortar, or remove paint from masonry by sand blasting or harsh chemical products.</p>
<p>1.2 Component of building</p> <p>Walls and Roof</p>	<p>Original materials and finishes of walls, particularly unpainted brickwork or stonework, should be retained. Materials closely consistent with original materials such as corrugated iron or terracotta tile roofing, should be used. Metal tiles or coloured concrete tiles should not be used.</p> <p>Contemporary features such as exhaust vents, air-conditioning units outlets and ducts, skylights and antennae should be located away from principal facades where possible.</p>
<p>Gutters and Downpipes</p>	<p>The original gutters and downpipes should be retained wherever possible. If replacement is necessary, new gutters and downpipes should closely match the original form. The profile of the metal guttering, round metal downpipes and rainheads should all be appropriate to the style of the house.</p>
<p>Windows and Doors</p>	<p>The original configuration of windows and doors should be retained to the principal elevation and any others which are viewed from the street. Original framing materials, usually timber, should be retained where practicable or replaced with the same materials.</p> <p>Modern or inappropriate features such as aluminium screens, security grilles, screen window combinations, and the replacement of timber framed windows with aluminium framed windows should be avoided.</p>

Component / Aspect / Issue	Guidelines for Conservation and Development
Verandas	The original form and detail of design of any veranda element should be maintained, including posts, brackets and friezes, projecting gables, veranda floors, entrance steps, and dividing panels, where relevant. Verandas to the principal front elevation should not be enclosed.
Colours	All painted surfaces should be maintained in good repair with use of colours of a kind similar to or compatible with original colours where they can be determined from physical or archival evidence.

2 Alterations of and additions to a building which contributes significantly to the historic character

Component / Aspect / Issue	Guidelines for Conservation and Development
Walls and Roof	<p>Materials used for walls should match or be closely compatible with the original materials of construction of the building concerned, including rendered masonry, face stone, face brick or lightweight timber construction associated with windows or glazed doors.</p> <p>Materials used for roofs should match or be closely compatible with the original materials of construction. Generally, painted corrugated iron or terra cotta tiles are the most appropriate roofing materials.</p>
Location of additions	Physical change to a building, through additions, should be in harmony with the character of the original building. The impact of visible change should be minimised and the historic architectural character of the portion of a building visible from the principal road frontage should not be significantly changed by any alteration or addition.
Form	<p>Additions should generally continue the form and elements of the building to which they are attached; for example the shape and pitch of the roof and the height and projection of eaves and gables.</p> <p>Side extensions should be sited well to the rear and should not detract from the front elevation.</p> <p>Second storey additions may be appropriate provided they repeat elements of the form of the original building, are not prominently visible from a street, do not visually dominate the existing building and do not interfere with the essential character of the building. Incorporation of attic rooms into existing large roof spaces of some styles of dwellings may be appropriate, but large scale roof conversions are generally inappropriate.</p>

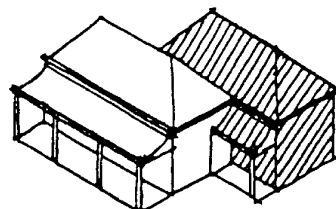
3 Appropriate Forms of Additions

Component / Aspect / Issue

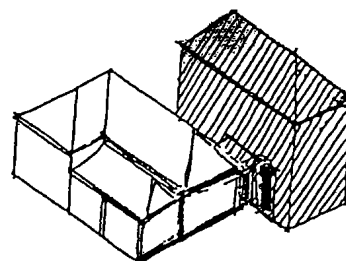
Guidelines for Conservation and Development

3.1 Pre-1870s Houses and Cottages

- (a) Simple cottage with veranda - extension repeats form and verandah element

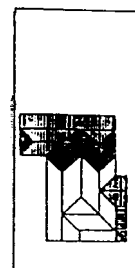


- (b) Simple cottage with side wing - two storey extension repeats form and does not visually dominate the original cottage when viewed from the street.

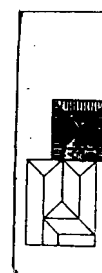
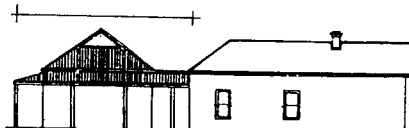


3.2 Victorian Houses and Cottages - 1870s - 1890s

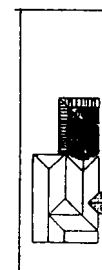
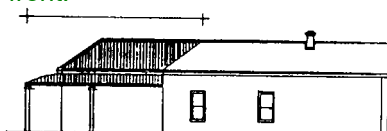
- (a) Extension of original form at rear and side - continues gutter line, roof ridges and roof form.



- (b) Pavilion addition - this creates a separately roofed element linked to the original building with a lower roofed section.



- (c) Wing addition - This extends a parallel roof ridge, maintaining the existing gutter line, ridge height and ceiling height, any separate veranda echoing the use of a veranda at the front.

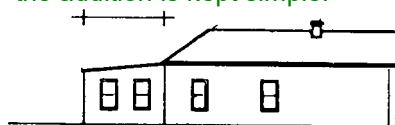


Component / Aspect / Issue	Guidelines for Conservation and Development
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- (d) Skillion and wing addition - This combines a simple skillion addition with an additional wing. This form of addition is not desirable on a site with frontage to more than one road, other than a lane.

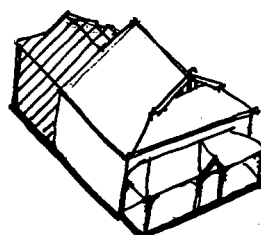


- (e) Skillion addition - The lower ceiling height and discontinuous gutter line of this traditional form of addition are generally acceptable if the form of the addition is kept simple.

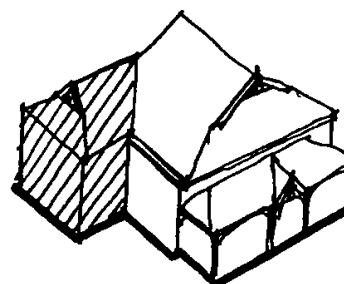


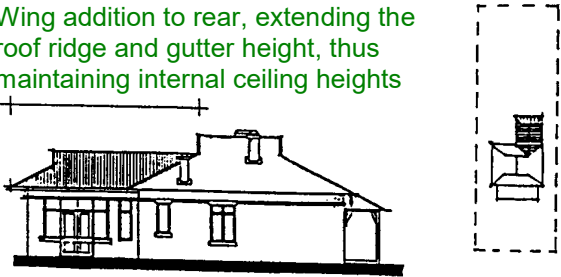
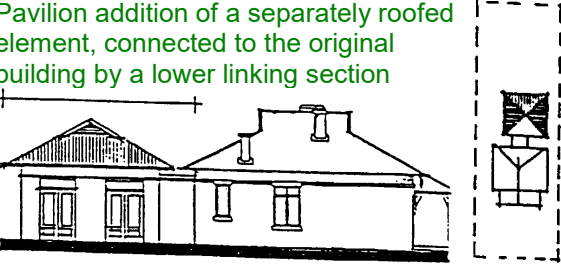
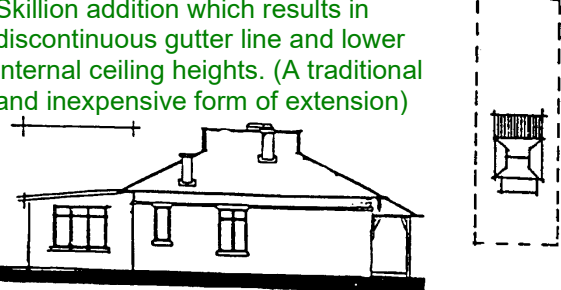
3.3 Edwardian Houses and Cottages 1900-1920s

- (a) Extension of complicated roof forms as rear addition continues the design form of the residence.



- (b) Addition to side of building, set back from front elevation, repeating roof and veranda form and detailing.



Component / Aspect / Issue	Guidelines for Conservation and Development	
3.4 Bungalows and Tudor Revival– 1920s-1930s	<p>(a) Wing addition to rear, extending the roof ridge and gutter height, thus maintaining internal ceiling heights</p> 	
	<p>(b) Pavilion addition of a separately roofed element, connected to the original building by a lower linking section</p> 	
	<p>(c) Skillion addition which results in discontinuous gutter line and lower internal ceiling heights. (A traditional and inexpensive form of extension)</p> 	
	<p>(d) Two storey additions should be set back from the verandah and front rooms of the existing dwelling, and repeat roof forms and other significant elements. The front elevation should not be substantially altered or overshadowed.</p> <p>Side extensions should preserve gable and verandah fronts, and steeply pitched roofs.</p>	
3.5 Early SA Housing Trust Houses	<p>Additions to these simple and austere houses should be to the rear and respect the low pitch of the roofs and repeat roof proportions, height and pitch. Wall materials should be face red brick or rendered masonry in appropriate colours.</p> <p>Two storey extensions should be set well back from the principal elevation and should not dominate the front section of the dwelling</p>	
3.6 Post War Houses	<p>Additions to post war houses should be complementary to the design of the original building and should follow the general principles for additions to buildings which contribute significantly to the historic character of the Zone.</p>	

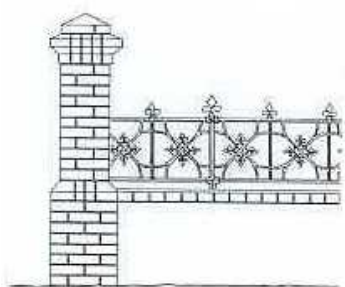
4 Fences and Gates

Component / Aspect / Issue	Guidelines for Conservation and Development
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4.1 General

Original fences and gates should be retained and reinstated where possible. Where evidence regarding the original fence is not available, a fence sympathetic to the style of the dwelling on the site to be fenced should be erected. Except on the frontage to an arterial road, the erection of high walling in concrete, masonry or timber is not appropriate where it is liable to obscure a dwelling from the street, or disrupt the existing open landscaped character of the locality.

Relatively low and open fencing is appropriate to enable visually attractive detailing of the design of a dwelling to remain visible from a road. Solid side fencing should be built of tradition materials such as timber, corrugated iron (pre-painted if desired), brush or well-detailed masonry.

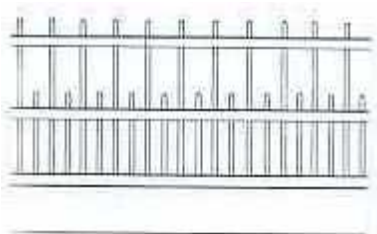


Masonry & cast iron fence

Typical traditional types of fences for house periods are:

Pre 1880 Cottages

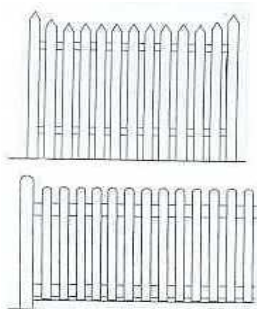
- Timber picket
- Simple masonry and cast iron palisade
- Timber dowelling



Timber dowel

1880-1900 Victorian Houses and Villas

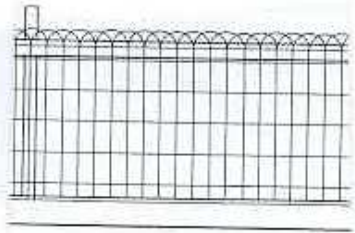
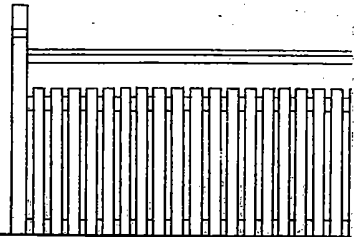
- Timber picket or dowelling
- Masonry and cast iron palisade
- Corrugated iron or mini orb within timber framing



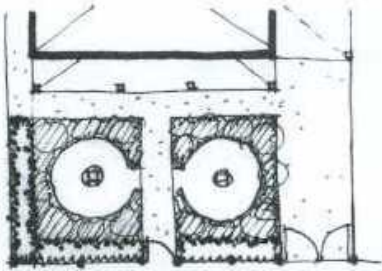
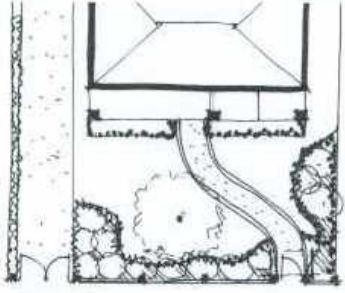
Timber pickets with round or pointed tops & posts

1900-1920 Edwardian houses

- Timber picket or dowelling
- Timber paling with timber top rail
- Corrugated iron or mini orb within timber framing

Component / Aspect / Issue	Guidelines for Conservation and Development
 <p style="color: green;">Woven crimped wire</p>	<p style="color: green;">1920s-1930s Between the Wars houses (Bungalows, Tudors, State Bank, Housing Trust)</p> <ul style="list-style-type: none"> <li style="color: green;">• Wire mesh with timber or galvanised tube framing <li style="color: green;">• Woven crimped wire <li style="color: green;">• Masonry with galvanised steel ribbon <li style="color: green;">• Brick to match house detail <li style="color: green;">• Timber paling with timber top rail
 <p style="color: green;">Timber paling with timber top rail</p>	<p style="color: green;">Post 1940s</p> <ul style="list-style-type: none"> <li style="color: green;">• Masonry base with wrought steel top band <li style="color: green;">• Wire mesh with timber or tube framing

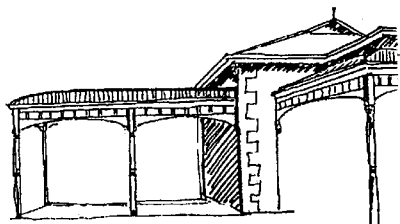
5 Gardens

Component / Aspect / Issue	Guidelines for Conservation and Development
 <p style="color: green;">Typical Victorian garden layout</p>	<p style="color: green;">Mature landmark trees, for example, of the species <i>araucaria</i>, should be retained. Planting of native species of trees close to buildings is not appropriate and may be hazardous to old buildings in highly reactive soils.</p> <p style="color: green;">Established garden areas of significance should be retained. The garden style can be designed to suit the period of the house.</p>
 <p style="color: green;">Typical 1920s garden layout</p>	

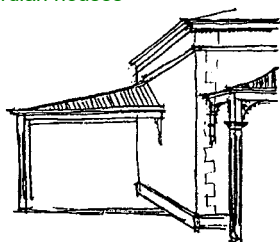
6 Carports and Garages

Component / Aspect / Issue

Guidelines for Conservation and Development



Single and double side carports suitable for Victorian and Edwardian houses



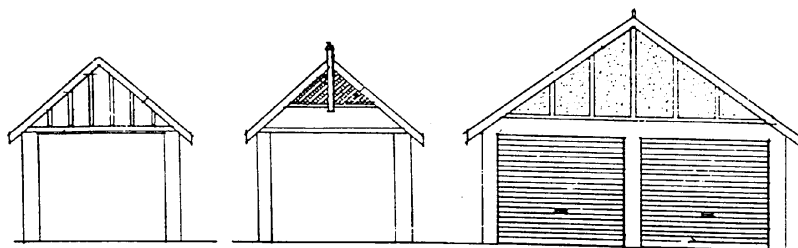
The design of new carports or garages attached to a dwelling should be an integral part of the character of the existing building, reflecting existing materials, forms, colours and detailing.

Any carport attached to the side of a house should be carefully sited in order not to obscure the front elevation and veranda form of the residence. A flat-roofed carport near to or in front of the facade of a dwelling is not appropriate.

Retention of original garages is encouraged, if these remain. A new free-standing garage should generally be located to the rear of the existing dwelling, with access past the side of the house or by a rear lane.

Front veranda extensions for carports and garages are inappropriate.

Double garages under the main roof of the dwelling are inappropriate.



Appropriate free standing garages or carports with pitched roofs and appropriate detailing to suit the house layout

7 Conversion to Semi-Detached Dwellings

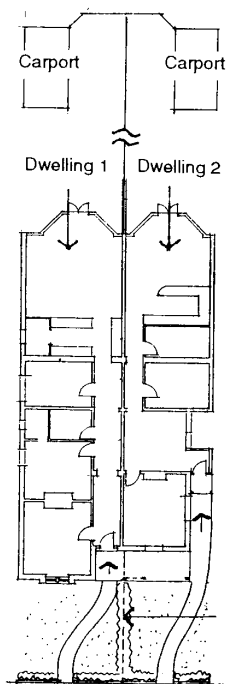
Component / Aspect / Issue

Guidelines for Conservation and Development

Conversion of an existing dwelling should be designed so that the historical architectural qualities of the dwelling which give that dwelling its contributory significance are not diminished or obscured.

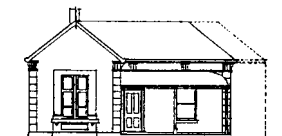
Division of the front garden of the property should be carefully aligned with a significant element of the front elevation such as a veranda post or corner of the building.

The following diagrams illustrate examples of acceptable forms of conversion of a detached dwelling to semi-detached dwellings.

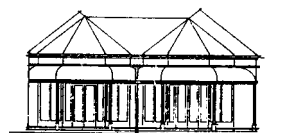


fence division to be low

traditional picket and/or hedging to align with veranda post

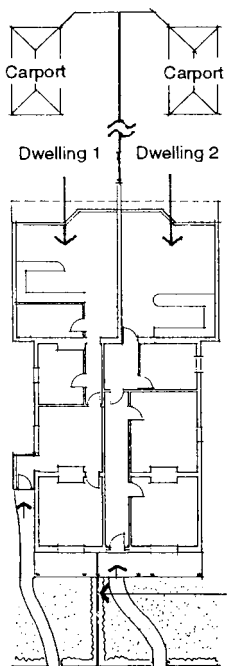


Front Elevation
(extension shown dotted)



Rear Elevation

Typical asymmetrical villa c. 1890

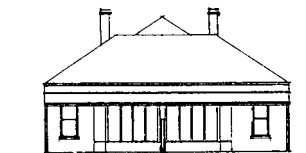


fence division to be low

traditional picket and/or hedging to align with veranda post



Front Elevation
(extension shown dotted)



Rear Elevation

Typical straight fronted villa c. 1905

8 New Residential Development Guidelines

Component / Aspect / Issue	Guidelines for Conservation and Development
<p>It is not intended to create new buildings which look old in the Historic Conservation Area. New building design should be complementary to the heritage character of adjoining buildings and the surrounding area by relating sympathetically to the established character of the street.</p>	<p>Note: Each policy area has specific minimum site area requirements</p>
<p>Scale, proportions and mass</p>	<p>The height, proportions and mass of any new development should reflect such elements predominant in the immediate area. The height of eaves of the ground floor level of a dwelling should be similar to the height of the eaves of any adjoining dwellings which contribute significantly to the historic character of the area. Two-storeyed dwellings may therefore be appropriate adjacent to dwellings which contribute significantly to the historic character of an area if elements of the form, scale and building height of the new building are compatible with those existing dwellings.</p>
<p>Materials, colours and details</p>	<p>The materials, colours and detailing used on new development should relate to the adjacent buildings and the surrounding area. Materials of a traditional kind, such as stone, red brick and rendered masonry, should be used to complement the materials used in buildings contributing significantly to the historic character of the area. The use of imitation cast iron decorative elements, or stone or slate veneer used to resemble stone is not appropriate.</p>
<p>Siting and setback</p>	<p>The set-back of a dwelling from a street should match any consistent or generally consistent set-back of existing dwellings in order to maintain the traditional arrangement of buildings in the road concerned. New development should not visually obscure views or sight lines to nearby heritage places. The relative location of two or more dwellings on a site or adjoining sites should ensure that the traditional pattern of development is maintained.</p>
<p>Openings, windows and doors</p>	<p>The proportions and spacing of door and window openings should relate to those of adjacent heritage places and the surrounding residential character. Openings in walls adjacent to a road frontage should generally have proportions similar to those of existing dwellings which contribute significantly to the historic character of the area. Materials used for framing openings should be of traditional or complementary materials. In masonry buildings, simple rendered surrounds of windows and doors are preferable to elaborate projecting quoins. Large areas of reflective glass are generally not appropriate in Historic Conservation Area.</p>
<p>Veranda</p>	<p>A front veranda should be incorporated into the design of a new dwelling, if this is a predominant element in the character of the Historic Conservation Area in which the new dwelling is to be located.</p>

Component / Aspect / Issue	Guidelines for Conservation and Development
<p>Roof form and materials</p>	<p>Corrugated iron roofing, which may be pre-painted, and terra cotta tiles should be used in preference to concrete or metal tiles. New tiling may be used if the type is carefully selected to harmonise with nearby roofing. The predominant pitch of roofs in the area, on buildings which contribute significantly to the historic character of the area should be repeated. This will generally be between 27 and 45 degrees, and new development should reflect the appropriate pitch.</p>
<p>Design of semi-detached dwellings</p>	<p>The design of semi-detached dwellings should be consistent with the guidelines for the erection of new buildings and should reinforce the historic character of the Historic Conservation Area.</p>

Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
2 Barham Street ALLENBY GARDENS	House	CT 5499/9	10748
3 Barham Street ALLENBY GARDENS	House	CT 5316/658	10749
4 Barham Street ALLENBY GARDENS	House	CT 5741/249	10750
5 Barham Street ALLENBY GARDENS	House	CT 5240/120	10751
9 Barham Street ALLENBY GARDENS	House	CT 5808/905	10756
10 Barham Street ALLENBY GARDENS	House	CT 5753/558	10757
1/ 11-15 Barham Street ALLENBY GARDENS	Single Storey Flats	CT 5183/123	10758
12 Barham Street ALLENBY GARDENS	House	CT 5753/557	10759
14 Barham Street ALLENBY GARDENS	House	CT 5749/416	10760
16 Barham Street ALLENBY GARDENS	House	CT 5207/162	10761
17 Barham Street ALLENBY GARDENS	Maisonette	CT 5069/892	10762
18 Barham Street ALLENBY GARDENS	House	CT 5377/369	10763
19 Barham Street ALLENBY GARDENS	Maisonette	CT 5512/687	10764
20 Barham Street ALLENBY GARDENS	House	CT 5709/624	10765
21 Barham Street ALLENBY GARDENS	Maisonette	CT 5714/592	10766
22 Barham Street ALLENBY GARDENS	House	CT 5340/338	10767
23 Barham Street ALLENBY GARDENS	Maisonette	CT 5714/591	10768
24 Barham Street ALLENBY GARDENS	Single Storey Flats	CT 5742/952	10769
25 Barham Street ALLENBY GARDENS	House	CT 5923/338	10770
29 Barham Street ALLENBY GARDENS	House	CT 5211/157	10771
30 Barham Street ALLENBY GARDENS	Single Storey Flats	CT 5252/908	10772
32 Barham Street ALLENBY GARDENS	Home Unit	CT 5159/168	10773
34-34A Barham Street ALLENBY GARDENS	Maisonettes	CT 5731/284	10775
35 Barham Street ALLENBY GARDENS	House	CT 5715/947	10776
36 Barham Street ALLENBY GARDENS	Maisonette	CT 5372/884	10777
36a Barham Street ALLENBY GARDENS	Maisonette	CT 5257/261	10778
37 Barham Street ALLENBY GARDENS	House	CT 1172/115	10779
39 Barham Street ALLENBY GARDENS	House	CT 5404/22	10781
40 Barham Street ALLENBY GARDENS	House	CT 5228/429	10782
41 Barham Street ALLENBY GARDENS	House	CT 5258/308	10783
42 Barham Street ALLENBY GARDENS	House	CT 5190/711	10784
43 Barham Street ALLENBY GARDENS	House	CT 5467/151	10785
45 Barham Street ALLENBY GARDENS	House	CT 5358/802	10787
47 Barham Street ALLENBY GARDENS	House	CT 5815/151	10788
48 Barham Street ALLENBY GARDENS	House	CT 5275/705	10789

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
49 Barham Street ALLENBY GARDENS	House	CT 5072/29	10790
50 Barham Street ALLENBY GARDENS	House	CT 5150/503	10791
51 Barham Street ALLENBY GARDENS	House	CT 5264/769	10792
53 Barham Street ALLENBY GARDENS	House	CT 5260/419	10793
54 Barham Street ALLENBY GARDENS	House	CT 5808/968	10794
55 Barham Street ALLENBY GARDENS	House	CT 5178/481	10795
1/ 56 Barham Street ALLENBY GARDENS	Single Storey Flats	CT 5537/180	10796
57 Barham Street ALLENBY GARDENS	House	CT 5463/468	10797
1 Birdwood Avenue ALLENBY GARDENS	House	CT 5439/588	10798
2 Birdwood Avenue ALLENBY GARDENS	House	CT 5497/806	10799
3 Birdwood Avenue ALLENBY GARDENS	House	CT 5381/510	10800
4 Birdwood Avenue ALLENBY GARDENS	House	CT 5741/697	10801
5 Birdwood Avenue ALLENBY GARDENS	House	CT 5759/216	10802
6 Birdwood Avenue ALLENBY GARDENS	House	CT 5741/698	10803
7 Birdwood Avenue ALLENBY GARDENS	House	CT 5316/819	10804
9 Birdwood Avenue ALLENBY GARDENS	House	CT 5814/474	10805
7 Brand Avenue ALLENBY GARDENS	House	CT 5423/896	10807
9 Brand Avenue ALLENBY GARDENS	House	CT 5420/482	10808
11 Brand Avenue ALLENBY GARDENS	House	CT 5751/171	10809
12 Brand Avenue ALLENBY GARDENS	House	CT 5370/870	10810
13 Brand Avenue ALLENBY GARDENS	House	CT 5751/172	10811
14 Brand Avenue ALLENBY GARDENS	House	CT 5800/99	10812
16 Brand Avenue ALLENBY GARDENS	House	CT 5758/303	10813
17-19 Brand Avenue ALLENBY GARDENS	Maisonettes & House	CT 5137/160	10814
18 Brand Avenue ALLENBY GARDENS	House	CT 5800/98	10815
20 Brand Avenue ALLENBY GARDENS	House	CT 5756/235	10816
24 Brand Avenue ALLENBY GARDENS	House	CT 5758/318	10817
1 Bridges Street ALLENBY GARDENS	House	CT 5707/811	10818
3 Bridges Street ALLENBY GARDENS	House	CT 5294/347	10819
5 Bridges Street ALLENBY GARDENS	House	CT 5250/416	10820
5 Coombe Road ALLENBY GARDENS	House	CT 5744/990	10821
7 Coombe Road ALLENBY GARDENS	House	CT 5204/486	10822
9 Coombe Road ALLENBY GARDENS	House	CT 5446/397	10823
11 Coombe Road ALLENBY GARDENS	House	CT 5454/535	10825
12 Coombe Road ALLENBY GARDENS	House	CT 5239/431	10826
13 Coombe Road ALLENBY GARDENS	House	CT 5562/941	10827
14 Coombe Road ALLENBY GARDENS	House	CT 5349/856	10828
15 Coombe Road ALLENBY GARDENS	House	CT 5563/110	10829

Charles Sturt Council
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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Coombe Road ALLENBY GARDENS	House	CT 5556/850	10830
17 Coombe Road ALLENBY GARDENS	Maisonette	CT 5330/850	10831
18 Coombe Road ALLENBY GARDENS	House	CT 5325/850	10832
19 Coombe Road ALLENBY GARDENS	Maisonette	CT 2668/159	10833
20 Coombe Road ALLENBY GARDENS	House	CT 5838/740	10834
22 Coombe Road ALLENBY GARDENS	House	CT 5532/998	10835
23 Coombe Road ALLENBY GARDENS	Maisonette	CT 5134/64	10836
25 Coombe Road ALLENBY GARDENS	Maisonette	CT 5134/65	10837
26 Coombe Road ALLENBY GARDENS	House	CT 5171/83	10838
27 Coombe Road ALLENBY GARDENS	House	CT 5551/161	10839
28 Coombe Road ALLENBY GARDENS	House	CT 5171/84	10840
29 Coombe Road ALLENBY GARDENS	House	CT 5393/12	10841
30 Coombe Road ALLENBY GARDENS	House	CT 5707/810	10842
31 Coombe Road ALLENBY GARDENS	House	CT 5205/475	10843
33 Coombe Road ALLENBY GARDENS	House	CT 5236/820	10844
34 Coombe Road ALLENBY GARDENS	House	CT 5717/947	10845
35 Coombe Road ALLENBY GARDENS	House	CT 5886/82	10846
36 Coombe Road ALLENBY GARDENS	House	CT 5761/842	10847
37 Coombe Road ALLENBY GARDENS	Maisonette	CT 5853/985	10848
37a Coombe Road ALLENBY GARDENS	Maisonette	CT 2821/39	10849
38-40 Coombe Road ALLENBY GARDENS	Maisonettes	CT 5578/970	10850
39 Coombe Road ALLENBY GARDENS	Maisonette	CT 5747/228	10851
41 Coombe Road ALLENBY GARDENS	Maisonette	CT 5747/227	10852
42 Coombe Road ALLENBY GARDENS	House	CT 5744/380	10853
43 Coombe Road ALLENBY GARDENS	House	CT 5333/206	10854
44 Coombe Road ALLENBY GARDENS	House	CT 5744/385	10855
45 Coombe Road ALLENBY GARDENS	House	CT 5533/663	10856
46 Coombe Road ALLENBY GARDENS	House	CT 5528/303	10857
47-49 Coombe Road ALLENBY GARDENS	Maisonettes	CT 5352/125	10858
50 Coombe Road ALLENBY GARDENS	Building	CT 5730/705	10859
51 Coombe Road ALLENBY GARDENS	House	CT 5713/45	10860
52 Coombe Road ALLENBY GARDENS	House	CT 5511/612	10861
53 Coombe Road ALLENBY GARDENS	House	CT 5576/902	10862
54 Coombe Road ALLENBY GARDENS	House	CT 5261/266	10863
56 Coombe Road ALLENBY GARDENS	House	CT 5748/8	10864
57 Coombe Road ALLENBY GARDENS	House	CT 5337/151	10865
58 Coombe Road ALLENBY GARDENS	House	CT 5742/7	10866
59 Coombe Road ALLENBY GARDENS	House	CT 5197/203	10867

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
60 Coombe Road ALLENBY GARDENS	House	CT 5748/6	10868
61 Coombe Road ALLENBY GARDENS	House	CT 5754/231	10869
62 Coombe Road ALLENBY GARDENS	House	CT 5541/748	10870
63 Coombe Road ALLENBY GARDENS	House	CT 5213/794	10871
64 Coombe Road ALLENBY GARDENS	House	CT 5742/5	10872
65 Coombe Road ALLENBY GARDENS	House	CT 5804/899	10873
66 Coombe Road ALLENBY GARDENS	Maisonette	CT 5845/877	10874
68 Coombe Road ALLENBY GARDENS	Maisonette	CT 5258/325	10875
70 Coombe Road ALLENBY GARDENS	House	CT 5340/114	10877
71 Coombe Road ALLENBY GARDENS	House	CT 5512/472	10878
72 Coombe Road ALLENBY GARDENS	House	CT 5069/61	10879
74 Coombe Road ALLENBY GARDENS	House	CT 5760/277	10880
76 Coombe Road ALLENBY GARDENS	House	CT 5754/779	10881
77 Coombe Road ALLENBY GARDENS	House	CT 5320/221	10882
79 Coombe Road ALLENBY GARDENS	Maisonette	CT 5398/831	10883
80 Coombe Road ALLENBY GARDENS	House	CT 5354/549	10884
81 Coombe Road ALLENBY GARDENS	Maisonette	CT 2175/42	10885
82 Coombe Road ALLENBY GARDENS	House	CT 5300/661	10886
83 Coombe Road ALLENBY GARDENS	House and Granny Flat	CT 5231/980	10887
84 Coombe Road ALLENBY GARDENS	House	CT 5365/856	10888
87 Coombe Road ALLENBY GARDENS	House	CT 5810/913	10889
88 Coombe Road ALLENBY GARDENS	House	CT 5701/993	10890
1/ 89 Coombe Road ALLENBY GARDENS	Home Unit	CT 5058/510	10891
91 Coombe Road ALLENBY GARDENS	House	CT 5471/522	10895
93 Coombe Road ALLENBY GARDENS	House	CT 5365/376	10896
93a Coombe Road ALLENBY GARDENS	House	CT 5205/23	10897
94-94A Coombe Road ALLENBY GARDENS	Maisonettes	CT 5728/968	10898
95 Coombe Road ALLENBY GARDENS	House	CT 5681/236	10899
96 Coombe Road ALLENBY GARDENS	House	CT 5728/969	10900
97 Coombe Road ALLENBY GARDENS	House	CT 5790/515	10901
98 Coombe Road ALLENBY GARDENS	House	CT 5816/247	10902
99 Coombe Road ALLENBY GARDENS	House	CT 5233/935	10903
101-103 Coombe Road ALLENBY GARDENS	Maisonettes	CT 5596/421	10905
102 Coombe Road ALLENBY GARDENS	House	CT 5856/895	10906
104 Coombe Road ALLENBY GARDENS	House	CT 5810/634	10907
105 Coombe Road ALLENBY GARDENS	House	CT 5753/70	10908
106 Coombe Road ALLENBY GARDENS	House	CT 5219/538	10909
107 Coombe Road ALLENBY GARDENS	House	CT 5764/189	10910

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
108 Coombe Road ALLENBY GARDENS	Primary School	CT 5731/564	10911
109 Coombe Road ALLENBY GARDENS	House	CT 5232/783	10912
110 Coombe Road ALLENBY GARDENS	Kindergarten & Child Minding Services	CT 5731/764	10913
111 Coombe Road ALLENBY GARDENS	House	CT 5261/433	10914
113 Coombe Road ALLENBY GARDENS	House	CT 1307/193	10915
115 Coombe Road ALLENBY GARDENS	House	CT 5616/788	10916
117 Coombe Road ALLENBY GARDENS	House	CT 5173/211	10917
118 Coombe Road ALLENBY GARDENS	House	CT 5552/540	10918
119 Coombe Road ALLENBY GARDENS	House	CT 5338/369	10919
120 Coombe Road ALLENBY GARDENS	House	CT 6029/665	10920
121 Coombe Road ALLENBY GARDENS	House	CT 5321/676	10921
122 Coombe Road ALLENBY GARDENS	House	CT 5826/254	10922
123 Coombe Road ALLENBY GARDENS	House	CT 5281/342	10923
125 Coombe Road ALLENBY GARDENS	House	CT 5647/121	10924
126 Coombe Road ALLENBY GARDENS	House	CT 5170/615	10925
127 Coombe Road ALLENBY GARDENS	House	CT 5748/606	10926
128 Coombe Road ALLENBY GARDENS	House	CT 5314/222	10927
130 Coombe Road ALLENBY GARDENS	House	CT 5607/198	10928
131 Coombe Road ALLENBY GARDENS	House	CT 5530/193	10929
132 Coombe Road ALLENBY GARDENS	House	CT 5758/304	10930
134 Coombe Road ALLENBY GARDENS	House	CT 5774/103	10931
136 Coombe Road ALLENBY GARDENS	House	CT 5761/841	10932
1 Foster Street ALLENBY GARDENS	House	CT 5578/719	10933
5 Foster Street ALLENBY GARDENS	House	CT 5788/837	10935
7 Foster Street ALLENBY GARDENS	House	CT 5538/390	10936
13 Foster Street ALLENBY GARDENS	House	CT 5182/842	10937
15 Foster Street ALLENBY GARDENS	House	CT 5213/887	10938
17 Foster Street ALLENBY GARDENS	House	CT 4113/2	10939
8 Knight Street ALLENBY GARDENS	House	CT 5820/61	10940
10 Knight Street ALLENBY GARDENS	House	CT 5533/662	10941
11 Knight Street ALLENBY GARDENS	House	CT 5235/982	10942
12 Knight Street ALLENBY GARDENS	House	CT 5658/172	10943
14 Knight Street ALLENBY GARDENS	House	CT 5481/57	10944
19 Knight Street ALLENBY GARDENS	House	CT 5757/665	10946
21 Knight Street ALLENBY GARDENS	House	CT 5427/261	10947
23 Knight Street ALLENBY GARDENS	House	CT 5454/175	10948
25 Knight Street ALLENBY GARDENS	House	CT 5189/425	10949

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
29 Knight Street ALLENBY GARDENS	House	CT 5284/961	10950
31 Knight Street ALLENBY GARDENS	House	CT 5089/200	10951
1 Leane Avenue ALLENBY GARDENS	House	CT 5328/101	10952
3 Leane Avenue ALLENBY GARDENS	House	CT 5729/638	10953
5 Leane Avenue ALLENBY GARDENS	House	CT 5761/846	10954
7 Leane Avenue ALLENBY GARDENS	House	CT 5220/805	10955
8 Leane Avenue ALLENBY GARDENS	House	CT 5339/678	10956
9 Leane Avenue ALLENBY GARDENS	House	CT 5314/641	10957
10 Leane Avenue ALLENBY GARDENS	Maisonette	CT 5740/440	10958
11 Leane Avenue ALLENBY GARDENS	House	CT 5214/917	10959
12 Leane Avenue ALLENBY GARDENS	Maisonette	CT 5349/853	10960
13 Leane Avenue ALLENBY GARDENS	House	CT 5449/834	10961
14 Leane Avenue ALLENBY GARDENS	House	CT 5748/462	10962
15 Leane Avenue ALLENBY GARDENS	House	CT 5545/380	10963
16 Leane Avenue ALLENBY GARDENS	House	CT 5749/903	10964
17 Leane Avenue ALLENBY GARDENS	Maisonette	CT 2052/100	10965
17a Leane Avenue ALLENBY GARDENS	Maisonette	CT 5709/654	10966
18 Leane Avenue ALLENBY GARDENS	House	CT 5626/180	10967
19 Leane Avenue ALLENBY GARDENS	House	CT 5472/524	10968
20 Leane Avenue ALLENBY GARDENS	House	CT 5517/677	10969
2 Lewanick Street ALLENBY GARDENS	Maisonette	CT 5264/547	10970
2a Lewanick Street ALLENBY GARDENS	Maisonette	CT 5724/557	10971
4 Lewanick Street ALLENBY GARDENS	House	CT 5347/279	10972
6 Lewanick Street ALLENBY GARDENS	House	CT 5249/887	10973
8 Lewanick Street ALLENBY GARDENS	House	CT 5756/237	10974
10 Lewanick Street ALLENBY GARDENS	House	CT 5334/124	10975
12 Lewanick Street ALLENBY GARDENS	House	CT 5332/81	10976
14 Lewanick Street ALLENBY GARDENS	House	CT 5539/989	10977
16 Lewanick Street ALLENBY GARDENS	House	CT 5193/350	10978
18 Lewanick Street ALLENBY GARDENS	House	CT 5704/552	10979
20 Lewanick Street ALLENBY GARDENS	House	CT 5172/370	10980
21 Lewanick Street ALLENBY GARDENS	House	CT 5744/953	10981
22 Lewanick Street ALLENBY GARDENS	House	CT 5574/311	10982
23 Lewanick Street ALLENBY GARDENS	House	CT 5758/905	10983
24 Lewanick Street ALLENBY GARDENS	House	CT 5078/69	10984
25 Lewanick Street ALLENBY GARDENS	House	CT 5744/821	10985
26 Lewanick Street ALLENBY GARDENS	House	CT 5764/116	10986
27 Lewanick Street ALLENBY GARDENS	House	CT 5673/561	10987

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
28 Lewanick Street ALLENBY GARDENS	House	CT 5572/430	10988
29 Lewanick Street ALLENBY GARDENS	House	CT 5752/983	10989
30 Lewanick Street ALLENBY GARDENS	House	CT 5167/280	10990
31 Lewanick Street ALLENBY GARDENS	House	CT 5468/535	10991
32 Lewanick Street ALLENBY GARDENS	House	CT 5612/969	10992
33 Lewanick Street ALLENBY GARDENS	House	CT 5293/360	10993
34 Lewanick Street ALLENBY GARDENS	House	CT 5493/948	10994
35 Lewanick Street ALLENBY GARDENS	House	CT 5764/115	10995
36 Lewanick Street ALLENBY GARDENS	House	CT 5764/118	10996
37 Lewanick Street ALLENBY GARDENS	House	CT 5751/940	10997
38 Lewanick Street ALLENBY GARDENS	House	CT 5753/577	10998
39 Lewanick Street ALLENBY GARDENS	House	CT 5751/939	10999
40 Lewanick Street ALLENBY GARDENS	House	CT 5535/740	11000
41 Lewanick Street ALLENBY GARDENS	House	CT 5518/343	11001
42 Lewanick Street ALLENBY GARDENS	House	CT 5820/997	11002
43 Lewanick Street ALLENBY GARDENS	House	CT 5802/674	11003
44 Lewanick Street ALLENBY GARDENS	House	CT 5756/902	11004
45 Lewanick Street ALLENBY GARDENS	House	CT 5722/733	11005
46 Lewanick Street ALLENBY GARDENS	House	CT 5764/117	11006
47 Lewanick Street ALLENBY GARDENS	House	CT 5449/270	11007
48 Lewanick Street ALLENBY GARDENS	House	CT 5426/827	11008
49 Lewanick Street ALLENBY GARDENS	House	CT 5754/38	11009
50 Lewanick Street ALLENBY GARDENS	House	CT 5377/704	11010
51 Lewanick Street ALLENBY GARDENS	House	CT 5739/905	11011
52 Lewanick Street ALLENBY GARDENS	House	CT 5454/566	11012
54 Lewanick Street ALLENBY GARDENS	Maisonette	CT 2268/21	11014
56 Lewanick Street ALLENBY GARDENS	Maisonette	CT 5689/726	11015
1 Maclagan Avenue ALLENBY GARDENS	House	CT 5104/329	11016
2 Maclagan Avenue ALLENBY GARDENS	House	CT 5648/810	11017
4 Maclagan Avenue ALLENBY GARDENS	House	CT 5858/890	11018
5 Maclagan Avenue ALLENBY GARDENS	House	CT 5244/313	11019
6 Maclagan Avenue ALLENBY GARDENS	House	CT 5761/562	11020
7 Maclagan Avenue ALLENBY GARDENS	House	CT 5576/751	11021
8 Maclagan Avenue ALLENBY GARDENS	House	CT 5124/153	11022
9 Maclagan Avenue ALLENBY GARDENS	House	CT 5345/279	11023
10 Maclagan Avenue ALLENBY GARDENS	House	CT 5409/613	11024
12 Maclagan Avenue ALLENBY GARDENS	House	CT 5717/948	11026
14 Maclagan Avenue ALLENBY GARDENS	House	CT 5753/556	11027

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
15 Maclagan Avenue ALLENBY GARDENS	House	CT 5394/914	11028
16 Maclagan Avenue ALLENBY GARDENS	House	CT 5753/555	11029
18 Maclagan Avenue ALLENBY GARDENS	House	CT 5270/724	11030
20 Maclagan Avenue ALLENBY GARDENS	House	CT 5746/726	11031
22 Maclagan Avenue ALLENBY GARDENS	House	CT 5750/163	11032
24 Maclagan Avenue ALLENBY GARDENS	House	CT 5749/259	11033
1 Price Weir Avenue ALLENBY GARDENS	House	CT 5194/665	11034
3 Price Weir Avenue ALLENBY GARDENS	House	CT 5392/541	11035
5 Price Weir Avenue ALLENBY GARDENS	House	CT 5162/763	11036
6 Price Weir Avenue ALLENBY GARDENS	House	CT 5074/975	11037
7 Price Weir Avenue ALLENBY GARDENS	House	CT 5278/811	11038
8 Price Weir Avenue ALLENBY GARDENS	House	CT 5742/891	11039
11 Price Weir Avenue ALLENBY GARDENS	House	CT 5687/881	11040
11a Price Weir Avenue ALLENBY GARDENS	House	CT 5700/744	11041
12 Price Weir Avenue ALLENBY GARDENS	House	CT 5734/320	11042
6 Way Terrace ALLENBY GARDENS	House	CT 5420/123	11500
8 Way Terrace ALLENBY GARDENS	House	CT 5301/254	11501
1 White Street ALLENBY GARDENS	House		11043
2 White Street ALLENBY GARDENS	House	CT 5142/637 CT 5610/50	11044
4 White Street ALLENBY GARDENS	House		11045
5 White Street ALLENBY GARDENS	House		11046
6 White Street ALLENBY GARDENS	House	CT 5552/323	11047
79 Drayton Street BOWDEN	House	CT 5237/791	9348
95 Drayton Street BOWDEN	House	CT 1040/158	9349
101 Drayton Street BOWDEN	House	CT 5869/422	20652
103 Drayton Street BOWDEN	House	CT 5285/196	20656
104 Drayton Street BOWDEN	House	CT 5686/450	20657
105 Drayton Street BOWDEN	House	CT 5248/774	9371
106 Drayton Street BOWDEN	House	CT 5484/232	20658
109 Drayton Street BOWDEN	House	CT 5097/320	20659
110 Drayton Street BOWDEN	House	CT 5407/534	20660
114 Drayton Street BOWDEN	House	CT 5119/771	9352
118 Drayton Street BOWDEN	House	CT 5096/495	9353
120 Drayton Street BOWDEN	Maisonette	CT 5657/836	20661
122 Drayton Street BOWDEN	Maisonette	CT 5607/294	9354
124 Drayton Street BOWDEN	Maisonette	CT 5595/527	9355
125 Drayton Street BOWDEN	House	CT 5713/196	9356

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
126 Drayton Street BOWDEN	Maisonette	CT 5595/527	9357
127 Drayton Street BOWDEN	House	CT 5125/611	9358
128 Drayton Street BOWDEN	House	CT 5544/443	9359
129 Drayton Street BOWDEN	House	CT 5545/64	9360
130 Drayton Street BOWDEN	House	CT 5347/872	20662
131 Drayton Street BOWDEN	House	CT 5193/975	9361
133 Drayton Street BOWDEN	House	CT 5441/186	9362
141 Drayton Street BOWDEN	House	CT 1157/196	9364
142 Drayton Street BOWDEN	House	CT 5714/925	9365
143 Drayton Street BOWDEN	House	CT 5697/984	9366
144 Drayton Street BOWDEN	House	CT 5319/975	9367
145 Drayton Street BOWDEN	House	CT 5732/947	9368
146 Drayton Street BOWDEN	House	CT 5679/780	9369
148 Drayton Street BOWDEN	House	CT 5507/541	9370
150 Drayton Street BOWDEN	House	CT 5507/542	20663
152 Drayton Street BOWDEN	House	CT 5828/998	9372
154 Drayton Street BOWDEN	House	CT 4162/241	9373
156 Drayton Street BOWDEN	House	CT 5659/872	9374
162 Drayton Street BOWDEN	House	CT 5323/927	9375
29-39 Eighth Street BOWDEN	Attached Houses	CT 6154/122 CT 6154/120 CT 6154/118 CT 6154/119 CT 6154/124 CT 6154/121	13208
5 Eleventh Street BOWDEN	House	CT 5604/477	9389
7 Eleventh Street BOWDEN	House	CT 5479/875	9390
9 Eleventh Street BOWDEN	Maisonette	CT 2265/53	9391
11-13 Eleventh Street BOWDEN	Maisonettes	CT 5314/117	9392
12 Eleventh Street BOWDEN	Maisonette	CT 3861/29	9393
14 Eleventh Street BOWDEN	Maisonette	CT 5781/896	9394
16 Eleventh Street BOWDEN	House	CT 5180/760	9396
18 Eleventh Street BOWDEN	House	CT 5446/453	9397
21 Eleventh Street BOWDEN	House	CT 5358/563	9398
1A Fifth Street BOWDEN	House	CT 5082/273	9399
1 Gething Crescent BOWDEN	House	CT 5253/114	9402
2 Gething Crescent BOWDEN	House	CT 5673/694	9403
3 Gething Crescent BOWDEN	House	CT 1157/197	9404
4 Gething Crescent BOWDEN	House	CT 5837/297	9405

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
7 Gething Crescent BOWDEN	House	CT 1166/52	9406
9 Gething Crescent BOWDEN	House	CT 5313/736	9407
11 Gething Crescent BOWDEN	House	CT 1349/97	9408
12 Gething Crescent BOWDEN	House	CT 5240/405	9409
13 Gething Crescent BOWDEN	House	CT 5334/452	9410
15 Gething Crescent BOWDEN	House	CT 5153/81	9411
91 Gibson Street BOWDEN	House	CT 5655/920	9412
93 Gibson Street BOWDEN	House	CT 5569/882	9413
95 Gibson Street BOWDEN	House	CT 5148/133	9414
97 Gibson Street BOWDEN	House	CT 5558/455	9415
111 Gibson Street BOWDEN	House	CT 5423/454	9417
113 Gibson Street BOWDEN	House	CT 731/172	9418
114 Gibson Street BOWDEN	Maisonettes	CT 5624/854	9419
115 Gibson Street BOWDEN	House	CT 5676/942	9420
1/ 118 Gibson Street BOWDEN	Single Storey Flats	CT 5503/469	9422
119 Gibson Street BOWDEN	House	CT 5335/771	9423
121 Gibson Street BOWDEN	House	CT 5474/234	9424
128 Gibson Street BOWDEN	Floor Home Unit	CT 5482/310	9425
130 Gibson Street BOWDEN	Home Unit	CT 5482/311	9426
134 Gibson Street BOWDEN	Single storey flats	CT 5331/602	9427
136 Gibson Street BOWDEN	House	CT 5366/158	9428
140 Gibson Street BOWDEN	House	CT 4128/477	9429
140A Gibson Street BOWDEN	House	CT 5758/414	9430
146 Gibson Street BOWDEN	Aged Accommodation	CT 5676/491 CT 5683/308 CT 5683/373 CT 5830/37	9431
14 Market Place BOWDEN	Single Storey Flats	CT 1662/105	9487
11 Ninth Street BOWDEN	House	CT 5611/823	9488
17 Ninth Street BOWDEN	House	CT 5824/186	9490
20 Ninth Street BOWDEN	House	CT 5328/383	13209
24 Ninth Street BOWDEN	House	CT 5217/475	9492
24 Tenth Street BOWDEN	House	CT 5361/225	9564
8 Thirteenth Street BOWDEN	Maisonette	CT 5882/678	9568
10 Thirteenth Street BOWDEN	Maisonette	CT 5882/679	9569
12 Thirteenth Street BOWDEN	Maisonette	CT 5186/118	9570
14 Thirteenth Street BOWDEN	Maisonette	CT 5297/766	9571
16 Thirteenth Street BOWDEN	House	CT 5831/871	9572
21 Trembath Street BOWDEN	House	CT 4208/461	9582

Charles Sturt Council
Table Section
Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
48 Trembath Street BOWDEN	House	CT 5558/586	9583
52 Trembath Street BOWDEN	House	CT 4250/266	9584
54 Trembath Street BOWDEN	House	CT 5659/139	9585
58 Trembath Street BOWDEN	Maisonettes	CT 5548/315	9586
39 Blight Street BROMPTON	Maisonette	CT 5419/742	9743
41 Blight Street BROMPTON	Maisonette	CT 5786/38	9744
43 Blight Street BROMPTON	Maisonette	CT 5594/872	9745
45 Blight Street BROMPTON	Maisonette	CT 5594/872	9746
47 Blight Street BROMPTON	Maisonette	CT 5594/872	9747
49 Blight Street BROMPTON	Maisonette	CT 5594/872	9748
51 Blight Street BROMPTON	Maisonette	CT 5594/872	9749
53 Blight Street BROMPTON	Maisonette	CT 5088/580	9750
55 Blight Street BROMPTON	Maisonette	CT 5594/872	9751
57 Blight Street BROMPTON	Maisonette	CT 5594/872	9752
59 Blight Street BROMPTON	Maisonette	CT 5594/872	9753
61 Blight Street BROMPTON	Maisonette	CT 5594/872	9754
63 Blight Street BROMPTON	Maisonette	CT 5594/872	9755
65 Blight Street BROMPTON	Maisonette	CT 5594/872	9756
67 Blight Street BROMPTON	Maisonette	CT 5432/511	9757
69 Blight Street BROMPTON	Maisonette	CT 5432/708	9758
71 Blight Street BROMPTON	Maisonette	CT 5594/872	9759
73 Blight Street BROMPTON	Maisonette	CT 5594/871	9760
75 Blight Street BROMPTON	Maisonette	CT 5594/872	9765
77 Blight Street BROMPTON	Maisonette	CT 5594/872	9767
79 Blight Street BROMPTON	Maisonette	CT 5594/872	9769
81 Blight Street BROMPTON	Maisonette	CT 5594/872	9771
83 Blight Street BROMPTON	Maisonette	CT 5594/872	9773
85 Blight Street BROMPTON	Maisonette	CT 5594/872	9775
87 Blight Street BROMPTON	Maisonette	CT 5594/872	9777
89 Blight Street BROMPTON	Maisonette	CT 5594/872	9779
91 Blight Street BROMPTON	Maisonette	CT 5594/872	9780
93 Blight Street BROMPTON	Maisonette	CT 5594/872	9781
95 Blight Street BROMPTON	Maisonette	CT 5594/872	9782
97 Blight Street BROMPTON	Maisonette	CT 5594/872	9783
99 Blight Street BROMPTON	Maisonette	CT 5594/872	9784
101 Blight Street BROMPTON	Maisonette	CT 5594/872	9761
103 Blight Street BROMPTON	Maisonette	CT 5594/872	9762
105 Blight Street BROMPTON	Maisonette	CT 5594/872	9763

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
1 Brown Street BROMPTON	House	CT 5204/99	9278
3 Brown Street BROMPTON	House	CT 5419/158	9279
87 Chief Street BROMPTON	House	CT 5784/690	9280
91 Chief Street BROMPTON	House	CT 5182/34	9281
93 Chief Street BROMPTON	Club	CT 4138/547	9282
95a Chief Street BROMPTON	House	CT 5817/513	9283
97 Chief Street BROMPTON	House	CT 5807/621	9284
99 Chief Street BROMPTON	House	CT 5388/987	9285
101 Chief Street BROMPTON	House	CT 5233/754	9286
5 Coglin Street BROMPTON	House	CT 5816/250	9609
7 Coglin Street BROMPTON	House	CT 5629/392	9610
26 Coglin Street BROMPTON	House	CT 5569/885	9287
35 Coglin Street BROMPTON	Maisonette	CT 5470/877	9613
37 Coglin Street BROMPTON	Maisonette	CT 5530/596	9614
39 Coglin Street BROMPTON	Maisonette	CT 5527/852	9615
40-40a Coglin Street BROMPTON	House	CT 5392/431	9288
41 Coglin Street BROMPTON	Maisonette	CT 5209/176	9616
42 Coglin Street BROMPTON	House	CT 5539/456	9289
43 Coglin Street BROMPTON	Maisonette	CT 5268/968	9617
44-46 Coglin Street BROMPTON	House	CT 5707/1	9290
45 Coglin Street BROMPTON	Maisonette	CT 5267/19	9618
48 Coglin Street BROMPTON	Maisonette	CT 5432/862	9291
50 Coglin Street BROMPTON	Maisonette	CT 4335/727	9292
53 Coglin Street BROMPTON	Maisonette	CT 5221/345	9619
55-57 Coglin Street BROMPTON	House	CT 5813/998	9293
58 Coglin Street BROMPTON	House	CT 5084/544	9295
61 Coglin Street BROMPTON	House	CT 5689/646	9296
62 Coglin Street BROMPTON	House	CT 5480/44	9297
64 Coglin Street BROMPTON	House	CT 5294/57	9298
65 Coglin Street BROMPTON	Single Storey Flats	CT 5230/702	9299
72 Coglin Street BROMPTON	House	CT 5218/907	9300
73 Coglin Street BROMPTON	House	CT 5311/987	9301
74 Coglin Street BROMPTON	House	CT 5221/470	9302
76 Coglin Street BROMPTON	House	CT 5574/317	9303
77 Coglin Street BROMPTON	House	CT 5328/347	9304
78 Coglin Street BROMPTON	House	CT 5837/291	9305
79 Coglin Street BROMPTON	House	CT 5550/260 CT 5809/904	9306

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
85 Coglin Street BROMPTON	House	CT 5339/759	9307
86 Coglin Street BROMPTON	House	CT 5509/56	9308
90 Coglin Street BROMPTON	House	CT 5200/162	9309
99 Coglin Street BROMPTON	House	CT 5612/875	9310
101 Coglin Street BROMPTON	House	CT 5596/610	9311
102 Coglin Street BROMPTON	House	CT 5250/79	9312
103 Coglin Street BROMPTON	House	CT 5682/320	9313
105 Coglin Street BROMPTON	House	CT 5786/744	9314
107 Coglin Street BROMPTON	House	CT 4330/907	9315
109 Coglin Street BROMPTON	House	CT 3766/17	9316
111 Coglin Street BROMPTON	House	CT 5839/185	9318
113 Coglin Street BROMPTON	House	CT 5509/57	9319
115 Coglin Street BROMPTON	House	CT 5473/252	9320
121 Coglin Street BROMPTON	House	CT 5336/393	9322
124 Coglin Street BROMPTON	House	CT 589/30	9323
130 Coglin Street BROMPTON	House	CT 5547/299	9325
133 Coglin Street BROMPTON	House & Office	CT 2444/103 CT 5669/869	9326
134 Coglin Street BROMPTON	House	CT 5826/175	9327
135 Coglin Street BROMPTON	House	CT 5796/889	9328
137 Coglin Street BROMPTON	House	CT 5208/852	9329
139 Coglin Street BROMPTON	House	CT 5362/127	9331
141 Coglin Street BROMPTON	House	CT 5501/583	9332
147 Coglin Street BROMPTON	House	CT 5824/207	9333
155 Coglin Street BROMPTON	House	CT 5445/738	9334
157 Coglin Street BROMPTON	House	CT 5816/712	9335
159 Coglin Street BROMPTON	Maisonette	CT 5629/751	9336
161 Coglin Street BROMPTON	Maisonette	CT 4210/398	9337
163 Coglin Street BROMPTON	Home Unit	CT 5006/411	9338
165 Coglin Street BROMPTON	Home Unit	CT 5006/412	9339
167 Coglin Street BROMPTON	Home Unit	CT 5006/413	9340
169 Coglin Street BROMPTON	Home Unit	CT 5006/414	9341
171 Coglin Street BROMPTON	Home Unit	CT 5006/415	9342
173 Coglin Street BROMPTON	House	CT 5824/786	9343
175 Coglin Street BROMPTON	House	CT 5104/781	9344
179 Coglin Street BROMPTON	House	CT 5650/75	9345
East Street BROMPTON	Chemistry Laboratory		13199
East Street BROMPTON	Gas Control Building		13200

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
East Street BROMPTON	Museum		13197
East Street BROMPTON	Former Carpenters Workshop		13198
Off East Street BROMPTON	Amenities Block	CT 176/137	13202
107 East Street BROMPTON	House	CT 5502/368	9377
108 East Street BROMPTON	House	CT 5641/354	9378
110 East Street BROMPTON	House	CT 5542/52	9380
113 East Street BROMPTON	House	CT 1208/197	9381
116 East Street BROMPTON	House	CT 1270/116	9383
118 East Street BROMPTON	House	CT 5852/724	9384
120 East Street BROMPTON	House	CT 5884/663	9385
122 East Street BROMPTON	House	CT 5482/390	9386
124 East Street BROMPTON	House	CT 5238/64	9387
126 East Street BROMPTON	House	CT 5182/789	9388
12 First Street BROMPTON	House	CT 5099/143 CT 5099/144	9620
13 First Street BROMPTON	House	CT 5759/219	9621
14 First Street BROMPTON	House	CT 5564/265	9622
15 First Street BROMPTON	House	CT 5805/73	9623
17 First Street BROMPTON	House	CT 5579/923	9624
19 First Street BROMPTON	House	CT 5073/293	9625
21 First Street BROMPTON	House	CT 5193/473	9626
27 First Street BROMPTON	Maisonette	CT 5087/565	9627
29 First Street BROMPTON	Maisonette	CT 5476/898	9628
31 First Street BROMPTON	Maisonette	CT 5221/495	9629
33 First Street BROMPTON	Maisonette	CT 5221/498	9630
6 Fiveash Place BROMPTON	House	CT 5260/551	9401
3 Goyder Place BROMPTON	Maisonette	CT 5449/10	9461
5 Goyder Place BROMPTON	Maisonette	CT 5177/486	9462
12 Green Street BROMPTON	House	CT 5545/99	9463
16 Green Street BROMPTON	House	CT 5365/158	9464
18 Green Street BROMPTON	House	CT 5104/61	9465
22 Green Street BROMPTON	House	CT 5435/9	9466
30 Green Street BROMPTON	House	CT 5155/932	20665
31 Green Street BROMPTON	House	CT 4210/941	9467
32 Green Street BROMPTON	House	CT 5404/384	9468
34 Green Street BROMPTON	Maisonette	CT 5198/504	9469
36 Green Street BROMPTON	Maisonette	CT 5215/435	9470

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
37 Green Street BROMPTON	House	CT 5221/778	9471
1/ 39 Green Street BROMPTON	Maisonette	CT 5286/418	9472
2/ 39 Green Street BROMPTON	Maisonette	CT 5286/419	9473
44 Green Street BROMPTON	Home Unit	CT 5834/620	9474
45 Green Street BROMPTON	Maisonette	CT 5224/355	9475
45a Green Street BROMPTON	Maisonette	CT 5223/951	9476
46 Green Street BROMPTON	Home Unit	CT 5834/619	9477
48 Green Street BROMPTON	House	CT 5214/316	9478
50 Green Street BROMPTON	House	CT 5390/888	20666
51 Green Street BROMPTON	House	CT 5813/721	9479
55 Green Street BROMPTON	House	CT 5336/418	9480
57 Green Street BROMPTON	House	CT 5103/790	9481
59A-59B Green Street BROMPTON	Maisonettes	CT 5812/322	9482
60 Green Street BROMPTON	House	CT 5780/494	20667
62 Green Street BROMPTON	House	CT 5440/878	9483
70 Green Street BROMPTON	House	CT 5221/583	20668
1 Hocking Street BROMPTON	House	CT 316/155	9631
11 Hocking Street BROMPTON	House	CT 5798/501	9632
Near Railway BROMPTON	Reticulated Gas Workshop		13204
Near Railway BROMPTON	Maintenance Building		13203
8 Pickering Street BROMPTON	House	CT 5692/169 CT 5759/383	9633
14 Pickering Street BROMPTON	House	CT 5632/784	9635
19 Pickering Street BROMPTON	House	CT 5136/488	9636
24 Pickering Street BROMPTON	Maisonette	CT 5274/926	9637
26 Pickering Street BROMPTON	Maisonette	CT 5480/728	9639
28-30 Pickering Street BROMPTON	House	CT 5397/964 CT 5397/965	9640
39 Pickering Street BROMPTON	House	CT 5136/476	9641
28 Second Street BROMPTON	House	CT 5436/242	9642
29 Second Street BROMPTON	House	CT 5590/11	9525
31 Second Street BROMPTON	House	CT 5193/391	9526
32 Second Street BROMPTON	House	CT 234/58	9644
33 Second Street BROMPTON	Row House	CT 5007/915	9527
35 Second Street BROMPTON	Home Unit	CT 5007/916	9528
37 Second Street BROMPTON	House	CT 4332/727	9529
39 Second Street BROMPTON	House	CT 5193/392	9530
41 Second Street BROMPTON	House	CT 5796/857	9531

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
43 Second Street BROMPTON	House	CT 5236/580	9532
45 Second Street BROMPTON	House	CT 5280/763	9533
47 Second Street BROMPTON	House	CT 5289/911	9534
48 Second Street BROMPTON	House	CT 5189/33 CT 5189/34	9645
49 Second Street BROMPTON	House	CT 1168/88	9535
50 Second Street BROMPTON	House	CT 5671/786	9646
52 Second Street BROMPTON	House	CT 5328/515	9647
53 Second Street BROMPTON	House	CT 5586/896	9536
54 Second Street BROMPTON	Maisonette	CT 5221/504	9648
55 Second Street BROMPTON	House	CT 5240/537	9537
1 Taylor Street BROMPTON	Maisonette	CT 5437/899	9649
2 Taylor Street BROMPTON	Home Unit	CT 5005/387	9650
3 Taylor Street BROMPTON	Maisonette	CT 5851/692	9651
4 Taylor Street BROMPTON	Home Unit	CT 5005/388	9652
5 Taylor Street BROMPTON	House	CT 3404/43	9653
7 Taylor Street BROMPTON	House	CT 5422/666 CT 5508/971 CT 5721/62	9654
8 Taylor Street BROMPTON	Maisonette	CT 5104/982	9655
10 Taylor Street BROMPTON	House	CT 5730/263	9656
11 Taylor Street BROMPTON	House	CT 5883/95	9657
12 Taylor Street BROMPTON	House	CT 5338/923	9658
14 Taylor Street BROMPTON	House	CT 5605/285	9659
15 Taylor Street BROMPTON	House	CT 5507/747 CT 5507/748 CT 5507/749 CT 5507/750	9660
16 Taylor Street BROMPTON	House	CT 5678/149	9661
18 Taylor Street BROMPTON	House	CT 5719/825	9662
20 Taylor Street BROMPTON	House	CT 5676/877	9663
22 Taylor Street BROMPTON	House	CT 5412/270	9664
23 Taylor Street BROMPTON	House	CT 5752/464	9665
25 Taylor Street BROMPTON	Maisonette	CT 5463/243	9666
32 Third Street BROMPTON	House	CT 5612/582	9566
145 Torrens Road BROMPTON	Maisonette	CT 5263/364	9836
147 Torrens Road BROMPTON	Maisonette	CT 5263/364	9837
149 Torrens Road BROMPTON	Maisonette	CT 5263/364	9838
151 Torrens Road BROMPTON	Maisonette	CT 5263/364	9839
1 Wadham Street BROMPTON	House	CT 5549/640	9592

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
6 Wadham Street BROMPTON	Maisonette	CT 5012/332	9590
8 Wadham Street BROMPTON	Maisonette	CT 5012/333	9591
10 Wadham Street BROMPTON	House	CT 5740/611	13201
18 West Street BROMPTON	House		9667
30 West Street BROMPTON	House	CT 5136/481	9670
32 West Street BROMPTON	House	CT 5136/482	9671
34 West Street BROMPTON	House	CT 5136/483	9672
36 West Street BROMPTON	House	CT 5316/121	9673
54 West Street BROMPTON	Maisonette	CT 5441/593	27068
84 West Street BROMPTON	Maisonette	CT 5222/370	9599
86 West Street BROMPTON	Maisonette	CT 5222/366	9600
88 West Street BROMPTON	Maisonette	CT 5994/486	9601
90 West Street BROMPTON	Maisonette	CT 5994/486	9602
98 West Street BROMPTON	House	CT 5808/567	9603
100 West Street BROMPTON	House	CT 5659/431	9604
102 West Street BROMPTON	House	CT 5265/194	9605
104 West Street BROMPTON	House	CT 5265/229	9606
106 West Street BROMPTON	House	CT 5548/884	9607
108 West Street BROMPTON	House	CT 5814/59	9608
22 Brougham Place CHELTENHAM	House	CT 5090/197	12419
24 Brougham Place CHELTENHAM	House	CT 1196/80	12420
28 Brougham Place CHELTENHAM	House	CT 5623/504	12421
19 Buller Terrace CHELTENHAM	House & Granny Flat	CT 5285/198	12422
21 Buller Terrace CHELTENHAM	House	CT 5134/784	12423
22 Buller Terrace CHELTENHAM	House	CT 5740/75	12424
24 Buller Terrace CHELTENHAM	House	CT 5779/912	12425
25 Buller Terrace CHELTENHAM	House	CT 5803/892	12426
26 Buller Terrace CHELTENHAM	House	CT 5206/545	12427
28 Buller Terrace CHELTENHAM	House	CT 5096/722	12428
29 Buller Terrace CHELTENHAM	House	CT 5809/770	12429
30 Buller Terrace CHELTENHAM	House	CT 5809/769	12430
31 Buller Terrace CHELTENHAM	House	CT 5720/937	12431
32 Buller Terrace CHELTENHAM	House	CT 5797/524	12432
33 Buller Terrace CHELTENHAM	House	CT 5274/156	12433
34 Buller Terrace CHELTENHAM	House	CT 5721/301	12434
35 Buller Terrace CHELTENHAM	House	CT 5363/775	12435
36 Buller Terrace CHELTENHAM	House	CT 5321/845	12436
37 Buller Terrace CHELTENHAM	House	CT 5445/161	12437

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
38 Buller Terrace CHELTENHAM	House	CT 5579/922	12438
39-40 Buller Terrace CHELTENHAM	House	CT 5160/391	12439
41 Buller Terrace CHELTENHAM	House	CT 5752/77	12440
43 Buller Terrace CHELTENHAM	House	CT 5752/76	12441
44 Buller Terrace CHELTENHAM	House	CT 5827/658	12442
45 Buller Terrace CHELTENHAM	House	CT 5704/906	12443
1 Colston Street CHELTENHAM	House	CT 5459/833	12444
3 Colston Street CHELTENHAM	House	CT 5566/302	12445
5 Colston Street CHELTENHAM	House	CT 5428/864	12446
7 Colston Street CHELTENHAM	House	CT 5592/294	12447
11 Colston Street CHELTENHAM	House	CT 5112/193	12449
15 Colston Street CHELTENHAM	House	CT 5781/843	12450
17 Colston Street CHELTENHAM	House	CT 5314/214	12451
19 Colston Street CHELTENHAM	House	CT 5437/9	12452
21 Colston Street CHELTENHAM	House	CT 5798/866	12453
23 Colston Street CHELTENHAM	House	CT 5798/867	12454
25 Colston Street CHELTENHAM	House	CT 5248/84	12455
1 Cricksdale Street CHELTENHAM	House	CT 5215/987	12276
2 Durham Terrace CHELTENHAM	House	CT 5205/963	12457
8 Durham Terrace CHELTENHAM	House	CT 5616/591	12458
10 Durham Terrace CHELTENHAM	House	CT 5727/970	12459
12 Durham Terrace CHELTENHAM	House	CT 5349/955	12460
16 Durham Terrace CHELTENHAM	House	CT 5818/884	12461
10 Earle Street CHELTENHAM	House	CT 3496/172	12462
1 Fifth Avenue CHELTENHAM	House	CT 5806/812	12463
3 Fifth Avenue CHELTENHAM	House	CT 5806/813	12464
4 Fifth Avenue CHELTENHAM	House	CT 5821/698	12465
5 Fifth Avenue CHELTENHAM	House & Granny Flat	CT 5438/391	12466
6 Fifth Avenue CHELTENHAM	House	CT 5180/948	12467
7 Fifth Avenue CHELTENHAM	House	CT 1221/168	12468
8 Fifth Avenue CHELTENHAM	House	CT 5260/329	12469
10 Fifth Avenue CHELTENHAM	House	CT 5844/145	12470
12 Fifth Avenue CHELTENHAM	House	CT 4123/581	12471
14 Fifth Avenue CHELTENHAM	House	CT 5813/177	12472
15 Fifth Avenue CHELTENHAM	House	CT 5349/130	12473
16 Fifth Avenue CHELTENHAM	House	CT 1237/27	12474
17 Fifth Avenue CHELTENHAM	House	CT 5817/516	12475
19 Fifth Avenue CHELTENHAM	House	CT 5370/993	12476

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
20 Fifth Avenue CHELTENHAM	House	CT 5176/387	12477
21 Fifth Avenue CHELTENHAM	House	CT 5381/296	12478
22 Fifth Avenue CHELTENHAM	House	CT 5204/342	12479
23 Fifth Avenue CHELTENHAM	House	CT 5134/720	12480
24 Fifth Avenue CHELTENHAM	House	CT 5761/790	12481
25 Fifth Avenue CHELTENHAM	House	CT 5335/151	12482
26 Fifth Avenue CHELTENHAM	House	CT 1240/169	12483
27 Fifth Avenue CHELTENHAM	House	CT 5102/73	12484
29 Fifth Avenue CHELTENHAM	House	CT 5346/681	12485
31 Fifth Avenue CHELTENHAM	House	CT 1166/19	12486
32 Fifth Avenue CHELTENHAM	House	CT 5462/264	12487
34 Fifth Avenue CHELTENHAM	House	CT 1263/107	12488
36 Fifth Avenue CHELTENHAM	House	CT 5416/1	12489
38 Fifth Avenue CHELTENHAM	House	CT 5759/461	12490
40 Fifth Avenue CHELTENHAM	House	CT 5796/519	12491
1 First Avenue CHELTENHAM	House	CT 5754/281	12277
2 First Avenue CHELTENHAM	House	CT 5821/289	12278
3 First Avenue CHELTENHAM	House	CT 5578/551	12279
4 First Avenue CHELTENHAM	House	CT 1217/64	12280
5 First Avenue CHELTENHAM	House	CT 5821/289	12281
6 First Avenue CHELTENHAM	House	CT 5578/551	12282
7 First Avenue CHELTENHAM	House	CT 1217/64	12283
8 First Avenue CHELTENHAM	House	CT 1248/194	12284
9 First Avenue CHELTENHAM	House	CT 5298/792	12285
10 First Avenue CHELTENHAM	House	CT 5603/752	12286
11 First Avenue CHELTENHAM	House	CT 5749/750	12287
12 First Avenue CHELTENHAM	House	CT 5814/934	12288
13 First Avenue CHELTENHAM	House	CT 5681/306	12289
14 First Avenue CHELTENHAM	House	CT 5203/191	12290
15 First Avenue CHELTENHAM	House	CT 5312/955	12291
16 First Avenue CHELTENHAM	House	CT 1148/157	12292
17 First Avenue CHELTENHAM	House	CT 5752/394	12293
18 First Avenue CHELTENHAM	House	CT 5642/856	12294
19 First Avenue CHELTENHAM	House	CT 5754/802	12295
21 First Avenue CHELTENHAM	House	CT 5124/332	12296
22 First Avenue CHELTENHAM	House	CT 5160/132	12297
23 First Avenue CHELTENHAM	House	CT 5789/616	12298
24 First Avenue CHELTENHAM	House	CT 5816/30	12299

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
26 First Avenue CHELTENHAM	House	CT 5198/995	12300
27 First Avenue CHELTENHAM	House	CT 5568/611	12301
28 First Avenue CHELTENHAM	House	CT 5474/880	12302
29 First Avenue CHELTENHAM	House	CT 5076/219	12303
30 First Avenue CHELTENHAM	House	CT 5442/217	12304
31 First Avenue CHELTENHAM	House	CT 5531/120	12305
33 First Avenue CHELTENHAM	House	CT 5253/747	12306
34 First Avenue CHELTENHAM	House	CT 5722/984	12307
35 First Avenue CHELTENHAM	House	CT 4138/101	12308
36 First Avenue CHELTENHAM	House	CT 5086/138	12309
37 First Avenue CHELTENHAM	House	CT 5559/24	12310
39 First Avenue CHELTENHAM	House	CT 5481/742	12311
41 First Avenue CHELTENHAM	House	CT 5808/601	12312
42 First Avenue CHELTENHAM	House	CT 5314/216	12313
43 First Avenue CHELTENHAM	House	CT 5269/869	12314
44 First Avenue CHELTENHAM	House	CT 5361/995	12315
46 First Avenue CHELTENHAM	House	CT 5345/73	12316
48 First Avenue CHELTENHAM	House	CT 5496/487	12317
51 First Avenue CHELTENHAM	House	CT 5658/752	12318
52 First Avenue CHELTENHAM	House	CT 5253/430	12319
58 First Avenue CHELTENHAM	House	CT 5760/689	12320
1 Fourth Avenue CHELTENHAM	House	CT 5191/159	12492
3 Fourth Avenue CHELTENHAM	House	CT 5803/299	12494
7 Fourth Avenue CHELTENHAM	House	CT 5766/528	12497
9 Fourth Avenue CHELTENHAM	House	CT 5795/493	12499
11 Fourth Avenue CHELTENHAM	House	CT 5760/258	12500
13 Fourth Avenue CHELTENHAM	House	CT 5808/14	12501
15 Fourth Avenue CHELTENHAM	House	CT 5455/91	12502
17 Fourth Avenue CHELTENHAM	House	CT 5806/792	12503
19 Fourth Avenue CHELTENHAM	House	CT 5761/641	12504
21 Fourth Avenue CHELTENHAM	House	CT 5598/654	12506
25 Fourth Avenue CHELTENHAM	House	CT 5309/441	12508
27 Fourth Avenue CHELTENHAM	House	CT 5722/981	12509
29 Fourth Avenue CHELTENHAM	House	CT 5148/41	12510
30 Fourth Avenue CHELTENHAM	House	CT 5696/752	12511
31 Fourth Avenue CHELTENHAM	House	CT 5253/423	12512
32 Fourth Avenue CHELTENHAM	House	CT 1341/142	12513
33 Fourth Avenue CHELTENHAM	House	CT 5378/328	12514

Charles Sturt Council
Table Section
Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
34 Fourth Avenue CHELTENHAM	House	CT 5460/590	12515
35 Fourth Avenue CHELTENHAM	House	CT 5118/679	12516
36 Fourth Avenue CHELTENHAM	House	CT 5356/205	12517
37 Fourth Avenue CHELTENHAM	House	CT 5813/932	12518
38 Fourth Avenue CHELTENHAM	House	CT 5754/737	12519
39 Fourth Avenue CHELTENHAM	House	CT 5162/739	12520
41 Fourth Avenue CHELTENHAM	House	CT 5816/87	12521
42 Fourth Avenue CHELTENHAM	House	CT 1318/191	12522
43 Fourth Avenue CHELTENHAM	House	CT 1314/185	12523
44 Fourth Avenue CHELTENHAM	House	CT 5825/60	12524
45 Fourth Avenue CHELTENHAM	House	CT 5437/613	12525
46 Fourth Avenue CHELTENHAM	House	CT 5462/131	12526
47 Fourth Avenue CHELTENHAM	House	CT 5333/977	12527
48 Fourth Avenue CHELTENHAM	House	CT 5800/66	12528
49 Fourth Avenue CHELTENHAM	House	CT 5743/828	12529
50 Fourth Avenue CHELTENHAM	House	CT 5340/193	12530
51 Fourth Avenue CHELTENHAM	House	CT 5743/829	12531
52 Fourth Avenue CHELTENHAM	House	CT 5077/605	12532
54 Fourth Avenue CHELTENHAM	House	CT 5665/306	12533
55 Fourth Avenue CHELTENHAM	House	CT 5136/353	12534
56 Fourth Avenue CHELTENHAM	House	CT 5208/233	12535
58 Fourth Avenue CHELTENHAM	House	CT 5793/214	12536
2 Herbert Street CHELTENHAM	House	CT 5430/84	12537
3 Herbert Street CHELTENHAM	House	CT 5216/245	12538
8 Herbert Street CHELTENHAM	House	CT 5825/742	12539
9 Herbert Street CHELTENHAM	House	CT 5819/971	12540
11 Herbert Street CHELTENHAM	House	CT 5482/463	12541
12 Herbert Street CHELTENHAM	House	CT 5787/819	12542
14 Herbert Street CHELTENHAM	House	CT 5268/613	12543
15 Herbert Street CHELTENHAM	House	CT 5813/915	12544
16 Herbert Street CHELTENHAM	House	CT 5194/832	12545
17 Herbert Street CHELTENHAM	House	CT 5814/456	12546
18 Herbert Street CHELTENHAM	House	CT 5259/275	12547
19 Herbert Street CHELTENHAM	House	CT 5631/522	12548
20 Herbert Street CHELTENHAM	House	CT 5816/317	12549
22 Herbert Street CHELTENHAM	House	CT 5779/928	12550
23 Herbert Street CHELTENHAM	House	CT 5215/114	12551
24 Herbert Street CHELTENHAM	House	CT 5804/620	12552

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
25 Herbert Street CHELTENHAM	House	CT 5820/291	12553
26 Herbert Street CHELTENHAM	House	CT 5518/748	12554
27 Herbert Street CHELTENHAM	House	CT 5748/368	12555
32 Herbert Street CHELTENHAM	House	CT 5784/696	12556
34 Herbert Street CHELTENHAM	House	CT 5601/268	12557
36 Herbert Street CHELTENHAM	House	CT 5541/174	12558
15 High Street CHELTENHAM	House	CT 5207/639	12321
2 James Street CHELTENHAM	House	CT 5726/974	12559
4 James Street CHELTENHAM	House	CT 5720/938	12560
5 James Street CHELTENHAM	House	CT 5218/696	12561
6 James Street CHELTENHAM	Aged Accommodation	CT 5263/177	12562
7 James Street CHELTENHAM	House	CT 5548/535	12563
9 James Street CHELTENHAM	House	CT 5121/914	12564
11 James Street CHELTENHAM	House	CT 5466/560	12565
12 James Street CHELTENHAM	House	CT 1159/99	12566
14 James Street CHELTENHAM	House	CT 5548/539	12567
15 James Street CHELTENHAM	House	CT 5792/258	12568
16 James Street CHELTENHAM	House	CT 5789/672	12569
18 James Street CHELTENHAM	House	CT 5591/185	12570
19 James Street CHELTENHAM	House	CT 5818/965	12571
20 James Street CHELTENHAM	House	CT 5380/142	12572
1 Percy Street CHELTENHAM	House	CT 5287/659	12573
2 Percy Street CHELTENHAM	House	CT 5682/795	12574
3 Percy Street CHELTENHAM	House	CT 5273/520	12575
4 Percy Street CHELTENHAM	House	CT 5796/910	12576
5 Percy Street CHELTENHAM	House	CT 5395/693	12577
6 Percy Street CHELTENHAM	House	CT 5065/313	12578
7 Percy Street CHELTENHAM	House	CT 5716/774	12579
8 Percy Street CHELTENHAM	House	CT 5796/375	12580
10 Percy Street CHELTENHAM	House	CT 5308/979	12581
11 Percy Street CHELTENHAM	House	CT 5813/843	12582
14 Percy Street CHELTENHAM	House	CT 5708/324	12583
15 Percy Street CHELTENHAM	House	CT 5814/509	12584
16 Percy Street CHELTENHAM	House	CT 5796/240	12585
17 Percy Street CHELTENHAM	House	CT 5376/122	12586
18 Percy Street CHELTENHAM	House	CT 5173/408	12587
19 Percy Street CHELTENHAM	House	CT 5796/103	12588
23 Percy Street CHELTENHAM	House	CT 5701/699	12589

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
1 Railway Terrace CHELTENHAM	House	CT 5700/572	12590
2 Railway Terrace CHELTENHAM	House	CT 5700/571	12591
3 Railway Terrace CHELTENHAM	House	CT 5700/573	12592
4 Railway Terrace CHELTENHAM	House	CT 5813/880	12593
6 Railway Terrace CHELTENHAM	House	CT 5533/551	12594
7 Railway Terrace CHELTENHAM	House	CT 5837/317	12595
8 Railway Terrace CHELTENHAM	House	CT 5422/610	12596
9 Railway Terrace CHELTENHAM	House	CT 1105/147	12597
10 Railway Terrace CHELTENHAM	House	CT 5815/71	12598
11 Railway Terrace CHELTENHAM	House	CT 5314/292	12599
12 Railway Terrace CHELTENHAM	House & Granny Flat	CT 1112/91	12600
13 Railway Terrace CHELTENHAM	House	CT 5810/724	12601
16 Railway Terrace CHELTENHAM	House	CT 5795/336	12603
17 Railway Terrace CHELTENHAM	House	CT 5741/597	12604
18 Railway Terrace CHELTENHAM	House	CT 5545/850	12605
20 Railway Terrace CHELTENHAM	House	CT 5361/301	12606
21 Railway Terrace CHELTENHAM	House	CT 5837/237	12607
23 Railway Terrace CHELTENHAM	House	CT 5808/993	12608
24 Railway Terrace CHELTENHAM	House	CT 5452/939	12609
26 Railway Terrace CHELTENHAM	House	CT 5067/334	12610
1 Second Avenue CHELTENHAM	House	CT 5517/5	12322
3 Second Avenue CHELTENHAM	House	CT 5212/445	12324
5 Second Avenue CHELTENHAM	House	CT 5491/901	12326
7 Second Avenue CHELTENHAM	House	CT 5154/886	12328
9 Second Avenue CHELTENHAM	House	CT 5821/288	12330
11 Second Avenue CHELTENHAM	House	CT 5789/609	12332
12 Second Avenue CHELTENHAM	House	CT 5704/89	12333
14 Second Avenue CHELTENHAM	House	CT 5225/134	12334
15 Second Avenue CHELTENHAM	House	CT 5789/608	12335
16 Second Avenue CHELTENHAM	House	CT 5789/442	12336
19 Second Avenue CHELTENHAM	House	CT 5320/552	12337
21 Second Avenue CHELTENHAM	House	CT 5085/758	12338
23 Second Avenue CHELTENHAM	House	CT 5500/608	12339
24 Second Avenue CHELTENHAM	House	CT 5688/871	12340
25 Second Avenue CHELTENHAM	House	CT 5398/953	12341
26 Second Avenue CHELTENHAM	House	CT 5392/601	12342
28 Second Avenue CHELTENHAM	House	CT 5752/351	12343
29 Second Avenue CHELTENHAM	House	CT 5080/789	12344

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
30 Second Avenue CHELTENHAM	House	CT 5500/553	12345
31 Second Avenue CHELTENHAM	House	CT 5353/538	12346
32 Second Avenue CHELTENHAM	House	CT 5335/925	12347
33 Second Avenue CHELTENHAM	House	CT 5741/451	12348
34 Second Avenue CHELTENHAM	House	CT 5830/802	12349
35 Second Avenue CHELTENHAM	House	CT 5382/769	12350
36 Second Avenue CHELTENHAM	House	CT 5592/178	12351
37 Second Avenue CHELTENHAM	House	CT 5126/478	12352
38 Second Avenue CHELTENHAM	House	CT 5276/655	12353
39 Second Avenue CHELTENHAM	House	CT 5577/215 CT 5577/216	12354
40 Second Avenue CHELTENHAM	House	CT 5523/836	12355
41 Second Avenue CHELTENHAM	House	CT 5185/532	12356
43 Second Avenue CHELTENHAM	House	CT 5118/447	12357
44 Second Avenue CHELTENHAM	House	CT 5269/745	12358
47 Second Avenue CHELTENHAM	House	CT 5427/164	12359
49 Second Avenue CHELTENHAM	House	CT 5787/577 CT 951/115	12360
1 Seventh Avenue CHELTENHAM	House	CT 5782/276	12611
2 Seventh Avenue CHELTENHAM	House	CT 5696/751	12612
3 Seventh Avenue CHELTENHAM	House	CT 1231/94	12613
4 Seventh Avenue CHELTENHAM	House	CT 5215/708	12614
6 Seventh Avenue CHELTENHAM	House	CT 5839/122	12615
7 Seventh Avenue CHELTENHAM	House	CT 5276/693	12616
8 Seventh Avenue CHELTENHAM	House	CT 5107/469	12617
9 Seventh Avenue CHELTENHAM	House	CT 5700/860	12618
10 Seventh Avenue CHELTENHAM	House	CT 5752/27	12619
11 Seventh Avenue CHELTENHAM	House	CT 5618/920	12620
12 Seventh Avenue CHELTENHAM	House & Granny Flat	CT 5453/802	12621
13 Seventh Avenue CHELTENHAM	House	CT 5817/602	12622
14 Seventh Avenue CHELTENHAM	House	CT 5296/588	12623
15 Seventh Avenue CHELTENHAM	House	CT 5597/114	12624
16 Seventh Avenue CHELTENHAM	House	CT 5305/707	12625
17 Seventh Avenue CHELTENHAM	House	CT 5757/33	12626
20 Seventh Avenue CHELTENHAM	House	CT 5490/53	12627
21 Seventh Avenue CHELTENHAM	House	CT 5805/852	12628
22 Seventh Avenue CHELTENHAM	House	CT 5838/90	12629
25 Seventh Avenue CHELTENHAM	House	CT 1124/41	12630

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
26 Seventh Avenue CHELTENHAM	House	CT 5817/797	12631
27 Seventh Avenue CHELTENHAM	House	CT 5155/329	12632
29 Seventh Avenue CHELTENHAM	House	CT 5805/510	12633
31 Seventh Avenue CHELTENHAM	House	CT 5214/836	12635
33 Seventh Avenue CHELTENHAM	House	CT 5182/645	12636
35 Seventh Avenue CHELTENHAM	House	CT 5424/463	12637
37 Seventh Avenue CHELTENHAM	House	CT 5815/780	12638
39 Seventh Avenue CHELTENHAM	House	CT 5132/139	12639
41 Seventh Avenue CHELTENHAM	House	CT 5712/555	12640
43 Seventh Avenue CHELTENHAM	House	CT 5128/883	12641
45 Seventh Avenue CHELTENHAM	House	CT 5333/923	12642
1 Sixth Avenue CHELTENHAM	House	CT 5385/710	12643
2 Sixth Avenue CHELTENHAM	House	CT 5818/218	12644
5 Sixth Avenue CHELTENHAM	House	CT 5452/347	12645
6 Sixth Avenue CHELTENHAM	House	CT 5145/242	12646
7 Sixth Avenue CHELTENHAM	House	CT 1240/193	12647
8 Sixth Avenue CHELTENHAM	House	CT 5163/722	12648
9 Sixth Avenue CHELTENHAM	House	CT 5784/346	12649
10 Sixth Avenue CHELTENHAM	House	CT 5191/59	12650
11 Sixth Avenue CHELTENHAM	House	CT 5817/465	12651
12 Sixth Avenue CHELTENHAM	House	CT 1141/1	12652
14 Sixth Avenue CHELTENHAM	House	CT 5177/503	12653
15 Sixth Avenue CHELTENHAM	House	CT 1251/169	12654
16 Sixth Avenue CHELTENHAM	House	CT 5807/645	12655
17 Sixth Avenue CHELTENHAM	House	CT 5165/999	12656
18 Sixth Avenue CHELTENHAM	House	CT 5275/668	12657
19 Sixth Avenue CHELTENHAM	House	CT 5699/899	12658
20 Sixth Avenue CHELTENHAM	House	CT 5816/111	12659
21 Sixth Avenue CHELTENHAM	House	CT 5365/974	12660
22 Sixth Avenue CHELTENHAM	House	CT 5330/641	12661
23 Sixth Avenue CHELTENHAM	House	CT 5790/448	12662
25 Sixth Avenue CHELTENHAM	House	CT 1298/189	12663
27 Sixth Avenue CHELTENHAM	House	CT 5760/950	12664
28 Sixth Avenue CHELTENHAM	House	CT 5087/391	12665
30 Sixth Avenue CHELTENHAM	House	CT 5812/323	12666
35 Sixth Avenue CHELTENHAM	House	CT 5734/431	12667
37 Sixth Avenue CHELTENHAM	House	CT 5273/768	12668
2 Stroud Street South CHELTENHAM	House	CT 5739/993	12361

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
4 Stroud Street South CHELTENHAM	House	CT 5476/878	12362
5 Stroud Street South CHELTENHAM	House	CT 5809/529	12695
6 Stroud Street South CHELTENHAM	House	CT 5445/206	12363
7 Stroud Street South CHELTENHAM	House	CT 5163/411	12696
8 Stroud Street South CHELTENHAM	House	CT 5672/97	12364
10 Stroud Street South CHELTENHAM	House	CT 5720/670	12365
11a Stroud Street South CHELTENHAM	House	CT 5602/895	12697
12 Stroud Street South CHELTENHAM	House	CT 5699/96	12366
14 Stroud Street South CHELTENHAM	House	CT 5711/130	12367
15 Stroud Street South CHELTENHAM	House	CT 5713/280	12698
16 Stroud Street South CHELTENHAM	House	CT 5682/287	12368
17 Stroud Street South CHELTENHAM	House	CT 5180/321	12699
18 Stroud Street South CHELTENHAM	House	CT 5136/756	12369
19 Stroud Street South CHELTENHAM	House	CT 5550/262	12700
22 Stroud Street South CHELTENHAM	House	CT 5417/238	12370
24 Stroud Street South CHELTENHAM	House	CT 5512/729	12371
31 Stroud Street North CHELTENHAM	House	CT 5756/213	12669
33 Stroud Street North CHELTENHAM	House	CT 5179/82	12670
36 Stroud Street South CHELTENHAM	House	CT 5639/668	12374
37 Stroud Street North CHELTENHAM	House	CT 5814/350	12671
38 Stroud Street South CHELTENHAM	House	CT 5495/946	12375
39 Stroud Street North CHELTENHAM	House	CT 5499/301	12672
41 Stroud Street North CHELTENHAM	House	CT 5312/576	12673
43 Stroud Street North CHELTENHAM	House	CT 5209/625	12674
44 Stroud Street South CHELTENHAM	House	CT 5811/592	12376
46 Stroud Street South CHELTENHAM	House	CT 5225/364	12377
47 Stroud Street North CHELTENHAM	House	CT 5224/76	12675
48 Stroud Street South CHELTENHAM	House	CT 5255/951	12378
50 Stroud Street South CHELTENHAM	House	CT 5133/693	12379
51 Stroud Street North CHELTENHAM	House	CT 5787/938	12676
54 Stroud Street South CHELTENHAM	House	CT 5602/981	12380
56 Stroud Street South CHELTENHAM	House	CT 5396/658	12381
61 Stroud Street North CHELTENHAM	House	CT 5816/178	12677
64 Stroud Street North CHELTENHAM	House	CT 5804/348	12679
65 Stroud Street North CHELTENHAM	House	CT 5489/452	12680
66 Stroud Street North CHELTENHAM	House	CT 5741/763	12681
68 Stroud Street North CHELTENHAM	House	CT 5434/670	12682
70 Stroud Street North CHELTENHAM	House	CT 5409/311	12683

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
72 Stroud Street North CHELTENHAM	House	CT 5818/766	12684
74 Stroud Street North CHELTENHAM	House	CT 5784/358	12685
76 Stroud Street North CHELTENHAM	House	CT 5792/183	12686
80 Stroud Street North CHELTENHAM	House	CT 5657/203	12687
84 Stroud Street North CHELTENHAM	House	CT 5789/674	12688
86 Stroud Street North CHELTENHAM	House	CT 5825/429	12689
88 Stroud Street North CHELTENHAM	House	CT 5826/844	12690
90 Stroud Street North CHELTENHAM	House	CT 5350/292	12691
92 Stroud Street North CHELTENHAM	House	CT 5607/358	12692
94 Stroud Street North CHELTENHAM	House	CT 5837/669	12693
96 Stroud Street North CHELTENHAM	House	CT 5475/661	12694
21 Stroud Street South CHELTENHAM	House	CT 5825/420	12701
19 Tewkesbury Street CHELTENHAM	House	CT 5725/630	12382
25 Tewkesbury Street CHELTENHAM	House	CT 5687/392	12383
29 Tewkesbury Street CHELTENHAM	House	CT 5812/188 CT 5814/426	12384
31 Tewkesbury Street CHELTENHAM	House	CT 5078/88	12385
33 Tewkesbury Street CHELTENHAM	House	CT 5979/809	12386
37 Tewkesbury Street CHELTENHAM	House	CT 5752/326	12387
39 Tewkesbury Street CHELTENHAM	House	CT 5536/283	12388
41 Tewkesbury Street CHELTENHAM	House	CT 5582/307	12389
43 Tewkesbury Street CHELTENHAM	House	CT 5824/695	12390
1 Third Avenue CHELTENHAM	House	CT 5316/475	12391
2 Third Avenue CHELTENHAM	House	CT 5250/281	12392
3 Third Avenue CHELTENHAM	House	CT 5217/779	12393
4 Third Avenue CHELTENHAM	House	CT 5787/973	12394
5 Third Avenue CHELTENHAM	House	CT 1115/85	12395
11 Third Avenue CHELTENHAM	House	CT 5157/868	12396
14 Third Avenue CHELTENHAM	House	CT 5568/45	12397
15 Third Avenue CHELTENHAM	House	CT 5137/202	12398
16 Third Avenue CHELTENHAM	House	CT 5093/663	12399
19 Third Avenue CHELTENHAM	House	CT 5668/887	12400
21 Third Avenue CHELTENHAM	House	CT 5246/746	12402
22 Third Avenue CHELTENHAM	House	CT 5208/352	12403
23 Third Avenue CHELTENHAM	House	CT 5242/751	12404
24 Third Avenue CHELTENHAM	House	CT 5739/774	12405
25 Third Avenue CHELTENHAM	House	CT 5781/504	12406
26 Third Avenue CHELTENHAM	House	CT 1237/136	12407

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
27 Third Avenue CHELTENHAM	House	CT 5350/422	12408
28 Third Avenue CHELTENHAM	House	CT 5129/64	12409
29 Third Avenue CHELTENHAM	House	CT 5731/510	12410
31 Third Avenue CHELTENHAM	House	CT 5487/635 CT 5487/881	12411
33 Third Avenue CHELTENHAM	House	CT 5270/813 CT 5270/975	12412
37 Third Avenue CHELTENHAM	House	CT 5671/927	12413
667 Torrens Road CHELTENHAM	House	CT 5717/926	12702
669 Torrens Road CHELTENHAM	House	CT 1278/176	12703
671 Torrens Road CHELTENHAM	House	CT 5789/585	12704
673 Torrens Road CHELTENHAM	House	CT 5235/2	12705
675 Torrens Road CHELTENHAM	House	CT 5741/285	12706
677 Torrens Road CHELTENHAM	House	CT 5592/23	12707
685 Torrens Road CHELTENHAM	House	CT 5209/226	12709
687 Torrens Road CHELTENHAM	House	CT 5723/388	12710
689 Torrens Road CHELTENHAM	House	CT 5343/368	12711
691 Torrens Road CHELTENHAM	House	CT 5228/913	12712
693 Torrens Road CHELTENHAM	House	CT 5826/865	12713
695 Torrens Road CHELTENHAM	House	CT 5631/478	12714
697 Torrens Road CHELTENHAM	House	CT 5588/810	12715
699 Torrens Road CHELTENHAM	House	CT 5085/73	12716
701 Torrens Road CHELTENHAM	House	CT 5304/585	12717
703 Torrens Road CHELTENHAM	House	CT 5807/620	12718
711 Torrens Road CHELTENHAM	House	CT 5112/338	12719
715 Torrens Road CHELTENHAM	House	CT 5310/150	12720
719-721 Torrens Road CHELTENHAM	House	CT 5574/933	12721
723 Torrens Road CHELTENHAM	House	CT 5835/186	12722
725 Torrens Road CHELTENHAM	House	CT 5357/748	12723
727 Torrens Road CHELTENHAM	House	CT 5215/679	12724
729 Torrens Road CHELTENHAM	House	CT 5831/195	12725
731 Torrens Road CHELTENHAM	House	CT 5367/183 CT 5367/184	12726
733 Torrens Road CHELTENHAM	House	CT 5347/932	12727
735-737 Torrens Road CHELTENHAM	House	CT 5520/669	12728
1 Whitney Street CHELTENHAM	House	CT 5292/621	12729
2 Whitney Street CHELTENHAM	House	CT 5585/281	12730
3 Whitney Street CHELTENHAM	House	CT 5613/814	12731
4 Whitney Street CHELTENHAM	House	CT 5819/31	12732

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
5 Whitney Street CHELTENHAM	House	CT 5761/653	12733
6 Whitney Street CHELTENHAM	House	CT 5816/708	12734
8 Whitney Street CHELTENHAM	House	CT 5161/763	12735
10 Whitney Street CHELTENHAM	House	CT 5717/266	12736
1 Woodstock Street CHELTENHAM	House	CT 5823/397	12414
3 Woodstock Street CHELTENHAM	House	CT 5105/834	12417
4 Woodstock Street CHELTENHAM	House	CT 5812/203	12418
1 Cedar Avenue CROYDON	House	CT 5715/368	9968
2 Cedar Avenue CROYDON	House	CT 5816/91	9969
3 Cedar Avenue CROYDON	House	CT 1154/144	9970
4 Cedar Avenue CROYDON	House	CT 5389/284	9971
5 Cedar Avenue CROYDON	House	CT 5807/44	9972
6 Cedar Avenue CROYDON	House	CT 5498/325	9973
7 Cedar Avenue CROYDON	House	CT 5485/691	9974
8 Cedar Avenue CROYDON	House	CT 5233/594	9975
10 Cedar Avenue CROYDON	House	CT 5486/913	9976
12 Cedar Avenue CROYDON	House	CT 5837/686	9977
13 Cedar Avenue CROYDON	House	CT 5807/674	9978
14 Cedar Avenue CROYDON	House	CT 5817/589	9979
15 Cedar Avenue CROYDON	House	CT 5421/196	9980
16 Cedar Avenue CROYDON	House	CT 5783/423	9981
17 Cedar Avenue CROYDON	House	CT 5202/525	9982
18 Cedar Avenue CROYDON	House	CT 5790/494	9983
19 Cedar Avenue CROYDON	House	CT 5574/860	9984
20 Cedar Avenue CROYDON	House	CT 5790/493	9985
21 Cedar Avenue CROYDON	House	CT 5346/664	9986
22 Cedar Avenue CROYDON	House	CT 5806/232	9987
7 Day Terrace CROYDON	House	CT 5708/152	9995
8 Day Terrace CROYDON	House	CT 5718/410	9996
9 Day Terrace CROYDON	House	CT 615/16	9997
10 Day Terrace CROYDON	House	CT 5641/462	9998
11 Day Terrace CROYDON	House	CT 5613/645	9999
12 Day Terrace CROYDON	House	CT 5492/505	10000
13 Day Terrace CROYDON	House	CT 5729/500	10001
14 Day Terrace CROYDON	House	CT 5837/694	10002
15 Day Terrace CROYDON	House	CT 5321/967	10003
16 Day Terrace CROYDON	House	CT 4233/605	10004
20 Day Terrace CROYDON	Home Unit	CT 5001/699	10005

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
21 Day Terrace CROYDON	Home Unit	CT 5001/698	10006
22 Day Terrace CROYDON	House	CT 5742/291	10007
23 Day Terrace CROYDON	House	CT 5787/633	10008
24 Day Terrace CROYDON	House	CT 5390/114	10009
25 Day Terrace CROYDON	House	CT 5217/730	10010
26 Day Terrace CROYDON	House	CT 972/82	10011
1/ 27 Day Terrace CROYDON	Home Unit	CT 5037/324	10012
2/ 27 Day Terrace CROYDON	Home Unit	CT 5037/325	10013
29 Day Terrace CROYDON	Maisonette	CT 5465/869	10014
30 Day Terrace CROYDON	Maisonette	CT 5858/757	10015
31 Day Terrace CROYDON	House	CT 1214/94	10016
32 Day Terrace CROYDON	House	CT 5731/782	10017
33 Day Terrace CROYDON	House	CT 5698/872	10018
34 Day Terrace CROYDON	House	CT 5173/816	10019
35 Day Terrace CROYDON	House	CT 5266/981	10020
36 Day Terrace CROYDON	House	CT 5814/492	10021
37 Day Terrace CROYDON	House	CT 5837/699	10022
38 Day Terrace CROYDON	House	CT 5244/737	10023
39 Day Terrace CROYDON	House	CT 5507/886	10024
40 Day Terrace CROYDON	House	CT 5698/873	10025
41 Day Terrace CROYDON	House	CT 5718/25	10026
1-5 Elizabeth Street CROYDON	Shops & Dwelling	CT 5348/550	20784
2-10 Elizabeth Street CROYDON	Shops & Dwelling	CT 5212/424	20786
9 Elizabeth Street CROYDON	House	CT 5295/295	10108
11 Elizabeth Street CROYDON	House	CT 5813/517	10109
12-22 Elizabeth Street CROYDON	Shops	CT 5506/788	20787
13 Elizabeth Street CROYDON	House	CT 5302/767	10110
15 Elizabeth Street CROYDON	Shop & Dwelling	CT 5183/303	20785
17 Elizabeth Street CROYDON	House	CT 5501/73	10111
1/ 19 Elizabeth Street CROYDON	Single Storey Flats	CT 5178/962	10112
21 Elizabeth Street CROYDON	House	CT 5258/295	10113
23 Elizabeth Street CROYDON	House	CT 5814/932	10114
24 Elizabeth Street CROYDON	House	CT 5852/800	10115
25 Elizabeth Street CROYDON	House	CT 5279/558	10116
26 Elizabeth Street CROYDON	House	CT 5852/801	10117
27 Elizabeth Street CROYDON	House	CT 5797/844	10118
1/ 28 Elizabeth Street CROYDON	Single Storey Flats	CT 5448/279	10119
29 Elizabeth Street CROYDON	House	CT 1978/78	10120

Charles Sturt Council
Table Section
Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
30 Elizabeth Street CROYDON	House	CT 5233/683	10121
32-34 Elizabeth Street CROYDON	Shop	CT 5181/443	10122
33 Elizabeth Street CROYDON	House	CT 5715/899	10123
36 Elizabeth Street CROYDON	House	CT 5496/379	10124
38 Elizabeth Street CROYDON	House	CT 5790/803	10126
40 Elizabeth Street CROYDON	House	CT 5641/232	10128
42 Elizabeth Street CROYDON	House	CT 5582/556	10130
44 Elizabeth Street CROYDON	House	CT 633/98	10131
46 Elizabeth Street CROYDON	House	CT 5796/55	10132
48 Elizabeth Street CROYDON	House	CT 5329/636	10134
50 Elizabeth Street CROYDON	House	CT 5800/554	10136
52 Elizabeth Street CROYDON	House	CT 5552/873	10137
56 Elizabeth Street CROYDON	House	CT 5811/377	10139
58 Elizabeth Street CROYDON	House	CT 5808/605	10140
2 Euston Terrace CROYDON	House	CT 5281/200	10152
3 Euston Terrace CROYDON	House	CT 5536/120	10153
4 Euston Terrace CROYDON	House	CT 5731/500	10154
5 Euston Terrace CROYDON	House	CT 5273/411	10155
6 Euston Terrace CROYDON	House	CT 5689/780	10156
8 Euston Terrace CROYDON	House	CT 5791/478	10157
9 Euston Terrace CROYDON	House	CT 5828/314	10158
10 Euston Terrace CROYDON	House	CT 5405/297	10159
11 Euston Terrace CROYDON	House	CT 5406/952	10160
12 Euston Terrace CROYDON	House	CT 1148/32	10161
13 Euston Terrace CROYDON	House	CT 5709/468	10162
14 Euston Terrace CROYDON	House	CT 5181/542	10163
15 Euston Terrace CROYDON	House	CT 5183/885	10164
16 Euston Terrace CROYDON	House	CT 5227/136	10165
1/ 17 Euston Terrace CROYDON	Ground Home Unit Only	CT 5268/442	10166
2/ 17 Euston Terrace CROYDON	Ground Home Unit Only	CT 5268/443	10167
18 Euston Terrace CROYDON	House	CT 4261/594	10168
21 Euston Terrace CROYDON	House	CT 5806/611	10169
23 Euston Terrace CROYDON	House	CT 5723/887	10170
24 Euston Terrace CROYDON	House	CT 5815/197 CT 5824/687	10171
25 Euston Terrace CROYDON	House	CT 5782/905	10172
26 Euston Terrace CROYDON	House	CT 5825/801	10173
34 Euston Terrace CROYDON	House	CT 5333/573	10174

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
1 Harriet Street CROYDON	House	CT 5194/790	10209
2 Harriet Street CROYDON	House	CT 5567/566	10210
3 Harriet Street CROYDON	House	CT 5188/984	10211
4 Harriet Street CROYDON	House	CT 5413/993	10212
5 Harriet Street CROYDON	House	CT 5793/861	10213
6 Harriet Street CROYDON	House	CT 5476/279	10214
7 Harriet Street CROYDON	House	CT 5785/528	10215
8 Harriet Street CROYDON	House	CT 5725/671	10216
9 Harriet Street CROYDON	House	CT 5347/779	10217
10 Harriet Street CROYDON	House	CT 5203/278	10218
11 Harriet Street CROYDON	House	CT 5533/505	10219
12 Harriet Street CROYDON	House	CT 5807/540	10220
13 Harriet Street CROYDON	House	CT 5361/936	10221
14 Harriet Street CROYDON	House	CT 5527/602	10222
15 Harriet Street CROYDON	House	CT 5305/349	10223
16 Harriet Street CROYDON	House	CT 5796/738	10224
17 Harriet Street CROYDON	House	CT 5293/98	10225
19 Harriet Street CROYDON	House	CT 5782/784	10226
20 Harriet Street CROYDON	House	CT 5323/427	10227
21 Harriet Street CROYDON	House	CT 5616/906	10228
23 Harriet Street CROYDON	House	CT 938/171	10229
5 Henry Street CROYDON	House	CT 5714/418	10251
7 Henry Street CROYDON	House	CT 5246/613	10253
8 Henry Street CROYDON	House	CT 5477/663	10254
9 Henry Street CROYDON	House	CT 4302/923	10255
10 Henry Street CROYDON	House	CT 5502/826	10256
11 Henry Street CROYDON	House	CT 5164/424	10257
12 Henry Street CROYDON	House	CT 3301/198	10258
13 Henry Street CROYDON	House	CT 5536/393	10259
14 Henry Street CROYDON	House	CT 5328/17	10260
15 Henry Street CROYDON	House	CT 5551/34	10261
16 Henry Street CROYDON	House	CT 5502/533	10262
17 Henry Street CROYDON	House	CT 5821/714	10263
18 Henry Street CROYDON	House	CT 5347/839	10264
19 Henry Street CROYDON	House	CT 5795/875	10265
20 Henry Street CROYDON	House	CT 5730/869	10266
20a Henry Street CROYDON	House	CT 5506/813	10267
20b Henry Street CROYDON	House	CT 5388/339	10268

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
22 Henry Street CROYDON	House	CT 5682/90	10269
22a Henry Street CROYDON	House	CT 5675/755	10270
24 Henry Street CROYDON	House	CT 5699/909	10271
26 Henry Street CROYDON	House	CT 914/178	10272
28 Henry Street CROYDON	House	CT 5091/772	10273
30 Henry Street CROYDON	House	CT 5700/493	10274
32 Henry Street CROYDON	House	CT 5403/365	10275
2 Hurtle Street CROYDON	House	CT 5838/46	10353
4 Hurtle Street CROYDON	House	CT 5786/952	10354
6 Hurtle Street CROYDON	House	CT 5785/529	10355
8 Hurtle Street CROYDON	House	CT 5583/488	10356
9 Hurtle Street CROYDON	House	CT 5670/349	10357
10 Hurtle Street CROYDON	House	CT 5282/345	10358
11 Hurtle Street CROYDON	House	CT 5837/572	10359
12 Hurtle Street CROYDON	House	CT 5797/701	10360
13 Hurtle Street CROYDON	House	CT 5837/573	10361
14 Hurtle Street CROYDON	House	CT 5785/593	10362
15 Hurtle Street CROYDON	House	CT 1266/174	10363
16 Hurtle Street CROYDON	House	CT 5787/837	10364
17 Hurtle Street CROYDON	House	CT 5186/92	10365
18 Hurtle Street CROYDON	House	CT 5267/66	10366
19 Hurtle Street CROYDON	House	CT 5524/44	10367
20 Hurtle Street CROYDON	House	CT 5600/85	10368
21 Hurtle Street CROYDON	House	CT 5755/291	10369
22 Hurtle Street CROYDON	House	CT 5789/433	10370
23 Hurtle Street CROYDON	House	CT 1321/182	10372
24 Hurtle Street CROYDON	House	CT 5648/171	10373
2 King Street CROYDON	House	CT 5168/536	10374
4 King Street CROYDON	House	CT 5727/404	10375
6 King Street CROYDON	Single Storey Flats	CT 5445/372	10376
417 Port Road CROYDON	House	CT 5832/179	10422
421 Port Road CROYDON	House	CT 5267/573	10423
429 Port Road CROYDON	House	CT 5539/643	10424
431 Port Road CROYDON	House	CT 5731/867	10425
433 Port Road CROYDON	House	CT 5718/71	10426
435b Port Road CROYDON	House	CT 5792/341	10427
437 Port Road CROYDON	House	CT 5667/94	10428
439 Port Road CROYDON	House	CT 5526/65	10429

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
441 Port Road CROYDON	House	CT 5708/898	10430
487 Port Road CROYDON	House	CT 5535/694	10431
493 Port Road CROYDON	House	CT 5291/964	10432
6 Princes Street CROYDON	House	CT 5236/427	10434
7 Princes Street CROYDON	House	CT 5393/705	10435
11 Princes Street CROYDON	House	CT 5300/251	10436
12 Princes Street CROYDON	House	CT 5819/56	10437
13 Princes Street CROYDON	House	CT 5332/98	10438
15 Princes Street CROYDON	House	CT 5681/971	10439
17 Princes Street CROYDON	House	CT 5591/980	10440
18 Princes Street CROYDON	House	CT 5410/175	10441
19 Princes Street CROYDON	House	CT 5586/276	10442
21 Princes Street CROYDON	House	CT 5331/919	10443
23 Princes Street CROYDON	House	CT 5442/107	10444
27 Princes Street CROYDON	House	CT 5175/580	10446
28 Princes Street CROYDON	House	CT 5433/638	10447
29 Princes Street CROYDON	House	CT 5242/866	10448
35 Princes Street CROYDON	House	CT 5314/86	10450
4/ 45 Princes Street CROYDON	Home Unit	CT 5036/68	10451
47 Princes Street CROYDON	House	CT 5222/113	10452
49 Princes Street CROYDON	House	CT 5501/900	10453
51 Princes Street CROYDON	House	CT 5820/164	10454
53 Princes Street CROYDON	House	CT 5884/850	10455
55 Princes Street CROYDON	House	CT 5727/104	10456
56 Princes Street CROYDON	House	CT 5297/93	10457
57 Princes Street CROYDON	House	CT 5805/414	10458
58 Princes Street CROYDON	House	CT 5708/402	10459
59 Princes Street CROYDON	House	CT 5695/164	10460
60 Princes Street CROYDON	House	CT 5824/34	10461
61 Princes Street CROYDON	House	CT 5722/849	10462
62 Princes Street CROYDON	House	CT 5325/342	10463
63 Princes Street CROYDON	House	CT 5337/701	10464
64 Princes Street CROYDON	House	CT 5804/369	10465
66 Princes Street CROYDON	Single Storey Flats	CT 5598/857	10466
67 Princes Street CROYDON	House	CT 5514/751	10467
68 Princes Street CROYDON	Single Storey Flats	CT 5671/435	10468
70 Princes Street CROYDON	House	CT 5238/28	10469
72 Princes Street CROYDON	House	CT 5354/53	10470

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
74 Princes Street CROYDON	House	CT 5786/677	10471
76 Princes Street CROYDON	House	CT 5782/855	10472
1/ 2 Queen Street CROYDON	Single Storey Flats	CT 5512/380	10473
03 Queen Street CROYDON	House	CT 5263/773	10474
04 Queen Street CROYDON	House	CT 5656/436	10475
05 Queen Street CROYDON	House	CT 5632/874	10476
13 Queen Street CROYDON	House	CT 5748/393	10477
6 Robert Street CROYDON	House	CT 5533/406	10483
8 Robert Street CROYDON	House	CT 5151/189	10485
9 Robert Street CROYDON	House	CT 5522/770	10486
10 Robert Street CROYDON	House	CT 5837/773	10487
11 Robert Street CROYDON	House	CT 5364/131	10488
12 Robert Street CROYDON	House	CT 5305/710	10489
14 Robert Street CROYDON	House	CT 5837/753	10490
14a Robert Street CROYDON	House	CT 5781/163	10491
15 Robert Street CROYDON	House	CT 5514/281	10492
16 Robert Street CROYDON	House	CT 5189/809	10493
17 Robert Street CROYDON	House	CT 5809/740	10494
20 Robert Street CROYDON	House	CT 5794/803	10495
21 Robert Street CROYDON	House	CT 5813/836	10496
22 Robert Street CROYDON	House	CT 5794/804	10497
23 Robert Street CROYDON	House	CT 5668/174	10498
24 Robert Street CROYDON	House	CT 5682/852	10499
25 Robert Street CROYDON	House	CT 5234/256	10500
26 Robert Street CROYDON	House	CT 5478/985	10501
27 Robert Street CROYDON	House	CT 5698/869	10502
28 Robert Street CROYDON	House	CT 5200/685	10503
29 Robert Street CROYDON	House	CT 5672/510	10504
30 Robert Street CROYDON	House	CT 1218/56	10505
31 Robert Street CROYDON	House	CT 5197/595	10506
31a Robert Street CROYDON	House	CT 1316/173	10507
32 Robert Street CROYDON	House	CT 5687/115	10508
33 Robert Street CROYDON	House	CT 5698/870	10509
34 Robert Street CROYDON	House	CT 5714/582	10510
35 Robert Street CROYDON	House	CT 5699/312	10511
36 Robert Street CROYDON	House	CT 5837/695	10512
37 Robert Street CROYDON	House	CT 5622/955	10513
37a Robert Street CROYDON	House	CT 5819/340	10514

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
38 Robert Street CROYDON	House	CT 5354/453	10515
39 Robert Street CROYDON	House	CT 5807/632	10516
44 Robert Street CROYDON	House	CT 5654/989	10517
46 Robert Street CROYDON	House	CT 5367/962	10518
48 Robert Street CROYDON	House	CT 5359/301	10519
50 Robert Street CROYDON	House	CT 5558/458	10520
1 St Lawrence Avenue CROYDON	House	CT 5442/115	10618
2 St Lawrence Avenue CROYDON	House	CT 5807/191	10619
3 St Lawrence Avenue CROYDON	House	CT 4069/242	10620
5 St Lawrence Avenue CROYDON	House	CT 1260/22	10621
6 St Lawrence Avenue CROYDON	House	CT 5230/716	10622
1 Thomas Street CROYDON	House	CT 5197/596	10623
2 Thomas Street CROYDON	House	CT 5575/326	10624
3 Thomas Street CROYDON	House	CT 5349/268	10625
4 Thomas Street CROYDON	House	CT 5814/925	10626
5 Thomas Street CROYDON	House	CT 2630/140	10627
6 Thomas Street CROYDON	House	CT 5195/204	10628
7 Thomas Street CROYDON	House	CT 5724/129	10629
7a Thomas Street CROYDON	House	CT 5647/804	10630
8 Thomas Street CROYDON	House	CT 5160/737	10631
9-11 Thomas Street CROYDON	Maisonettes	CT 5821/211	10632
10 Thomas Street CROYDON	House	CT 5516/930	10633
12 Thomas Street CROYDON	House	CT 5160/407	10634
13 Thomas Street CROYDON	House	CT 5837/933	10635
14 Thomas Street CROYDON	House	CT 5725/705	10636
15 Thomas Street CROYDON	House	CT 5257/307	10637
16 Thomas Street CROYDON	House	CT 5569/266	10638
17 Thomas Street CROYDON	House	CT 5780/480	10639
18 Thomas Street CROYDON	House	CT 5393/513	10640
19 Thomas Street CROYDON	House	CT 5789/603	10641
20 Thomas Street CROYDON	House	CT 5547/157	10642
21 Thomas Street CROYDON	House	CT 5789/604	10643
22 Thomas Street CROYDON	House	CT 5478/539	10644
23 Thomas Street CROYDON	House	CT 5632/415	10645
24 Thomas Street CROYDON	House	CT 5076/877	10646
25 Thomas Street CROYDON	House	CT 5389/828	10647
26 Thomas Street CROYDON	House	CT 5725/285	10648
27 Thomas Street CROYDON	House	CT 5124/539	10649

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
28 Thomas Street CROYDON	House	CT 5807/48	10650
29 Thomas Street CROYDON	House	CT 5666/8	10651
30 Thomas Street CROYDON	House	CT 5305/951	10652
31 Thomas Street CROYDON	House	CT 5625/43	10653
32 Thomas Street CROYDON	House	CT 5797/952	10654
33-35 Thomas Street CROYDON	Maisonettes	CT 5631/480	10655
34 Thomas Street CROYDON	House	CT 5583/738	10656
36 Thomas Street CROYDON	House	CT 694/170	10657
37 Thomas Street CROYDON	House	CT 5568/601	10658
38 Thomas Street CROYDON	House	CT 5816/52	10659
39 Thomas Street CROYDON	House	CT 5806/447	10660
40 Thomas Street CROYDON	House	CT 5632/368	10661
41 Thomas Street CROYDON	House	CT 5834/376	10662
41a Thomas Street CROYDON	House	CT 5614/281	10663
42 Thomas Street CROYDON	House	CT 5806/449	10664
43 Thomas Street CROYDON	House	CT 5148/876	10665
44 Thomas Street CROYDON	House	CT 5175/818	10666
45 Thomas Street CROYDON	House	CT 5303/72	10667
46 Thomas Street CROYDON	House	CT 5537/600	10668
46a Thomas Street CROYDON	House	CT 5127/754	10669
46b Thomas Street CROYDON	House	CT 5781/503	10670
47 Thomas Street CROYDON	House	CT 5532/599	10671
48 Thomas Street CROYDON	House	CT 5195/347	10672
49 Thomas Street CROYDON	House	CT 5538/388	10673
51 Thomas Street CROYDON	House	CT 5644/401	10674
52 Thomas Street CROYDON	House	CT 5807/664	10675
1 William Street CROYDON	House	CT 5339/571	10688
3 William Street CROYDON	House	CT 5471/119	10690
5 William Street CROYDON	House	CT 5789/180	10693
6 William Street CROYDON	House	CT 5526/108	10694
7 William Street CROYDON	House	CT 5351/982	10695
8 William Street CROYDON	House	CT 5551/243	10696
9 William Street CROYDON	House	CT 5792/739	10697
10 William Street CROYDON	House	CT 5670/943	10698
11 William Street CROYDON	House	CT 1214/144	10699
12 William Street CROYDON	House	CT 5814/507	10700
13 William Street CROYDON	House	CT 5794/656	10701
14 William Street CROYDON	House	CT 5450/382	10702

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
15 William Street CROYDON	House	CT 1214/142	10703
16 William Street CROYDON	House	CT 5442/304	10704
17 William Street CROYDON	House	CT 4267/176	10705
18 William Street CROYDON	House	CT 5795/278	10706
19 William Street CROYDON	House	CT 5796/256	10707
20 William Street CROYDON	House	CT 5173/288	10708
21 William Street CROYDON	House	CT 5559/787	10709
22 William Street CROYDON	House	CT 5314/364	10710
23 William Street CROYDON	House	CT 5553/325	10711
24 William Street CROYDON	House	CT 5783/651	10712
26 William Street CROYDON	House	CT 5417/337	10713
28 William Street CROYDON	House	CT 5822/167	10714
2 Beach Street GRANGE	House	CT 5224/865	13017
1/ 3 Beach Street GRANGE	Single Storey Flats	CT 5455/744	13018
3/ 3 Beach Street GRANGE	Single Storey Flats	CT 5466/744	13019
12 Beach Street GRANGE	House	CT 4374/700	13020
14 Beach Street GRANGE	House	CT 5526/391	13021
16 Beach Street GRANGE	House	CT 5321/212	13022
19 Charles Sturt Avenue GRANGE	House	CT 5725/52	13023
21 Charles Sturt Avenue GRANGE	House	CT 5726/607	13024
23 Charles Sturt Avenue GRANGE	House	CT 5481/935	13025
25 Charles Sturt Avenue GRANGE	House	CT 5276/297	13026
27 Charles Sturt Avenue GRANGE	House	CT 5519/237	13027
33-35 Charles Sturt Avenue GRANGE	Maisonettes	CT 5609/557	13030
37 Charles Sturt Avenue GRANGE	House	CT 5367/617	13031
39 Charles Sturt Avenue GRANGE	House	CT 5491/762	13032
41 Charles Sturt Avenue GRANGE	House	CT 5380/611	13033
46 Charles Sturt Avenue GRANGE	House	CT 5150/504	13034
48 Charles Sturt Avenue GRANGE	House	CT 5272/723	13035
49 Charles Sturt Avenue GRANGE	House	CT 5509/705	13036
50 Charles Sturt Avenue GRANGE	House	CT 5617/941	13037
53 Charles Sturt Avenue GRANGE	House	CT 5218/521	13038
455 Esplanade GRANGE	House	CT 5337/613	13039
623 Grange Road GRANGE	House	CT 5489/775	13040
625 Grange Road GRANGE	Veterinary Surgeons	CT 5333/349	13041
627 Grange Road GRANGE	House	CT 5506/733	13042
629 Grange Road GRANGE	House	CT 5386/343	13043
631 Grange Road GRANGE	House	CT 5137/531	13044

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
633 Grange Road GRANGE	House	CT 5523/128	13046
637 Grange Road GRANGE	House	CT 5493/449	13047
639 Grange Road GRANGE	House	CT 5083/609	13048
641 Grange Road GRANGE	House	CT 5128/783	13049
645 Grange Road GRANGE	House	CT 5522/960	13050
655 Grange Road GRANGE	House	CT 5249/499	13052
657 Grange Road GRANGE	House	CT 5520/520	13053
659 Grange Road GRANGE	House	CT 5248/96	13054
28 High Street GRANGE	House	CT 5682/116	13065
29 High Street GRANGE	House	CT 5496/461	13066
30 High Street GRANGE	House	CT 5154/771	13067
31 High Street GRANGE	House	CT 5133/63	13068
32 High Street GRANGE	House	CT 5399/124	13069
33 High Street GRANGE	House	CT 5400/317	13070
34 High Street GRANGE	House	CT 5150/66	13071
35 High Street GRANGE	House	CT 5524/978	13072
36 High Street GRANGE	House	CT 5206/631	13073
37 High Street GRANGE	House	CT 5493/821	13074
38 High Street GRANGE	House	CT 5522/247	13075
39 High Street GRANGE	House	CT 5424/574	13076
40 High Street GRANGE	House	CT 5448/580	13077
41 High Street GRANGE	House	CT 5502/736	13078
43 High Street GRANGE	House	CT 5086/477	13079
45 High Street GRANGE	House	CT 5283/555	13080
47 High Street GRANGE	House	CT 5417/139	13081
49 High Street GRANGE	House	CT 5510/937	13082
52 High Street GRANGE	House	CT 858/14	13083
53 High Street GRANGE	House	CT 4075/796	13084
54 High Street GRANGE	House	CT 5826/778	13085
11 Jetty Street GRANGE	Maisonette	CT 5390/693	13086
13 Jetty Street GRANGE	Maisonette	CT 5464/891	13087
15 Jetty Street GRANGE	House	CT 5103/602	13088
18 Jetty Street GRANGE	House	CT 5090/181	13090
19 Jetty Street GRANGE	House	CT 5525/736	13091
20 Jetty Street GRANGE	House	CT 5136/238	13092
23 Jetty Street GRANGE	House	CT 5500/40	13093
26 Jetty Street GRANGE	House	CT 5069/634	13096
27 Jetty Street GRANGE	House	CT 5158/85	13097

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
28 Jetty Street GRANGE	House & Flat	CT 5501/249 CT 5501/250	13098
29 Jetty Street GRANGE	House	CT 5702/89	13099
31 Jetty Street GRANGE	House	CT 5431/453	13100
32 Jetty Street GRANGE	House	CT 5124/708	13101
33 Jetty Street GRANGE	House	CT 5128/207	13102
38 Jetty Street GRANGE	House	CT 5371/891	13104
42 Jetty Street GRANGE	House	CT 5086/449	13105
44 Jetty Street GRANGE	House	CT 5515/841	13106
46 Jetty Street GRANGE	House	CT 5839/914	13107
48 Jetty Street GRANGE	House	CT 5831/606	13108
50 Jetty Street GRANGE	House	CT 5076/146	13109
52 Jetty Street GRANGE	House	CT 5495/403	13110
542 Seaview Road GRANGE	House	CT 5148/419	13113
1/ 544 Seaview Road GRANGE	Maisonette	CT 5358/776	13111
546 Seaview Road GRANGE	House	CT 5634/320	13114
548 Seaview Road GRANGE	House	CT 5515/526	13115
596 Seaview Road GRANGE	Maisonette	CT 5512/404	13116
598 Seaview Road GRANGE	Maisonette	CT 5363/794	13117
1/ 604 Seaview Road GRANGE	Single Storey Flats	CT 5825/228	13112
606 Seaview Road GRANGE	House	CT 5609/245	13119
608 Seaview Road GRANGE	House	CT 5136/250	13120
610 Seaview Road GRANGE	House	CT 5209/718	13121
1 Surrey Street GRANGE	House	CT 5515/834	13122
2 Surrey Street GRANGE	House & Granny Flat	CT 5062/626	13123
3 Surrey Street GRANGE	House	CT 5832/745	13124
4 Surrey Street GRANGE	House	CT 5784/974	13125
5 Surrey Street GRANGE	House	CT 5159/803	13126
6 Surrey Street GRANGE	House	CT 5499/578	13127
7 Surrey Street GRANGE	House	CT 5488/295	13128
8 Surrey Street GRANGE	House	CT 5075/251	13129
9 Surrey Street GRANGE	House	CT 5120/464	13130
10 Surrey Street GRANGE	House	CT 5264/972	13131
11 Surrey Street GRANGE	House	CT 5508/859	13132
12 Surrey Street GRANGE	House	CT 5393/892	13133
13 Surrey Street GRANGE	House	CT 5173/742	13134
14 Surrey Street GRANGE	House	CT 5649/289	13135
15 Surrey Street GRANGE	House	CT 5434/981	13136

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Surrey Street GRANGE	House	CT 5517/96	13137
17 Surrey Street GRANGE	House	CT 5468/473	13138
18 Surrey Street GRANGE	House	CT 5163/323	13139
19 Surrey Street GRANGE	House	CT 5086/923	13140
20 Surrey Street GRANGE	House	CT 5153/871	13141
2 Wilson Court GRANGE	House	CT 5086/392	13142
4 Wilson Court GRANGE	House	CT 5576/731	13143
6 Wilson Court GRANGE	Home Unit	CT 5146/954	13144
8 Wilson Court GRANGE	Home Unit	CT 5146/955	13145
10 Wilson Court GRANGE	House	CT 5189/354	13146
12 Wilson Court GRANGE	Maisonette	CT 5133/762	13147
14 Wilson Court GRANGE	Maisonette	CT 5133/762	13148
16 Wilson Court GRANGE	House	CT 5140/52	13149
21 Anthony Street HENLEY BEACH	House	CT 5523/353	13007
22 Anthony Street HENLEY BEACH	House	CT 5271/907	13008
23 Anthony Street HENLEY BEACH	House	CT 5156/350	13009
24 Anthony Street HENLEY BEACH	House	CT 5525/834	13010
25 Anthony Street HENLEY BEACH	House	CT 5128/855	13011
26 Anthony Street HENLEY BEACH	House	CT 5097/818	13012
28 Anthony Street HENLEY BEACH	House	CT 5283/163	13013
29 Anthony Street HENLEY BEACH	House	CT 5188/568	13014
31 Anthony Street HENLEY BEACH	House	CT 5057/102	13015
34 Anthony Street HENLEY BEACH	House	CT 5236/791	13016
1 Crewe Street HENLEY BEACH	House	CT 5150/367	12737
3 Crewe Street HENLEY BEACH	House	CT 5487/501	12738
5 Crewe Street HENLEY BEACH	House	CT 5080/124	12739
7 Crewe Street HENLEY BEACH	House	CT 5504/351	12740
9 Crewe Street HENLEY BEACH	House	CT 5284/842	12741
10 Crewe Street HENLEY BEACH	House	CT 5838/118	12742
12 Crewe Street HENLEY BEACH	House	CT 5731/481	12743
13-15 Crewe Street HENLEY BEACH	Maisonettes	CT 5208/264	12744
14 Crewe Street HENLEY BEACH	House	CT 5641/636	12745
17 Crewe Street HENLEY BEACH	House	CT 5128/55	12746
18 Crewe Street HENLEY BEACH	House	CT 1121/57	12747
19 Crewe Street HENLEY BEACH	House	CT 5751/116	12748
20 Crewe Street HENLEY BEACH	House	CT 5459/560	12749
21 Crewe Street HENLEY BEACH	House	CT 979/59	12750
23 Crewe Street HENLEY BEACH	House	CT 5749/354	12752

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
24 Crewe Street HENLEY BEACH	House	CT 5240/17	12753
3 Durham Street HENLEY BEACH	House	CT 5448/757	12754
5 Durham Street HENLEY BEACH	House	CT 5520/636	12755
10 Durham Street HENLEY BEACH	House	CT 5074/805	12756
12 Durham Street HENLEY BEACH	House	CT 5090/745	12757
16 Durham Street HENLEY BEACH	House	CT 5727/662	12758
18 Durham Street HENLEY BEACH	House	CT 5511/931	12759
20 Durham Street HENLEY BEACH	House	CT 5669/330	12760
5 East Terrace HENLEY BEACH	House	CT 5310/490	12761
9 East Terrace HENLEY BEACH	House	CT 5309/947	12762
12 East Terrace HENLEY BEACH	House	CT 2347/193	12763
13 East Terrace HENLEY BEACH	House	CT 5644/890	12764
14 East Terrace HENLEY BEACH	House	CT 5702/33	12765
15 East Terrace HENLEY BEACH	House	CT 5074/152	12766
21 East Terrace HENLEY BEACH	House	CT 5679/303	12767
22 East Terrace HENLEY BEACH	House	CT 5816/694	12768
23 East Terrace HENLEY BEACH	Home Unit	CT 5008/207	12769
24 East Terrace HENLEY BEACH	House	CT 5839/824	12770
25 East Terrace HENLEY BEACH	Home Unit	CT 5008/208	12771
26 East Terrace HENLEY BEACH	House	CT 5494/299	12772
27 East Terrace HENLEY BEACH	House	CT 5516/473	12773
28 East Terrace HENLEY BEACH	House	CT 5223/115	12774
30 East Terrace HENLEY BEACH	House	CT 5617/598	12775
32 East Terrace HENLEY BEACH	House	CT 5471/865	12776
34 East Terrace HENLEY BEACH	House	CT 5202/461	12777
38 East Terrace HENLEY BEACH	House	CT 5285/405	12778
39 East Terrace HENLEY BEACH	House	CT 5657/954	12779
40 East Terrace HENLEY BEACH	House	CT 1256/101	12780
42 East Terrace HENLEY BEACH	House	CT 5786/530	12781
43 East Terrace HENLEY BEACH	House	CT 5499/170	12782
44 East Terrace HENLEY BEACH	House	CT 5122/648	12783
46 East Terrace HENLEY BEACH	House	CT 5228/642	12785
47 East Terrace HENLEY BEACH	House	CT 1237/87	12786
48 East Terrace HENLEY BEACH	House	CT 5094/686	12787
49 East Terrace HENLEY BEACH	House	CT 5790/489	12788
50 East Terrace HENLEY BEACH	House	CT 5157/519	12789
51 East Terrace HENLEY BEACH	House	CT 5790/485	12790
52 East Terrace HENLEY BEACH	House	CT 5131/266	12791

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
53 East Terrace HENLEY BEACH	House	CT 5525/892	12792
55 East Terrace HENLEY BEACH	House	CT 5186/724	12793
56 East Terrace HENLEY BEACH	House	CT 5200/328	12794
57 East Terrace HENLEY BEACH	House	CT 5087/691	12795
59 East Terrace HENLEY BEACH	House	CT 5495/431	12796
61 East Terrace HENLEY BEACH	House	CT 5502/760	12797
63 East Terrace HENLEY BEACH	House	CT 5163/347	12798
64 East Terrace HENLEY BEACH	House	CT 5727/371	12799
65 East Terrace HENLEY BEACH	Home Unit	CT 5027/934	12800
66 East Terrace HENLEY BEACH	House	CT 5282/328	12801
67 East Terrace HENLEY BEACH	Home Unit	CT 5027/935	12802
68 East Terrace HENLEY BEACH	House	CT 5671/900	12803
71 East Terrace HENLEY BEACH	House	CT 5604/139	12804
72 East Terrace HENLEY BEACH	House	CT 5065/355	12805
73 East Terrace HENLEY BEACH	Telephone Exchange Operation	CT 5547/3132	12806
75 East Terrace HENLEY BEACH	House	CT 5425/869	12807
76 East Terrace HENLEY BEACH	House	CT 5497/988	12808
77 East Terrace HENLEY BEACH	House	CT 5358/608	12809
78 East Terrace HENLEY BEACH	House	CT 5333/769	12810
79 East Terrace HENLEY BEACH	House	CT 4389/976	12811
80 East Terrace HENLEY BEACH	House	CT 5591/925	12812
82 East Terrace HENLEY BEACH	House	CT 5831/14	12813
83 East Terrace HENLEY BEACH	House	CT 5335/562	12814
84 East Terrace HENLEY BEACH	House	CT 5507/323	12815
85 East Terrace HENLEY BEACH	House	CT 5743/335	12816
86 East Terrace HENLEY BEACH	House	CT 5145/65	12817
87 East Terrace HENLEY BEACH	House	CT 5159/320	12818
88 East Terrace HENLEY BEACH	Maisonette	CT 5118/622	12819
89 East Terrace HENLEY BEACH	House	CT 5589/742	12820
90 East Terrace HENLEY BEACH	Maisonette	CT 5124/935	12821
92 East Terrace HENLEY BEACH	House	CT 5431/601	12822
94 East Terrace HENLEY BEACH	House	CT 5606/549	12823
97 East Terrace HENLEY BEACH	House	CT 5220/154	12824
100 East Terrace HENLEY BEACH	House	CT 5260/697	12826
102 East Terrace HENLEY BEACH	House	CT 1080/27	12827
106 East Terrace HENLEY BEACH	House	CT 5520/395	12828
108 East Terrace HENLEY BEACH	House	CT 5274/358	12829

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
110 East Terrace HENLEY BEACH	House	CT 5816/776	12830
112 East Terrace HENLEY BEACH	House	CT 5804/874	12831
114 East Terrace HENLEY BEACH	House	CT 1270/142	12832
116 East Terrace HENLEY BEACH	House	CT 5336/886	12833
118 East Terrace HENLEY BEACH	House	CT 5293/106	12834
120 East Terrace HENLEY BEACH	House	CT 5769/305	12835
122 East Terrace HENLEY BEACH	House	CT 5721/282	12836
128 East Terrace HENLEY BEACH	House	CT 5352/770	12837
130 East Terrace HENLEY BEACH	House	CT 5269/72	12838
132 East Terrace HENLEY BEACH	House	CT 5961/892	12839
138 East Terrace HENLEY BEACH	House	CT 5255/752	12840
142 East Terrace HENLEY BEACH	House	CT 5151/310	12841
144 East Terrace HENLEY BEACH	House	CT 5082/280	12842
146 East Terrace HENLEY BEACH	House	CT 5463/966	12843
148 East Terrace HENLEY BEACH	House	CT 5785/661	12844
150 East Terrace HENLEY BEACH	House	CT 5397/949	12845
152 East Terrace HENLEY BEACH	House	CT 5129/898	12846
154 East Terrace HENLEY BEACH	House	CT 5561/329	12847
608 Grange Road HENLEY BEACH	House	CT 5502/876	13056
610 Grange Road HENLEY BEACH	House	CT 5132/722	13057
612 Grange Road HENLEY BEACH	House	CT 5525/682	13058
614 Grange Road HENLEY BEACH	House	CT 5782/885	13059
616 Grange Road HENLEY BEACH	House	CT 5527/990	13060
618 Grange Road HENLEY BEACH	House	CT 5431/499	13061
620 Grange Road HENLEY BEACH	House	CT 5071/466	13062
622 Grange Road HENLEY BEACH	House	CT 3095/38	13063
628 Grange Road HENLEY BEACH	House	CT 5267/816	13064
632 Grange Road HENLEY BEACH	House	CT 5464/870	13045
20 Henley Beach Road HENLEY BEACH	House	CT 5837/391	12848
22 Henley Beach Road HENLEY BEACH	House	CT 5464/313	12849
24 Henley Beach Road HENLEY BEACH	House	CT 5332/933	12850
28 Henley Beach Road HENLEY BEACH	House	CT 5178/760	12851
1 Main Street HENLEY BEACH	Home Unit	CT 5431/830	12857
3 Main Street HENLEY BEACH	Home Unit	CT 5431/831	12858
9 Main Street HENLEY BEACH	House	CT 5119/208	12859
11 Main Street HENLEY BEACH	House	CT 5790/486	12860
13 Main Street HENLEY BEACH	House	CT 5138/773	12861
15 Main Street HENLEY BEACH	House	CT 1267/120	12862

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
17 Main Street HENLEY BEACH	House	CT 5251/451	12863
34 Main Street HENLEY BEACH	House	CT 5504/85	12864
36-38 Main Street HENLEY BEACH	Maisonettes	CT 5710/910	12865
40-42 Main Street HENLEY BEACH	House	CT 5513/240	12866
44 Main Street HENLEY BEACH	Maisonette	CT 5868/676	12867
46 Main Street HENLEY BEACH	Maisonette	CT 5868/677	20878
48 Main Street HENLEY BEACH	Maisonette	CT 5868/678	12868
50 Main Street HENLEY BEACH	Maisonette	CT 5868/679	20879
52 Main Street HENLEY BEACH	House	CT 5524/627	12869
54 Main Street HENLEY BEACH	House	CT 5839/924	12870
56 Main Street HENLEY BEACH	House	CT 5885/237	12871
58 Main Street HENLEY BEACH	House	CT 5382/190	12872
114 Marlborough Street HENLEY BEACH	House	CT 5154/594	12873
116 Marlborough Street HENLEY BEACH	House	CT 5702/31	12874
118 Marlborough Street HENLEY BEACH	House & Granny Flat	CT 5216/335	12875
120 Marlborough Street HENLEY BEACH	House	CT 5491/866	12876
121 Marlborough Street HENLEY BEACH	House	CT 5599/117	12877
122 Marlborough Street HENLEY BEACH	House	CT 5330/567	12878
123 Marlborough Street HENLEY BEACH	House	CT 5255/898	12879
124 Marlborough Street HENLEY BEACH	House	CT 5842/528	12880
125 Marlborough Street HENLEY BEACH	House	CT 4187/430	12881
127 Marlborough Street HENLEY BEACH	House	CT 5119/599	12882
128 Marlborough Street HENLEY BEACH	House	CT 5783/492	12883
130 Marlborough Street HENLEY BEACH	House	CT 5095/266	12884
131 Marlborough Street HENLEY BEACH	House	CT 5230/614	12885
132 Marlborough Street HENLEY BEACH	House	CT 5530/701	12886
134 Marlborough Street HENLEY BEACH	House	CT 4126/546	12887
135 Marlborough Street HENLEY BEACH	House	CT 1027/95	12888
136 Marlborough Street HENLEY BEACH	House	CT 5506/486	12889
137 Marlborough Street HENLEY BEACH	House	CT 5184/828	12890
138 Marlborough Street HENLEY BEACH	House	CT 5170/58	12891
139 Marlborough Street HENLEY BEACH	House	CT 5516/941	12892
140 Marlborough Street HENLEY BEACH	House	CT 5296/362	12893
141 Marlborough Street HENLEY BEACH	House	CT 5848/450	12894
142 Marlborough Street HENLEY BEACH	House	CT 5268/153	12895
143 Marlborough Street HENLEY BEACH	House	CT 5497/99	12896
145 Marlborough Street HENLEY BEACH	House	CT 5367/60	12897
146 Marlborough Street HENLEY BEACH	House	CT 5140/413	12898

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
147 Marlborough Street HENLEY BEACH	Maisonette	CT 5285/330	12899
148 Marlborough Street HENLEY BEACH	House	CT 5154/224	12900
149 Marlborough Street HENLEY BEACH	Maisonette	CT 5072/903	12901
150 Marlborough Street HENLEY BEACH	House	CT 5627/24	12902
151 Marlborough Street HENLEY BEACH	House	CT 5551/244	12903
152 Marlborough Street HENLEY BEACH	House	CT 5474/485	12904
156 Military Road HENLEY BEACH	House	CT 5094/972	12906
160 Military Road HENLEY BEACH	House	CT 5211/570	12907
162 Military Road HENLEY BEACH	House	CT 5177/510	12909
164 Military Road HENLEY BEACH	House	CT 5511/502	12910
168 Military Road HENLEY BEACH	House	CT 5826/357	12911
170 Military Road HENLEY BEACH	House	CT 5522/890	12912
184 Military Road HENLEY BEACH	House	CT 5315/13	12913
188 Military Road HENLEY BEACH	House	CT 5790/490	12914
212 Military Road HENLEY BEACH	House	CT 5635/252	12915
220 Military Road HENLEY BEACH	House	CT 5157/914	12916
235 Military Road HENLEY BEACH	House	CT 5482/844	12917
237 Military Road HENLEY BEACH	House	CT 5158/437	12918
239 Military Road HENLEY BEACH	House	CT 5352/594	12919
241 Military Road HENLEY BEACH	House	CT 5141/270	12920
243 Military Road HENLEY BEACH	House	CT 5392/359	12921
245 Military Road HENLEY BEACH	House	CT 5713/76	12922
247 Military Road HENLEY BEACH	House	CT 5234/855	12923
249 Military Road HENLEY BEACH	House	CT 5128/885	12924
257 Military Road HENLEY BEACH	Maisonette	CT 3649/198	12927
257a Military Road HENLEY BEACH	Maisonette	CT 3649/198	12928
259 Military Road HENLEY BEACH	House	CT 5681/741	12929
263 Military Road HENLEY BEACH	House	CT 5197/103	12930
265 Military Road HENLEY BEACH	House	CT 5499/35	12931
267 Military Road HENLEY BEACH	House	CT 5372/467	12932
269 Military Road HENLEY BEACH	House	CT 5485/799	12933
65 North Street HENLEY BEACH	House	CT 5599/980	12934
67 North Street HENLEY BEACH	House	CT 5599/677	12935
69 North Street HENLEY BEACH	House	CT 5261/110	12936
71 North Street HENLEY BEACH	House	CT 5090/188	12937
73 North Street HENLEY BEACH	House	CT 5468/754	12938
75 North Street HENLEY BEACH	House	CT 5819/90	12939
77 North Street HENLEY BEACH	House	CT 5172/670	12940

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
81 North Street HENLEY BEACH	House	CT 861/191	12941
83 North Street HENLEY BEACH	House	CT 5149/878	12942
85 North Street HENLEY BEACH	House	CT 5820/81	12943
87 North Street HENLEY BEACH	House	CT 5350/522	12944
95 North Street HENLEY BEACH	Home Unit	CT 5009/148	12945
97 North Street HENLEY BEACH	House	CT 5095/879	12946
99 North Street HENLEY BEACH	Maisonette	CT 5181/267	12947
101 North Street HENLEY BEACH	Maisonette	CT 5151/432	12948
102 North Street HENLEY BEACH	House	CT 5321/717	12949
103 North Street HENLEY BEACH	Maisonette	CT 5117/705	12950
104 North Street HENLEY BEACH	House	CT 5502/927	12951
105 North Street HENLEY BEACH	Maisonette	CT 5326/879	12952
106 North Street HENLEY BEACH	House	CT 5791/946	12953
108 North Street HENLEY BEACH	House	CT 5210/118	12954
110 North Street HENLEY BEACH	House	CT 5168/72	12955
114 North Street HENLEY BEACH	House	CT 5461/915	12956
116 North Street HENLEY BEACH	House	CT 5126/115	12957
118 North Street HENLEY BEACH	House	CT 5180/711	12958
120 North Street HENLEY BEACH	House	CT 5523/937	12959
1/ 124-126 North Street HENLEY BEACH	Home Unit	CT 5001/317	12961
2/ 124-126 North Street HENLEY BEACH	Home Unit	CT 5001/318	12962
128 North Street HENLEY BEACH	Home Unit	CT 5852/797	12963
130 North Street HENLEY BEACH	Home Unit	CT 5852/798	12964
132 North Street HENLEY BEACH	House	CT 5236/544	12965
134 North Street HENLEY BEACH	House	CT 5147/368	12966
3-5 South Street HENLEY BEACH	House	CT 5204/696	12969
4 South Street HENLEY BEACH	House	CT 5512/234	12968
6 South Street HENLEY BEACH	Maisonette	CT 5890/736	12970
6A South Street HENLEY BEACH	Maisonette	CT 5890/737	20880
7 South Street HENLEY BEACH	House	CT 5085/352	12971
8 South Street HENLEY BEACH	House	CT 5422/453	12972
9 South Street HENLEY BEACH	Home Unit	CT 5023/982	12974
11 South Street HENLEY BEACH	Home Unit	CT 5023/983	12975
13 South Street HENLEY BEACH	House	CT 5212/216	12976
15 South Street HENLEY BEACH	House	CT 5545/379	12977
19 South Street HENLEY BEACH	House	CT 5343/920	12978
21 South Street HENLEY BEACH	House	CT 5508/38	12979
9 Sussex Street HENLEY BEACH	House	CT 5085/96	12980

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
1 White Street HENLEY BEACH	House		12981
2 White Street HENLEY BEACH	House		12982
3 White Street HENLEY BEACH	House	CT 5436/967	12983
4 White Street HENLEY BEACH	House		12984
5 White Street HENLEY BEACH	House		12985
7 White Street HENLEY BEACH	House	CT 5693/979	12986
9 White Street HENLEY BEACH	Home Unit	CT 5788/71	12987
11 White Street HENLEY BEACH	Home Unit	CT 5788/70	12988
13 White Street HENLEY BEACH	Maisonette	CT 5071/696	12989
15 White Street HENLEY BEACH	Maisonette	CT 5071/696	12990
17 White Street HENLEY BEACH	House	CT 5431/381	12991
19 White Street HENLEY BEACH	House	CT 5211/566	12992
21-23 White Street HENLEY BEACH	Maisonettes	CT 5435/229	12993
2 York Street HENLEY BEACH	House	CT 5096/95	12994
3 York Street HENLEY BEACH	House	CT 5129/120	12995
4 York Street HENLEY BEACH	House	CT 5289/504	12996
5 York Street HENLEY BEACH	House	CT 5785/474	12997
6 York Street HENLEY BEACH	House	CT 5524/190	12998
7 York Street HENLEY BEACH	House	CT 5256/878	12999
8 York Street HENLEY BEACH	House	CT 5515/355	13000
9-11 York Street HENLEY BEACH	House	CT 5839/379	13001
10 York Street HENLEY BEACH	Maisonette	CT 5338/60	13002
12 York Street HENLEY BEACH	Maisonette	CT 5338/61	13003
13 York Street HENLEY BEACH	House	CT 5074/784	13004
14 York Street HENLEY BEACH	House	CT 894/7	13005
15 York Street HENLEY BEACH	House	CT 5134/848	13006
25 Henley Beach Road HENLEY BEACH SOUTH	House	CT 5855/134	12852
27 Henley Beach Road HENLEY BEACH SOUTH	House	CT 5503/502	12853
29 Henley Beach Road HENLEY BEACH SOUTH	House	CT 5302/358	12854
31 Henley Beach Road HENLEY BEACH SOUTH	House	CT 5256/695	12855
33 Henley Beach Road HENLEY BEACH SOUTH	House	CT 5798/618	12856
1-3 Chapel Street HINDMARSH	Row dwellings	CT 1945/187	13207
2 Hindmarsh Place HINDMARSH	Cottage	CT 2159/50 CT 2159/51	13206
31 Holden Street HINDMARSH	Dwelling	CT 2461/113	13188

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
35 Holden Street HINDMARSH	Attached Cottages	CT 5218/373	13189
3 Milner Street HINDMARSH	Dwelling	CT 5127/733	13150
5 Milner Street HINDMARSH	Office	CT 1931/134 CT 2321/143	13151
41 Orsmond Street HINDMARSH	Office	CT 5175/743	13156
47 Orsmond Street HINDMARSH	Office	CT 5261/536	13159
49-51 Orsmond Street HINDMARSH	Shop/Dwelling	CT 2463/17	13160
130 Port Road HINDMARSH	Former Bank	CT 5285/988	13164
132 Port Road HINDMARSH	Shop	CT 5204/747	13165
134-138 Port Road HINDMARSH	Shops	CT 5204/752	13166
140 Port Road HINDMARSH	Shop	CT 5179/974	13167
142-144 Port Road HINDMARSH	Shop	CT 5179/973	13168
146 Port Road HINDMARSH	Shop	CT 5728/447	13169
158-162 Port Road HINDMARSH	Shop	CT 5524/686	13170
172 Port Road HINDMARSH	Land of Promise Hotel	CT 5135/98 CT 5137/967	13171
184-186 Port Road HINDMARSH	Shop	CT 5523/422	13172
190 Port Road HINDMARSH	Shop	CT 1098/20	13173
199 Port Road HINDMARSH	Shop	CT 5231/220	13186
224-226 Port Road HINDMARSH	Shop	CT 5201/745	13175
280-282A Port Road HINDMARSH	Shop	CT 5320/725	13178
284-288 Port Road HINDMARSH	Shop	CT 5254/231	13179
290 Port Road HINDMARSH	Shop	CT 5254/230	13180
292-294 Port Road HINDMARSH	Shop	CT 5212/612	13181
296 Port Road HINDMARSH	Shop	CT 5206/761	13182
298 Port Road HINDMARSH	Shop	CT 5206/758	13183
300-302 Port Road HINDMARSH	Shops	CT 5206/760 CT 5260/759	13184
7 Arkaba Road KILKENNY	Maisonette	CT 5491/290	11695
9 Arkaba Road KILKENNY	Maisonette	CT 5491/413	11696
17 Arkaba Road KILKENNY	House	CT 5838/988	11698
1/ 19 Arkaba Road KILKENNY	Maisonette	CT 5685/915	11699
2/ 19 Arkaba Road KILKENNY	Maisonette	CT 5685/916	11700
11 Aroona Road KILKENNY	House	CT 4173/9	11701
13 Aroona Road KILKENNY	House	CT 5541/953	11702
15 Aroona Road KILKENNY	House	CT 5796/762	11703
23 Aroona Road KILKENNY	House	CT 5797/938	11704
27 Aroona Road KILKENNY	House	CT 5694/437	11705
29 Aroona Road KILKENNY	House	CT 5555/477	11706

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
31 Aroona Road KILKENNY	House	CT 5718/784	11707
35 Aroona Road KILKENNY	House	CT 5570/376	11709
37 Aroona Road KILKENNY	House	CT 5422/117	11710
39 Aroona Road KILKENNY	House	CT 5672/53	11711
41 Aroona Road KILKENNY	House	CT 5309/811	11712
1 Kupman Street KILKENNY	House	CT 5714/569	11713
2 Kupman Street KILKENNY	House	CT 5609/546	11714
3 Kupman Street KILKENNY	House	CT 5536/114	11715
5 Kupman Street KILKENNY	House	CT 5661/606	11716
7 Kupman Street KILKENNY	House	CT 5197/742	11717
9 Kupman Street KILKENNY	House	CT 5754/642	11718
11 Kupman Street KILKENNY	House	CT 5770/890	11719
13 Kupman Street KILKENNY	House	CT 5328/295	11721
14 Kupman Street KILKENNY	House	CT 5567/377	11722
15 Kupman Street KILKENNY	House	CT 5800/644	11723
16 Kupman Street KILKENNY	House	CT 5514/12	11724
17 Kupman Street KILKENNY	House	CT 5532/551	11725
18 Kupman Street KILKENNY	House	CT 5649/611	11726
19 Kupman Street KILKENNY	House	CT 5806/689	11727
21 Kupman Street KILKENNY	House	CT 5385/922	11728
23 Kupman Street KILKENNY	House	CT 5518/681	11729
26 Kupman Street KILKENNY	House	CT 5700/902	11730
28 Kupman Street KILKENNY	House	CT 5668/135	11731
10 Mundulla Street KILKENNY	House	CT 5588/211	11732
12 Mundulla Street KILKENNY	Maisonette	CT 3949/82	11733
14 Mundulla Street KILKENNY	Maisonette	CT 1126/30	11734
16 Mundulla Street KILKENNY	Maisonette	CT 5014/413	11735
17 Mundulla Street KILKENNY	House	CT 5394/265	11736
18 Mundulla Street KILKENNY	Maisonette	CT 5014/412	11737
19 Mundulla Street KILKENNY	House	CT 5129/915	11738
21-23 Mundulla Street KILKENNY	Maisonette	CT 5396/124	11739
22 Mundulla Street KILKENNY	House	CT 5412/686	11740
24 Mundulla Street KILKENNY	House	CT 5806/669	11741
25 Mundulla Street KILKENNY	Maisonette	CT 5379/94	11742
26 Mundulla Street KILKENNY	House	CT 5209/144	11743
30 Mundulla Street KILKENNY	House	CT 5729/124	11745
32 Mundulla Street KILKENNY	House	CT 5204/443	11746
1 Myponga Terrace KILKENNY	House	CT 5391/203	11747

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
2 Myponga Terrace KILKENNY	House	CT 5686/309	11748
4 Myponga Terrace KILKENNY	House	CT 5082/66	11749
5 Myponga Terrace KILKENNY	House	CT 5818/666	11750
7 Myponga Terrace KILKENNY	House	CT 5325/623	11751
8 Myponga Terrace KILKENNY	House	CT 5461/900	11752
9 Myponga Terrace KILKENNY	House	CT 5465/134	11753
10 Myponga Terrace KILKENNY	Maisonette	CT 5761/793	11754
11 Myponga Terrace KILKENNY	House	CT 5686/273	20874
11a Myponga Terrace KILKENNY	House	CT 5645/625	11755
12 Myponga Terrace KILKENNY	Maisonette	CT 5516/955	11756
14 Myponga Terrace KILKENNY	House	CT 5256/572	11757
15 Myponga Terrace KILKENNY	Home Unit	CT 5006/993	11758
17 Myponga Terrace KILKENNY	Home Unit	CT 5006/992	11759
18 Myponga Terrace KILKENNY	House	CT 5188/109	11760
19 Myponga Terrace KILKENNY	House	CT 5678/365	11761
1 Penola Street KILKENNY	House	CT 1227/97	11762
2 Penola Street KILKENNY	House	CT 5751/815	11763
3 Penola Street KILKENNY	House	CT 5339/467	11764
4a Penola Street KILKENNY	House	CT 5144/142	11765
6 Penola Street KILKENNY	House	CT 5292/676	11767
7 Penola Street KILKENNY	House	CT 5405/556	11768
8 Penola Street KILKENNY	House	CT 5761/816	11769
9 Penola Street KILKENNY	House	CT 5496/444	11770
10 Penola Street KILKENNY	House	CT 5725/451	11771
12 Penola Street KILKENNY	House	CT 5162/686	11772
13 Penola Street KILKENNY	House	CT 5501/371	11773
14 Penola Street KILKENNY	House	CT 5365/119	11774
15 Penola Street KILKENNY	House	CT 5399/573	11775
16 Penola Street KILKENNY	House	CT 5422/929	11776
17 Penola Street KILKENNY	House	CT 5793/497	11777
18-20 Penola Street KILKENNY	Building	CT 5623/774 CT 5623/775	11778
4 Tarcowie Street KILKENNY	House	CT 5667/24	11781
8 Tarcowie Street KILKENNY	House	CT 5832/182	11782
12 Tarcowie Street KILKENNY	House	CT 5546/570	11783
14 Tarcowie Street KILKENNY	House	CT 5220/854	11784
15 Tarcowie Street KILKENNY	House	CT 5153/204	11785
18 Tarcowie Street KILKENNY	House	CT 5331/308	11787

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
19 Tarcowie Street KILKENNY	House	CT 5273/600	11788
21 Tarcowie Street KILKENNY	House	CT 5181/875	11789
1 Wilpena Terrace KILKENNY	House	CT 5731/725	11791
2 Wilpena Terrace KILKENNY	House	CT 5827/239	11792
3 Wilpena Terrace KILKENNY	Maisonette	CT 5470/692	11793
5 Wilpena Terrace KILKENNY	Maisonette	CT 5470/693	11795
6 Wilpena Terrace KILKENNY	House	CT 5422/149	11796
7 Wilpena Terrace KILKENNY	Maisonette	CT 5323/520	11797
8 Wilpena Terrace KILKENNY	House	CT 5301/448	11798
9 Wilpena Terrace KILKENNY	Maisonette	CT 5323/519	11799
11 Wilpena Terrace KILKENNY	Maisonette	CT 5334/485	11801
13 Wilpena Terrace KILKENNY	Maisonette	CT 5302/187	11803
14 Wilpena Terrace KILKENNY	Maisonette	CT 5367/450	11804
15 Wilpena Terrace KILKENNY	Maisonette	CT 5503/504	11805
16 Wilpena Terrace KILKENNY	Maisonette	CT 1473/59	11806
17 Wilpena Terrace KILKENNY	Maisonette	CT 5503/376	11807
21 Wilpena Terrace KILKENNY	Maisonette	CT 5211/16	11811
22 Wilpena Terrace KILKENNY	House	CT 5827/947	11812
23 Wilpena Terrace KILKENNY	Maisonette	CT 5116/561	11813
25 Wilpena Terrace KILKENNY	House	CT 5187/482	11815
26 Wilpena Terrace KILKENNY	House	CT 5285/172	11816
27 Wilpena Terrace KILKENNY	House	CT 5273/600	11817
27a Wilpena Terrace KILKENNY	Hall	CT 5827/928	11818
28 Wilpena Terrace KILKENNY	House	CT 5786/157	11819
29 Wilpena Terrace KILKENNY	House	CT 5266/281	11820
30 Wilpena Terrace KILKENNY	House	CT 5734/749	11821
32 Wilpena Terrace KILKENNY	House	CT 5512/678	11824
34 Wilpena Terrace KILKENNY	House	CT 5732/270	11826
46-48 Wilpena Terrace KILKENNY	Maisonettes	CT 5627/636	11832
51 Wilpena Terrace KILKENNY	House	CT 5412/360	11833
53 Wilpena Terrace KILKENNY	House	CT 5191/175	11834
54 Wilpena Terrace KILKENNY	Single Storey Flats	CT 5668/134	11835
56 Wilpena Terrace KILKENNY	House	CT 1176/15	11837
57 Wilpena Terrace KILKENNY	House	CT 5669/882	11838
58 Wilpena Terrace KILKENNY	House	CT 5202/567	11839
61 Wilpena Terrace KILKENNY	House	CT 5390/980	11842
1/ 62 Wilpena Terrace KILKENNY	Single Storey Flat	CT 5755/280	11843
2/ 62 Wilpena Terrace KILKENNY	Single Storey Flat	CT 5755/280	11844

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
66 Wilpena Terrace KILKENNY	House	CT 5409/900	11846
68 Wilpena Terrace KILKENNY	House	CT 5781/912	11848
69 Wilpena Terrace KILKENNY	House	CT 5195/277	11849
70 Wilpena Terrace KILKENNY	House	CT 4119/640 CT 4119/641	11850
71 Wilpena Terrace KILKENNY	House	CT 5800/584	11851
72 Wilpena Terrace KILKENNY	House & Granny Flat	CT 5126/542	11852
73 Wilpena Terrace KILKENNY	House	CT 5614/244	11853
74 Wilpena Terrace KILKENNY	House	CT 5815/292	11854
76 Wilpena Terrace KILKENNY	House	CT 5820/348	11856
77 Wilpena Terrace KILKENNY	House	CT 1316/188	11857
78 Wilpena Terrace KILKENNY	House	CT 5651/362	11858
79 Wilpena Terrace KILKENNY	House	CT 5412/450	11859
80 Wilpena Terrace KILKENNY	House	CT 5813/356	11860
81 Wilpena Terrace KILKENNY	House	CT 5253/780	11861
82 Wilpena Terrace KILKENNY	House	CT 5752/136	11862
83 Wilpena Terrace KILKENNY	House	CT 5500/351	11863
85 Wilpena Terrace KILKENNY	House	CT 5300/578 CT 5300/579	11865
2 Yallum Terrace KILKENNY	House	CT 5672/61	11867
3 Yallum Terrace KILKENNY	Maisonette	CT 5469/823	11868
4 Yallum Terrace KILKENNY	House	CT 5101/633	11869
5 Yallum Terrace KILKENNY	Maisonette	CT 5469/823	11870
6 Yallum Terrace KILKENNY	House	CT 4138/711	11871
7 Yallum Terrace KILKENNY	Maisonette	CT 5011/864	11872
8 Yallum Terrace KILKENNY	House	CT 5784/715	11873
9 Yallum Terrace KILKENNY	Maisonette	CT 5899/121	11874
10 Yallum Terrace KILKENNY	House	CT 5520/644	11875
11 Yallum Terrace KILKENNY	House	CT 5573/983	11876
13 Yallum Terrace KILKENNY	House	CT 5787/230	11878
15 Yallum Terrace KILKENNY	House	CT 5211/951	11881
19 Yallum Terrace KILKENNY	House	CT 5590/681	11884
21 Yallum Terrace KILKENNY	House	CT 5215/556	11885
1-3 Albert Street Ovingham	Maisonettes	CT 5795/494	9276
5 Albert Street Ovingham	House	CT 5827/913	9277
3 Coombe Street Ovingham	House	CT 5841/23	9346
4 Coombe Street Ovingham	House	CT 4356/310	9347
15 Gilbert Street Ovingham	House	CT 5313/194	9433

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Gilbert Street OVINHAM	Maisonette	CT 5089/204	9434
16a Gilbert Street OVINHAM	Maisonette	CT 5089/203	9435
17 Gilbert Street OVINHAM	House	CT 5611/234	9436
18 Gilbert Street OVINHAM	House	CT 5334/627	9437
19 Gilbert Street OVINHAM	Maisonette	CT 5611/228	9438
20 Gilbert Street OVINHAM	House	CT 5816/717	9439
21 Gilbert Street OVINHAM	Maisonette	CT 1745/43	9440
1/ 23 Gilbert Street OVINHAM	Single Storey Flat	CT 5387/576	9441
2/ 23 Gilbert Street OVINHAM	Single Storey Flat	CT 4212/336	9442
24 Gilbert Street OVINHAM	House	CT 5263/179	9443
26 Gilbert Street OVINHAM	House	CT 5339/455	9444
28 Gilbert Street OVINHAM	House	CT 5201/564	9445
29 Gilbert Street OVINHAM	House	CT 5781/865	9446
30 Gilbert Street OVINHAM	House	CT 5469/573	9447
33 Gilbert Street OVINHAM	House	CT 5780/417	9448
34 Gilbert Street OVINHAM	House	CT 5408/37	9449
36 Gilbert Street OVINHAM	House	CT 5832/417	9450
37 Gilbert Street OVINHAM	House	CT 5646/182	9451
38 Gilbert Street OVINHAM	House	CT 5738/57	9452
40 Gilbert Street OVINHAM	House	CT 5480/847	9453
41-43 Gilbert Street OVINHAM	House	CT 5782/963	9454
45 Gilbert Street OVINHAM	House	CT 5269/746	9455
47 Gilbert Street OVINHAM	House	CT 5387/118	9456
49 Gilbert Street OVINHAM	House	CT 5355/974	9457
51 Gilbert Street OVINHAM	House	CT 5270/988	9458
53 Gilbert Street OVINHAM	House	CT 5227/65	9459
55 Gilbert Street OVINHAM	House	CT 5661/237	9460
2 Guthrie Street OVINHAM	House	CT 5744/328	9484
4 Guthrie Street OVINHAM	House	CT 5427/690	9485
37 Guthrie Street OVINHAM	House	CT 1212/168	9486
1 Noble Street OVINHAM	House	CT 5805/75	9495
3 Noble Street OVINHAM	House	CT 5805/74	9496
9 Noble Street OVINHAM	House	CT 5335/149	9497
11 Noble Street OVINHAM	House	CT 5329/257	9498
12 Noble Street OVINHAM	Maisonette	CT 5847/162	9499
13 Noble Street OVINHAM	House	CT 5329/256	9500
14 Noble Street OVINHAM	Maisonette	CT 5327/895	9501
15 Noble Street OVINHAM	Floor Home Unit	CT 5082/614	9502

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
15a Noble Street Ovingham	Floor Home Unit	CT 5082/615	9503
17 Noble Street Ovingham	House	CT 5509/54	9504
32 Park Terrace Ovingham	House	CT 5275/516	9505
33 Park Terrace Ovingham	House	CT 5161/549	9506
34 Park Terrace Ovingham	House	CT 5560/955	9507
35 Park Terrace Ovingham	Maisonette	CT 5139/893	9508
36 Park Terrace Ovingham	Maisonette	CT 2467/38	9509
37 Park Terrace Ovingham	House	CT 5149/842	9510
39 Park Terrace Ovingham	House	CT 5409/608	9511
41 Park Terrace Ovingham	House	CT 1651/17	9512
42 Park Terrace Ovingham	House	CT 5132/503	9513
43 Park Terrace Ovingham	House	CT 5204/392	9514
44 Park Terrace Ovingham	House	CT 5514/229 CT 5514/230	9515
47 Park Terrace Ovingham	House	CT 5092/265	9517
48 Park Terrace Ovingham	Maisonette	CT 5123/777	9518
48a Park Terrace Ovingham	Maisonette	CT 5270/659	9519
50 Park Terrace Ovingham	House	CT 5547/15	20669
52 Park Terrace Ovingham	House	CT 5100/154	9521
53 Park Terrace Ovingham	House	CT 5798/494 CT 5845/127	9522
55 Park Terrace Ovingham	House	CT 5868/406	9523
56A Park Terrace Ovingham	House	CT 5881/864	9524
1 Telford Street Ovingham	House	CT 5347/895	9538
2 Telford Street Ovingham	House	CT 5151/969	9550
3 Telford Street Ovingham	House	CT 5344/493	9539
5 Telford Street Ovingham	House	CT 5517/356	9540
8 Telford Street Ovingham	House	CT 5369/550	9542
9 Telford Street Ovingham	House	CT 5249/309	9543
11 Telford Street Ovingham	House	CT 5683/966	9544
13-15 Telford Street Ovingham	Maisonettes	CT 5827/914	9545
14 Telford Street Ovingham	House	CT 5395/36	9546
16 Telford Street Ovingham	House	CT 5344/282	9547
17 Telford Street Ovingham	Maisonette	CT 5710/207	9548
19 Telford Street Ovingham	Maisonette	CT 4347/644	9549
21 Telford Street Ovingham	House	CT 5607/810	9551
22 Telford Street Ovingham	House	CT 5263/724	9552
23 Telford Street Ovingham	Maisonette	CT 5209/414	9553

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
25 Telford Street Ovingham	Maisonette	CT 5209/267	9554
27 Telford Street Ovingham	House	CT 5687/180	9555
29 Telford Street Ovingham	House	CT 5627/43	9556
31 Telford Street Ovingham	House	CT 5149/323	9557
33 Telford Street Ovingham	House	CT 5553/874	9558
1/ 41 Telford Street Ovingham	Home Unit	CT 5008/366	9560
2/ 41 Telford Street Ovingham	Home Unit	CT 5008/367	9561
49 Telford Street Ovingham	House	CT 5111/957	9562
3 Torrens Road Ovingham	House	CT 5242/741	9573
11-17 Torrens Road Ovingham	Social/Entertainment Club	CT 5282/288 CT 5282/289	9574
21 Torrens Road Ovingham	House	CT 4183/366	9575
23 Torrens Road Ovingham	House	CT 5360/680	9576
25 Torrens Road Ovingham	House	CT 5363/478	9577
27 Torrens Road Ovingham	House	CT 5381/507	9578
31 Torrens Road Ovingham	House	CT 5195/183	9580
33 Torrens Road Ovingham	House	CT 5750/888	9581
62 Addison Road Pennington	Maisonette	CT 5733/560	9676
64 Addison Road Pennington	Maisonette	CT 5733/560	9677
66 Addison Road Pennington	Maisonette	CT 3069/131	9678
68 Addison Road Pennington	Maisonette	CT 5733/560	9679
70 Addison Road Pennington	Maisonette	CT 5733/560	9680
72 Addison Road Pennington	Maisonette	CT 5733/560	9681
74 Addison Road Pennington	Maisonette	CT 5733/560	9682
76 Addison Road Pennington	Maisonette	CT 5733/560	9683
78 Addison Road Pennington	Maisonette	CT 5733/552	9684
80 Addison Road Pennington	Maisonette	CT 5733/552	9685
82 Addison Road Pennington	Maisonette	CT 5733/552	9686
84 Addison Road Pennington	Maisonette	CT 5733/552	9687
2 Arthur Street Pennington	Maisonette	CT 5733/552	9688
4 Arthur Street Pennington	Maisonette	CT 5733/552	9689
6 Arthur Street Pennington	Maisonette	CT 5733/552	9690
8 Arthur Street Pennington	Maisonette	CT 5733/552	9691
10 Arthur Street Pennington	Maisonette	CT 5224/302	9692
12 Arthur Street Pennington	Maisonette	CT 5397/995	9693
14 Arthur Street Pennington	Maisonette	CT 5733/552	9694
16 Arthur Street Pennington	Maisonette	CT 5733/552	9695
18 Arthur Street Pennington	Maisonette	CT 5428/362	9696

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
20 Arthur Street PENNINGTON	Maisonette	CT 5428/71	9697
22 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9698
24 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9699
26 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9700
28 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9701
30 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9702
32 Arthur Street PENNINGTON	Maisonette	CT 5733/556	9703
34 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9704
36 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9705
38 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9706
40 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9707
42 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9708
44 Arthur Street PENNINGTON	Maisonette	CT 5733/557	9709
46 Arthur Street PENNINGTON	Maisonette	CT 5733/555	9710
48 Arthur Street PENNINGTON	Maisonette	CT 5733/555	9711
50 Arthur Street PENNINGTON	Maisonette	CT 5733/555	9712
52 Arthur Street PENNINGTON	Maisonette	CT 5733/555	9713
54 Arthur Street PENNINGTON	Maisonette	CT 5733/555	9714
56 Arthur Street PENNINGTON	Maisonette	CT 5356/865	9715
58 Arthur Street PENNINGTON	Maisonette	CT 5356/867	9716
60 Arthur Street PENNINGTON	Maisonette	CT 5356/867	9717
1 Bell Street PENNINGTON	Maisonette	CT 5733/553	9718
2 Bell Street PENNINGTON	Maisonette	CT 5679/608	9719
3 Bell Street PENNINGTON	Maisonette	CT 5733/553	9720
4 Bell Street PENNINGTON	Maisonette	CT 5363/308	9721
5 Bell Street PENNINGTON	Maisonette	CT 5733/553	9722
6 Bell Street PENNINGTON	Maisonette	CT 5356/867	9723
7 Bell Street PENNINGTON	Maisonette	CT 5733/553	9724
8 Bell Street PENNINGTON	Maisonette	CT 5356/867	9725
9 Bell Street PENNINGTON	Maisonette	CT 5733/553	9726
10 Bell Street PENNINGTON	Maisonette	CT 5356/867	9727
11 Bell Street PENNINGTON	Maisonette	CT 5733/553	9728
12 Bell Street PENNINGTON	Maisonette	CT 5356/867	9729
13 Bell Street PENNINGTON	Maisonette	CT 5733/553	9730
14 Bell Street PENNINGTON	Maisonette	CT 5356/867	9731
15 Bell Street PENNINGTON	Maisonette	CT 5416/959	9732
16 Bell Street PENNINGTON	Maisonette	CT 5356/867	9733
17 Bell Street PENNINGTON	Maisonette	CT 5417/78	9734

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
18 Bell Street PENNINGTON	Maisonette	CT 5356/867	9735
19 Bell Street PENNINGTON	Maisonette	CT 5855/402	9736
20 Bell Street PENNINGTON	Maisonette	CT 5356/867	9742
21 Bell Street PENNINGTON	Maisonette	CT 5855/403	9737
22 Bell Street PENNINGTON	Maisonette	CT 5356/867	9738
23 Bell Street PENNINGTON	Maisonette	CT 5733/555	9739
24 Bell Street PENNINGTON	Maisonette	CT 5356/867	9740
26 Bell Street PENNINGTON	Maisonette	CT 5356/866	9741
9 Butler Avenue PENNINGTON	Maisonette	CT 5733/559	9789
11 Butler Avenue PENNINGTON	Maisonette	CT 5733/559	9790
15 Butler Avenue PENNINGTON	Maisonette	CT 5733/559	9791
17 Butler Avenue PENNINGTON	Maisonette	CT 5733/559	9792
21 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9793
23 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9794
25 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9795
27 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9796
29 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9797
31 Butler Avenue PENNINGTON	Maisonette	CT 5098/3	9798
33 Butler Avenue PENNINGTON	Maisonette	CT 5434/237	9799
35 Butler Avenue PENNINGTON	Maisonette	CT 5434/215	9800
37 Butler Avenue PENNINGTON	Maisonette	CT 5733/553	9801
39 Butler Avenue PENNINGTON	Maisonette	CT 5733/553	9802
41 Butler Avenue PENNINGTON	House	CT 5733/553	9803
19 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9804
21 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9805
23 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9806
25 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9807
27 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9808
29 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9809
31 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9810
33 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9811
35 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9812
37 Gilligan Street PENNINGTON	Maisonette	CT 5733/561	9813
39 Gilligan Street PENNINGTON	Maisonette	CT 5414/165	9814
41 Gilligan Street PENNINGTON	Maisonette	CT 5414/164	9815
1 Helen Street PENNINGTON	House	CT 5363/990	11226
2 Helen Street PENNINGTON	House	CT 5548/29	11227
3 Helen Street PENNINGTON	House	CT 5784/185	11228

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
4 Helen Street PENNINGTON	House	CT 5488/549	11229
5 Helen Street PENNINGTON	House	CT 5807/760	11230
6 Helen Street PENNINGTON	House	CT 5841/945	11231
7 Helen Street PENNINGTON	House	CT 5164/183	11232
8 Helen Street PENNINGTON	House	CT 5671/825	11233
10 Helen Street PENNINGTON	House	CT 5841/946	11235
11 Helen Street PENNINGTON	House	CT 5467/425 CT 5546/194	11236
12 Helen Street PENNINGTON	House	CT 5803/875	11237
13 Helen Street PENNINGTON	House	CT 5550/405	11238
14 Helen Street PENNINGTON	House	CT 5367/192	11239
15 Helen Street PENNINGTON	House	CT 5783/825	11240
16 Helen Street PENNINGTON	House	CT 5784/714	11241
17 Helen Street PENNINGTON	House	CT 5789/122	11242
18 Helen Street PENNINGTON	House	CT 1293/123	11243
20 Helen Street PENNINGTON	House	CT 5817/68	11244
21 Helen Street PENNINGTON	House	CT 5092/977	11245
22 Helen Street PENNINGTON	House	CT 5828/849	11246
24 Helen Street PENNINGTON	House	CT 5215/964	11247
25 Helen Street PENNINGTON	House	CT 1272/146	11248
26 Helen Street PENNINGTON	House	CT 5793/715	11249
27 Helen Street PENNINGTON	House	CT 5761/561	11250
28 Helen Street PENNINGTON	House	CT 5691/586	11251
29 Helen Street PENNINGTON	House	CT 5129/828	11252
31 Helen Street PENNINGTON	House	CT 5203/570	11253
33 Helen Street PENNINGTON	House	CT 5788/324	11254
34 Helen Street PENNINGTON	House	CT 5788/172	11255
35 Helen Street PENNINGTON	House	CT 5796/68	11256
36 Helen Street PENNINGTON	House	CT 5217/998	11257
37 Helen Street PENNINGTON	House	CT 5355/88	11258
6 Hodgeman Road PENNINGTON	House	CT 5460/898	11259
8 Hodgeman Road PENNINGTON	House	CT 5807/851	11260
9 Hodgeman Road PENNINGTON	House	CT 5146/139	11261
10 Hodgeman Road PENNINGTON	House	CT 5246/721	11262
12 Hodgeman Road PENNINGTON	House	CT 5159/815	11263
13 Hodgeman Road PENNINGTON	House	CT 5785/407	11264
14 Hodgeman Road PENNINGTON	House	CT 5373/224	11265
15 Hodgeman Road PENNINGTON	House	CT 5437/675	11266

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Hodgeman Road PENNINGTON	House	CT 5795/882	11267
17 Hodgeman Road PENNINGTON	House	CT 5433/99	11268
18 Hodgeman Road PENNINGTON	House	CT 5807/749	11269
19 Hodgeman Road PENNINGTON	House	CT 5552/612	11270
20 Hodgeman Road PENNINGTON	House	CT 5814/499	11271
21 Hodgeman Road PENNINGTON	House	CT 5660/878	11272
22 Hodgeman Road PENNINGTON	House	CT 5486/370	11273
23 Hodgeman Road PENNINGTON	House	CT 5227/41	11274
24 Hodgeman Road PENNINGTON	House	CT 5347/680	11275
27 Hodgeman Road PENNINGTON	House	CT 5807/750	11276
31 Hodgeman Road PENNINGTON	House	CT 5252/858	11277
32 Hodgeman Road PENNINGTON	House & Granny Flat	CT 5340/349	11278
33 Hodgeman Road PENNINGTON	House	CT 5792/273	11279
34 Hodgeman Road PENNINGTON	House	CT 5819/467	11280
35 Hodgeman Road PENNINGTON	House	CT 5534/317	11281
36 Hodgeman Road PENNINGTON	House	CT 5276/767	11282
39 Hodgeman Road PENNINGTON	House	CT 5638/85	11284
40 Hodgeman Road PENNINGTON	House	CT 5360/466	11285
42 Hodgeman Road PENNINGTON	House	CT 5807/649	11286
43 Hodgeman Road PENNINGTON	House	CT 5819/468	11287
44 Hodgeman Road PENNINGTON	House	CT 5115/154	11288
45 Hodgeman Road PENNINGTON	House	CT 5302/252	11289
46 Hodgeman Road PENNINGTON	House	CT 5807/648	11290
47 Hodgeman Road PENNINGTON	House	CT 5361/663	11291
49 Hodgeman Road PENNINGTON	House	CT 5696/566	11292
51 Hodgeman Road PENNINGTON	House	CT 5433/86	11293
53 Hodgeman Road PENNINGTON	House	CT 5359/463	11294
55 Hodgeman Road PENNINGTON	House	CT 5801/828	11295
4 Pennington Terrace PENNINGTON	House	CT 5395/250	11325
6 Pennington Terrace PENNINGTON	House	CT 5820/957	11326
8 Pennington Terrace PENNINGTON	House	CT 5338/318	11327
10 Pennington Terrace PENNINGTON	House	CT 5340/162	11328
12 Pennington Terrace PENNINGTON	House	CT 5564/575	11329
14 Pennington Terrace PENNINGTON	House	CT 5410/721	11330
16 Pennington Terrace PENNINGTON	House	CT 5425/915	11331
18 Pennington Terrace PENNINGTON	House	CT 5799/351	11333
1 Sinclair Square PENNINGTON	Maisonette	CT 5445/271	9816
2 Sinclair Square PENNINGTON	Maisonette	CT 5453/837	9817

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
3 Sinclair Square PENNINGTON	Maisonette	CT 5445/139	9818
4 Sinclair Square PENNINGTON	Maisonette	CT 5187/621	9819
5 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9820
6 Sinclair Square PENNINGTON	Maisonette	CT 5098/3	9821
7 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9822
8 Sinclair Square PENNINGTON	Maisonette	CT 5098/3	9823
9 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9824
10 Sinclair Square PENNINGTON	Maisonette	CT 5403/47	9825
11 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9826
12 Sinclair Square PENNINGTON	Maisonette	CT 5179/19	9827
13 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9828
14 Sinclair Square PENNINGTON	Maisonette	CT 5098/3	9829
15 Sinclair Square PENNINGTON	Maisonette	CT 5733/556	9830
16 Sinclair Square PENNINGTON	Maisonette	CT 5098/3	9831
17 Sinclair Square PENNINGTON	Maisonette	CT 5733/557	9832
18 Sinclair Square PENNINGTON	Maisonette	CT 5098/1	9833
19 Sinclair Square PENNINGTON	Maisonette	CT 5733/557	9834
20 Sinclair Square PENNINGTON	Maisonette	CT 5098/2	9835
1 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9844
2 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9845
3 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9846
4 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9847
5 Tulloch Avenue PENNINGTON	Maisonette	CT 5153/569	9848
6 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9849
7 Tulloch Avenue PENNINGTON	Maisonette	CT 5276/505	9850
8 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9851
9 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9852
10 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9853
11 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9854
12 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9855
14 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9856
15 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9857
16 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/554	9858
17 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9859
18 Tulloch Avenue PENNINGTON	Maisonette	CT 5407/202	9860
19 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9861
20 Tulloch Avenue PENNINGTON	Maisonette	CT 5152/736	9862
21 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9863

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
22 Tulloch Avenue PENNINGTON	Maisonette	CT 5145/918	9864
23 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/558	9865
24 Tulloch Avenue PENNINGTON	Maisonette	CT 5093/522	9866
25 Tulloch Avenue PENNINGTON	Maisonette	CT 3069/131	9867
26 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/556	9868
28 Tulloch Avenue PENNINGTON	Maisonette	CT 5733/556	9870
30 Tulloch Avenue PENNINGTON	Maisonette	CT 6122/448	9872
32 Tulloch Avenue PENNINGTON	Maisonette	CT 6122/448	9874
74 Blight Street RIDLEYTON	Maisonette	CT 5262/599	9764
76 Blight Street RIDLEYTON	Maisonette	CT 5262/599	9766
78 Blight Street RIDLEYTON	Maisonette	CT 5262/600	9768
80 Blight Street RIDLEYTON	Maisonette	CT 5262/600	9770
82 Blight Street RIDLEYTON	Maisonette	CT 5262/601	9772
84 Blight Street RIDLEYTON	Maisonette	CT 5262/601	9774
86 Blight Street RIDLEYTON	Maisonette	CT 5262/602	9776
88 Blight Street RIDLEYTON	Maisonette	CT 5262/603	9778
153 Torrens Road RIDLEYTON	Maisonette	CT 5451/917	9840
155 Torrens Road RIDLEYTON	Maisonette	CT 4296/100	9841
157 Torrens Road RIDLEYTON	Maisonette	CT 4296/99	9842
159 Torrens Road RIDLEYTON	Maisonette	CT 5451/918	9843
6 Arlington Terrace WELLAND	House	CT 5104/726	11385
8 Arlington Terrace WELLAND	House	CT 5492/53	11386
10 Arlington Terrace WELLAND	House	CT 5604/283	11387
12 Arlington Terrace WELLAND	House	CT 5102/138	11388
14 Arlington Terrace WELLAND	House	CT 5491/765	11389
16 Arlington Terrace WELLAND	House	CT 5815/126	11390
18 Arlington Terrace WELLAND	House	CT 1220/176	11391
20 Arlington Terrace WELLAND	House	CT 5657/440	11392
22 Arlington Terrace WELLAND	House	CT 5474/97	11393
24 Arlington Terrace WELLAND	House	CT 5733/641	11394
26 Arlington Terrace WELLAND	House	CT 1149/47	11395
28 Arlington Terrace WELLAND	House	CT 5268/974	11396
30 Arlington Terrace WELLAND	House	CT 5348/861	11397
1/ 32 Arlington Terrace WELLAND	Single Storey Flats	CT 5203/455	11398
34 Arlington Terrace WELLAND	House	CT 5645/302	11399
36 Arlington Terrace WELLAND	House	CT 5812/279	11400
38 Arlington Terrace WELLAND	House	CT 5687/404	11401
40 Arlington Terrace WELLAND	House	CT 5826/587	11402

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
42 Arlington Terrace WELLAND	House	CT 5820/436	11403
44 Arlington Terrace WELLAND	House	CT 5547/765	11404
46 Arlington Terrace WELLAND	House	CT 5337/265	11405
48 Arlington Terrace WELLAND	House	CT 5258/266	11406
50 Arlington Terrace WELLAND	House	CT 5557/190	11407
52 Arlington Terrace WELLAND	House	CT 5796/61	11408
54 Arlington Terrace WELLAND	House	CT 5818/941	11409
56 Arlington Terrace WELLAND	House	CT 5820/768	11410
58 Arlington Terrace WELLAND	House	CT 5662/255	11411
60 Arlington Terrace WELLAND	House	CT 5175/475	11412
64 Arlington Terrace WELLAND	House	CT 5746/663	11413
66 Arlington Terrace WELLAND	House	CT 5241/664	11414
1A-1B Fischer Street WELLAND	House	CT 5939/669	20872
3 Fischer Street WELLAND	House	CT 2034/99	11416
4 Fischer Street WELLAND	House	CT 5682/613	11417
5 Fischer Street WELLAND	House	CT 5595/310	11418
6 Fischer Street WELLAND	House	CT 5123/724	11419
7 Fischer Street WELLAND	House	CT 5261/769	11420
8 Fischer Street WELLAND	House	CT 5555/586	11421
9 Fischer Street WELLAND	House	CT 5781/914	11422
10 Fischer Street WELLAND	House	CT 5731/167	11423
11 Fischer Street WELLAND	House	CT 5473/999	11424
12 Fischer Street WELLAND	House	CT 5794/141	11425
14 Fischer Street WELLAND	House	CT 5514/589	11426
15 Fischer Street WELLAND	Maisonette	CT 5293/351	11427
16 Fischer Street WELLAND	House	CT 5582/620	11428
17 Fischer Street WELLAND	Maisonette	CT 1837/139	11429
18 Fischer Street WELLAND	House	CT 4080/685	11430
19 Fischer Street WELLAND	House	CT 5460/672	11431
20 Fischer Street WELLAND	House	CT 5793/773	11432
21 Fischer Street WELLAND	House	CT 5441/393	11433
22 Fischer Street WELLAND	House	CT 5806/382	11434
23 Fischer Street WELLAND	House	CT 5804/592	11435
2 Frederick Street WELLAND	Maisonette	CT 5751/542	11437
4 Frederick Street WELLAND	Maisonette	CT 5626/186	11438
5 Frederick Street WELLAND	House	CT 5493/93	11439
6 Frederick Street WELLAND	House	CT 5752/566	11440
7 Frederick Street WELLAND	House	CT 5124/322	11441

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
8 Frederick Street WELLAND	House	CT 5395/702	11442
9 Frederick Street WELLAND	House	CT 5797/672	11443
10 Frederick Street WELLAND	House	CT 5788/230	11444
12 Frederick Street WELLAND	Maisonette	CT 5953/949	11445
13 Frederick Street WELLAND	House	CT 5272/650	11446
14 Frederick Street WELLAND	Maisonette	CT 5953/950	11447
16-18 Frederick Street WELLAND	Maisonettes	CT 5788/492	11448
17 Frederick Street WELLAND	House	CT 5797/674	11449
19 Frederick Street WELLAND	House	CT 5797/150	11450
21 Frederick Street WELLAND	House	CT 5423/205	11451
23 Frederick Street WELLAND	House	CT 5797/670	11452
25 Frederick Street WELLAND	House	CT 5762/504	11453
26 Frederick Street WELLAND	House	CT 5533/436	11454
28 Frederick Street WELLAND	House	CT 5838/44	11455
30 Frederick Street WELLAND	House	CT 5797/669	11456
32 Frederick Street WELLAND	House	CT 5287/541	11457
33a Frederick Street WELLAND	Single Storey Flats	CT 5422/896	11458
33b Frederick Street WELLAND	Single Storey Flats		11459
34 Frederick Street WELLAND	House	CT 5611/810	11460
35 Frederick Street WELLAND	House	CT 5351/491	11461
36 Frederick Street WELLAND	House	CT 5222/69	11462
37 Frederick Street WELLAND	House	CT 5531/379	11463
38 Frederick Street WELLAND	House	CT 5740/50	11464
39 Frederick Street WELLAND	House	CT 5467/499	11465
41 Frederick Street WELLAND	House	CT 5812/258	11467
42 Frederick Street WELLAND	House	CT 5381/8	11468
43 Frederick Street WELLAND	House	CT 5812/268	11469
44 Frederick Street WELLAND	House	CT 5779/594	11470
45 Frederick Street WELLAND	House	CT 5352/462	11471
46 Frederick Street WELLAND	House	CT 5779/595	11472
47 Frederick Street WELLAND	House	CT 5178/615	11473
48 Frederick Street WELLAND	House	CT 5455/986	11474
49 Frederick Street WELLAND	House	CT 1166/83	11475
50 Frederick Street WELLAND	House	CT 5105/980	11476
51 Frederick Street WELLAND	House	CT 1166/84	11477
52 Frederick Street WELLAND	House	CT 5270/189	11478
54 Frederick Street WELLAND	House	CT 5756/180	11479
55 Frederick Street WELLAND	House	CT 5807/817	11480

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
56 Frederick Street WELLAND	House	CT 5786/635	11481
57 Frederick Street WELLAND	House	CT 5815/700	11482
58 Frederick Street WELLAND	House	CT 5571/56	11483
59 Frederick Street WELLAND	House	CT 5263/741	11484
60 Frederick Street WELLAND	House	CT 5480/840	11485
61 Frederick Street WELLAND	House	CT 5378/527	11486
62 Frederick Street WELLAND	House	CT 5504/971	11487
63 Frederick Street WELLAND	House	CT 5213/177	11488
64 Frederick Street WELLAND	House	CT 5504/946	11489
66 Frederick Street WELLAND	House	CT 5785/554	11490
68 Frederick Street WELLAND	House	CT 5128/640	11491
70 Frederick Street WELLAND	House	CT 5814/973	11492
72 Frederick Street WELLAND	House	CT 5814/974	11493
74 Frederick Street WELLAND	House	CT 5177/931	11494
76 Frederick Street WELLAND	House	CT 5486/433	11495
78 Frederick Street WELLAND	House	CT 5394/860	11496
80 Frederick Street WELLAND	House	CT 5408/685	11497
84 Frederick Street WELLAND	House	CT 5302/146	11498
86 Frederick Street WELLAND	House	CT 5492/827	11499
5 Welland Avenue WELLAND	House	CT 5788/469	11502
6 Welland Avenue WELLAND	House	CT 5269/728	11503
7 Welland Avenue WELLAND	House	CT 5785/968	11504
8 Welland Avenue WELLAND	House	CT 5509/301	11505
9 Welland Avenue WELLAND	House	CT 5400/758	11506
10 Welland Avenue WELLAND	House	CT 5746/749	11507
11 Welland Avenue WELLAND	House	CT 5711/337	11508
13 Welland Avenue WELLAND	House	CT 5794/132	11509
14 Welland Avenue WELLAND	House	CT 5214/889	11510
15 Welland Avenue WELLAND	House	CT 5850/1	11511
16 Welland Avenue WELLAND	House	CT 5819/622	11512
17 Welland Avenue WELLAND	House	CT 5573/875	11513
18 Welland Avenue WELLAND	House	CT 5788/215	11514
19 Welland Avenue WELLAND	House	CT 5446/397	11515
20 Welland Avenue WELLAND	House	CT 5389/704	11516
22 Welland Avenue WELLAND	House	CT 5271/50	11517
23 Welland Avenue WELLAND	House	CT 5814/275	11518
24 Welland Avenue WELLAND	House	CT 5735/684	11519
25 Welland Avenue WELLAND	House	CT 5348/842	11520

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
26 Welland Avenue WELLAND	House	CT 5819/69	11521
27 Welland Avenue WELLAND	House	CT 5532/335	11522
28 Welland Avenue WELLAND	House	CT 5405/230	11523
29 Welland Avenue WELLAND	House	CT 5335/476	11524
30 Welland Avenue WELLAND	House	CT 5203/819	11525
31 Welland Avenue WELLAND	House	CT 5215/827	11526
33 Welland Avenue WELLAND	House	CT 5638/432	11528
34 Welland Avenue WELLAND	House	CT 5240/466	11529
35 Welland Avenue WELLAND	House	CT 5487/591	11530
36 Welland Avenue WELLAND	House	CT 5783/827	11531
38 Welland Avenue WELLAND	House	CT 5574/340	11533
39 Welland Avenue WELLAND	House	CT 5246/78	11534
40 Welland Avenue WELLAND	House	CT 5788/862	11535
41 Welland Avenue WELLAND	House	CT 5194/643	11536
42 Welland Avenue WELLAND	House	CT 5837/480	11537
43 Welland Avenue WELLAND	House	CT 5281/332	11538
44 Welland Avenue WELLAND	House	CT 5603/766	11539
45 Welland Avenue WELLAND	House	CT 5210/309	11540
46 Welland Avenue WELLAND	House	CT 5625/571	11541
47 Welland Avenue WELLAND	House	CT 5146/548	11542
48 Welland Avenue WELLAND	House	CT 5356/434	11543
50 Welland Avenue WELLAND	House	CT 5788/515	11544
51 Welland Avenue WELLAND	Maisonette	CT 5272/156	11545
52 Welland Avenue WELLAND	House	CT 5677/856	11546
53 Welland Avenue WELLAND	Maisonette	CT 5444/226	11547
54 Welland Avenue WELLAND	House	CT 5787/674	11548
56 Welland Avenue WELLAND	House	CT 5251/963	11549
58 Welland Avenue WELLAND	House	CT 5826/191	11550
59 Welland Avenue WELLAND	Maisonette	CT 5783/132	11551
60 Welland Avenue WELLAND	House	CT 5695/431	11552
61 Welland Avenue WELLAND	Maisonette	CT 5785/122	11553
62 Welland Avenue WELLAND	Home Unit	CT 5004/304	11554
63 Welland Avenue WELLAND	House	CT 5787/180	11555
64 Welland Avenue WELLAND	Home Unit	CT 5004/305	11556
65 Welland Avenue WELLAND	House	CT 5272/658	11557
6/ 79 Euston Terrace WEST CROYDEN	Home Unit	CT 5486/821	20671
1 Alfred Road WEST CROYDON	House	CT 5220/265	9876
2 Alfred Road WEST CROYDON	House	CT 5269/239	9877

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
4 Alfred Road WEST CROYDON	House	CT 5761/660	9878
5 Alfred Road WEST CROYDON	House	CT 5814/674	9879
6 Alfred Road WEST CROYDON	House	CT 5231/188	9880
7 Alfred Road WEST CROYDON	House	CT 5517/444	9881
8 Alfred Road WEST CROYDON	House	CT 5573/539	9882
9 Alfred Road WEST CROYDON	House	CT 5278/898	9883
10 Alfred Road WEST CROYDON	House	CT 5732/877	9884
11 Alfred Road WEST CROYDON	House	CT 5551/394	9885
12 Alfred Road WEST CROYDON	House	CT 5797/411	9886
14 Alfred Road WEST CROYDON	Maisonette	CT 5709/810	9887
15 Alfred Road WEST CROYDON	House	CT 5731/632	9888
16 Alfred Road WEST CROYDON	Maisonette	CT 5709/811	9889
17 Alfred Road WEST CROYDON	House	CT 5732/310	9890
18 Alfred Road WEST CROYDON	House	CT 5724/576	9891
19 Alfred Road WEST CROYDON	House	CT 5733/530	9892
20 Alfred Road WEST CROYDON	House	CT 5214/291	9893
21 Alfred Road WEST CROYDON	House	CT 5202/469	9894
22 Alfred Road WEST CROYDON	House	CT 5322/413	9895
23 Alfred Road WEST CROYDON	House	CT 5732/992	9896
24 Alfred Road WEST CROYDON	House	CT 5173/735	9897
25 Alfred Road WEST CROYDON	House	CT 5632/416	9898
26 Alfred Road WEST CROYDON	House	CT 5297/979	9899
27 Alfred Road WEST CROYDON	House	CT 5732/993	9900
28 Alfred Road WEST CROYDON	House	CT 5142/309	9901
29 Alfred Road WEST CROYDON	House	CT 5192/551	9902
31 Alfred Road WEST CROYDON	House	CT 5520/494	9903
1 Blanford Street WEST CROYDON	House	CT 5565/590	9904
2 Blanford Street WEST CROYDON	House	CT 5809/287	9905
3 Blanford Street WEST CROYDON	House	CT 5249/984	9906
5 Blanford Street WEST CROYDON	House	CT 5249/985	9907
7 Blanford Street WEST CROYDON	House	CT 5246/250	9908
8 Blanford Street WEST CROYDON	House	CT 5600/992	9909
9 Blanford Street WEST CROYDON	House	CT 5814/935	9910
10 Blanford Street WEST CROYDON	House	CT 5786/637	9911
11 Blanford Street WEST CROYDON	House	CT 5833/620	9912
12 Blanford Street WEST CROYDON	House	CT 5309/423	9913
14 Blanford Street WEST CROYDON	House	CT 5783/771	9914
15 Blanford Street WEST CROYDON	House	CT 5181/442	9915

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Blanford Street WEST CROYDON	House	CT 5814/400	9916
17 Blanford Street WEST CROYDON	House	CT 5213/115	9917
19 Blanford Street WEST CROYDON	House	CT 5783/676	9918
20 Blanford Street WEST CROYDON	House	CT 5155/303	9919
21 Blanford Street WEST CROYDON	House	CT 5806/456	9920
22 Blanford Street WEST CROYDON	House	CT 5511/611	9921
23 Blanford Street WEST CROYDON	House	CT 5806/457	9922
24 Blanford Street WEST CROYDON	House	CT 5887/963	9923
25 Blanford Street WEST CROYDON	House	CT 5159/711	9924
26 Blanford Street WEST CROYDON	House	CT 5584/923	9925
28 Blanford Street WEST CROYDON	House	CT 5789/15	9927
30 Blanford Street WEST CROYDON	House	CT 5788/688	9928
32 Blanford Street WEST CROYDON	House	CT 5116/132	9929
40 Blanford Street WEST CROYDON	House	CT 5419/474	9930
44 Blanford Street WEST CROYDON	House	CT 5402/653	9931
2 Brown Street WEST CROYDON	Community Centre	CT 5651/862	9932
3 Brown Street WEST CROYDON	House	CT 5679/704	9933
23 Brown Street WEST CROYDON	House	CT 5222/361	9934
27 Brown Street WEST CROYDON	House	CT 5112/569	9935
29 Brown Street WEST CROYDON	House	CT 1225/138	9936
37 Brown Street WEST CROYDON	House	CT 5322/275	9938
39 Brown Street WEST CROYDON	House	CT 1242/167	9939
41 Brown Street WEST CROYDON	House	CT 5078/402	9940
43 Brown Street WEST CROYDON	House	CT 5459/706	9941
45 Brown Street WEST CROYDON	House	CT 5754/65	9942
49 Brown Street WEST CROYDON	Home Unit	CT 5013/340	9943
51 Brown Street WEST CROYDON	Home Unit	CT 5013/339	9944
1 Burke Street WEST CROYDON	House	CT 5172/852	9945
3 Burke Street WEST CROYDON	House	CT 5729/403	9946
5 Burke Street WEST CROYDON	House	CT 5246/676	9947
7 Burke Street WEST CROYDON	House	CT 5065/279	9948
11 Burke Street WEST CROYDON	House	CT 5156/731	9949
15 Burke Street WEST CROYDON	House	CT 5659/364	9950
16 Burke Street WEST CROYDON	House	CT 5532/868	9951
17 Burke Street WEST CROYDON	House	CT 5084/869	9952
18 Burke Street WEST CROYDON	House	CT 5759/456	9953
19 Burke Street WEST CROYDON	House	CT 1100/176	9954
20 Burke Street WEST CROYDON	House	CT 5807/752	9955

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
21 Burke Street WEST CROYDON	House	CT 5179/912	9956
22 Burke Street WEST CROYDON	House	CT 5103/213	9957
23 Burke Street WEST CROYDON	House	CT 5185/22	9958
24 Burke Street WEST CROYDON	House	CT 5169/643	9959
25 Burke Street WEST CROYDON	House	CT 5365/395	9960
26 Burke Street WEST CROYDON	House	CT 5576/794	9961
27 Burke Street WEST CROYDON	House	CT 4066/323	9962
28 Burke Street WEST CROYDON	House	CT 5204/460	9963
30 Burke Street WEST CROYDON	House	CT 5214/99	9964
33 Burke Street WEST CROYDON	House	CT 5818/537	9965
35 Burke Street WEST CROYDON	House	CT 5220/425	9966
37 Burke Street WEST CROYDON	House	CT 5194/456	9967
45 Day Terrace WEST CROYDON	House	CT 5790/787	10027
46 Day Terrace WEST CROYDON	House	CT 5535/530	10028
47 Day Terrace WEST CROYDON	House	CT 5466/302	10029
48 Day Terrace WEST CROYDON	Maisonette	CT 5796/594	10030
49 Day Terrace WEST CROYDON	Maisonette	CT 1716/70	10031
50 Day Terrace WEST CROYDON	House	CT 5302/990	10032
51 Day Terrace WEST CROYDON	House	CT 5173/398	10033
52 Day Terrace WEST CROYDON	House	CT 824/136	10034
53 Day Terrace WEST CROYDON	House	CT 5466/950	10035
54 Day Terrace WEST CROYDON	House	CT 5148/666	10036
55 Day Terrace WEST CROYDON	House	CT 5607/367	10037
57 Day Terrace WEST CROYDON	House	CT 5813/116	10038
58 Day Terrace WEST CROYDON	House	CT 5729/602	10039
59 Day Terrace WEST CROYDON	House	CT 5362/190	10040
60 Day Terrace WEST CROYDON	House	CT 5461/890	10041
61 Day Terrace WEST CROYDON	House	CT 5433/820	10042
62 Day Terrace WEST CROYDON	House	CT 5352/803	10043
63 Day Terrace WEST CROYDON	House	CT 5116/560	10044
64 Day Terrace WEST CROYDON	House	CT 5796/51	10045
65 Day Terrace WEST CROYDON	House	CT 5782/771	10046
66 Day Terrace WEST CROYDON	House	CT 5651/863	10047
68 Day Terrace WEST CROYDON	Community Centre	CT 5651/862	10048
69 Day Terrace WEST CROYDON	House	CT 5501/843	10049
70 Day Terrace WEST CROYDON	House	CT 5460/629	10050
71 Day Terrace WEST CROYDON	House	CT 5848/302	10051
72 Day Terrace WEST CROYDON	House	CT 5850/647	10052

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
73 Day Terrace WEST CROYDON	House	CT 5268/340	10053
74 Day Terrace WEST CROYDON	House	CT 5568/596	10054
75 Day Terrace WEST CROYDON	House	CT 5598/803	10055
76 Day Terrace WEST CROYDON	House	CT 5405/11	10056
77 Day Terrace WEST CROYDON	House	CT 5502/419	10057
1/ 78-81 Day Terrace WEST CROYDON	Single Storey Flats	CT 5575/963	10058
84 Day Terrace WEST CROYDON	House	CT 5197/363	10059
85 Day Terrace WEST CROYDON	House	CT 5355/543	10060
86 Day Terrace WEST CROYDON	House	CT 5433/1	10061
87 Day Terrace WEST CROYDON	House	CT 5554/12	10062
88 Day Terrace WEST CROYDON	House	CT 1077/19	10063
89 Day Terrace WEST CROYDON	House	CT 5836/726	10064
90 Day Terrace WEST CROYDON	House	CT 5076/106	10065
91 Day Terrace WEST CROYDON	House	CT 5183/522	10066
92 Day Terrace WEST CROYDON	House	CT 5336/325	10067
93 Day Terrace WEST CROYDON	House	CT 5684/326	10068
94 Day Terrace WEST CROYDON	House	CT 5552/802	10069
95 Day Terrace WEST CROYDON	House	CT 5661/634	10070
97 Day Terrace WEST CROYDON	House	CT 5571/934	10071
98 Day Terrace WEST CROYDON	House	CT 5413/494	10072
99 Day Terrace WEST CROYDON	House	CT 5533/376	10073
100 Day Terrace WEST CROYDON	House & Granny Flat	CT 5440/821 CT 5440/864 CT 5440/865	10074
101 Day Terrace WEST CROYDON	House	CT 5270/879	10075
102 Day Terrace WEST CROYDON	House	CT 5676/391	10076
103 Day Terrace WEST CROYDON	House	CT 5533/316	10077
104 Day Terrace WEST CROYDON	House	CT 5514/835	10078
105 Day Terrace WEST CROYDON	House	CT 5276/168	10079
106 Day Terrace WEST CROYDON	House	CT 1137/127	10080
107 Day Terrace WEST CROYDON	House	CT 5411/829	10081
108 Day Terrace WEST CROYDON	House	CT 4066/324	10082
109 Day Terrace WEST CROYDON	House	CT 5423/901	10083
110 Day Terrace WEST CROYDON	House	CT 5555/415	10084
111 Day Terrace WEST CROYDON	House	CT 5757/512	10085
1 Donald Avenue WEST CROYDON	Maisonette	CT 3953/46	10086
2 Donald Avenue WEST CROYDON	Maisonette	CT 2300/96	10087
3 Donald Avenue WEST CROYDON	Maisonette	CT 3953/45	10088

Charles Sturt Council
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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
4 Donald Avenue WEST CROYDON	Maisonette	CT 2300/97	10089
5 Donald Avenue WEST CROYDON	House	CT 5142/257	10090
6 Donald Avenue WEST CROYDON	Maisonette	CT 5852/988	10091
7 Donald Avenue WEST CROYDON	House	CT 5725/733	10092
8 Donald Avenue WEST CROYDON	Maisonette	CT 5963/916	10093
9 Donald Avenue WEST CROYDON	House	CT 5725/734	10094
10 Donald Avenue WEST CROYDON	Maisonette	CT 5731/917	10095
11 Donald Avenue WEST CROYDON	House	CT 5246/817	10096
12 Donald Avenue WEST CROYDON	Maisonette	CT 5731/917	10097
14 Donald Avenue WEST CROYDON	Maisonette	CT 5867/127	10098
16 Donald Avenue WEST CROYDON	Maisonette	CT 5523/690	10099
18 Donald Avenue WEST CROYDON	Home Unit	CT 5006/483	10100
20 Donald Avenue WEST CROYDON	Home Unit	CT 5006/482	10101
1 Duncan Road WEST CROYDON	House	CT 5179/842	10102
2 Duncan Road WEST CROYDON	Maisonette	CT 2287/71	10103
3 Duncan Road WEST CROYDON	House	CT 5792/268	10104
4 Duncan Road WEST CROYDON	Maisonette	CT 5290/26	10105
6 Duncan Road WEST CROYDON	Maisonette	CT 5220/318	10106
8 Duncan Road WEST CROYDON	Maisonette	CT 5493/556	10107
1 Ellen Street WEST CROYDON	House	CT 5727/960	10147
3 Ellen Street WEST CROYDON	House	CT 5803/659	10148
5 Ellen Street WEST CROYDON	House	CT 5194/119	10149
7 Ellen Street WEST CROYDON	House	CT 5219/587	10150
36 Euston Terrace WEST CROYDON	House	CT 5794/115	10175
37 Euston Terrace WEST CROYDON	House	CT 5814/30	10176
38 Euston Terrace WEST CROYDON	House	CT 5381/338	10177
39 Euston Terrace WEST CROYDON	House	CT 5166/97	10178
45 Euston Terrace WEST CROYDON	House	CT 5400/46	10179
49 Euston Terrace WEST CROYDON	House	CT 5815/107	10180
50 Euston Terrace WEST CROYDON	House	CT 5815/108	10181
51 Euston Terrace WEST CROYDON	House	CT 5188/867	10182
53 Euston Terrace WEST CROYDON	House	CT 5806/76	10183
54 Euston Terrace WEST CROYDON	House	CT 5806/77	10184
55 Euston Terrace WEST CROYDON	House	CT 5447/45	10185
56 Euston Terrace WEST CROYDON	House	CT 5436/781	10186
57 Euston Terrace WEST CROYDON	House	CT 5821/281	10187
58 Euston Terrace WEST CROYDON	House	CT 5694/137	10188
59 Euston Terrace WEST CROYDON	House	CT 5750/315	10189

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
60 Euston Terrace WEST CROYDON	House	CT 5183/928	10190
61 Euston Terrace WEST CROYDON	House	CT 5255/679	10191
62 Euston Terrace WEST CROYDON	House	CT 5820/510	10192
63 Euston Terrace WEST CROYDON	House	CT 5391/457	10193
65 Euston Terrace WEST CROYDON	House	CT 5721/907	10194
66 Euston Terrace WEST CROYDON	House	CT 5757/622	10195
67 Euston Terrace WEST CROYDON	House	CT 5480/699	10196
69 Euston Terrace WEST CROYDON	House	CT 5358/64	10197
70 Euston Terrace WEST CROYDON	House	CT 5687/314	10198
72 Euston Terrace WEST CROYDON	House	CT 5814/160	10199
74 Euston Terrace WEST CROYDON	House	CT 5339/70	10200
75 Euston Terrace WEST CROYDON	House	CT 5336/700	10201
76 Euston Terrace WEST CROYDON	House	CT 5655/958	10202
77 Euston Terrace WEST CROYDON	House	CT 5812/598	10203
78 Euston Terrace WEST CROYDON	House	CT 5596/120	10204
5/ 79 Euston Terrace WEST CROYDON	Home Unit	CT 5486/820	10205
80 Euston Terrace WEST CROYDON	House	CT 5412/147	10206
81 Euston Terrace WEST CROYDON	House	CT 5747/317	10207
82 Euston Terrace WEST CROYDON	House	CT 5850/733	10208
2 Heading Street WEST CROYDON	House	CT 5785/744	10230
4 Heading Street WEST CROYDON	House	CT 5734/791	10231
5 Heading Street WEST CROYDON	House	CT 5729/869	10232
6 Heading Street WEST CROYDON	House	CT 5346/475	10233
7 Heading Street WEST CROYDON	House	CT 5742/922	10234
8 Heading Street WEST CROYDON	House	CT 5547/796	10235
9 Heading Street WEST CROYDON	House	CT 5364/92	10236
10 Heading Street WEST CROYDON	House	CT 5733/1	10237
11 Heading Street WEST CROYDON	House	CT 5335/867	10238
12 Heading Street WEST CROYDON	House	CT 5707/278	10239
14 Heading Street WEST CROYDON	House	CT 5735/186	10240
15 Heading Street WEST CROYDON	House	CT 5591/190	10241
16 Heading Street WEST CROYDON	House	CT 5733/120	10242
17 Heading Street WEST CROYDON	House	CT 5725/574	10243
18 Heading Street WEST CROYDON	House	CT 5551/122	10244
19 Heading Street WEST CROYDON	House	CT 5442/626	10245
39 Henry Street WEST CROYDON	House	CT 5638/80	10276
41 Henry Street WEST CROYDON	House	CT 5202/741 CT 5236/851	10277

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
42 Henry Street WEST CROYDON	House	CT 5087/758	10278
43 Henry Street WEST CROYDON	House	CT 5817/765	10279
44 Henry Street WEST CROYDON	House	CT 5839/764	10280
45 Henry Street WEST CROYDON	House	CT 5168/547	10281
46 Henry Street WEST CROYDON	House	CT 5139/260	10282
47 Henry Street WEST CROYDON	House	CT 5647/134	10283
48 Henry Street WEST CROYDON	House	CT 2144/6	10284
49 Henry Street WEST CROYDON	House	CT 5645/885	10285
50 Henry Street WEST CROYDON	House	CT 5855/107	10286
51 Henry Street WEST CROYDON	House	CT 5360/465	10287
52 Henry Street WEST CROYDON	House	CT 5606/686	10288
53 Henry Street WEST CROYDON	House	CT 5381/373	10289
54 Henry Street WEST CROYDON	House	CT 1079/179	10290
55 Henry Street WEST CROYDON	House	CT 5691/261	10291
56 Henry Street WEST CROYDON	House	CT 1236/120	10292
57 Henry Street WEST CROYDON	House	CT 5806/690	10293
58 Henry Street WEST CROYDON	House	CT 5098/721	10294
59 Henry Street WEST CROYDON	House	CT 5678/495	10295
60 Henry Street WEST CROYDON	House	CT 5193/637	10296
61 Henry Street WEST CROYDON	House	CT 5759/291	10297
62 Henry Street WEST CROYDON	House	CT 5489/967	10298
63 Henry Street WEST CROYDON	House	CT 5759/281	10299
64 Henry Street WEST CROYDON	House	CT 5349/303	10300
65 Henry Street WEST CROYDON	House	CT 5710/581	10301
66 Henry Street WEST CROYDON	House	CT 5760/313	10302
68 Henry Street WEST CROYDON	Maisonette	CT 5155/566	10304
69 Henry Street WEST CROYDON	House	CT 5168/71	10305
70 Henry Street WEST CROYDON	Maisonette	CT 5732/756	10306
71 Henry Street WEST CROYDON	House	CT 5713/87	10307
72 Henry Street WEST CROYDON	House	CT 5214/895	10308
73 Henry Street WEST CROYDON	House	CT 5789/190	10309
74 Henry Street WEST CROYDON	House	CT 5183/822	10310
75 Henry Street WEST CROYDON	House	CT 5154/875	10312
77 Henry Street WEST CROYDON	House	CT 5730/267	10314
78 Henry Street WEST CROYDON	House	CT 5533/539	10315
79 Henry Street WEST CROYDON	House	CT 5196/793	10316
80 Henry Street WEST CROYDON	House	CT 5524/17	10317
81 Henry Street WEST CROYDON	House	CT 5577/116	10318

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
82 Henry Street WEST CROYDON	House	CT 5154/171	10319
83 Henry Street WEST CROYDON	House	CT 5805/808	10320
84 Henry Street WEST CROYDON	House	CT 5556/935	10321
85 Henry Street WEST CROYDON	House	CT 5494/830	10322
86 Henry Street WEST CROYDON	House	CT 5239/434	10323
87 Henry Street WEST CROYDON	House	CT 5105/822	10324
88 Henry Street WEST CROYDON	House	CT 5713/90	10325
90 Henry Street WEST CROYDON	House	CT 5728/27	10326
92 Henry Street WEST CROYDON	House	CT 5786/506	10327
2 Herbert Road WEST CROYDON	House	CT 5300/195	10328
4 Herbert Road WEST CROYDON	House	CT 5750/845	10329
6 Herbert Road WEST CROYDON	House	CT 5357/576	10330
7 Herbert Road WEST CROYDON	House	CT 5228/617	10331
8 Herbert Road WEST CROYDON	House	CT 5754/671	10332
1/ 9 Herbert Road WEST CROYDON	Home Unit	CT 5104/108	10333
10 Herbert Road WEST CROYDON	House	CT 5852/490	10334
11 Herbert Road WEST CROYDON	House	CT 5151/673	10335
12 Herbert Road WEST CROYDON	House	CT 5237/277	10336
14 Herbert Road WEST CROYDON	House	CT 5757/679	10337
15 Herbert Road WEST CROYDON	House	CT 5502/560	10338
19 Herbert Road WEST CROYDON	House	CT 5548/246	10339
22 Herbert Road WEST CROYDON	House	CT 5485/340	10340
24 Herbert Road WEST CROYDON	House	CT 5161/295	10341
25 Herbert Road WEST CROYDON	House	CT 5227/237 CT 5227/238	10342
26 Herbert Road WEST CROYDON	House	CT 5743/351	10343
27 Herbert Road WEST CROYDON	House	CT 5818/999	10344
28 Herbert Road WEST CROYDON	House	CT 5386/371	10345
29 Herbert Road WEST CROYDON	House	CT 4120/150	10346
30 Herbert Road WEST CROYDON	House	CT 5197/351	10347
31 Herbert Road WEST CROYDON	House	CT 5279/702	10348
32 Herbert Road WEST CROYDON	House	CT 5728/286	10349
33 Herbert Road WEST CROYDON	House	CT 5533/7	10350
34 Herbert Road WEST CROYDON	House	CT 5317/24	10351
35 Herbert Road WEST CROYDON	House	CT 5173/802	10352
1 Margaret Avenue WEST CROYDON	House	CT 5492/332	10377
2 Margaret Avenue WEST CROYDON	House	CT 5212/518	10378
3 Margaret Avenue WEST CROYDON	House	CT 5841/100	10379

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Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
4 Margaret Avenue WEST CROYDON	House	CT 5803/474	10380
5 Margaret Avenue WEST CROYDON	House	CT 5303/884	10381
6 Margaret Avenue WEST CROYDON	House	CT 5099/354	10382
7 Margaret Avenue WEST CROYDON	House	CT 5286/646	10383
8 Margaret Avenue WEST CROYDON	House	CT 5476/343	10384
9 Margaret Avenue WEST CROYDON	House	CT 5848/41	10385
11 Margaret Avenue WEST CROYDON	House	CT 5803/856	10386
12 Margaret Avenue WEST CROYDON	House	CT 5754/37	10387
13 Margaret Avenue WEST CROYDON	House	CT 5330/69	10388
14 Margaret Avenue WEST CROYDON	House	CT 5821/686	10389
15 Margaret Avenue WEST CROYDON	House	CT 5290/496	10390
16 Margaret Avenue WEST CROYDON	House	CT 5361/836	10391
17 Margaret Avenue WEST CROYDON	House	CT 5726/674	10392
19 Margaret Avenue WEST CROYDON	House	CT 5653/553	10393
20 Margaret Avenue WEST CROYDON	House	CT 5594/535	10394
21 Margaret Avenue WEST CROYDON	House	CT 5728/189	10395
22 Margaret Avenue WEST CROYDON	House	CT 5727/279	10396
23 Margaret Avenue WEST CROYDON	House	CT 5176/521	10397
24 Margaret Avenue WEST CROYDON	House	CT 5728/190	10398
25 Margaret Avenue WEST CROYDON	House	CT 5160/404	10399
26 Margaret Avenue WEST CROYDON	House	CT 5191/411	10400
27 Margaret Avenue WEST CROYDON	House	CT 5529/820	10401
28 Margaret Avenue WEST CROYDON	House	CT 5204/992	10402
29 Margaret Avenue WEST CROYDON	House	CT 5302/781	10403
30 Margaret Avenue WEST CROYDON	House	CT 5235/128	10404
31 Margaret Avenue WEST CROYDON	House	CT 5726/445	10405
32 Margaret Avenue WEST CROYDON	House	CT 1729/89	10406
33 Margaret Avenue WEST CROYDON	House	CT 5899/67	10407
34 Margaret Avenue WEST CROYDON	House	CT 5726/673	10408
36 Margaret Avenue WEST CROYDON	House	CT 5724/924	10409
38 Margaret Avenue WEST CROYDON	House	CT 5724/925	10410
1 Marion Street WEST CROYDON	House	CT 5665/558	10411
4 Marion Street WEST CROYDON	House	CT 5638/544	10412
5 Marion Street WEST CROYDON	House	CT 5756/767	10413
6 Marion Street WEST CROYDON	House	CT 5189/937	10414
7 Marion Street WEST CROYDON	House	CT 5238/158	10415
8 Marion Street WEST CROYDON	House	CT 1154/22	10416
9 Marion Street WEST CROYDON	House	CT 5629/100	10417

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
1 Para Street WEST CROYDON	House	CT 5409/446	10418
2 Para Street WEST CROYDON	House	CT 5790/925	10419
3 Para Street WEST CROYDON	House	CT 5418/726	10420
4 Para Street WEST CROYDON	House	CT 5364/407	10421
55 Robert Street WEST CROYDON	House	CT 2187/176	10521
57 Robert Street WEST CROYDON	House	CT 5216/816	10522
59 Robert Street WEST CROYDON	House	CT 5599/161	10524
60 Robert Street WEST CROYDON	House	CT 5179/334	10525
61 Robert Street WEST CROYDON	House	CT 5410/162	10526
62 Robert Street WEST CROYDON	House	CT 5414/632	10527
63 Robert Street WEST CROYDON	House	CT 5319/593	10528
64 Robert Street WEST CROYDON	House	CT 5154/316	10529
65 Robert Street WEST CROYDON	House	CT 5837/565	10530
68 Robert Street WEST CROYDON	House	CT 5163/501	10531
69 Robert Street WEST CROYDON	House	CT 5569/953	10532
70 Robert Street WEST CROYDON	House	CT 2014/166	10533
71 Robert Street WEST CROYDON	House	CT 5444/268	10534
73 Robert Street WEST CROYDON	House	CT 5849/658	10535
75 Robert Street WEST CROYDON	House	CT 5797/675	10536
76 Robert Street WEST CROYDON	House	CT 5853/66	10537
77 Robert Street WEST CROYDON	House	CT 5569/807	10538
78 Robert Street WEST CROYDON	House	CT 5448/319	10539
79 Robert Street WEST CROYDON	Maisonette	CT 5276/730	10540
80 Robert Street WEST CROYDON	House	CT 5545/776	10541
81 Robert Street WEST CROYDON	Maisonette	CT 5711/920	10542
82 Robert Street WEST CROYDON	House	CT 5740/126	10543
84 Robert Street WEST CROYDON	Maisonette	CT 5852/989	10544
85 Robert Street WEST CROYDON	House	CT 5546/917	10545
86 Robert Street WEST CROYDON	Maisonette	CT 5610/46	10546
87 Robert Street WEST CROYDON	House	CT 5188/520	10547
88-94 Robert Street WEST CROYDON	Private Hospital	CT 4319/468 CT 5458/361 CT 5476/317 CT 5516/966	10548
89 Robert Street WEST CROYDON	Maisonette	CT 5173/419	10549
91 Robert Street WEST CROYDON	Maisonette	CT 5226/958	10550
93a Robert Street WEST CROYDON	House	CT 5331/327	10551
95 Robert Street WEST CROYDON	House	CT 5130/319	10552
96 Robert Street WEST CROYDON	House	CT 5625/905	10553

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
98 Robert Street WEST CROYDON	House	CT 5779/602	10554
99 Robert Street WEST CROYDON	House	CT 5703/703	10555
100 Robert Street WEST CROYDON	House	CT 5823/877	10556
101 Robert Street WEST CROYDON	Maisonette	CT 5216/246	10557
102 Robert Street WEST CROYDON	House	CT 5692/378	10558
103 Robert Street WEST CROYDON	Maisonette	CT 5225/534	10559
105 Robert Street WEST CROYDON	House	CT 5820/567	10560
1/ 5 Rosetta Street WEST CROYDON	Home Unit	CT 5002/735	10561
2/ 5 Rosetta Street WEST CROYDON	Home Unit	CT 5002/736	10562
7 Rosetta Street WEST CROYDON	House	CT 1184/131	10563
8-16 Rosetta Street WEST CROYDON	Shops & Dwelling	CT 5715/125	20783
18 Rosetta Street WEST CROYDON	House	CT 5532/457	10564
22 Rosetta Street WEST CROYDON	House	CT 5780/524	10565
23 Rosetta Street WEST CROYDON	House	CT 5666/754 CT 5742/971	10566
24 Rosetta Street WEST CROYDON	House	CT 5814/677	10567
26 Rosetta Street WEST CROYDON	House	CT 5397/721	10568
27 Rosetta Street WEST CROYDON	House	CT 5478/477	10569
28 Rosetta Street WEST CROYDON	House	CT 5726/350	10570
29 Rosetta Street WEST CROYDON	House	CT 5732/308	10571
30 Rosetta Street WEST CROYDON	House	CT 5272/475	10572
31 Rosetta Street WEST CROYDON	House	CT 5246/844	10573
32 Rosetta Street WEST CROYDON	House	CT 5760/868	10574
33 Rosetta Street WEST CROYDON	House	CT 5360/961	10575
34 Rosetta Street WEST CROYDON	House	CT 5437/831	10576
36 Rosetta Street WEST CROYDON	House	CT 5786/275	10577
39 Rosetta Street WEST CROYDON	House	CT 5722/779 CT 5722/780	10579
41 Rosetta Street WEST CROYDON	House	CT 5278/712	10580
42 Rosetta Street WEST CROYDON	House	CT 5371/946	10581
44 Rosetta Street WEST CROYDON	House	CT 5727/411	10582
45A-45B Rosetta Street WEST CROYDON	Maisonettes	CT 5727/564	10583
49 Rosetta Street WEST CROYDON	House	CT 5442/256	10584
50 Rosetta Street WEST CROYDON	House	CT 5263/468	10585
51 Rosetta Street WEST CROYDON	House	CT 5492/648	10586
53 Rosetta Street WEST CROYDON	House	CT 5726/453	10587
54 Rosetta Street WEST CROYDON	House	CT 5477/37	10588
55 Rosetta Street WEST CROYDON	Home Unit	CT 5010/39	10589

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
56 Rosetta Street WEST CROYDON	House	CT 5355/106	10590
57 Rosetta Street WEST CROYDON	Home Unit	CT 5597/635	10591
59 Rosetta Street WEST CROYDON	Home Unit	CT 5023/212	10592
60 Rosetta Street WEST CROYDON	Shop & Dwelling	CT 5814/569	10593
61 Rosetta Street WEST CROYDON	Home Unit	CT 5023/211	10594
63 Rosetta Street WEST CROYDON	House	CT 5727/280	10595
1 Westbourne Street WEST CROYDON	House	CT 5751/630	10676
2 Westbourne Street WEST CROYDON	House	CT 5751/121	10677
4 Westbourne Street WEST CROYDON	House	CT 5706/803	10678
5 Westbourne Street WEST CROYDON	House	CT 5819/920	10679
7 Westbourne Street WEST CROYDON	House	CT 5076/791	10680
8 Westbourne Street WEST CROYDON	House	CT 5819/991	10681
9 Westbourne Street WEST CROYDON	House	CT 5754/669	10682
10 Westbourne Street WEST CROYDON	House	CT 5509/573	10683
11 Westbourne Street WEST CROYDON	House	CT 5749/721	10684
12 Westbourne Street WEST CROYDON	House	CT 5741/3108	10685
13 Westbourne Street WEST CROYDON	House	CT 5390/356	10686
14 Westbourne Street WEST CROYDON	House	CT 5742/578	10687
29 William Street WEST CROYDON	House	CT 5408/21	10715
31 William Street WEST CROYDON	House	CT 5760/235	10716
34 William Street WEST CROYDON	House	CT 5549/737	10717
36 William Street WEST CROYDON	House	CT 5338/316	10718
37 William Street WEST CROYDON	House	CT 5631/950	10719
38 William Street WEST CROYDON	House	CT 5804/349	10720
39 William Street WEST CROYDON	House	CT 5557/77	10721
41 William Street WEST CROYDON	House	CT 5339/325	10722
42 William Street WEST CROYDON	Maisonettes	CT 5710/234	10723
43 William Street WEST CROYDON	House	CT 5816/33	10724
44 William Street WEST CROYDON	House	CT 5632/172	10725
45 William Street WEST CROYDON	House	CT 5356/757	10726
46 William Street WEST CROYDON	House	CT 5792/274	10727
47 William Street WEST CROYDON	House	CT 5219/202	10728
48 William Street WEST CROYDON	House	CT 5793/761	10729
49 William Street WEST CROYDON	House	CT 5733/7	10730
50 William Street WEST CROYDON	House	CT 5780/560	10731
51 William Street WEST CROYDON	House	CT 5781/519	10732
52 William Street WEST CROYDON	House	CT 5780/559	10733
53 William Street WEST CROYDON	House	CT 5781/698	10734

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
56 William Street WEST CROYDON	House	CT 5813/314	10736
57 William Street WEST CROYDON	House	CT 5836/690	10737
58 William Street WEST CROYDON	House	CT 5779/563	10738
59 William Street WEST CROYDON	House	CT 5843/665	10739
60 William Street WEST CROYDON	House	CT 5259/912	10740
62 William Street WEST CROYDON	House	CT 5249/92	10741
64 William Street WEST CROYDON	House	CT 5511/200	10742
66 William Street WEST CROYDON	House	CT 5073/648	10743
68 William Street WEST CROYDON	House	CT 5407/2	10744
72 William Street WEST CROYDON	House	CT 5147/62	10745
74 William Street WEST CROYDON	House	CT 5781/827	10746
76 William Street WEST CROYDON	House	CT 5275/227	10747
10 Albemarle Street WEST HINDMARSH	House	CT 5601/300	11048
14 Albemarle Street WEST HINDMARSH	Maisonette	CT 5657/209	11049
16 Albemarle Street WEST HINDMARSH	Maisonette	CT 5657/209	11050
18 Albemarle Street WEST HINDMARSH	House	CT 5610/339	11051
20 Albemarle Street WEST HINDMARSH	House	CT 5610/339	11052
22 Albemarle Street WEST HINDMARSH	House	CT 5185/185	11053
24 Albemarle Street WEST HINDMARSH	House	CT 5792/530	11054
28 Albemarle Street WEST HINDMARSH	House	CT 5302/684	11055
30 Albemarle Street WEST HINDMARSH	House	CT 5789/989	11056
32 Albemarle Street WEST HINDMARSH	House	CT 4072/73	11057
34 Albemarle Street WEST HINDMARSH	House	CT 5163/997	11058
36 Albemarle Street WEST HINDMARSH	House	CT 5756/776	11059
37 Albemarle Street WEST HINDMARSH	House	CT 5197/667	11060
38 Albemarle Street WEST HINDMARSH	House	CT 5467/200	11061
39 Albemarle Street WEST HINDMARSH	House	CT 5793/903	11062
40 Albemarle Street WEST HINDMARSH	Maisonette	CT 5654/841	11063
41 Albemarle Street WEST HINDMARSH	House	CT 5577/55	11064
42 Albemarle Street WEST HINDMARSH	Maisonette	CT 5654/841	11065
43 Albemarle Street WEST HINDMARSH	House	CT 5346/697	11066
44 Albemarle Street WEST HINDMARSH	House	CT 5719/251	11067
46 Albemarle Street WEST HINDMARSH	House	CT 5814/937	11068
47 Albemarle Street WEST HINDMARSH	House	CT 5784/183	11069
49 Albemarle Street WEST HINDMARSH	House	CT 5439/984	11070
51 Albemarle Street WEST HINDMARSH	House	CT 5331/307	11071
53 Albemarle Street WEST HINDMARSH	House	CT 5556/28	11072
57 Arlington Terrace WEST HINDMARSH	House	CT 5439/817	11073

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
59 Arlington Terrace WEST HINDMARSH	House	CT 987/146	11074
1 Bertie Street WEST HINDMARSH	Maisonette	CT 5464/392	11075
1a Bertie Street WEST HINDMARSH	Maisonette	CT 5392/87	11076
4 Bertie Street WEST HINDMARSH	House	CT 5413/694	11077
5 Bertie Street WEST HINDMARSH	House	CT 5077/317	11078
6 Bertie Street WEST HINDMARSH	House	CT 4160/155	11079
7 Bertie Street WEST HINDMARSH	House	CT 4224/52	11080
8 Bertie Street WEST HINDMARSH	House	CT 5800/668	11081
8a Bertie Street WEST HINDMARSH	House	CT 5719/383	11082
9 Bertie Street WEST HINDMARSH	House	CT 4318/11	11083
11 Bertie Street WEST HINDMARSH	House	CT 5300/826	11084
11a Bertie Street WEST HINDMARSH	House	CT 5348/370	11085
12 Bertie Street WEST HINDMARSH	House	CT 5322/483	11086
14 Bertie Street WEST HINDMARSH	House	CT 5290/764	11087
15 Bertie Street WEST HINDMARSH	House	CT 5459/988	11088
16 Bertie Street WEST HINDMARSH	House	CT 5627/930	11089
17 Bertie Street WEST HINDMARSH	House	CT 4012/131	11090
18 Bertie Street WEST HINDMARSH	House	CT 5537/877	11091
19 Bertie Street WEST HINDMARSH	House	CT 5793/298	11092
20 Bertie Street WEST HINDMARSH	House	CT 5801/283	11093
21 Bertie Street WEST HINDMARSH	House	CT 5597/92	11094
22 Bertie Street WEST HINDMARSH	House	CT 5185/218	11095
23 Bertie Street WEST HINDMARSH	House	CT 5571/479	11096
25 Bertie Street WEST HINDMARSH	House	CT 5679/201	11097
26 Bertie Street WEST HINDMARSH	House	CT 5803/257	11098
27 Bertie Street WEST HINDMARSH	House	CT 5679/202	11099
28 Bertie Street WEST HINDMARSH	House	CT 5444/814	11100
29 Bertie Street WEST HINDMARSH	House	CT 5606/249	11101
29a Bertie Street WEST HINDMARSH	House	CT 5442/364	11102
30 Bertie Street WEST HINDMARSH	House	CT 5654/81	11103
31 Bertie Street WEST HINDMARSH	House	CT 5615/900	11104
35 Bertie Street WEST HINDMARSH	House	CT 5235/826	11105
2 Bond Street WEST HINDMARSH	House	CT 5783/640	11107
5 Bond Street WEST HINDMARSH	House	CT 5456/104	11108
5a Bond Street WEST HINDMARSH	House	CT 5523/802	11109
6 Bond Street WEST HINDMARSH	House	CT 5573/490	11110
7 Bond Street WEST HINDMARSH	House	CT 5797/93	11111
8 Bond Street WEST HINDMARSH	House	CT 5355/172	11112

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
9 Bond Street WEST HINDMARSH	House	CT 5189/300	11113
10 Bond Street WEST HINDMARSH	House	CT 5531/232	11114
11 Bond Street WEST HINDMARSH	House	CT 5837/464	11115
12 Bond Street WEST HINDMARSH	House	CT 5205/54	11116
13 Bond Street WEST HINDMARSH	House	CT 5477/7	11117
15 Bond Street WEST HINDMARSH	House	CT 5661/344	11119
16 Bond Street WEST HINDMARSH	House	CT 5274/712	11120
17 Bond Street WEST HINDMARSH	House	CT 5816/353	11121
18 Bond Street WEST HINDMARSH	House	CT 5670/673	11122
19 Bond Street WEST HINDMARSH	House	CT 5657/765	11123
20 Bond Street WEST HINDMARSH	House	CT 5733/230	11124
21 Bond Street WEST HINDMARSH	House	CT 5821/209	11125
23 Bond Street WEST HINDMARSH	House	CT 5589/552	11126
27 Bond Street WEST HINDMARSH	House	CT 5187/394	11128
28 Bond Street WEST HINDMARSH	House	CT 5576/898	11129
1 Bourn Avenue WEST HINDMARSH	House	CT 5244/346	11130
2 Bourn Avenue WEST HINDMARSH	House	CT 5814/700	11131
4 Bourn Avenue WEST HINDMARSH	House	CT 4063/638	11132
6 Bourn Avenue WEST HINDMARSH	House	CT 5139/713	11133
5 Cator Street WEST HINDMARSH	House	CT 5785/403	11134
7 Cator Street WEST HINDMARSH	House	CT 5384/367	11135
8 Cator Street WEST HINDMARSH	House	CT 5711/895	11136
1/ 9 Cator Street WEST HINDMARSH	Single Storey Flat	CT 5387/558	11137
2/ 9 Cator Street WEST HINDMARSH	Single Storey Flat	CT 3698/110	11138
10 Cator Street WEST HINDMARSH	House	CT 5304/372	11139
11 Cator Street WEST HINDMARSH	House	CT 5783/638	11140
13 Cator Street WEST HINDMARSH	House	CT 5496/583	11141
17 Cator Street WEST HINDMARSH	House	CT 5743/949	11142
21 Cator Street WEST HINDMARSH	House	CT 5807/604	11145
22 Cator Street WEST HINDMARSH	House	CT 5752/65	11146
23 Cator Street WEST HINDMARSH	House	CT 788/81	11147
24 Cator Street WEST HINDMARSH	Maisonette	CT 5887/143	11148
26 Cator Street WEST HINDMARSH	Maisonette	CT 5887/144	20871
27 Cator Street WEST HINDMARSH	House	CT 5316/821	11149
28 Cator Street WEST HINDMARSH	Maisonette	CT 5479/65	11150
29 Cator Street WEST HINDMARSH	Maisonette	CT 5215/324	11151
30 Cator Street WEST HINDMARSH	Maisonette	CT 5480/981	11152
31 Cator Street WEST HINDMARSH	Maisonette	CT 5215/323	11153

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
32 Cator Street WEST HINDMARSH	House	CT 5819/477	11154
1/ 33 Cator Street WEST HINDMARSH	Home Unit	CT 5329/732	11155
2/ 33 Cator Street WEST HINDMARSH	Home Unit	CT 5329/733	11156
34 Cator Street WEST HINDMARSH	House	CT 5290/86	11157
35 Cator Street WEST HINDMARSH	House	CT 5729/730	11158
36 Cator Street WEST HINDMARSH	House	CT 5657/774	11159
1 Cross Street WEST HINDMARSH	House	CT 5680/62	11160
2 Cross Street WEST HINDMARSH	House	CT 5838/612	11161
3 Cross Street WEST HINDMARSH	House	CT 5658/353	11162
4 Cross Street WEST HINDMARSH	House	CT 5363/685	11163
5 Cross Street WEST HINDMARSH	House	CT 4326/917	11164
6 Cross Street WEST HINDMARSH	House	CT 5199/838	11165
7 Cross Street WEST HINDMARSH	House	CT 5477/545	11166
9 Cross Street WEST HINDMARSH	House	CT 5570/885	11167
11 Cross Street WEST HINDMARSH	House	CT 5668/310	11168
13 Cross Street WEST HINDMARSH	House	CT 5517/964	11169
1 Glanton Street WEST HINDMARSH	House	CT 5362/189	11170
3 Glanton Street WEST HINDMARSH	House	CT 5143/759	11171
5 Glanton Street WEST HINDMARSH	House	CT 5206/83	11172
6 Glanton Street WEST HINDMARSH	House	CT 5833/210	11173
7 Glanton Street WEST HINDMARSH	House	CT 5359/134	11174
8 Glanton Street WEST HINDMARSH	House	CT 5197/254	11175
9 Glanton Street WEST HINDMARSH	House	CT 5321/23	11177
10 Glanton Street WEST HINDMARSH	House	CT 5098/464	11178
11 Glanton Street WEST HINDMARSH	House	CT 5213/920	11179
12 Glanton Street WEST HINDMARSH	House	CT 5603/271	11180
13 Glanton Street WEST HINDMARSH	House	CT 4204/712	11181
14 Glanton Street WEST HINDMARSH	House	CT 5675/445	11182
15 Glanton Street WEST HINDMARSH	House	CT 5719/333	11183
16 Glanton Street WEST HINDMARSH	House	CT 5675/444	11184
17 Glanton Street WEST HINDMARSH	House	CT 5649/562	11185
18 Glanton Street WEST HINDMARSH	House	CT 5736/344	11186
19 Glanton Street WEST HINDMARSH	House	CT 5431/880	11187
20 Glanton Street WEST HINDMARSH	House	CT 5736/345	11188
21 Glanton Street WEST HINDMARSH	House	CT 5440/306	11189
22 Glanton Street WEST HINDMARSH	House	CT 5798/698	11190
23 Glanton Street WEST HINDMARSH	House	CT 5085/541	11191
24 Glanton Street WEST HINDMARSH	House	CT 5461/796	11192

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
26 Glanton Street WEST HINDMARSH	House	CT 5727/998	11193
27 Glanton Street WEST HINDMARSH	House	CT 5226/571	11194
28 Glanton Street WEST HINDMARSH	House	CT 5727/999	11195
8 Grange Road WEST HINDMARSH	House	CT 5248/444	11200
10 Grange Road WEST HINDMARSH	House	CT 5839/194	11201
11 Grange Road WEST HINDMARSH	House	CT 5825/619	11202
13 Grange Road WEST HINDMARSH	Maisonette	CT 5825/618	11203
14 Grange Road WEST HINDMARSH	House	CT 2752/56	11204
15 Grange Road WEST HINDMARSH	Maisonette	CT 5825/618	11205
17 Grange Road WEST HINDMARSH	House	CT 5358/973	11206
22 Grange Road WEST HINDMARSH	House	CT 5447/156	11207
24 Grange Road WEST HINDMARSH	House	CT 5108/427	11208
26 Grange Road WEST HINDMARSH	House	CT 5837/888	11209
28 Grange Road WEST HINDMARSH	House	CT 4075/149	11210
30 Grange Road WEST HINDMARSH	House	CT 5577/97	11211
34 Grange Road WEST HINDMARSH	Private Hospital	CT 5734/386	11213
35 Grange Road WEST HINDMARSH	House	CT 5811/590	11214
37 Grange Road WEST HINDMARSH	House	CT 5598/580	11215
38 Grange Road WEST HINDMARSH	House	CT 5834/768	11216
39 Grange Road WEST HINDMARSH	House	CT 5839/308	11217
40 Grange Road WEST HINDMARSH	House	CT 5364/521	11218
41 Grange Road WEST HINDMARSH	House	CT 5222/588	11219
42 Grange Road WEST HINDMARSH	House	CT 5518/328	11220
44 Grange Road WEST HINDMARSH	House	CT 5797/442	11221
46 Grange Road WEST HINDMARSH	House	CT 5231/195	11222
47 Grange Road WEST HINDMARSH	House	CT 5643/548	11223
49 Grange Road WEST HINDMARSH	House	CT 5836/906	11224
1 Linden Street WEST HINDMARSH	House	CT 5738/670	11296
2 Linden Street WEST HINDMARSH	House	CT 5359/957	11297
3 Linden Street WEST HINDMARSH	House	CT 5359/344	11298
1 Morgan Street WEST HINDMARSH	House	CT 5555/651	11299
3 Morgan Street WEST HINDMARSH	House	CT 5313/980	11300
4 Morgan Street WEST HINDMARSH	House	CT 5367/471	11301
5 Morgan Street WEST HINDMARSH	House	CT 5213/925	11302
7 Morgan Street WEST HINDMARSH	House	CT 5370/526	11303
8 Morgan Street WEST HINDMARSH	House	CT 5326/931	11304
9 Morgan Street WEST HINDMARSH	House	CT 5820/478	11305
10 Morgan Street WEST HINDMARSH	House	CT 5292/688	11306

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
11 Morgan Street WEST HINDMARSH	House	CT 5730/954	11307
12 Morgan Street WEST HINDMARSH	House	CT 5582/446	11308
13 Morgan Street WEST HINDMARSH	House	CT 5533/203	11309
14 Morgan Street WEST HINDMARSH	House	CT 5825/437	11310
15 Morgan Street WEST HINDMARSH	House	CT 5812/769	11311
16 Morgan Street WEST HINDMARSH	House	CT 5813/870	11312
17 Morgan Street WEST HINDMARSH	House	CT 5530/221	11313
18 Morgan Street WEST HINDMARSH	House	CT 5151/677	11314
19 Morgan Street WEST HINDMARSH	House	CT 5794/142	11315
20 Morgan Street WEST HINDMARSH	House	CT 5581/948	11316
21 Morgan Street WEST HINDMARSH	House	CT 5190/45	11317
22 Morgan Street WEST HINDMARSH	House	CT 5495/417	11318
23 Morgan Street WEST HINDMARSH	House	CT 5713/967	11319
24 Morgan Street WEST HINDMARSH	House	CT 5827/681	11320
25 Morgan Street WEST HINDMARSH	House	CT 5628/672	11321
26 Morgan Street WEST HINDMARSH	House	CT 5401/454	11322
27 Morgan Street WEST HINDMARSH	House	CT 5464/329	11323
28 Morgan Street WEST HINDMARSH	House	CT 1539/184	11324
2 Ponsonby Street WEST HINDMARSH	House	CT 5820/671	11334
3 Ponsonby Street WEST HINDMARSH	House	CT 5735/623	11335
4 Ponsonby Street WEST HINDMARSH	House	CT 1056/136	11336
5 Ponsonby Street WEST HINDMARSH	House	CT 5551/985	11337
5a Ponsonby Street WEST HINDMARSH	House	CT 5103/925	11338
7 Ponsonby Street WEST HINDMARSH	House	CT 5847/678	11339
8 Ponsonby Street WEST HINDMARSH	House	CT 5779/821	11340
9 Ponsonby Street WEST HINDMARSH	House	CT 5556/226	11341
10 Ponsonby Street WEST HINDMARSH	House	CT 5546/198	11342
11 Ponsonby Street WEST HINDMARSH	House	CT 5080/902	11343
12 Ponsonby Street WEST HINDMARSH	House	CT 5290/779	11344
13 Ponsonby Street WEST HINDMARSH	House	CT 5715/895	11345
14 Ponsonby Street WEST HINDMARSH	House	CT 5782/142	11346
15 Ponsonby Street WEST HINDMARSH	House	CT 5309/728	11347
16 Ponsonby Street WEST HINDMARSH	House	CT 5451/593	11348
1 Romford Street WEST HINDMARSH	House	CT 5463/770	11349
3 Romford Street WEST HINDMARSH	House	CT 5573/261	11350
5 Romford Street WEST HINDMARSH	House	CT 5188/199	11351
6 Romford Street WEST HINDMARSH	House	CT 5827/676	11352
7 Romford Street WEST HINDMARSH	House	CT 5288/795	11353

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
9 Romford Street WEST HINDMARSH	House	CT 5239/379	11354
11 Romford Street WEST HINDMARSH	House	CT 1208/120	11355
13a Romford Street WEST HINDMARSH	House	CT 5677/518	11357
15 Romford Street WEST HINDMARSH	House	CT 5588/604	11358
17 Romford Street WEST HINDMARSH	Home Unit	CT 5039/721	11359
2 Beaufort Street WOODVILLE	House	CT 5356/286	12059
3 Beaufort Street WOODVILLE	House	CT 5726/59	12060
3a Beaufort Street WOODVILLE	House	CT 5089/122	12061
4 Beaufort Street WOODVILLE	House	CT 5228/72	12062
5 Beaufort Street WOODVILLE	House	CT 5783/326 CT 5810/280	12063
6 Beaufort Street WOODVILLE	House	CT 5787/204	12064
7 Beaufort Street WOODVILLE	House	CT 5417/387	12065
8 Beaufort Street WOODVILLE	House	CT 5734/757	12066
9 Beaufort Street WOODVILLE	House	CT 5144/747	12067
10 Beaufort Street WOODVILLE	House	CT 5454/517	12068
11 Beaufort Street WOODVILLE	House	CT 5672/470	12069
12 Beaufort Street WOODVILLE	House	CT 5797/566	12070
13 Beaufort Street WOODVILLE	House	CT 5650/255	12071
15 Beaufort Street WOODVILLE	House	CT 5454/97	12072
16 Beaufort Street WOODVILLE	House	CT 5301/520	12073
17 Beaufort Street WOODVILLE	House	CT 5119/302	12074
18 Beaufort Street WOODVILLE	House	CT 5467/238	12075
19 Beaufort Street WOODVILLE	Single Storey Flats	CT 5088/112	12076
20 Beaufort Street WOODVILLE	House	CT 5223/193	12077
21 Beaufort Street WOODVILLE	House	CT 5440/766	12078
22 Beaufort Street WOODVILLE	House	CT 5147/483	12079
23 Beaufort Street WOODVILLE	House	CT 5194/412	12080
24 Beaufort Street WOODVILLE	House	CT 5138/189	12081
25 Beaufort Street WOODVILLE	House	CT 5158/404	12082
27 Beaufort Street WOODVILLE	House	CT 5756/852	12084
28 Beaufort Street WOODVILLE	House	CT 5441/70	12085
30 Beaufort Street WOODVILLE	House	CT 5168/526	12086
31 Beaufort Street WOODVILLE	House	CT 5692/734	12087
32 Beaufort Street WOODVILLE	House	CT 5462/481	12088
1 Belmore Terrace WOODVILLE	House	CT 5302/871	12089
3 Belmore Terrace WOODVILLE	House	CT 5721/11	12090
5 Belmore Terrace WOODVILLE	House	CT 5411/122	12091

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
6-7 Belmore Terrace WOODVILLE	House	CT 5206/548	12092
8 Belmore Terrace WOODVILLE	House	CT 5705/967	12093
10 Belmore Terrace WOODVILLE	House	CT 5286/41	12094
11 Belmore Terrace WOODVILLE	House	CT 5167/809	12095
12 Belmore Terrace WOODVILLE	House	CT 5801/498	12096
13 Belmore Terrace WOODVILLE	House	CT 5823/513	12097
14 Belmore Terrace WOODVILLE	House	CT 5628/882	12098
2 Florence Street WOODVILLE	House	CT 5160/88	12104
4 Florence Street WOODVILLE	House	CT 5819/822	12105
6 Florence Street WOODVILLE	House	CT 5548/36	12106
5 Harvey Street West WOODVILLE	House	CT 5711/498	12118
7 Harvey Street West WOODVILLE	House	CT 5804/204	12119
9 Harvey Street West WOODVILLE	House	CT 5262/393	12120
11 Harvey Street West WOODVILLE	House	CT 5300/797	12121
13 Harvey Street West WOODVILLE	House	CT 5211/885	12122
20 Harvey Street West WOODVILLE	House	CT 5352/623	12123
22 Harvey Street West WOODVILLE	House	CT 5364/112	12124
24 Harvey Street West WOODVILLE	House	CT 5795/224	12125
26 Harvey Street West WOODVILLE	House	CT 5173/337	12126
28 Harvey Street West WOODVILLE	House	CT 5551/124	12127
19 Hughes Street South WOODVILLE	House	CT 5140/634	12149
20 Hughes Street South WOODVILLE	House	CT 5783/468 CT 5784/359	12150
21 Hughes Street South WOODVILLE	House	CT 5521/578	12151
22 Hughes Street South WOODVILLE	House	CT 5145/859	12152
23 Hughes Street South WOODVILLE	House	CT 5668/271	12153
24 Hughes Street South WOODVILLE	House	CT 5292/717	12154
25 Hughes Street South WOODVILLE	House	CT 5213/752	12155
26 Hughes Street South WOODVILLE	Maisonette	CT 5800/749	12156
27 Hughes Street South WOODVILLE	House	CT 5417/34	12157
28 Hughes Street South WOODVILLE	Maisonette	CT 2643/142	12158
34 Hughes Street North WOODVILLE	House	CT 5563/144	12128
35 Hughes Street North WOODVILLE	Telephone Exchange Building	CT 5551/150	12129
36 Hughes Street North WOODVILLE	House	CT 5620/392	12130
37 Hughes Street North WOODVILLE	House	CT 5551/151	12131
39 Hughes Street North WOODVILLE	House	CT 5211/748	12132
40 Hughes Street North WOODVILLE	House	CT 5187/174	12133

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
41 Hughes Street North WOODVILLE	House	CT 5761/32	12134
42 Hughes Street North WOODVILLE	House	CT 5791/999	12135
43 Hughes Street North WOODVILLE	House	CT 5337/605	12136
44 Hughes Street North WOODVILLE	House	CT 5627/809	12137
45 Hughes Street North WOODVILLE	House	CT 5799/613	12138
46 Hughes Street North WOODVILLE	House	CT 5687/308	12139
48 Hughes Street North WOODVILLE	House	CT 5129/928	12140
50 Hughes Street North WOODVILLE	House	CT 1189/176	12141
53 Hughes Street North WOODVILLE	House	CT 5472/130	12142
57 Hughes Street North WOODVILLE	House	CT 5454/567	12143
61 Hughes Street North WOODVILLE	House	CT 5423/933	12144
63 Hughes Street North WOODVILLE	House	CT 5220/480	12145
65 Hughes Street North WOODVILLE	House	CT 5726/394	12146
18 Hughes Street South WOODVILLE	House	CT 5806/563	12148
3 McColl Street WOODVILLE	House	CT 5270/87	12163
27 Norman Street WOODVILLE	Kindergarten	CT 5815/275	12164
32a Norman Street WOODVILLE	House	CT 5550/256	12165
33 Norman Street WOODVILLE	House	CT 5176/549	12166
34 Norman Street WOODVILLE	House	CT 5824/172	12167
35 Norman Street WOODVILLE	House	CT 1439/191	12168
36 Norman Street WOODVILLE	House	CT 5144/930	12169
40 Norman Street WOODVILLE	House	CT 5596/63	12170
15 Park Street South WOODVILLE	Maisonette	CT 5360/683	12174
17 Park Street South WOODVILLE	Maisonette	CT 5305/930	12175
19 Park Street South WOODVILLE	House	CT 5539/608	12176
21 Park Street South WOODVILLE	House	CT 5499/126	12177
23 Park Street North WOODVILLE	House	CT 5460/830	12171
25 Park Street North WOODVILLE	House	CT 5352/333	12172
27 Park Street North WOODVILLE	House	CT 5603/542	12173
5 Russell Terrace WOODVILLE	House	CT 4086/675 CT 4086/676	12184
6 Russell Terrace WOODVILLE	Single Storey Flats	CT 5809/763	12185
9 Russell Terrace WOODVILLE	House	CT 5814/78	12186
10 Russell Terrace WOODVILLE	House	CT 5809/771	12187
11 Russell Terrace WOODVILLE	House	CT 5809/764	12188
12 Russell Terrace WOODVILLE	House	CT 5814/779	12189
14 Russell Terrace WOODVILLE	House	CT 5547/324	12190
15 Russell Terrace WOODVILLE	Home Unit	CT 5013/278	12191

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
17 Russell Terrace WOODVILLE	House	CT 5234/359	12192
18 Russell Terrace WOODVILLE	House	CT 3128/147	12193
19 Russell Terrace WOODVILLE	Single Storey Flats	CT 3870/113	12194
20 Russell Terrace WOODVILLE	House	CT 5467/483	12195
21 Russell Terrace WOODVILLE	Maisonettes	CT 5796/837	12196
22 Russell Terrace WOODVILLE	House	CT 5731/865	12197
23 Russell Terrace WOODVILLE	House	CT 5633/241	12198
24 Russell Terrace WOODVILLE	House	CT 5794/869	12199
25 Russell Terrace WOODVILLE	House	CT 5821/330	12200
1 Stanley Street WOODVILLE	House	CT 2401/199	12203
3 Stanley Street WOODVILLE	House	CT 5440/54	12204
4 Stanley Street WOODVILLE	House	CT 5796/923	12205
5 Stanley Street WOODVILLE	House	CT 5172/505	12206
6 Stanley Street WOODVILLE	House	CT 5141/233	12207
7 Stanley Street WOODVILLE	House	CT 5811/7	12208
8 Stanley Street WOODVILLE	House	CT 5436/568	12209
9 Stanley Street WOODVILLE	House	CT 1220/40	12210
10 Stanley Street WOODVILLE	House	CT 5192/96	12211
11 Stanley Street WOODVILLE	House	CT 5340/509	12212
12 Stanley Street WOODVILLE	House	CT 5171/266	12213
13 Stanley Street WOODVILLE	House	CT 1280/125	12214
14 Stanley Street WOODVILLE	House	CT 5791/960	12215
15 Stanley Street WOODVILLE	House	CT 5178/356	12216
16 Stanley Street WOODVILLE	House	CT 5452/215	12217
17 Stanley Street WOODVILLE	House	CT 5212/706	12218
18 Stanley Street WOODVILLE	House	CT 5257/665	12219
19 Stanley Street WOODVILLE	House	CT 5807/607	12220
20 Stanley Street WOODVILLE	Home Units	CT 5739/559	12221
21 Stanley Street WOODVILLE	House	CT 5807/759	12222
21a Stanley Street WOODVILLE	House	CT 5716/279	12223
22 Stanley Street WOODVILLE	House	CT 5283/497	12224
24 Stanley Street WOODVILLE	House	CT 5650/836	12225
25 Stanley Street WOODVILLE	House	CT 5815/521	12226
26 Stanley Street WOODVILLE	House	CT 4332/352	12227
27 Stanley Street WOODVILLE	House	CT 5731/140	12228
28 Stanley Street WOODVILLE	House	CT 5191/767	12229
29 Stanley Street WOODVILLE	House	CT 5739/888	12230
30 Stanley Street WOODVILLE	House	CT 5611/170	12231

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
31 Stanley Street WOODVILLE	House	CT 5428/63 CT 5428/64	12232
32 Stanley Street WOODVILLE	House	CT 5715/632	12233
34 Stanley Street WOODVILLE	House	CT 1217/39	12235
36 Stanley Street WOODVILLE	House	CT 5124/479	12237
1 The Grove WOODVILLE	House	CT 5382/158	12245
2 The Grove WOODVILLE	House	CT 5787/338	12246
3 The Grove WOODVILLE	House	CT 5712/111	12247
5 The Grove WOODVILLE	House	CT 5116/988	12248
7 The Grove WOODVILLE	House	CT 5704/581	12249
8 The Grove WOODVILLE	House	CT 5305/696	12250
9 The Grove WOODVILLE	House	CT 5593/946	12251
11 The Grove WOODVILLE	House	CT 5791/105	12252
12 The Grove WOODVILLE	House	CT 5521/662	12253
14 The Grove WOODVILLE	House	CT 5442/878	12254
15 The Grove WOODVILLE	House	CT 5710/526	12255
16 The Grove WOODVILLE	House	CT 5797/397	12256
18 The Grove WOODVILLE	House	CT 5680/918	12257
20 The Grove WOODVILLE	House	CT 5300/673	12259
21 The Grove WOODVILLE	House	CT 5191/92	12260
22 The Grove WOODVILLE	House	CT 5827/600	12261
23 The Grove WOODVILLE	House	CT 5744/371	12262
24 The Grove WOODVILLE	House	CT 5133/450	12263
25 The Grove WOODVILLE	House	CT 5744/373	12264
27 The Grove WOODVILLE	House	CT 5744/370	12265
86 Woodville Road WOODVILLE	House	CT 5807/933	12266
86a Woodville Road WOODVILLE	House	CT 5406/76	12267
88 Woodville Road WOODVILLE	House	CT 5799/93 CT 5818/602	12268
92 Woodville Road WOODVILLE	House	CT 5219/967	12271
94 Woodville Road WOODVILLE	House	CT 5812/174	12272
22-25 Belmore Terrace WOODVILLE PARK	Shops	CT 5255/342	20670
26 Belmore Terrace WOODVILLE PARK	House	CT 5223/192	12100
28 Belmore Terrace WOODVILLE PARK	House	CT 5313/441	12101
29 Belmore Terrace WOODVILLE PARK	House	CT 5793/768	12102
30 Belmore Terrace WOODVILLE PARK	House	CT 5801/817	12103
31 Belmore Terrace WOODVILLE PARK	House	CT 5806/353	11886
32 Belmore Terrace WOODVILLE PARK	House	CT 5811/831	11887

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
33 Belmore Terrace WOODVILLE PARK	House	CT 5436/488	11888
34 Belmore Terrace WOODVILLE PARK	House	CT 5444/504	11889
35 Belmore Terrace WOODVILLE PARK	House	CT 5803/966	11890
36 Belmore Terrace WOODVILLE PARK	House	CT 5583/489	11891
37 Belmore Terrace WOODVILLE PARK	House	CT 5841/765	11892
38 Belmore Terrace WOODVILLE PARK	House	CT 5804/869	11893
39 Belmore Terrace WOODVILLE PARK	House	CT 5193/523	11894
40 Belmore Terrace WOODVILLE PARK	House	CT 5788/846	11895
41 Belmore Terrace WOODVILLE PARK	House	CT 5265/683	11896
42 Belmore Terrace WOODVILLE PARK	House	CT 5819/64	11897
43 Belmore Terrace WOODVILLE PARK	House	CT 5719/373	11898
44 Belmore Terrace WOODVILLE PARK	House	CT 5786/551	11899
1/ 45 Belmore Terrace WOODVILLE PARK	House	CT 5717/158	11900
46 Belmore Terrace WOODVILLE PARK	Maisonette	CT 5349/494	11901
47 Belmore Terrace WOODVILLE PARK	Maisonette	CT 5351/205	11902
48 Belmore Terrace WOODVILLE PARK	House	CT 5232/79	11903
49 Belmore Terrace WOODVILLE PARK	House	CT 5075/785	11904
50 Belmore Terrace WOODVILLE PARK	House	CT 5780/523	11905
52 Belmore Terrace WOODVILLE PARK	House	CT 1281/162	11906
53 Belmore Terrace WOODVILLE PARK	House	CT 5314/31	11907
21 Harvey Street East WOODVILLE PARK	House	CT 5798/6	12107
23 Harvey Street East WOODVILLE PARK	House	CT 5817/87	12108
25 Harvey Street East WOODVILLE PARK	House	CT 5547/169	12109
29 Harvey Street East WOODVILLE PARK	House	CT 1134/85	12110
31 Harvey Street East WOODVILLE PARK	Maisonette	CT 5537/263	12111
34 Harvey Street East WOODVILLE PARK	House	CT 5697/22	12112
35 Harvey Street East WOODVILLE PARK	House	CT 5748/674	11908
36 Harvey Street East WOODVILLE PARK	House	CT 5698/765	12113
37 Harvey Street East WOODVILLE PARK	House	CT 5212/117	11909
38 Harvey Street East WOODVILLE PARK	House	CT 5229/960	12114
39 Harvey Street East WOODVILLE PARK	House	CT 5800/625	11910
40 Harvey Street East WOODVILLE PARK	House	CT 5227/396	12115
41 Harvey Street East WOODVILLE PARK	House	CT 5722/666	11911
42 Harvey Street East WOODVILLE PARK	House	CT 5816/808	12116
43 Harvey Street East WOODVILLE PARK	House	CT 5227/324 CT 5227/447	11912
44 Harvey Street East WOODVILLE PARK	House	CT 5176/792	12117
45 Harvey Street East WOODVILLE PARK	House	CT 5817/421	11913

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
47 Harvey Street East WOODVILLE PARK	House	CT 5817/26	11914
48 Harvey Street East WOODVILLE PARK	House	CT 5724/947	11915
49 Harvey Street East WOODVILLE PARK	House	CT 5534/768	11916
51 Harvey Street East WOODVILLE PARK	House	CT 5191/39	11917
53 Harvey Street East WOODVILLE PARK	House	CT 5786/160	11918
55 Harvey Street East WOODVILLE PARK	House	CT 5184/358	11920
56 Harvey Street East WOODVILLE PARK	House	CT 5702/44	11921
57 Harvey Street East WOODVILLE PARK	House	CT 5240/514	11922
58 Harvey Street East WOODVILLE PARK	House	CT 5415/277	11923
59 Harvey Street East WOODVILLE PARK	House	CT 5301/531	11924
60 Harvey Street East WOODVILLE PARK	House	CT 5365/969	11925
61 Harvey Street East WOODVILLE PARK	House	CT 5831/192	11926
62 Harvey Street East WOODVILLE PARK	House	CT 5258/736	11927
63 Harvey Street East WOODVILLE PARK	House	CT 5818/767	11928
64 Harvey Street East WOODVILLE PARK	House	CT 5159/791	11929
65 Harvey Street East WOODVILLE PARK	House	CT 5782/927	11930
66 Harvey Street East WOODVILLE PARK	House	CT 5839/131	11931
67 Harvey Street East WOODVILLE PARK	House	CT 5818/416	11932
69 Harvey Street East WOODVILLE PARK	House	CT 5283/202	11934
70 Harvey Street East WOODVILLE PARK	House	CT 5802/62	11935
71 Harvey Street East WOODVILLE PARK	House	CT 5807/991	11936
72 Harvey Street East WOODVILLE PARK	House	CT 5802/63	11937
73 Harvey Street East WOODVILLE PARK	House	CT 5699/163	11938
74 Harvey Street East WOODVILLE PARK	House	CT 5229/958	11939
77 Harvey Street East WOODVILLE PARK	House	CT 5479/462	11940
78 Harvey Street East WOODVILLE PARK	House	CT 5839/307	11941
80 Harvey Street East WOODVILLE PARK	House	CT 5493/1	11942
82 Harvey Street East WOODVILLE PARK	House	CT 5342/243	11943
86 Harvey Street East WOODVILLE PARK	House	CT 5782/853	11944
88 Harvey Street East WOODVILLE PARK	House	CT 5844/415 CT 925/161	11945
90 Harvey Street East WOODVILLE PARK	House	CT 5820/125	11946
20 Marlborough Avenue WOODVILLE PARK	House	CT 5532/294	11947
22 Marlborough Avenue WOODVILLE PARK	House	CT 5457/407	11948
23 Marlborough Avenue WOODVILLE PARK	House	CT 5180/306	11949
24 Marlborough Avenue WOODVILLE PARK	House	CT 5374/314	11950
25 Marlborough Avenue WOODVILLE PARK	House	CT 5471/71	11951
26 Marlborough Avenue WOODVILLE PARK	House	CT 5136/648	11952

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
27 Marlborough Avenue WOODVILLE PARK	House	CT 5588/372	11953
28 Marlborough Avenue WOODVILLE PARK	House	CT 5793/470	11954
29 Marlborough Avenue WOODVILLE PARK	House	CT 5348/363	11955
31 Marlborough Avenue WOODVILLE PARK	House	CT 5504/362	11956
32 Marlborough Avenue WOODVILLE PARK	House	CT 5516/823 CT 5516/961	11957
1 Oxford Street WOODVILLE PARK	House	CT 5837/235	11958
2 Oxford Street WOODVILLE PARK	House	CT 5465/698	11959
3 Oxford Street WOODVILLE PARK	House	CT 5793/287	11960
4 Oxford Street WOODVILLE PARK	House	CT 5818/456	11961
5 Oxford Street WOODVILLE PARK	House	CT 5303/38	11962
6 Oxford Street WOODVILLE PARK	House	CT 5780/33	11963
7 Oxford Street WOODVILLE PARK	House	CT 5568/104	11964
9 Oxford Street WOODVILLE PARK	House	CT 5529/389	11965
22 Park Street South WOODVILLE PARK	House	CT 5406/536	12178
24 Park Street South WOODVILLE PARK	House	CT 5225/165	12179
26 Park Street South WOODVILLE PARK	House	CT 5298/817	12180
28 Park Street South WOODVILLE PARK	House	CT 5344/637	12181
30 Park Street South WOODVILLE PARK	House	CT 5239/250	12182
32 Park Street South WOODVILLE PARK	House	CT 5322/291	12183
29 Russell Terrace WOODVILLE PARK	House	CT 5290/882	12201
30 Russell Terrace WOODVILLE PARK	House	CT 5402/301	12202
31 Russell Terrace WOODVILLE PARK	House	CT 5501/379	11966
33 Russell Terrace WOODVILLE PARK	House	CT 5747/422	11967
34 Russell Terrace WOODVILLE PARK	House	CT 5366/972	11968
35 Russell Terrace WOODVILLE PARK	House	CT 5817/25	11969
37 Russell Terrace WOODVILLE PARK	House	CT 1129/118	11971
39 Russell Terrace WOODVILLE PARK	House	CT 5223/836	11972
40 Russell Terrace WOODVILLE PARK	House	CT 5662/561	11973
41 Russell Terrace WOODVILLE PARK	House	CT 5641/458	11974
42 Russell Terrace WOODVILLE PARK	House	CT 5272/110	11975
43 Russell Terrace WOODVILLE PARK	House	CT 5783/819	11976
44 Russell Terrace WOODVILLE PARK	House	CT 5085/928	11977
45 Russell Terrace WOODVILLE PARK	House	CT 5306/362	11978
46 Russell Terrace WOODVILLE PARK	House	CT 5793/264	11979
46a Russell Terrace WOODVILLE PARK	House	CT 5785/616	11980
47 Russell Terrace WOODVILLE PARK	House	CT 5702/68	11981
48 Russell Terrace WOODVILLE PARK	House	CT 5470/483	11982

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Property Address	Description of Contributory item	Certificate of Title	Heritage NR
49 Russell Terrace WOODVILLE PARK	House	CT 5265/775	11983
50 Russell Terrace WOODVILLE PARK	House	CT 5295/24	11984
51 Russell Terrace WOODVILLE PARK	House	CT 5792/277	11985
52 Russell Terrace WOODVILLE PARK	House	CT 5345/977	11986
53 Russell Terrace WOODVILLE PARK	House	CT 5333/494	11987
54 Russell Terrace WOODVILLE PARK	House	CT 5702/67	11988
55 Russell Terrace WOODVILLE PARK	House	CT 5096/606	11990
56 Russell Terrace WOODVILLE PARK	House	CT 5787/235	11991
35 Stanley Street WOODVILLE PARK	House	CT 5253/160	12236
38 Stanley Street WOODVILLE PARK	House	CT 5292/400	12238
39 Stanley Street WOODVILLE PARK	House	CT 5480/369	12239
40 Stanley Street WOODVILLE PARK	House	CT 5521/539	12240
41 Stanley Street WOODVILLE PARK	House	CT 5801/241	12241
43 Stanley Street WOODVILLE PARK	House	CT 5753/602	12242
45 Stanley Street WOODVILLE PARK	House	CT 5192/12	12243
46 Stanley Street WOODVILLE PARK	House	CT 5490/225	12244
47 Stanley Street WOODVILLE PARK	House	CT 5238/883	11992
49 Stanley Street WOODVILLE PARK	House	CT 5786/604	11993
50 Stanley Street WOODVILLE PARK	House	CT 5367/713	11994
51 Stanley Street WOODVILLE PARK	House	CT 5797/400	11995
52 Stanley Street WOODVILLE PARK	House	CT 5336/810	11996
53 Stanley Street WOODVILLE PARK	House	CT 5785/326	11997
54 Stanley Street WOODVILLE PARK	House	CT 5366/176	11998
55 Stanley Street WOODVILLE PARK	House	CT 5793/492	11999
56 Stanley Street WOODVILLE PARK	House	CT 5526/675	12000
57 Stanley Street WOODVILLE PARK	House	CT 5795/857	12001
58 Stanley Street WOODVILLE PARK	House	CT 5802/671	12002
59 Stanley Street WOODVILLE PARK	House	CT 5664/724	12003
61 Stanley Street WOODVILLE PARK	House	CT 5793/339	12004
62 Stanley Street WOODVILLE PARK	House	CT 5363/682	12005
63 Stanley Street WOODVILLE PARK	House	CT 5479/725	12006
64 Stanley Street WOODVILLE PARK	House	CT 5354/729	12007
65 Stanley Street WOODVILLE PARK	House	CT 5784/364	12008
66 Stanley Street WOODVILLE PARK	House	CT 5790/831	12009
67 Stanley Street WOODVILLE PARK	House	CT 5823/436	12010
68 Stanley Street WOODVILLE PARK	House	CT 5235/524	12011
69 Stanley Street WOODVILLE PARK	House	CT 4235/701	12012
70 Stanley Street WOODVILLE PARK	House	CT 5748/391	12013

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
71 Stanley Street WOODVILLE PARK	House	CT 5798/5	12014
74 Stanley Street WOODVILLE PARK	House	CT 5271/595	12015
76 Stanley Street WOODVILLE PARK	House	CT 5824/326	12016
78 Stanley Street WOODVILLE PARK	House	CT 5799/661	12017
39 Windsor Avenue WOODVILLE PARK	House	CT 5566/787	12018
41 Windsor Avenue WOODVILLE PARK	House	CT 5844/80	12019
42 Windsor Avenue WOODVILLE PARK	House	CT 1243/139	12020
43 Windsor Avenue WOODVILLE PARK	House	CT 5184/830	12021
45 Windsor Avenue WOODVILLE PARK	Maisonette	CT 3207/189	12022
45a Windsor Avenue WOODVILLE PARK	Maisonette	CT 5417/663	12023
47 Windsor Avenue WOODVILLE PARK	House	CT 5433/306	12024
48 Windsor Avenue WOODVILLE PARK	House	CT 5788/298	12025
50 Windsor Avenue WOODVILLE PARK	House	CT 5655/836	12026
51 Windsor Avenue WOODVILLE PARK	House	CT 5140/49	12027
52 Windsor Avenue WOODVILLE PARK	House	CT 5817/898	12028
53 Windsor Avenue WOODVILLE PARK	House	CT 5296/170	12029
54 Windsor Avenue WOODVILLE PARK	House	CT 913/171	12030
55 Windsor Avenue WOODVILLE PARK	House	CT 5676/398	12031
56 Windsor Avenue WOODVILLE PARK	House	CT 5218/599	12032
57 Windsor Avenue WOODVILLE PARK	House	CT 5534/339	12033
58 Windsor Avenue WOODVILLE PARK	House	CT 5665/152	12034
59 Windsor Avenue WOODVILLE PARK	House	CT 5794/507	12035
60 Windsor Avenue WOODVILLE PARK	House	CT 5384/811	12036
61 Windsor Avenue WOODVILLE PARK	House	CT 5484/339	12037
62 Windsor Avenue WOODVILLE PARK	House	CT 5800/430	12038
63 Windsor Avenue WOODVILLE PARK	House	CT 5570/99	12039
64 Windsor Avenue WOODVILLE PARK	House	CT 5786/972	12040
65 Windsor Avenue WOODVILLE PARK	House	CT 5818/443	12041
66 Windsor Avenue WOODVILLE PARK	House	CT 5833/786	12042
67 Windsor Avenue WOODVILLE PARK	House	CT 5155/112	12043
68 Windsor Avenue WOODVILLE PARK	House	CT 1183/42	12044
69 Windsor Avenue WOODVILLE PARK	House	CT 5837/682	12045
70 Windsor Avenue WOODVILLE PARK	House	CT 5185/373	12046
71 Windsor Avenue WOODVILLE PARK	House	CT 5804/392	12047
72 Windsor Avenue WOODVILLE PARK	House	CT 5817/396	12048
74 Windsor Avenue WOODVILLE PARK	House	CT 1289/129	12049
76 Windsor Avenue WOODVILLE PARK	House	CT 5122/81	12050
78 Windsor Avenue WOODVILLE PARK	House	CT 5759/356	12051

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80 Windsor Avenue WOODVILLE PARK	House	CT 5760/131	12052
82 Windsor Avenue WOODVILLE PARK	House	CT 5240/811	12053
84 Windsor Avenue WOODVILLE PARK	House	CT 5274/57	12054
86 Windsor Avenue WOODVILLE PARK	House	CT 5216/870	12055
1/ 88 Windsor Avenue WOODVILLE PARK	Single Storey Flats	CT 5785/359	12056
1 Angus Street WOODVILLE SOUTH	House	CT 5306/246	11558
2 Angus Street WOODVILLE SOUTH	House	CT 5599/190	11559
3 Angus Street WOODVILLE SOUTH	House	CT 5810/915	11560
4 Angus Street WOODVILLE SOUTH	House	CT 5401/152	11561
5 Angus Street WOODVILLE SOUTH	House	CT 5491/976	11562
6 Angus Street WOODVILLE SOUTH	House	CT 5810/916	11563
7 Angus Street WOODVILLE SOUTH	House	CT 5202/951	11564
8 Angus Street WOODVILLE SOUTH	House	CT 5602/915	11565
9 Angus Street WOODVILLE SOUTH	House	CT 5419/82	11566
10 Angus Street WOODVILLE SOUTH	House	CT 5533/929	11567
11 Angus Street WOODVILLE SOUTH	House	CT 5150/774	11568
12 Angus Street WOODVILLE SOUTH	House	CT 5220/782	11569
13 Angus Street WOODVILLE SOUTH	House	CT 5220/124	11570
14 Angus Street WOODVILLE SOUTH	House	CT 5191/482	11571
16 Angus Street WOODVILLE SOUTH	House	CT 5215/411	11572
18 Angus Street WOODVILLE SOUTH	House	CT 5252/234	11573
1 Cedar Avenue WOODVILLE SOUTH	House	CT 5190/777 CT 5715/368	11574
3 Cedar Avenue WOODVILLE SOUTH	House	CT 5816/295	11575
4 Cedar Avenue WOODVILLE SOUTH	House	CT 5795/225	11576
5 Cedar Avenue WOODVILLE SOUTH	House	CT 5358/741	11577
6 Cedar Avenue WOODVILLE SOUTH	House	CT 5530/17	11578
7 Cedar Avenue WOODVILLE SOUTH	House	CT 5758/443	11579
8 Cedar Avenue WOODVILLE SOUTH	House	CT 5175/195	11580
9 Cedar Avenue WOODVILLE SOUTH	House	CT 5758/442	11581
10 Cedar Avenue WOODVILLE SOUTH	House	CT 5484/144	11582
11 Cedar Avenue WOODVILLE SOUTH	House	CT 5745/261	11583
12 Cedar Avenue WOODVILLE SOUTH	House	CT 5180/414	11584
13 Cedar Avenue WOODVILLE SOUTH	House	CT 5535/553	11585
14 Cedar Avenue WOODVILLE SOUTH	Home Unit	CT 5571/172	11586
14a Cedar Avenue WOODVILLE SOUTH	Home Unit	CT 5571/171	11587
15 Cedar Avenue WOODVILLE SOUTH	House		11588
18 Cedar Avenue WOODVILLE SOUTH	Maisonette	CT 5130/848	11589

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
20 Cedar Avenue WOODVILLE SOUTH	Maisonette	CT 5130/847	11590
22 Cedar Avenue WOODVILLE SOUTH	Masionette	CT 5852/993	11591
24 Cedar Avenue WOODVILLE SOUTH	Maisonette	CT 2492/140	11592
26 Cedar Avenue WOODVILLE SOUTH	House	CT 5660/714	11593
28 Cedar Avenue WOODVILLE SOUTH	House	CT 5743/127	11594
30 Cedar Avenue WOODVILLE SOUTH	House	CT 5816/764	11595
1 Glen Eira Street WOODVILLE SOUTH	House	CT 5564/120	11596
2 Glen Eira Street WOODVILLE SOUTH	House	CT 5330/528	11597
3 Glen Eira Street WOODVILLE SOUTH	House	CT 5380/291	11598
4 Glen Eira Street WOODVILLE SOUTH	House	CT 5738/76	11599
6 Glen Eira Street WOODVILLE SOUTH	House	CT 5738/991	11600
7 Glen Eira Street WOODVILLE SOUTH	House	CT 5741/982	11601
8 Glen Eira Street WOODVILLE SOUTH	House	CT 5256/162	11602
10 Glen Eira Street WOODVILLE SOUTH	House	CT 5068/62	11603
12 Glen Eira Street WOODVILLE SOUTH	House	CT 5727/858	11604
14 Glen Eira Street WOODVILLE SOUTH	House	CT 5313/10	11605
16 Glen Eira Street WOODVILLE SOUTH	House	CT 5514/273	11606
1 Glen Rowan Road WOODVILLE SOUTH	House	CT 5072/388	11607
2 Glen Rowan Road WOODVILLE SOUTH	House	CT 5747/472	11609
2a Glen Rowan Road WOODVILLE SOUTH	House	CT 5083/622	11608
3 Glen Rowan Road WOODVILLE SOUTH	House	CT 5815/142	11610
4 Glen Rowan Road WOODVILLE SOUTH	House	CT 5738/903	11611
5 Glen Rowan Road WOODVILLE SOUTH	House	CT 5345/982	11612
6 Glen Rowan Road WOODVILLE SOUTH	House	CT 5805/899	11613
7 Glen Rowan Road WOODVILLE SOUTH	House	CT 5425/443	11614
8 Glen Rowan Road WOODVILLE SOUTH	House	CT 5178/715	11615
9 Glen Rowan Road WOODVILLE SOUTH	House	CT 5252/766	11616
10 Glen Rowan Road WOODVILLE SOUTH	House	CT 5521/609	11617
11 Glen Rowan Road WOODVILLE SOUTH	House	CT 5823/968	11618
12 Glen Rowan Road WOODVILLE SOUTH	House	CT 5515/579	11619
13 Glen Rowan Road WOODVILLE SOUTH	House	CT 5377/70	11620
14 Glen Rowan Road WOODVILLE SOUTH	House	CT 5680/582	11621
15 Glen Rowan Road WOODVILLE SOUTH	House	CT 5820/326	11622
16 Glen Rowan Road WOODVILLE SOUTH	House	CT 5720/915	11623
1 Glengarry Street WOODVILLE SOUTH	House	CT 5301/189	11624
2 Glengarry Street WOODVILLE SOUTH	House	CT 5753/562	11625
3 Glengarry Street WOODVILLE SOUTH	House	CT 5754/681	11626
4 Glengarry Street WOODVILLE SOUTH	House	CT 5818/750	11627

Charles Sturt Council
Table Section
Table ChSt/5 - Contributory Items

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
5 Glengarry Street WOODVILLE SOUTH	House	CT 5328/349	11628
6 Glengarry Street WOODVILLE SOUTH	House	CT 5820/327	11629
7 Glengarry Street WOODVILLE SOUTH	House	CT 5497/936	11630
8 Glengarry Street WOODVILLE SOUTH	House	CT 5273/547	11631
9 Glengarry Street WOODVILLE SOUTH	House	CT 5748/423	11632
10 Glengarry Street WOODVILLE SOUTH	House	CT 5625/596	11633
11 Glengarry Street WOODVILLE SOUTH	House	CT 5820/328	11634
12 Glengarry Street WOODVILLE SOUTH	House	CT 5745/11	11635
13 Glengarry Street WOODVILLE SOUTH	House	CT 5203/500	11636
14 Glengarry Street WOODVILLE SOUTH	House	CT 5163/552	11637
15 Glengarry Street WOODVILLE SOUTH	House	CT 5742/969	11638
16 Glengarry Street WOODVILLE SOUTH	House	CT 5074/607	11639
17 Glengarry Street WOODVILLE SOUTH	House	CT 5264/638	11640
18 Glengarry Street WOODVILLE SOUTH	House	CT 5741/641	11641
19 Glengarry Street WOODVILLE SOUTH	House	CT 5741/400	11642
20 Glengarry Street WOODVILLE SOUTH	House	CT 5821/163	11643
21 Glengarry Street WOODVILLE SOUTH	House	CT 5726/760	11644
22 Glengarry Street WOODVILLE SOUTH	House	CT 5432/714	11645
23 Glengarry Street WOODVILLE SOUTH	House	CT 5741/169	11646
24 Glengarry Street WOODVILLE SOUTH	House	CT 5632/201	11647
25 Glengarry Street WOODVILLE SOUTH	House	CT 5420/785	11648
26 Glengarry Street WOODVILLE SOUTH	House	CT 5154/555	11649
27 Glengarry Street WOODVILLE SOUTH	House	CT 5349/523	11650
28 Glengarry Street WOODVILLE SOUTH	House	CT 5726/396	11651
1 Glenhuntley Street WOODVILLE SOUTH	House	CT 5328/367	11652
2 Glenhuntley Street WOODVILLE SOUTH	House	CT 5541/321	11653
3 Glenhuntley Street WOODVILLE SOUTH	House	CT 5734/251	11654
4 Glenhuntley Street WOODVILLE SOUTH	House	CT 5368/688	11655
5 Glenhuntley Street WOODVILLE SOUTH	House	CT 5239/285	11656
6 Glenhuntley Street WOODVILLE SOUTH	House	CT 5751/511	11657
7 Glenhuntley Street WOODVILLE SOUTH	House	CT 5507/714	11658
8 Glenhuntley Street WOODVILLE SOUTH	House	CT 5564/648	11659
9 Glenhuntley Street WOODVILLE SOUTH	House	CT 5755/233	11660
10 Glenhuntley Street WOODVILLE SOUTH	House	CT 5555/155	11661
11 Glenhuntley Street WOODVILLE SOUTH	House	CT 5752/349	11662
12 Glenhuntley Street WOODVILLE SOUTH	House	CT 5749/449	11663
13 Glenhuntley Street WOODVILLE SOUTH	House	CT 5213/931	11664
15 Glenhuntley Street WOODVILLE SOUTH	House	CT 5741/988	11665

Property Address	Description of Contributory item	Certificate of Title	Heritage NR
16 Glenhuntley Street WOODVILLE SOUTH	House	CT 5749/451	11666
17 Glenhuntley Street WOODVILLE SOUTH	House	CT 5723/637	11667
20 Glenhuntley Street WOODVILLE SOUTH	House	CT 5433/245	11668
22 Glenhuntley Street WOODVILLE SOUTH	House	CT 5641/633	11669
23 Glenhuntley Street WOODVILLE SOUTH	House	CT 5242/766	11670
24 Glenhuntley Street WOODVILLE SOUTH	House	CT 5573/848	11671
25 Glenhuntley Street WOODVILLE SOUTH	House	CT 5514/332	11672
4 Oval Avenue WOODVILLE SOUTH	House	CT 5731/406	11673
6 Oval Avenue WOODVILLE SOUTH	House	CT 5692/806	11674
7 Oval Avenue WOODVILLE SOUTH	House	CT 5429/937	11675
10 Oval Avenue WOODVILLE SOUTH	House	CT 5740/676	11676
12 Oval Avenue WOODVILLE SOUTH	House	CT 5334/50	11677
14 Oval Avenue WOODVILLE SOUTH	House	CT 5360/774	11678
16 Oval Avenue WOODVILLE SOUTH	House	CT 5844/29	11679
18 Oval Avenue WOODVILLE SOUTH	House	CT 5844/30	11680
20 Oval Avenue WOODVILLE SOUTH	House	CT 5083/623	11681
22 Oval Avenue WOODVILLE SOUTH	House	CT 5170/542	11682
24 Oval Avenue WOODVILLE SOUTH	House	CT 5742/14	11683
26 Oval Avenue WOODVILLE SOUTH	House	CT 5391/121	11684
28 Oval Avenue WOODVILLE SOUTH	House	CT 5310/184	11685
30 Oval Avenue WOODVILLE SOUTH	House	CT 5105/560	11686
36 Oval Avenue WOODVILLE SOUTH	House	CT 5733/915	11687
38 Oval Avenue WOODVILLE SOUTH	House	CT 5730/758	11688
40 Oval Avenue WOODVILLE SOUTH	House	CT 5732/320	11689
42 Oval Avenue WOODVILLE SOUTH	House	CT 5731/952	11690
44 Oval Avenue WOODVILLE SOUTH	House	CT 5578/224	11691
46 Oval Avenue WOODVILLE SOUTH	House	CT 5740/184	11692
46b Oval Avenue WOODVILLE SOUTH	House	CT 5741/585	11693
48 Oval Avenue WOODVILLE SOUTH	House	CT 5738/921	11694

Table ChSt/6 - Local Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
7a Barham Street ALLENBY GARDENS	Former Allenby Gardens Uniting Church; Church under main intersecting CGI gable roof, fleche and front additions.	A11	D34338	CT 5352/774	a c	27616
33 Barham Street ALLENBY GARDENS	Allenby Gardens Primary School; Original 1926 two storey red brick school building under CGI hipped form roof, including stucco faced porches.	A195	D2918	CT 5843/126	a c d	27615
69 Coombe Road ALLENBY GARDENS	Dwelling; Whole of residence under tile roof, including carport. Excludes later tile roof additions to the rear.	A265	D2918	CT 5634/370	d	27617
53 Lewanick Street ALLENBY GARDENS	Dwelling; Whole of dwelling under tile roof, brick boundary fence to two street boundaries.	A122	D2918	CT 5448/902	d	27618
Howards Road BEVERLEY	Cemetery; All headstones, plaques and monuments, including existing open space setting and Pepper Tree (Schirius Molle)			CT 4096/947	c	9243
6 Jeanes Street BEVERLEY	Former York Church of Christ; Church building including all original features; include two brick stretcher bond gable structures on both sides and church building interior; exclude rear additions and fencing.			CT 4091/46	c	9147
140 Drayton Street BOWDEN	Dwellings; Former Factory Building and Rear Outbuilding Facing Fifth Street.	A70	F31153	CT 5092/866	a d	20791

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
46 Gibson Street BOWDEN	Former Shop and Attached House; External form, materials and detailing of the original shop and attached house, including the original shop front and verandah; exclude all other elements, including rear lean-to			CT 5240/536	a b d	9238
54 Gibson Street BOWDEN	Shop and Attached House; Shop, house, outbuildings and verandah with all original features and boundary walls to Second Street; exclude interior			CT 2893/108	b d	9237
78 Gibson Street BOWDEN	Former Shop and Attached House; Former shop, house and verandahs with all original features; exclude interior and fencing			CT 5232/578	b d	9246
102 Gibson Street BOWDEN	Former House and Shop; Excluding outbuildings and fencing.	A27 A28	F125487 F125487	CT 5232/474 CT 5232/474	a d	20792
103 Gibson Street BOWDEN	Dwelling; Former Shop and House, including Hawker Street Masonry Extension, excluding all other buildings on site.	A523	F211309	CT 5613/409	a d	20793
2 Ninth Street BOWDEN	House; Original external form and materials of the cottage; exclude later additions and alterations			CT 5131/591	a b d	9140
15 Ninth Street BOWDEN	House; Including Rear Lean-To, excluding fence and outbuildings.	A59	F121906	CT 5787/933	a d	20795
22 Ninth Street BOWDEN	House; Includes the two room cottage only.	A52	F121899	CT 5544/405	a d	20796
28 Ninth Street BOWDEN	Semi-Detached Dwelling (Attached House); Includes house but excludes masonry front fence, verandah and outbuildings.	A49	F121896	CT 5876/82	a d	26342

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
30 Ninth Street BOWDEN	Semi-Detached Dwelling (Attached House); Includes house but excludes masonry front fence, verandah and outbuildings.	A48	F121895	CT 5451/129	a d	26343
18 Park Terrace BOWDEN	Park View Hotel; Original form, materials and detailing of the hotel, including face red brick and verandah; exclude later additions.			CT 5340/311	a c d	9239
4-14 Trembath Street BOWDEN	Row Houses; External form and original materials of the row houses; exclude front fence, other fences and rear additions.			CT 5340/295 CT 5340/296 CT 5340/297 CT 5340/298 CT 5340/299 CT 5340/300	a b d	9138
34-36 Trembath Street BOWDEN	Attached Houses; Original external form, materials and detailing of the attached houses, Front fence of 34 Trembath Street; exclude later additions			CT 1165/150	a b d e	9139
Chief Street BROMPTON	Railway Underpass					9167
72 Chief Street BROMPTON	House; Excludes all fully detached outbuildings and rendered modern fence along the Chief Street property boundary and the eastern side masonry wall.	A73	F122020	CT 5720/420	a b d e	20798
110 Coglein Street BROMPTON	Excelsior Hotel; Two storey portion of hotel with balcony verandah structure facing Coglein Street. Excludes single storey portions.	A17	F103318	CT 6149/777	a b c d	27621
87-89 East Street BROMPTON	Attached Cottages; External form, material and details of the original cottages, including projecting party wall with plaster embellishment; exclude carport and later rear additions			CT 579/94	a d	9141

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
87 Torrens Road BROMPTON	Dwelling; Two-storey residential building including masonry elevations, original doors, windows, balcony and original features. Excludes front fence, altered wing to the west, later additions and shop front on Torrens Road.	A3	D3872	CT 5305/788	a	27623
17A West Street BROMPTON	Former Catholic Church; Original church structure and all original features; exclude fences and tin shed on the eastern side			CT 106/8	a b c d e f	9166
28 West Street BROMPTON	House; Original external form, materials and details including the walls, verandah form and roof form, excluding later additions.	A204	D31450	CT 5136/480	a e	25684
38 West Street BROMPTON	Semi-detached dwelling; Whole of dwelling, including chimneys, under gambrel CGI roof and bullnose verandah to front. Excludes rear lean-to additions.	A208	D31450	CT 5136/485	d	27624
40 West Street BROMPTON	Semi-detached dwelling; Whole of dwelling, including chimneys, under gambrel CGI roof and bullnose verandah to front. Excludes rear lean-to additions.	A209	D31450	CT 5136/485	d	27625
68-70 West Street BROMPTON	Row Dwellings (Attached Houses); Original house only, excluding all modern additions.	A3	D27452	CT 5445/376	a d e	20802
72-78 West Street BROMPTON	Row Dwellings (Attached Houses); Original form and fabric of the façade, side elevations, roof and chimneys as viewed from West Street and Third Street, excluding non-original fabric of verandah and rear additions.	A2	D27452	CT 5387/597	a d e	26218

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Table ChSt/6 - Local Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
9 Colston Street CHELTENHAM	Former shop and attached dwelling; Whole of shop and dwelling under CGI gambrel roof, including bullnose verandahs to dwelling and shop.	A1001	F11545	CT 6125/773	b	27626
6 Durham Terrace CHELTENHAM	House; House, outbuildings, verandah, front setting; exclude interior and front fence, pillars and gates.			CT 5089/478	d	9181
Port Road Section A, Drive B, Path 2(3), Site 43 CHELTENHAM	Grave of John Alexander Walker, Cheltenham Cemetery; Including grave headstone or monument only and any surviving stone surrounds.	30-63	F210066	CT 5961/23	e	20806
Port Road Section D, Drive B, Path 2(13) Site 49, Sites 50,51,52,53 & 54 CHELTENHAM	Grave of Adelaide Miethke, Cheltenham Cemetery; Extends to the grave stone only.	30-63	F210066	CT 5961/23	e	20807
Port Road Section C, Drive A, Path 4(15) Site 22 CHELTENHAM	Grave of Isaac Dewson, Cheltenham Cemetery; Including the grave headstone only and surviving stone surrounds.	30-63	F210066	CT 5961/23	e	20808
Port Road Section H, Drive B, Path 5(38), Sites 13,14&15 CHELTENHAM	Grave of Reverend Joseph Coles Kirby, Cheltenham Cemetery; Including pedestal monument.	30-63	F210066	CT 5961/23	e	20805
Port Road Section B, Drive A, Path 10(11), Site 326S, 326C, 326N CHELTENHAM	Grave of Firemen, Cheltenham Cemetery; Including statue and the tiled and marble grave components.	30-63	F210066	CT 5961/23	d e	20804
Port Road Section D, Drive B, Path 8(19), Sites 238S, 238N, 239S, 239N CHELTENHAM	Grave of David Bower, Cheltenham Cemetery; Including altar monument and surrounding cast iron balustrade.	30-63	F210066	CT 5961/23	d e	20803

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
Port Road Section M, Drive C, Path 9(31), Site 326S, Site 326C, Site 326N CHELTENHAM	Grave of John Carr, Cheltenham Cemetery; Including grave monument and any stone surrounds.	30-63	F210066	CT 5961/23	e	26220
Port Road Section I, Drive C, Path 3(4), Site 68 CHELTENHAM	Grave of Thomas Carr, Cheltenham Cemetery; Including grave monument and any stone surrounds.	30-63	F210066	CT 5961/23	e	26219
Port Road Section D, Drive B, Path 4(15), Sites 13, 14 &15 CHELTENHAM	Grave of Captain Patrick Weir, Cheltenham Cemetery; Including the double width grave site covered with horizontal grey granite tombstone and surrounds.	30-63	F210066	CT 5961/23	e	20868
Port Road Section B, Drive A, Path 6(7), Site 171S, 171N CHELTENHAM	Grave of Japanese Seamen, Toraichi Shirahma and Chuhichi Ikeyama, Cheltenham Cemetery; Includes the two memorials.	30-63	F210066	CT 5961/23	d e	20863
Port Road Section A, Drive B, Path 4(5), Site 8 section A, Drive B, Path 5(6) site 8 CHELTENHAM	Grave of John Barton Hack, Cheltenham Cemetery; Including the grave headstone only and surviving stone surrounds.	30-63	F210066	CT 5961/23	e	20864
Port Road Section H, Drive B, path 8(41), Sites 80S, 80C, 80N, 81S, 81C, 81Nm, 40AS, 40AC, 40AN, 41S, 41C 41N CHELTENHAM	Grave of Fletcher Family, Cheltenham Cemetery; Including grave headstones and any surviving stone surrounds.	30-63	F210066	CT 5961/23	e	20865
Port Road Section D, Drive B, Path 7(18), Sites 40,41&42 Section D Drive B Path 8(19) Sites 7,8&9 CHELTENHAM	Grave of Richard Honey, Cheltenham Cemetery; Including grave headstone, marble surround and cast iron fencing to the plot.	30-63	F210066	CT 5961/23	e	20866
Port Road Section C, Drive A, Path 5(16), Site 152S, Site 152C, Site 152N, Site 153S, Site 153C, Site 153N, Si CHELTENHAM	Grave of Richard Day, Cheltenham Cemetery; Including grave monument, plinth, stone surround and encircling cast iron fence and gate.	30-63	F210066	CT 5961/23	d e	20867

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
60A-62 Stroud Street North CHELTENHAM	Cheltenham Community Centre, former Cheltenham Congregational Church; Includes the former church building, excluding more recent additions.	A94 A2	F117206 D2031	CT 5566/96 CT 5804/347	a	20809
615 Torrens Road CHELTENHAM	Cheltenham Park Racecourse Entrance Gates and Fence (located on Cheltenham Parade, approximately 230 metres from the South Eastern corner of Cheltenham Parade and Torrens Road); Including gates, piers and masonry and associated cast iron fences that form the horse shoe shape.	A1011	D86910	CT 6080/517	a d f	20810
33 Princes Street CROYDON	House; Includes the house, earlier rear section and verandah, excluding timber fence.	A89	F115801	CT 5743/577	a d	20811
4a William Street CROYDON	St Barnabas Anglican Church and Hall; Church and porches under tiled roof; adjacent hall and entry porch under CGI gable roof. Excludes later additions and structures to rear.	A67 & A68	D1554	CT 5787/173	a d	27627
20 Crittenden Road FINDON	Clubrooms (RSL Memorial); Nissen' hut structure with rendered masonry front facade and sign; exclude fencing.			CT 2302/81	c d	9232
93 Crittenden Road FINDON	Methodist Church; Church building including 1922 rear extension; exclude hall, breezeway, asbestos outbuilding and fencing.			CT 5207/313	c	9148

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
373 Grange Road FINDON	Dwelling; Whole of original building, including fenestration, hood canopies and curved forms. Excludes non-original fabric including columns to verandah and carport.	A98	D3372	CT 5675/609	a d	27628
23 St Albyn's Street FINDON	House (St Albyn's); House with front and side verandahs, front setting and rear flat; exclude fencing.	A60	D3372	CT 5377/576	a b d e f	9149
Scott Avenue FLINDERS PARK	Jean Horan Memorial Kindergarten; Includes the whole site, in particular building with fibre cement sheet cladding and gabled corrugated iron roof.	A323	D3091	CT 5588/321	a c	20814
12 Westall Street FLINDERS PARK	Dwelling; Original building, including fenestration, trims and curved forms. Excludes non-original fabric, including the carport, later rear additions and front fence.	A428	D3400	CT 5721/469	a d	27629
1 Beach Street GRANGE	Shops, former Marrett's Store; Shops, former Marrett's Store; street facades, parapets and verandah			CT 5115/163	a c	9192
4-6 Beach Street GRANGE	Institute; Institute; facades and roof			CT 5784/976	a c	9193
5-11 Beach Street GRANGE	Grange Uniting Church; Grange Uniting Church; original brick and stone church building; exclude later additions and extensions.			CT 5522/781	a b c d	9194
47-49 Beach Street GRANGE	Grange Baptist Church; Grange Baptist Church; original 1881 stone church building (to depth of three window bays) and front porch; exclude rear hall, masonry additions and all other structures.			CT 3883/191	a b c d	9270

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Table ChSt/6 - Local Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
102 Beach Street GRANGE	House, excluding verandahs; House; facades, excluding verandah			CT 4154/920	a	9271
Esplanade GRANGE	Grange Jetty; Grange Jetty				a b e f	9260
451 Esplanade GRANGE	House; House; facades, roof, verandahs and wall; exclude corrugated iron buildings at rear.			CT 5309/154	b d e	9190
453 Esplanade GRANGE	House; House; 'Nautilus'; facades, verandah and roof; exclude later additions and alterations, brick garage and outbuildings.			CT 5603/772	a b d	9191
489-491 Esplanade GRANGE	Grange Hotel; Hotel, facades and balconies (2 storey building)			CT 3961/180	a c	9186
561 Esplanade GRANGE	House; "Jarrah Cottage" facades and verandah			CT 3558/33	d	9158
675-681 Grange Road GRANGE	Shops and House; Shops and House; (Shops and corner shop and house) street facades and verandah			CT 1558/25 CT 3801/85	c	9274
685 Grange Road GRANGE	Dwelling; Original external form and fabric of the building, including external walls, chimneys and verandah. Excludes later additions, fence and roof sheeting.	A200	D50013	CT 5592/339	a	27630
54-54A Jetty Road GRANGE	Former Grange Infant School; Grange Infant School; red brick building facing Jetty Street excluding "weatherboard" addition at rear. Stone and brick quoined buildings attached, excluding all brick and "weatherboard" flat roofed additions to rear of stone building.			CT 5585/73 CT 5585/74	a c	9189
Military Road GRANGE	Railway Siding; Concrete railway platform				a c	9188

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
270 Military Road GRANGE	St Agnes Church Hall; gabled brick building excluding additions			CT 504/118	a c	9273
280 Military Road GRANGE	Dwelling; Whole of dwelling under CGI gambrel hipped roof, including chimneys and bullnose return verandah. Excludes lean-to additions to rear.	A58	F42	CT 5147/74	a	27631
Circuit Drive cnr Phillips Crescent HENDON	Hendon Aerodrome Cairn; Includes stone and bronze plaque only.	A604	D20473	CT 5291/664	a e	20815
4 Circuit Drive HENDON	Former Hendon Ammunition Factory - Main Store; Whole of red brick building under hipped / gable form CGI roof.	A20	D27519	CT 5374/783	a b	27633
Esplanade HENLEY BEACH	Henley Jetty; Henley Jetty				a b e f	9259
157 Esplanade HENLEY BEACH	Henley Hotel; Henley Hotel; facades, parapet and balconies of the original hotel, including the two-storey section; exclude later infill and additions.			CT 5233/287	a b c d e f	9161
227 Esplanade HENLEY BEACH	Semi-detached dwelling; whole of dwelling under main hip and valley CGI roof, including wrap-around CGI verandah, cast iron and masonry front fence.	A19	F583	CT 5344/502	a	27642
229 Esplanade HENLEY BEACH	Semi-detached dwelling; Whole of dwelling under main hip and valley CGI roof, including wrap-around CGI verandah.	A18	F583	CT 5170/264	a	27643

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
231 Esplanade HENLEY BEACH	Former Doctor's Surgery; Single storey former surgery building facing Seaview Road only, including face brick walls and parapet decoration, CGI roof, cantilevered canopy over footpath, front door and window sash arrangement.	A17	F583	CT 5503/401	a	27644
255 Esplanade HENLEY BEACH	Former Kiosk; Restaurant, former Kiosk; facades and balconies			CT 4110/45	a b c d	9228
5/ 257 Esplanade HENLEY BEACH	Former Henley Beach Institute; Shop, former Henley Beach Institute; gabled brick building excluding additions			CT 1558/147	a b c	9227
289 Esplanade HENLEY BEACH	Dwelling; Whole of dwelling under CGI hip and valley roof, including front verandah.	A8	F288	CT 5131/536	a	27645
317 Esplanade HENLEY BEACH	'Windsor' Apartments; Two storey building under tiled hip form roof and rendered masonry front fence.	U1 U2 U3 U4 CP	S5242 S5242 S5242 S5242 S5242	CT 5016/89 CT 5016/90 CT 5016/91 CT 5016/92 CT 5016/93	a	27646
323 Esplanade HENLEY BEACH	Flats, Haighs Mansions; Includes original form and fabric as viewed from the Esplanade.	A200	D31903	CT 5280/561	a d	20816
385 Esplanade HENLEY BEACH	'Burnleigh' Apartments; Two storey red brick building under tiled, hip form roof and single storey red brick cottage facing Seaview Road.	U1 U2 U3 U4 U5 CP	S6460 S6460 S6460 S6460 S6460 S6460	CT 5013/705 CT 5013/706 CT 5013/707 CT 5013/708 CT 5013/709 CT 5013/710	a d	27648
188 Military Road HENLEY BEACH	Church and Hall; St Michael and All Angels Church and Hall; Brick church and hall buildings			CT 3062/83	a c	9183
206 Military Road HENLEY BEACH	Former Police Station; Veterinary Surgery, former Police Station; facades and verandah of main building excluding additions; brick cells at rear			CT 4110/796	a	9230

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
251 Military Road HENLEY BEACH	Dwelling; Original dwelling under terracotta tile gambrel hip roof, including verandahs and chimneys. Excludes later additions to rear and side.	A65 A66	F704 F704	CT 5464/998 CT 5823/781	a d	27649
311 Military Road HENLEY BEACH	Henley Guide Hall; Henley Guide Hall; facades			CT 1648/179	a c	9160
245A Seaview Road HENLEY BEACH	Shop and Dwelling; Two storey brick / stone building under CGI hipped roof, including French doors upstairs and re-entrant shopfront to ground floor. Excludes verandah.	A2	C21979	CT 5923/523	a b	27650
251-255 Seaview Road HENLEY BEACH	Shops; Shops; external facade			CT 2636/11 CT 4083/12 CT 4132/878 CT 4256/738 CT 4319/601		9229
257 Seaview Road Shop 2, HENLEY BEACH	Shop; Shop; facades, parapet and verandah			CT 1558/147	a b c	9226
410 Seaview Road HENLEY BEACH	Star of the Sea School; Two storey building under hipped, donut form roof, including bellcote.	A28 & A29	F252	CT 5476/344	a d f	27651
418 Seaview Road HENLEY BEACH	Former Our Lady of the Sacred Heart Church; Former Our Lady of Sacred Heart Church; brick building, excluding additions			CT 2324/188	a c	9159
8a South Street HENLEY BEACH	Hall; Main facades	A1	D51707	CT 5657/952	a d	12973
Swanwick Street HENLEY BEACH	Pump House; facade and roof detail			CT 4325/676	a	20681
188 Seaview Road HENLEY BEACH SOUTH	Dwelling; Whole of dwelling under terracotta tile roof, including open porch / verandahs and chimneys.	A60	F14992	CT 5459/573	a d	27652

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212 Seaview Road HENLEY BEACH SOUTH	Dwelling ; Whole of dwelling under terracotta tile roof, including verandahs. Excludes later additions.	A153	F14991	CT 5163/122	a d	27653
14 Adam Street HINDMARSH	Jolly Miller Hotel; Original hotel building including verandah and all original features and rear storeroom but exclude interior and later red brick additions, garage and fencing.			CT 5138/106 CT 5138/107 CT 5138/311 CT 5141/693	a f	9240
24 Adam Street HINDMARSH	Former WH Burford and Sons Factory; Original brick factory building and ll original features; exclude interior and verandah			CT 4280/167	a d	9241
34 Adam Street HINDMARSH	Former GH Michell and Sons Factory Complex; Two-storey frontage to Adam Street, font wall only; exclude all other structures.	A5 A92 A93	F1693 F120011 F120012	CT 5515/505 CT 5523/272 CT 5831/227	a	9242
3 Mary Street HINDMARSH	Free Serbian Orthodox Church; Church building and brick fence. Excludes attached hall and rooms.	A1	F15363	CT 5912/315	a b c d f	27655
35 Orsmond Street HINDMARSH	Dwelling, former Congregational Church Manse; Includes the former Manse, excluding the carport structure and reconstructed elements.	A2	D32979	CT 5963/112	a b c d	20818
37 Orsmond Street HINDMARSH	Former Congregational Church; Includes the former Church, excluding front fence and modern additions.	A1	D32979	CT 5963/111	a b c d f	20819
Port Road HINDMARSH	Caissons, former Hindmarsh Bridge; Including the four Caissons in their current location.	A50	D40657	CT 5812/420	a c d f	20820

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
118-120 Port Road HINDMARSH	Attached shops; Includes the original form and fabric of the façade, side elevations, roof and chimneys, excluding non-original verandah and rear additions.	A56	F120175	CT 5243/960	a b c d e	26221
122-124 Port Road HINDMARSH	Attached shops; Includes the original form and fabric of the façade, side elevations and roof, excluding non-original verandah and rear additions.	Part Lot 57	F120176	CT 5243/952	a d	25683
173 Port Road HINDMARSH	Former Convent; Includes the two storey building.	A220	F210196	CT 5727/596	a d e	20824
192 Port Road HINDMARSH	Former Bank building and masonry and cast iron fence to south side of facade	A4	D52380	CT 5789/107	a d	20825
252-258 Port Road HINDMARSH	Sacred Heart Church; Whole of church under main gable, cupola belltowers, including side chapels / porches and rear sacristy. Includes open space forward of church.	A3 A260	F120222 F211856	CT 5813/546 CT 5863/685	a b c d f	27656
252-258 Port Road HINDMARSH	Sacred Heart Church Priory; Whole of two storey building under terracotta tile hipped roof, including open arched colonnades around building.	A3 A260	F120222 F211856	CT 5813/546 CT 5863/685	a	27657
266 Port Road HINDMARSH	Former Hindmarsh Institute; Includes the single storey, red brick building.	A2	F120221	CT 5596/511	a c d	20826
348 Port Road HINDMARSH	Hope Inn Hotel; Original hotel structure and all original features including pavement lights, verandah and balcony; exclude the canopy, rear sheds and fencing			CT 5131/859	a b c d e	9216

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15 Richard Street HINDMARSH	Former Brewery; Includes the original brewery building, excluding all later additions.	A689	F211475	CT 5657/755	a b d	20827
Port Road reserve HINDMARSH	Former Mothers and Babies Health Centre; Listing extends to the building only.	A5805	H106100	CT 5753/909	a c e	20821
5 Riverway Place KIDMAN PARK	Kidman Park Stud (former Fulham Stud); House and verandah to front and side elevation including bay windows; exclude fencing, 1960's rear addition and 1960's bathroom addition under the side verandah.	11		CT 5317/116		9150
12 Arkaba Road KILKENNY	House, former St Edwards Anglican Church; Includes the former Church only.	A12	D67310	CT 5943/790	a d	20832
9-11 Bollen Street KILKENNY	Challa Gardens Infant School; Original school building; exclude fence			CT 2408/89 CT 4295/843	c	9248
60 David Terrace KILKENNY	Former Odeon Theatre; Includes the former cinema building only.	A1	F28051	CT 5322/323	a c f	20829
Humphries Terrace KILKENNY	Challa Gardens Primary School; Original two-storey school building; exclude car park and fencing			CT 2248/106 CT 4135/12	c	9249
Tarcowie Street North-west corner of Tarcowie Street and Wilpena Terrace KILKENNY	Ballet School, former Norman Memorial Congregational Church; Includes the former Church building and adjacent Hall, excluding non-original gates and pergola and non-original brick fence fronting Wilpena Terrace.	A30	F116842	CT 5827/928	a d	20831

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
Wilpena Terrace KILKENNY	Former Forwood Down Factory; Includes the earliest building on the site presumed to be the two storey brick building at the western end located close to the railway line, excluding all later buildings.	A20 A19	F116132 F116131	CT 5352/108 CT 5352/109	a b c d e f	20830
10 Wilpena Terrace KILKENNY	House; Includes the two storey building only.	A19	F116831	CT 5768/89	a d	20833
12 Wilpena Terrace KILKENNY	Former Shop and House; Includes the former shop and house, excluding the double garage.	A72	D1014	CT 5299/236	a	20834
18 Wilpena Terrace KILKENNY	Former Shops; Includes the shops only, excluding the rear house.	A2	S5957	CT 5020/192	a d	20835
19 Wilpena Terrace KILKENNY	House; Includes the original form and fabric of the façade, side elevations, and roof, excluding the non-original brick lean-to and garage.	A6	F129088	CT 5557/532	a d	20836
20 Wilpena TERRACE KILKENNY	Former Shops; Includes the shops only, excluding the rear house.	A1	S5957	CT 5020/191	a d	26483
24 Wilpena Terrace KILKENNY	Former Shop and House; Including the former shop and house only.	A28	F116840	CT 5550/721	a d	20837
84 Wilpena Terrace KILKENNY	House; Includes only the house.	A70	F116982	CT 5360/719	a d e	20838
9 Gilbert Street OVINGHAM	Church; Including attached outbuildings and masonry wall enclosing the site.	A56	F122003	CT 5869/309	a d e	20839
51 Park Terrace OVINGHAM	House; Includes the house only.	A64	F122011	CT 5541/909	a d e	20840
20 Telford Street OVINGHAM	House; Including house and original components of fence.	A100	D37837	CT 5142/879	a d	20841

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
29 Torrens Road OVINGHAM	Former Ovingham Hotel; Whole of two storey portion of hotel and external wall of single storey bluestone section of hotel facing Gilbert Street.	A10 A11 A9	F105673 F105673 F105673	CT 6124/845 CT 6124/845 CT 6124/845	a b c d	27659
Mary Street PENNINGTON	Pennington Junior Primary School and House; 1917 primary school building and dwelling with verandah; exclude 1928 infant school, all other buildings and fence.			CT 4389/963	c	9152
17 Pennington Terrace PENNINGTON	Mount Carmel Girls' Primary School and Convent; Includes the circa 1900 building, the 1917 extensions, the 1937 building.	A1	F10971	CT 5492/741	a d	20842
660 Torrens Road (rear) PENNINGTON	Cheltenham Child Health centre; Including the original external form, materials and details, the remaining signage to the front gable, red brick walls, entry portico, terracotta tiled roof, timber barges, , fascias and double hung windows.	A62	D2679	CT 5130/653	a	20843
712 Torrens Road PENNINGTON	Shop; Shop facade including verandah and parapet; exclude rear building and fencing.			CT 1286/19	b	9151
3 Honeyton Street cnr Alma Terrace SEATON	Greek Evangelical Church; Includes the brick building only, excluding all other additions to the sides or rear.	A91	F213191	CT 5566/869	a c	20844
18-20 Bray Avenue SEMAPHORE PARK	Baptist Church; Church building including interior exclude hall and fences.			CT 925/195	c	9154
29 Bray Avenue SEMAPHORE PARK	House; House and verandah; exclude fencing and outbuildings.			CT 4113/97	d	9153

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
36A Welland Avenue WELLAND	Former shop; Shop under gambrel hip form, shopfront, and projecting canopy awning verandah.	A36	F116248	CT 5767/800	a b	27660
5 Rockingham Street WEST BEACH	House; House; facades and fence			CT 1548/77	a d	9162
Seaview Road WEST BEACH	Torrens Outlet; River Torrens Outlet; concrete construction containing River Torrens				a f	9256
3 Alfred Road WEST CROYDON	House and Fence; House, garage, driveway, paving, garden, brick garden walls and front brick fence; exclude internal alterations.			CT 5236/876	d e	9175
32 Aroona Road WEST CROYDON	Dwelling; whole of dwelling under gambrel hipped CGI roof, including verandah.	A45	F116857	CT 5880/504	a d	27661
27 Blanford Street WEST CROYDON	Shop and dwelling; Shop and shopfront, including façade tiling, verandah awning to footpath, and attached dwelling under gambrel hip corrugated sheet roof.	A201	F15709	CT 5410/324	a b	27662
22A Cedar Avenue WEST CROYDON	Former Croydon Primary School; Original 1926 two storey school red brick building under CGI hipped form roof, including stucco faced porches.	A9	F116021	CT 5565/684	a c e	27664
617 Port Road WEST CROYDON	Offices (ACI); Exterior of two-storey office building.			CT 3333/158	a d	9244
38-40 Rosetta Street WEST CROYDON	Former West Croydon Baptist Church; Whole of church and entry porch under gable CGI roofs. Excludes additions to rear and side.	A28 A27	D2779 D2779	CT 5486/512 CT 5486/513	a	27666
Rowell Crescent & Miell Street Corner WEST CROYDON	Gelland War Memorial; Obelisk monument, stepped plinth, plaques and plaster ornamentation.				a c e	27668

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54 William Street WEST CROYDON	Former shop and dwelling; Shop, shopfront, awning verandah over footpath and wall tiling. Includes whole of dwelling under CGI gambrel hip roof and verandah.	A3	F107965	CT 5192/992	a b	27667
8A Glanton Street WEST HINDMARSH	Dwelling, former Methodist Church and Hall; Including the former Church and Hall, excluding all non-original buildings and front property boundary wall.	A89 A90	D1342 D1342	CT 5199/771 CT 5199/771	a c d f	20846
51 Grange Road WEST HINDMARSH	House; Includes the original form and fabric of the façade, side elevations, roof and chimneys, excluding the non-original roof fabric, rear pergolas and fences.	A4	D2159	CT 5424/485	a d	20848
23 Young Avenue WEST HINDMARSH	House (Elliot's); House and verandah including all original features and fencing to Young Avenue and Gawler Avenue; exclude interior.			CT 1953/100	d e	9146
16 Frederick Road WEST LAKES	Port Adelaide Treatment Works; The original form of the administration building, main plant building and the garden area located between the original administration building and Frederick Road for a distance of 50 metres to the north and south of the original administration building. Exclude sewerage ponds, all other structures and garden areas to the north, south and fronting Frederick Road.		H106400		a b d e f	9254
2 Belmore Terrace WOODVILLE	House; Original house; all other structures excluded.			CT 765/24	d	9180

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
19-21 Belmore Terrace WOODVILLE	Mareeba and former Nurses Home; Includes the Mareeba building and the former nurses home, excluding all outbuildings.	A2 A3 A275 A9 A268 A1 A1 A1	F125502 F125502 D689 F131917 D689 F131933 F123895 F131909	CT 5224/18 CT 5224/18 CT 5621/646 CT 5621/646 CT 5794/94 CT 5807/858 CT 5811/812 CT 5831/191	a d e f	20857
2 Jelley Street WOODVILLE	Woodville Private Hospital; External form, materials and details of original house, including concave verandah and decorative gablet; exclude fences, additions to the west and more recent verandahs.			CT 5493/722	a	9263
Leslie Street West WOODVILLE	Woodville High School; Original school building and iron gate and pillars, all other structures and buildings excluded			CT 1306/187 CT 1759/140 CT 3100/162 CT 3100/163 CT 5251/459	c	9202
20/ 6 Leslie Street WOODVILLE	Former dwelling; Whole of bluestone dwelling under projecting gable hip and valley CGI roof, including bullnose verandahs.	A3	F125904	CT 5675/856	a d	27669
765 Port Road WOODVILLE	Former Council Chambers; Original house excluding rear additions and fencing.			CT 246/42	a	9231
33 Stanley Street WOODVILLE	House "Irrawarra"; Includes the original form and fabric of the façade, roof, chimneys, verandah and fence.	A34	F124128	CT 5405/618	a d	26225
513 Torrens Road WOODVILLE	Former House; Original single-storey random coarsed stone building and raked verandah; exclude all other structures, including rear addition.			CT 700/89	c	9264

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
519 Torrens Road WOODVILLE	House; External form, material and details of the original house, including the verandah; exclude later additions, the garage and all other structures			CT 5204/494	a d	9265
Woodville Road WOODVILLE	St Clair Youth Complex; Original structure excluding later additions			CT 2232/196	c d	9203
Woodville Road WOODVILLE	Woodville Railway Station; Down platform shelter, up platform shelter with weatherboard ticket office, single-storey brick building.			CT 1800/86 CT 1805/26 CT 4361/941 CT 845/109 CT 882/50	a c	9198
65 Woodville Road WOODVILLE	State Bank; Office building and adjoining annexe to the south.			CT 1319/131	a c d	9197
72 Woodville Road WOODVILLE	Town Hall & Council Chambers; Entire building including former 1902 Council offices and chamber, Town Hall and later additions, hitching post, John Hanrahan Memorial Water Fountain and Memorial Clock.	A17 A18 A19 A52	D1834 D1834 D1834 F124046	CT 1591/65 CT 1591/65 CT 1591/65 CT 5739/135	a f	9200
92A Woodville Road WOODVILLE	Ukrainian Catholic Church of St Volodymyr & Olha; Whole of church building, including front porch and breeze block verandahs and roof domes.	A701	D90541	CT 6105/605	a b c d f	27671
90 Woodville Road WOODVILLE	Dwelling; Whole of dwelling under hipped terracotta tile roof, including verandahs. Excludes additions to rear, Stanley Street wing and garage.	A69	F124063	CT 5575/569	a d	27670
96 Woodville Road WOODVILLE	Former dwelling; Bluestone portion under 3 x hipped CGI roofs, including chimneys and concave verandah.	A18	F124212	CT 5728/116	a d	27672

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
106 Woodville Road WOODVILLE	Dwelling; Whole of dwelling under CGI hip and gable roofs, including chimney and bullnose verandahs.	A3	F124297	CT 5681/666	a d	27673
45-47 Beaufort Street WOODVILLE PARK	Whitefriars; Single-storey 1915 brick school building			CT 872/119 CT 905/16	c	9267
Kilkenny Road WOODVILLE PARK	Former Uniting Church; Church building including Mens Class room and 1866 foundation stone from Beverley Church but excluding additions adjacent Elgin Street.			CT 672/166	c	9234
Kilkenny Road WOODVILLE PARK	Former Wesley Hall; Hall building			CT 672/166	c	9233
649 Port Road WOODVILLE PARK	Former Police Station; House and verandah; exclude fences entrance structure and outbuildings.			CT 550/32	a c	9235
675-677 Port Road WOODVILLE PARK	St Sava Church; Whole of masonry church building and belltower.	A401	D55187	CT 5801/59	a b	27612
100 Ledger Road Cnr of Doon Street WOODVILLE SOUTH	House; Includes original form and fabric.	A26	D17183	CT 5252/699	a d	20789
Port Road WOODVILLE SOUTH	Woodville Primary School; Original school building and stone cottage to west.			CT 5218/845	c	9268
878-882 Port Road WOODVILLE SOUTH	Woodville Hotel; Hotel building, single-storey building adjoining to the west; exclude single-storey drive-through building adjoining two storey building to the east and rear additions.			CT 5140/751	a c f	9199
2-4 Tenterden Street Rear WOODVILLE SOUTH	House; Includes the house only, excluding rear outbuildings.	A35	F6608	CT 5732/862	a d	20861
44a Woodville Road WOODVILLE SOUTH	Uniting Church Complex; Church, former manse and three Sunday School classrooms; exclude fences and garages			CT 5128/240	c	9195

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 23(4) Criteria	Heritage NR
4 Findon Road WOODVILLE WEST	Former Fire Station; Original two-storey building excluding single-storey addition to rear and Outbuildings and fences			CT 414/55	c	9250
74 May Street WOODVILLE WEST	Dwelling; Original form of the building, including all decorative features, canopies and fenestration. Excludes later additions and outbuildings.	A2	D3991	CT 5718/354	a d	27613

Table ChSt/7 - State Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
1-3 Toogood Avenue BEVERLEY	Four Down-draught Brick Kilns and Four Chimneys, former James' Brickyard	A35	D30169	CT 5816/854	a b c	14675
100 Drayton Street BOWDEN	Anglican Church of the Good Shepherd Mission	A8	F122055	CT 5821/346		11811
123 Drayton Street BOWDEN	Dwelling (former Beehive Shirt Factory Complex comprising residence, factory and shop)	A1	D540	CT 5332/561		13689
69 - 77 Drayton Street BOWDEN	Dwelling (former Anderson's Cottages)	A56	F121903	CT 5732/525		13688
30 Sixth Street BOWDEN	Office (former Way Memorial Bible Christian Church)	A5	F125523	CT 5481/772		13682
Burley Griffin Boulevard BROMPTON	Former Hindmarsh Incinerator designed by Walter Burley Griffin	A100	D37062	CT 5133/137		10555
36 Chief Street BROMPTON	Gaslight Tavern Hotel	A29	F121876	CT 5217/461		11822
146 Coglin Street BROMPTON	Dwelling (former Cottage Homes)	U1 U5 U7 U6 U4 U2 U3 UCP	S5264 S5264 S5264 S5264 S5264 S5264 S5264 S5264	CT 5016/377 CT 5016/381 CT 5016/383 CT 5016/382 CT 5016/380 CT 5016/378 CT 5016/379 CT 5016/384		13686
East Street and Chief Street BROMPTON	Brompton Gas Works, 1879 Retort House, Remains of 1891 Retort House and Chimney Stack	AA A95- 97 & 99 A91	R2359 F218912 F207183	CT 5781/435 CT 6055/957 CT 6055/958		11823
5 First Street BROMPTON	Brompton Park Hotel	A313- 315	D459	CT 5113/183		11813
85A Torrens Road BROMPTON	Bowden Brompton Community School	A91, 92	F178456	CT 5349/873		13706
Port Road CHELTENHAM	Grave of Yoshikuma Kawakami (Japanese naval cadet), Cheltenham Cemetery	A39	F210066	CT 5961/23		14287

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Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
Jetty Street GRANGE	Charles Sturt Memorial Museum (former dwelling 'The Grange')	A209	F43	CT 5403/544		10567
270 Military Road GRANGE	St Agnes' Anglican Church	A44	F19	CT 5624/485	a e	14632
499-513 The Esplanade GRANGE	Dwelling ('The Marines')	A21 A15 A14 A17 A20 A18 A16 A19	F96 F96 F96 F96 F96 F96 F96 F96	CT 5082/129 CT 5363/496 CT 5414/745 CT 5350/396 CT 5125/942 CT 5620/717 CT 5648/432 CT 5863/694		14320
214 Military Road HENLEY BEACH	Temple Uniting (former Congregational) Church	A87	F288	CT 5352/771	b	14634
19 Reddie Street HENLEY BEACH	Scout Heritage Centre, sometime Grange Scout Hall (former Municipal Tramways Trust (MTT) Converter Station)	A271	F14	CT 5842/539	a e	14633
328 Seaview Road HENLEY BEACH	Ramsgate Hotel	A26	F583	CT 5216/44	a f	14637
378 Seaview Road HENLEY BEACH	Henley Beach Town Hall, Council Chambers & War Memorial	A32 A30	F288 F288	CT 5839/908 CT 5839/907		10558
209 The Esplanade HENLEY BEACH	Dwelling (former Del Monte Palace Guest House)	Q48,4 9,50 & 51 ACP A55 ACP	C25593 C25593 C25420 C25420	CT 6045/989 CT 6045/990 CT 6045/994 CT 6045/995	f	14638
32 Adam Street HINDMARSH	Hindmarsh Cemetery and Sexton's Cottage	S353	H106100	N/A		11799
33 Adam Street HINDMARSH	Omega Foods (Former GH Michell & Sons (originally Burnell & Co.) Fellmongery and Wool Scouring Factory)	A2	F1693	CT 5510/209		11788
3A Hindmarsh Place HINDMARSH	Hindmarsh Christian Church	A5	D51280	CT 6005/629		11800
Hindmarsh Place HINDMARSH	Hindmarsh Historical Society Museum (former Hindmarsh Fire Station & Christian Chapel)	A4	D51280	CT 6005/628		13693

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
34 Holden Street HINDMARSH	All Saints Anglican Church, Hall and Rectory	A91 & 92	F207016	CT 6005/631		11801
Holland Street HINDMARSH	Holland Street Tramway Bridge [Concrete Girder]	S1 A50	D1038 D40657	CT 400/195 CT 5812/420		10987
41 Manton Street HINDMARSH	Two Storey Dwelling	A5	F12422	CT 5851/942		11818
9 Manton Street HINDMARSH	Joiners' Arms Hotel	A29	F120148	CT 5954/201		11164
Manton Street HINDMARSH	Hall (former Hindmarsh Municipal Band Studio)	A4	D51280	CT 6005/628		13694
6 Mary Street HINDMARSH	Dwelling & Office (former Shop)	A54	F120173	CT 5243/975		13696
2 Milner Street HINDMARSH	Hindmarsh Town Hall (including Town Hall Building, Assembly Hall, Library and Facade of Skating Rink)	A3	D52380	CT 5789/106		11794
26 Orsmond Street HINDMARSH	Hindmarsh Church of Christ and Sunday School Hall	A43	F120162	CT 5243/967		11803
49A Orsmond Street HINDMARSH	Hindmarsh Resource Centre (former Hindmarsh Primary School)	A3 A4	F28819 F28819	CT 5281/310 CT 5281/309		11796
126 Port Road HINDMARSH	Lady Daly Hotel	A45	F120164	CT 5862/2		11817
155 Port Road HINDMARSH	Office (former Brompton Wesleyan Methodist Church)	A111	D64689	CT 5921/437		11805
183-185 Port Road HINDMARSH	Offices (Former Hindmarsh Police Station)	A30	F121777	CT 5451/935		11795
188 Port Road HINDMARSH	Two Storey Chemist Shop and Dwelling	A6	D52380	CT 5789/109		13701
212 Port Road HINDMARSH	Hindmarsh Post Office	A8	F120227	CT 5667/307		10556
240 Port Road HINDMARSH	Office (former Savings Bank of South Australia Hindmarsh Branch)	A1	F34191	CT 5182/709		11786
46 Port Road HINDMARSH	Norberto's Argentinian Bar & Grill (Former Commercial Hotel)	A	D1009	CT 5138/35		11821
59 Port Road HINDMARSH	Governor Hindmarsh Hotel	A19 A20 A21	F102586 F102586 F102586	CT 5606/422 CT 5606/423 CT 5606/424		11820

Charles Sturt Council
Table Section
Table ChSt/7 - State Heritage Places

Property Address	Description and/or Extent of Listed Place	Lot No. or Part Sec	Plan No.	Certificate of Title	Section 16 Criteria	SA Heritage Register ID
Port Road HINDMARSH	Lamp Standards, Port Road Reserve	S5807	H106100	CR 5753/911		12788
Port Road HINDMARSH	Bandstand, Port Road Reserve	S5807	H106100	CR 5753/911		13703
Port Road HINDMARSH	Drinking Fountain, Port Road Reserve	S5807	H106100	CR 5753/911		13704
Station Place HINDMARSH	Bowden Railway Station	A1 A12	D26891 D27470	CT 5658/199 CT 5997/440		10557
54 Park Terrace OVINGHAM	Dwelling	A7	F131685	CT 5490/188		13697
62 Park Terrace OVINGHAM	Dwelling ('Prospect Hall')	A43	F122090	CT 5381/374		11785
200 Bower Road SEMAPHORE PARK	Dwelling ('Bower Cottages')	A7	D51842	CT 5679/369	g	11958
349 Military Road SEMAPHORE PARK	Fort Glanville, Fort Glanville Conservation Park	A97	F215778	CT 5848/334		10569
175-177 Military Road TENNYSON	Dwelling (former Estcourt House)	A501	D71428	CT 5972/314		11971
45 Albermarle Street WEST HINDMARSH	Dwelling	A165	D1342	CT 5445/152		13120
12 Cross Street WEST HINDMARSH	Dwelling	A135	D1342	CT 5230/968		13687
29 Grange Road WEST HINDMARSH	St Joseph's Convent (former Dwelling)	A500	D81606	CT 6045/219		13691
9 Grange Road WEST HINDMARSH	Dwelling (Originally Job Hallett's House)	A5	D1837	CT 5157/157		13690
789-791 Port Road WOODVILLE	St Margaret's Anglican Church and Lychgate	NUA	H106100	N/A	a	11989
111 Woodville Road WOODVILLE	The Brocas Museum (former Dwelling 'The Brocas')	A10	F124204	CT 5596/992		10646

Note: this table was last updated on [7 June 2010](#) and is an extract from the South Australian Heritage Register established under Section 13 (1) of the *Heritage Places Act 1993*. In the event of a discrepancy between this extract and the South Australian Heritage Register, the South Australian Heritage Register shall prevail.

Mapping Section

Structure Plan Maps

Spatial Extent Maps

Concept Plan Maps

Map Reference Tables

Index Maps

Index Map Reference

[Council Index Map](#)

Zone Maps

Zone Name	Zone Map Numbers
Adelaide Shores Zone	ChSt/23
Airfield Zone	ChSt/23
Coastal Open Space Zone	ChSt/2, ChSt/6, ChSt/11, ChSt/17, ChSt/22, ChSt/23
District Centre Zone	ChSt/6, ChSt/8, ChSt/9, ChSt/15, ChSt/18, ChSt/20, ChSt/21
Education Zone	ChSt/17
Home Industry Zone	ChSt/3, ChSt/4, ChSt/5
Local Centre Zone	ChSt/2, ChSt/4, ChSt/5, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/22, ChSt/24
Metropolitan Open Space System Zone	ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/24
Mixed Use Zone	ChSt/3, ChSt/4, ChSt/7, ChSt/8, ChSt/9, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21
Neighbourhood Centre Zone	ChSt/3, ChSt/7, ChSt/13, ChSt/15, ChSt/17, ChSt/19, ChSt/24
Residential Character Zone	ChSt/4, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/20, ChSt/21, ChSt/22
Residential Zone	ChSt/2, ChSt/3, ChSt/4, ChSt/5, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/23, ChSt/24
Special Uses Zone	ChSt/2, ChSt/3, ChSt/4, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/11, ChSt/12, ChSt/14, ChSt/15, ChSt/19, ChSt/20, ChSt/21
Stadium Zone	ChSt/14
Suburban Activity Node Zone	ChSt/12, ChSt/13
Urban Core Zone	ChSt/6, ChSt/7, ChSt/15, ChSt/16, ChSt/21
Urban Employment Zone	ChSt/3, ChSt/4, ChSt/5, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/19, ChSt/20, ChSt/21, ChSt/24

Historic Conservation Area Maps

Area Name	<i>shown within</i> Overlay Maps - Heritage
Historic Conservation Area	ChSt/4, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/20, ChSt/21, ChSt/22

Policy Area Maps

Policy Area Name	Policy Map Numbers
Fulham Gardens Policy Area 1	ChSt/18
Hindmarsh Policy Area 2	ChSt/15, ChSt/20, ChSt/21
Kilkenny Policy Area 3	ChSt/9
West Lakes Policy Area 4	ChSt/6
Woodville Policy Area 5	ChSt/8, ChSt/9
Bulky Goods Policy Area 6	ChSt/3, ChSt/4, ChSt/5, ChSt/8, ChSt/9
Local Shopping Policy Area 7	ChSt/2, ChSt/4, ChSt/5, ChSt/6, ChSt/8, ChSt/9, ChSt/11, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/22
Linear Park (River Torrens/Karrawirra Pari) Policy Area 8	ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/24
Urban Village Policy Area 9	ChSt/19
Findon Policy Area 10	ChSt/13, ChSt/19
Findon Road Policy Area 11	ChSt/13, ChSt/24
Henley Square Policy Area 12	ChSt/17
Royal Park Policy Area 13	ChSt/3, ChSt/7
Welland Policy Area 14	ChSt/15
Inner Suburban Policy Area 15	ChSt/15, ChSt/16, ChSt/21
Mid Suburban Policy Area 16	ChSt/2, ChSt/3, ChSt/4, ChSt/5, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/22, ChSt/23, ChSt/24
Western Edge Policy Area 17	ChSt/2, ChSt/6, ChSt/11, ChSt/17, ChSt/22, ChSt/23
West Lakes General Policy Area 18	ChSt/2, ChSt/3, ChSt/6, ChSt/7, ChSt/11, ChSt/12, ChSt/24
West Lakes Medium Density Policy Area 19	ChSt/2, ChSt/3, ChSt/6, ChSt/7, ChSt/11, ChSt/12, ChSt/18, ChSt/24
Integrated Medium Density Policy Area 20	ChSt/5, ChSt/7, ChSt/9, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/24
Woodville Medium Density Policy Area 21	ChSt/9
Cheltenham Park Policy Area 22	ChSt/4, ChSt/5, ChSt/8, ChSt/9

Policy Area Name	Policy Map Numbers
Woodville West Policy Area 23	ChSt/8
Main Street Policy Area 24	ChSt/6, ChSt/21
Main Road Commercial Policy Area 25	ChSt/3, ChSt/5, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/14, ChSt/15, ChSt/20, ChSt/21
Core Industry Policy Area 26	ChSt/3, ChSt/4, ChSt/5, ChSt/7, ChSt/8, ChSt/9, ChSt/14, ChSt/15, ChSt/20, ChSt/21

Precinct Maps

Precinct Name	Precinct Map Numbers
Precinct 1 Fort Glanville	ChSt/2
Precinct 2 Retail Core Fulham Gardens	ChSt/18
Precinct 3 Commercial Fringe Fulham Gardens	ChSt/18
Precinct 4 Recreation Fulham Gardens	ChSt/18
Precinct 5 Historic Hub	ChSt/15, ChSt/21
Precinct 6 Port Road Office	ChSt/15, ChSt/20, ChSt/21
Precinct 7 Manton Street	ChSt/15, ChSt/21
Precinct 8 Gateway	ChSt/21
Precinct 9 Recreation Hindmarsh	ChSt/21
Precinct 10 Port Road Gardens	ChSt/15, ChSt/21
Precinct 11 Retail Core Kilkenny	ChSt/9
Precinct 12 Regency Road	ChSt/9
Precinct 13 Hanson Road	ChSt/9
Precinct 14 Commercial Fringe Kilkenny	ChSt/9
Precinct 15 Residential	ChSt/9
Precinct 16 Retail Core West Lakes	ChSt/6
Precinct 17 Lakefront	ChSt/6
Precinct 18 Woodville Road Medical	ChSt/8, ChSt/9
Precinct 19 Woodville Road Gardens	ChSt/8, ChSt/9
Precinct 20 Civic	ChSt/8, ChSt/9
Precinct 21 Recreation/Education	ChSt/9
Precinct 44 Woodville Bulky Goods	ChSt/8
Precinct 45 Old Port Road	ChSt/3
Precinct 46 Stations	ChSt/9, ChSt/15

Precinct Name	Precinct Map Numbers
Precinct 47 Mixed Use	ChSt/3, ChSt/4, ChSt/7, ChSt/8, ChSt/9, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21
Precinct 48 Hindmarsh Place	ChSt/21
Precinct 49 South Road	ChSt/15
Precinct 50 Village Employment and Living	ChSt/19
Precinct 51 Village Living Medium	ChSt/19
Precinct 52 Village Living Low	ChSt/19
Precinct 53 Retail Core Findon	ChSt/13, ChSt/19
Precinct 54 Winona Street	ChSt/13
Precinct 55 Retail Core Findon Road	ChSt/13, ChSt/24
Precinct 56 Commercial Fringe Findon Road	ChSt/13, ChSt/24
Precinct 57 Henley Square	ChSt/17
Precinct 58 Seaview Road	ChSt/17
Precinct 59 Retail Core Royal Park	ChSt/3, ChSt/7
Precinct 60 Commercial Fringe Royal Park	ChSt/7
Precinct 61 Retail Core Welland	ChSt/15
Precinct 62 Commercial Fringe Welland	ChSt/15
Precinct 66 Bowden/Brompton	ChSt/15, ChSt/16, ChSt/21
Precinct 67 Brompton Park	ChSt/15
Precinct 68 Early SA Housing Trust	ChSt/4, ChSt/15, ChSt/16
Precinct 69 Croydon/West Croydon	ChSt/14, ChSt/15
Precinct 70 Allenby Gardens	ChSt/14, ChSt/15
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Precinct 72 Welland Character	ChSt/15
Precinct 73 Woodville South Character	ChSt/8, ChSt/9, ChSt/13, ChSt/14
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Precinct 76 Woodville	ChSt/9
Precinct 77 Cheltenham East	ChSt/4, ChSt/8
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Precinct 79 Henley Beach	ChSt/17, ChSt/22
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Precinct Name	Precinct Map Numbers
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Precinct 82 Football Park	
Precinct 83 Woodville North	ChSt/4, ChSt/5, ChSt/9
Precinct 84 Findon Centre East	ChSt/13, ChSt/19

Overlay Maps

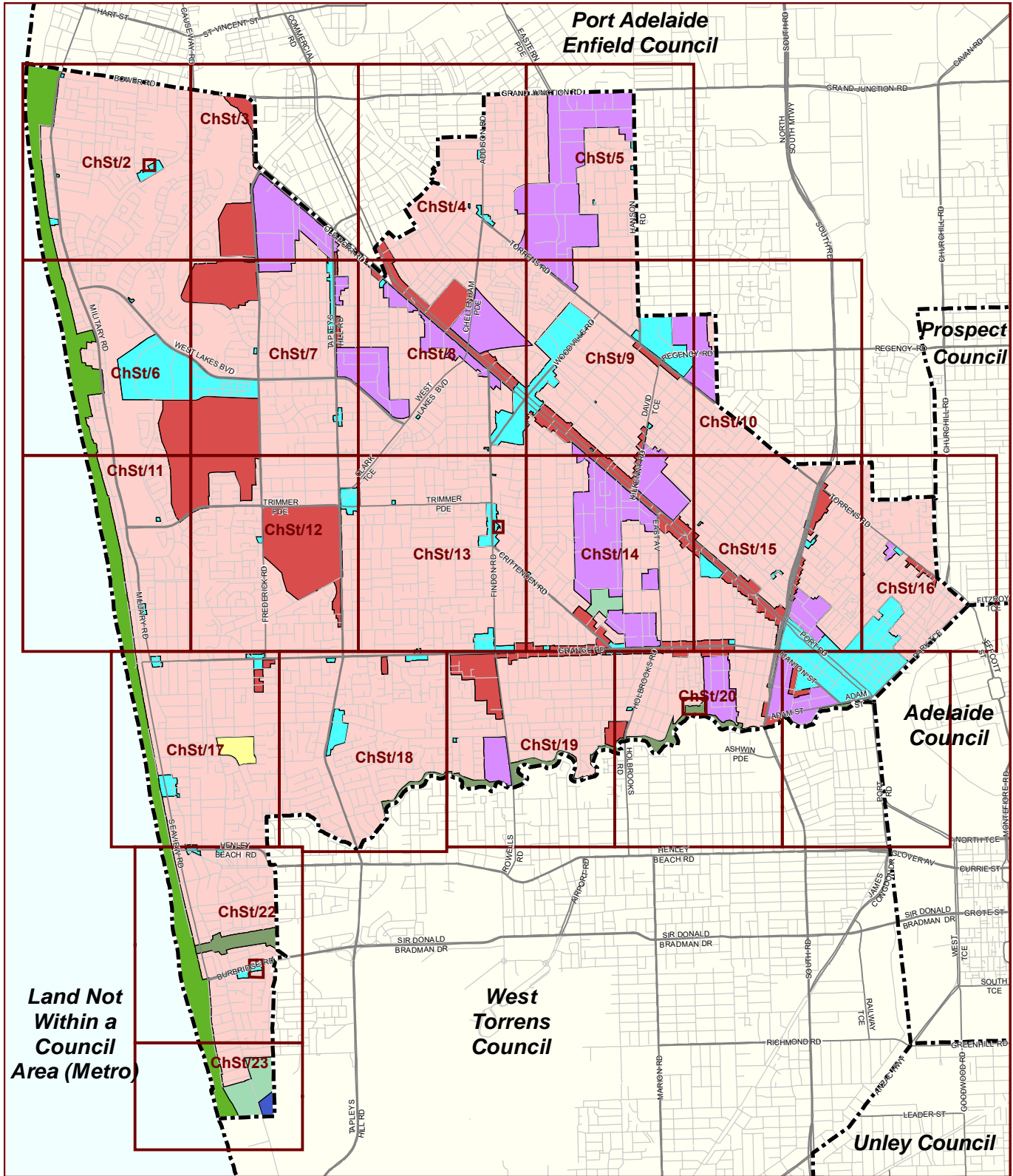
Issue	Overlay Map Numbers
Location	ChSt/1, ChSt/2, ChSt/3, ChSt/4, ChSt/5, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/23
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Heritage	ChSt/2, ChSt/3, ChSt/4, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/20, ChSt/21, ChSt/22, ChSt/23
Development Constraints	ChSt/1, ChSt/2, ChSt/3, ChSt/4, ChSt/5, ChSt/6, ChSt/7, ChSt/8, ChSt/9, ChSt/10, ChSt/11, ChSt/12, ChSt/13, ChSt/14, ChSt/15, ChSt/16, ChSt/17, ChSt/18, ChSt/19, ChSt/20, ChSt/21, ChSt/22, ChSt/23
Affordable Housing	ChSt/6, ChSt/7, ChSt/9, ChSt/12, ChSt/13, ChSt/15, ChSt/16, ChSt/19, ChSt/21
Noise and Air Emissions	ChSt/6, ChSt/7, ChSt/9, ChSt/12, ChSt/13, ChSt/15, ChSt/16, ChSt/19, ChSt/21
Strategic Transport Routes	ChSt/12, ChSt/21

Concept Plan Maps

Concept Plan Title	Concept Plan Map Numbers
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Hindmarsh Policy Area 2	ChSt/2
Kilkenny Policy Area 3	ChSt/3
West Lakes Policy Area 4	ChSt/4
Woodville Policy Area 5	ChSt/5
Urban Village Policy Area 9	ChSt/6
Findon Policy Area 10	ChSt/7
Findon Road Policy Area 11	ChSt/8
Henley Square Policy Area 12	ChSt/9
Royal Park Policy Area 13	ChSt/10
Welland Policy Area 14	ChSt/11

Concept Plan Title	Concept Plan Map Numbers
Allotments Affected by Aircraft Noise	ChSt/12
Allotments Affected by Foundry Noise	ChSt/13
Building Restriction Area	ChSt/14
Building Restriction Areas	ChSt/15
Building Restriction Areas	ChSt/16
Building Restriction Areas	ChSt/17
Cheltenham Park and Woodville Medium Density	ChSt/18
Former Underdale Campus Site	ChSt/19
Precinct 81 Stadium Beverley	ChSt/20
Former West Lakes Council Depot Land	ChSt/21
Woodville West	ChSt/22
Bowden Urban Village Urban Core Zone	ChSt/23
Precinct 21 Railway Station	ChSt/24
West Lakes Urban Core Zone	ChSt/25
Devon Park Medium Density Redevelopment Site	ChSt/26
Adelaide Shores	ChSt/27
Old Port Road, Royal Park	ChSt/28
Torrens Road, Woodville North	ChSt/29
Suburban Activity Node Zone Seaton	ChSt/30
Bowden/Brompton Urban Core Zone	ChSt/31

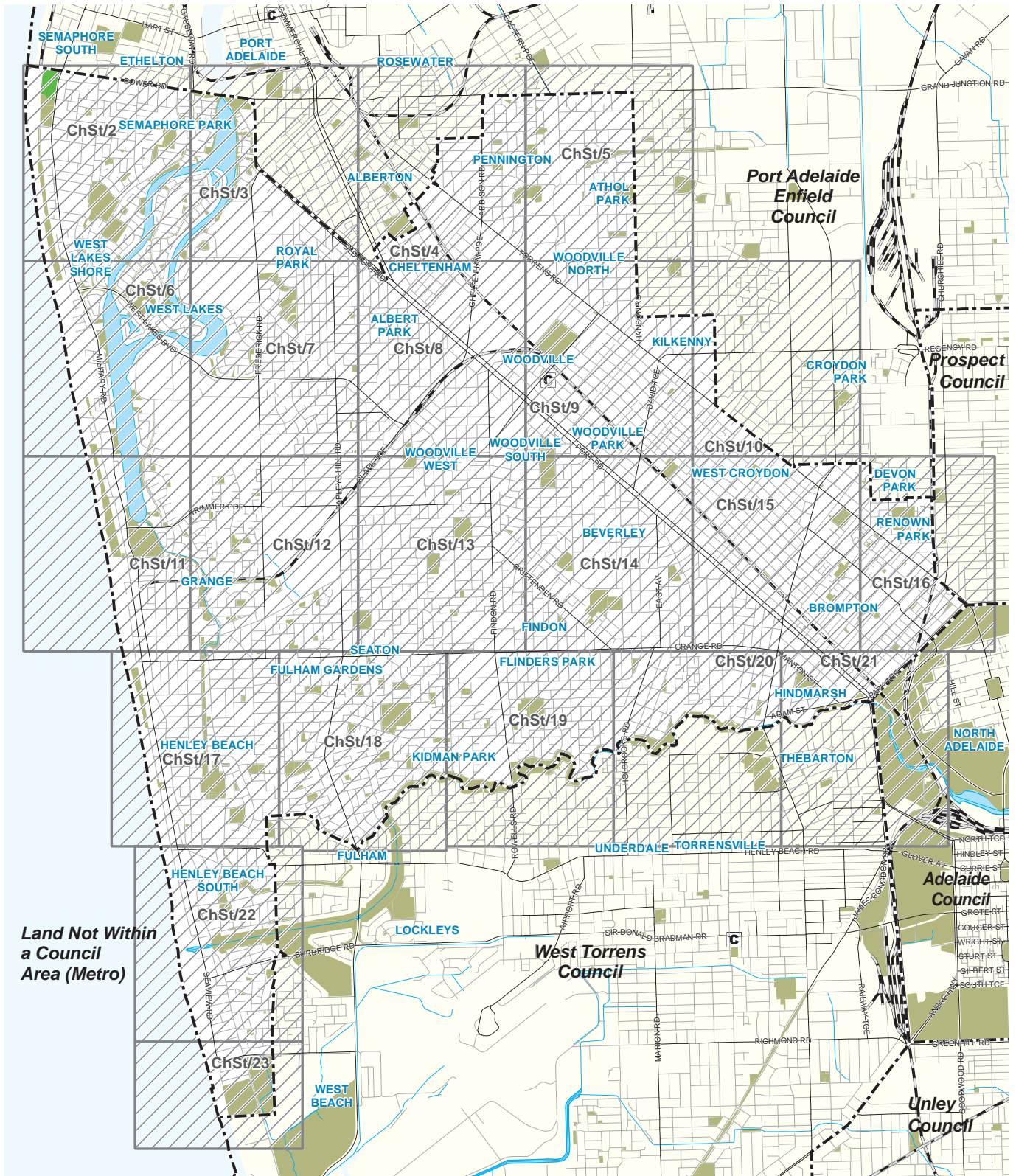
Spatial Extent Maps



For the purposes of the Development Plan unless otherwise clearly indicated, the zone/policy area/precinct boundaries depicted on or intended to be fixed by Maps ChSt/1 to Map ChSt/24 inclusive shall be read as conforming in all respects (as the case may require) to the land division boundaries, to the centre line of roads or drain reserves or to the title boundaries, or to imaginary straight lines joining the positions defined by survey or by the measurements shown on the said maps against which the said zone/policy area/precinct boundaries are shown or otherwise indicated.








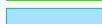

Council Index Map



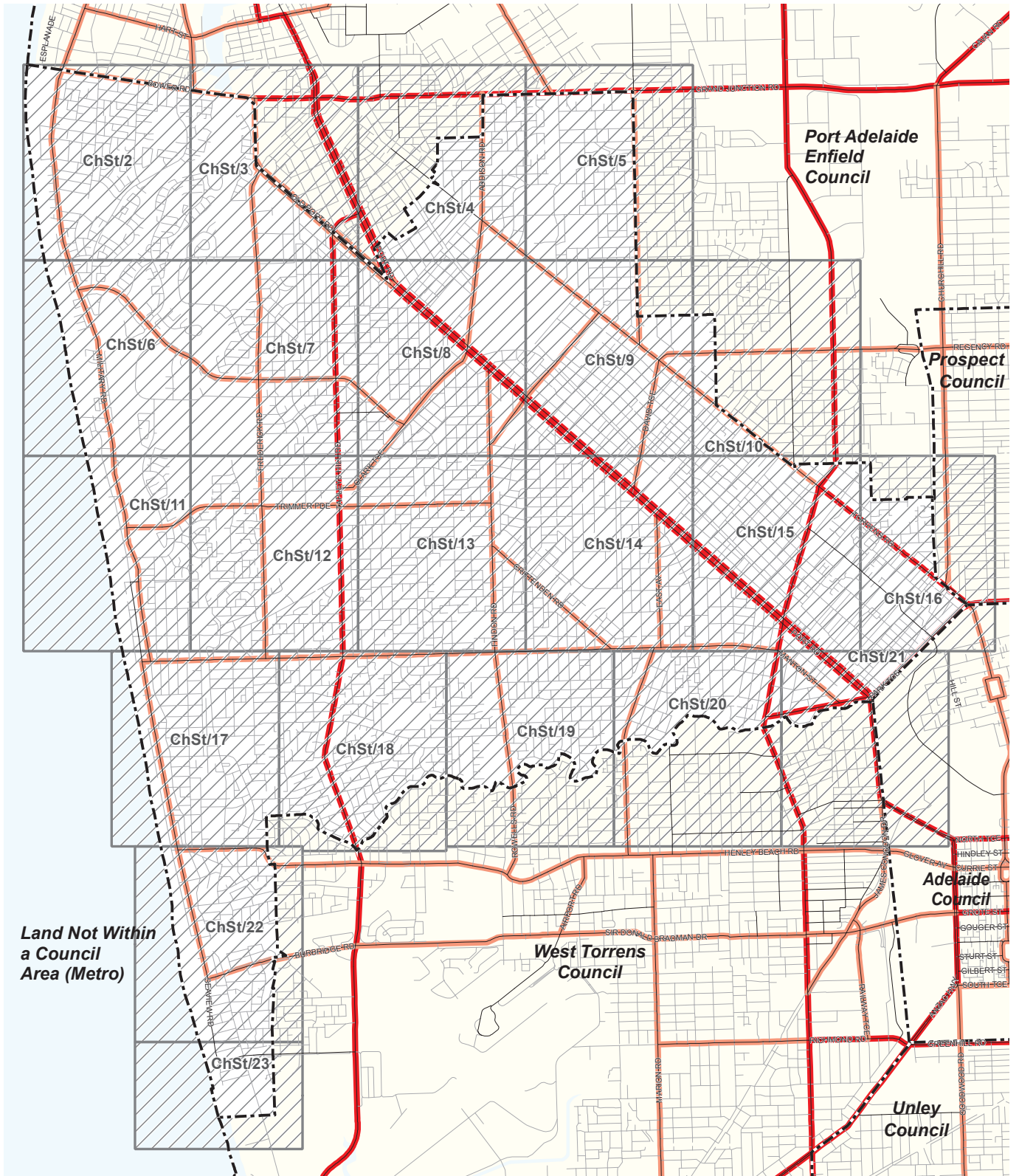
Land Not Within a Council Area (Metro)



0 5km

-  Council Office
-  Railways
-  Waterway / drain / channel
-  Local Reserves
-  Conservation Park
-  Waterbodies
-  Development Plan Boundary

Location Map ChSt/1



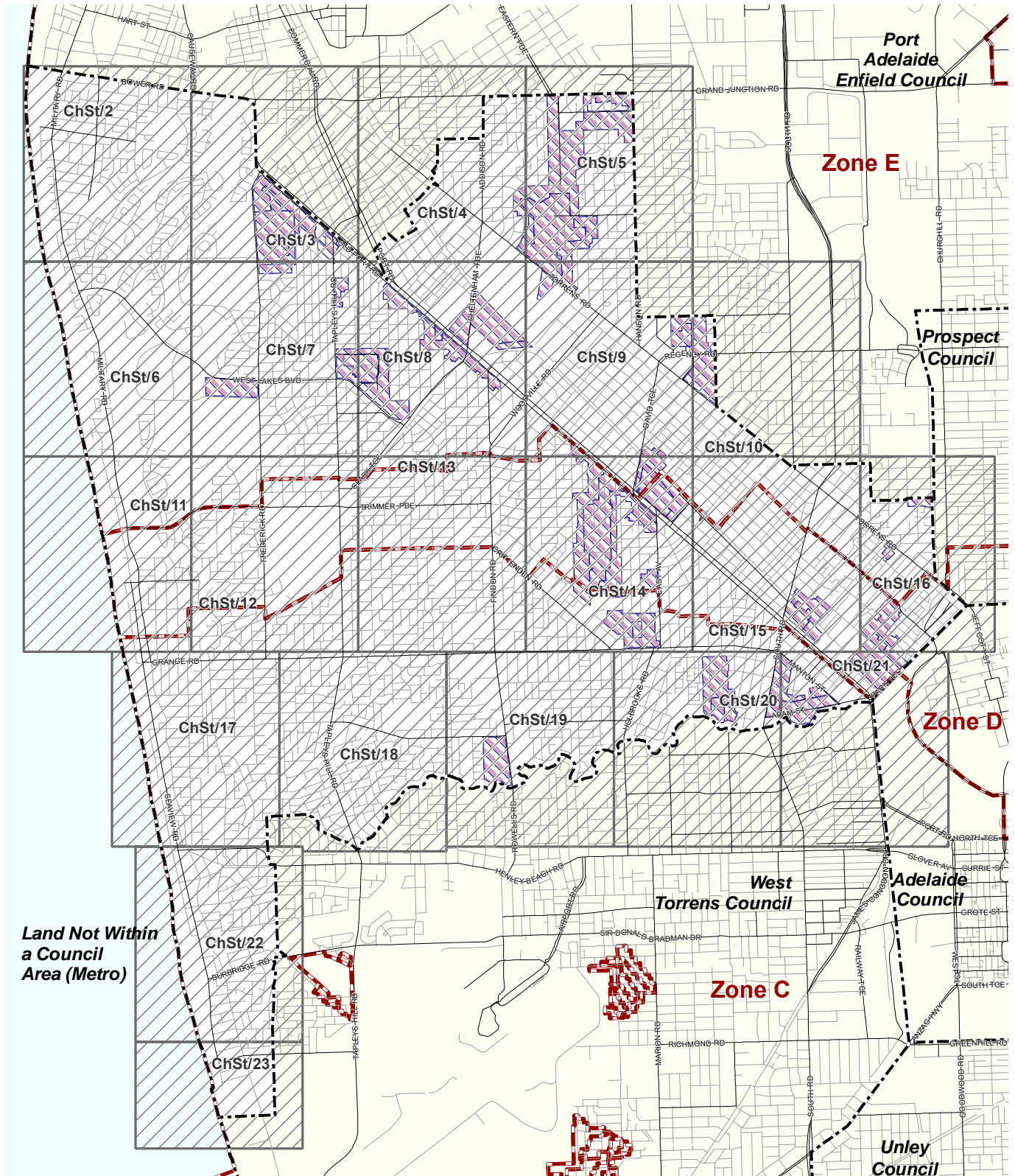
Land Not Within a Council Area (Metro)



Overlay Map ChSt/1

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Development Plan Boundary



Airport Building Heights

4 - 14m All Structures restricted to height identified on maps (above existing ground level, measured from the top of the nearest roadside curb)

Zone C All Structures Exceeding 15 metres above existing ground level

Zone D All Structures Exceeding 45 metres above existing ground level

Zone E All Structures Exceeding 100 metres above existing ground level

0



5 km

-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary

Overlay Map ChSt/1

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



MAP ChSt/6 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/3 Adjoins

Land Not Within a Council Area (Metro)

0 500 m

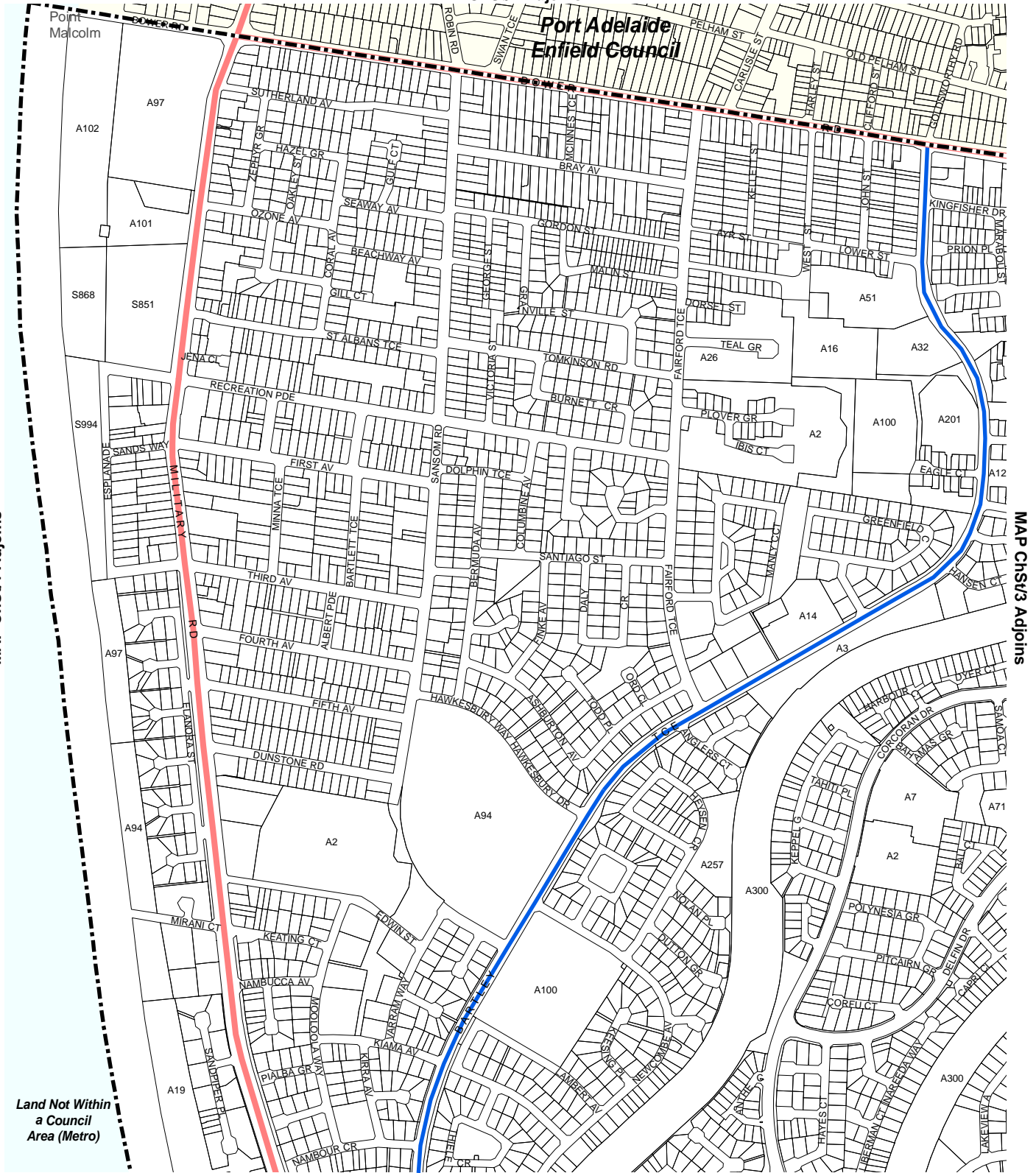
S School
P Post Office
 Railways
 Local Reserves
 Conservation Park
 Waterbodies
 Development Plan Boundary

Location Map ChSt/2

CHARLES STURT COUNCIL
 Consolidated - 14 January 2021

MAP ChSt/1 Adjoins

Port Adelaide
Enfield Council



Land Not Within
a Council
Area (Metro)

MAP ChSt/6 Adjoins



0 500m

Overlay Map ChSt/2 TRANSPORT

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/1 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/1 Adjoins

MAP ChSt/3 Adjoins

Land Not Within
a Council
Area (Metro)

MAP ChSt/6 Adjoins

Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

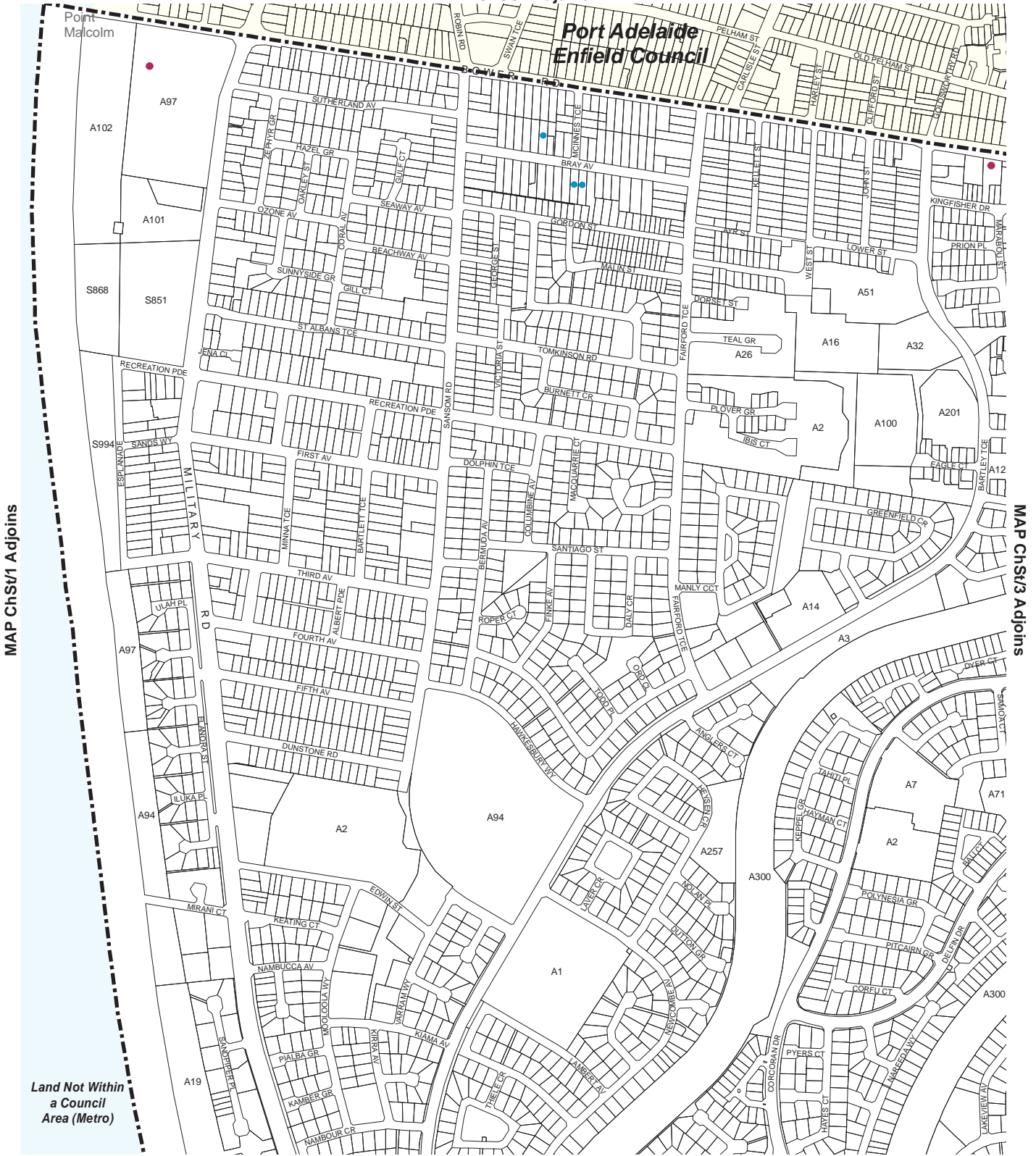


Overlay Map ChSt/2 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP ChSt/1 Adjoins

Port Adelaide
Enfield Council



Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



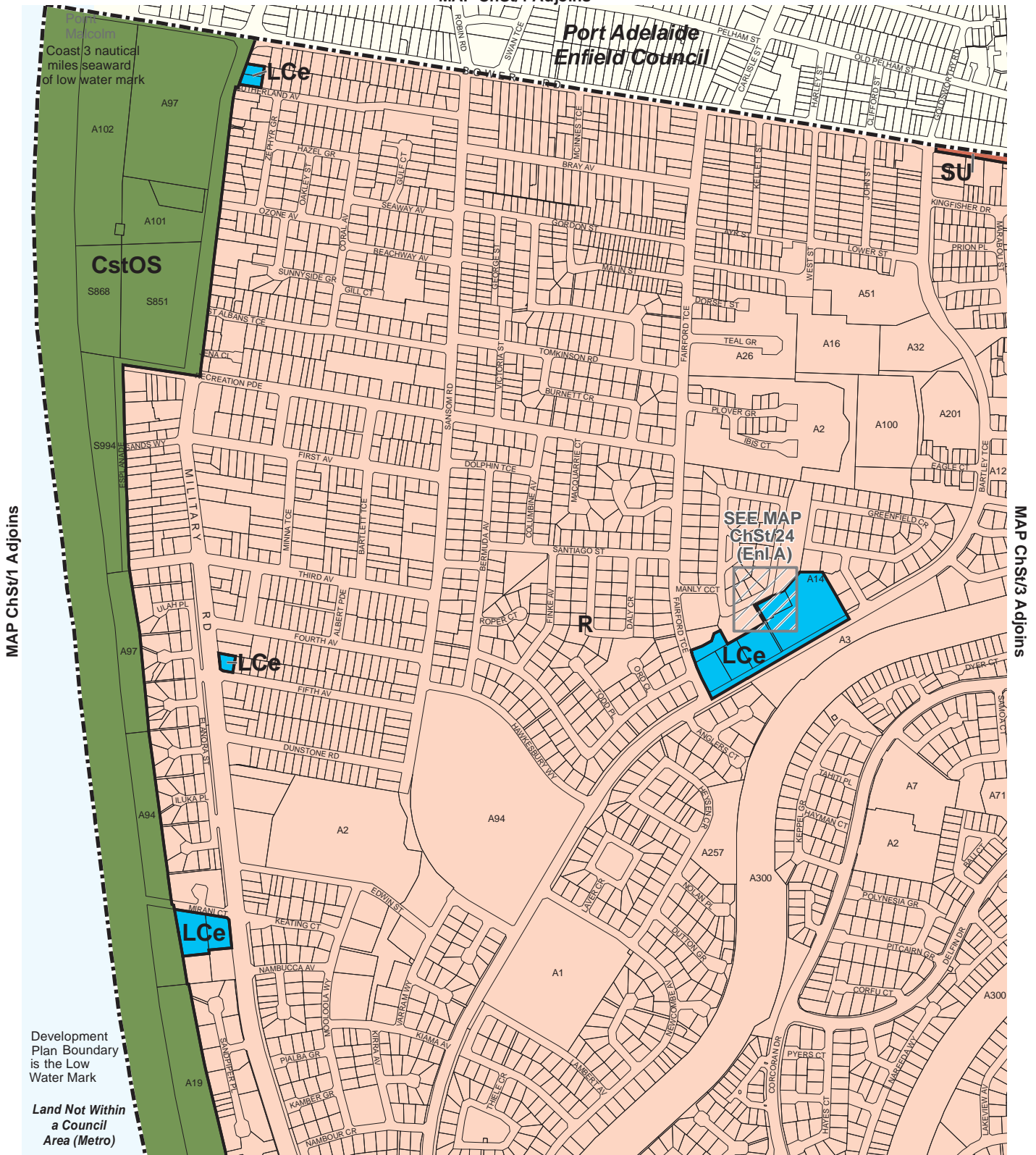
Overlay Map ChSt/2 HERITAGE

CHARLES STURT COUNCIL
Consolidated - 14 January 2021

- State heritage place
- Local heritage place
- Development Plan Boundary

MAP ChSt/1 Adjoins

Port Adelaide
Enfield Council



Development
Plan Boundary
is the Low
Water Mark

Land Not Within
a Council
Area (Metro)

MAP ChSt/6 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

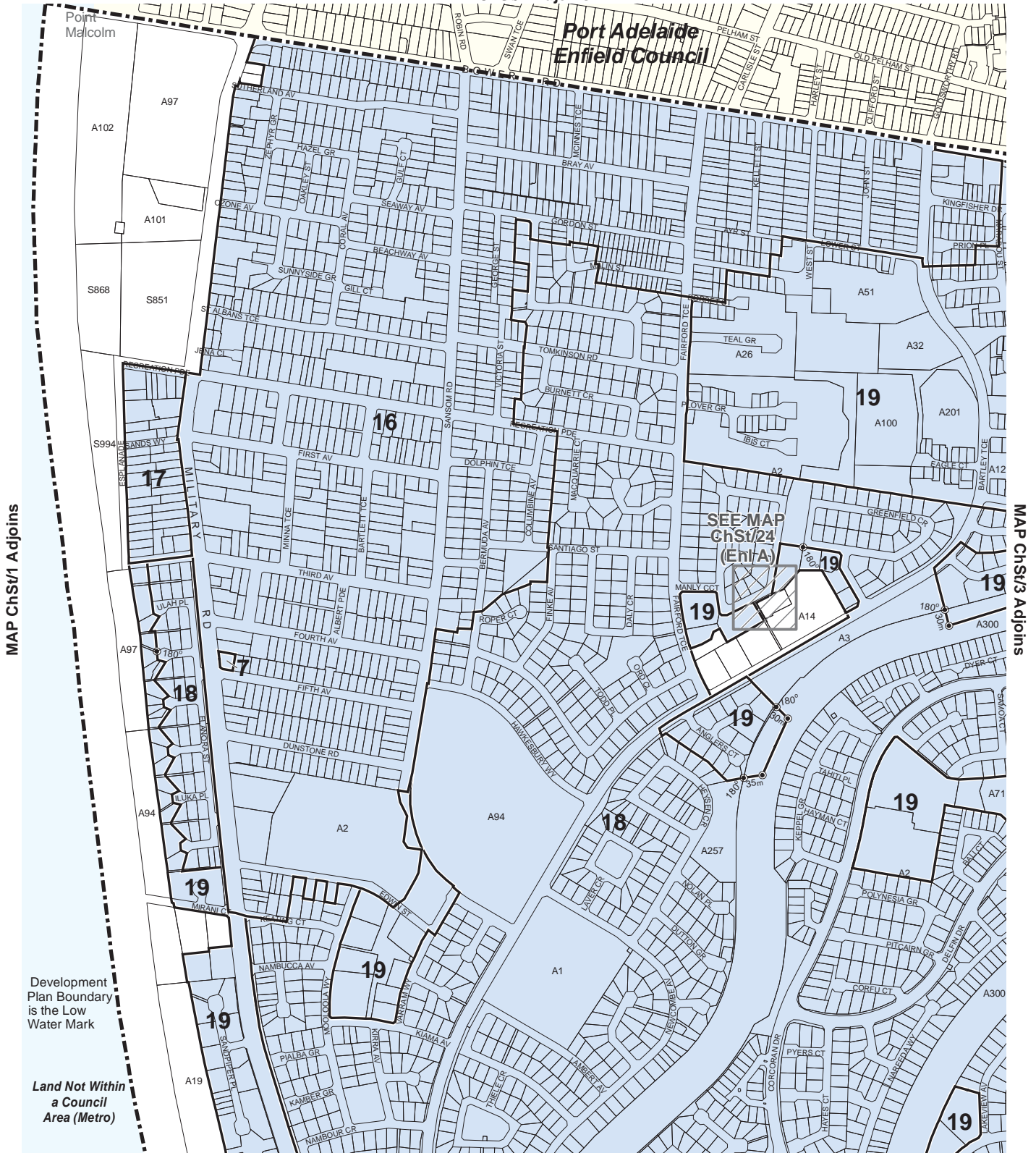


Zones

- CstOS Coastal Open Space
- LCe Local Centre
- R Residential
- SU Special Use
- Zone Boundary
- Development Plan Boundary

Zone Map ChSt/2

MAP ChSt/1 Adjoins



MAP ChSt/6 Adjoins

See enlargement map for accurate representation.
 Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 16 Mid Suburban
 - 17 Western Edge
 - 18 West Lakes General
 - 19 West Lakes Medium Density
 - 7 Local Shopping

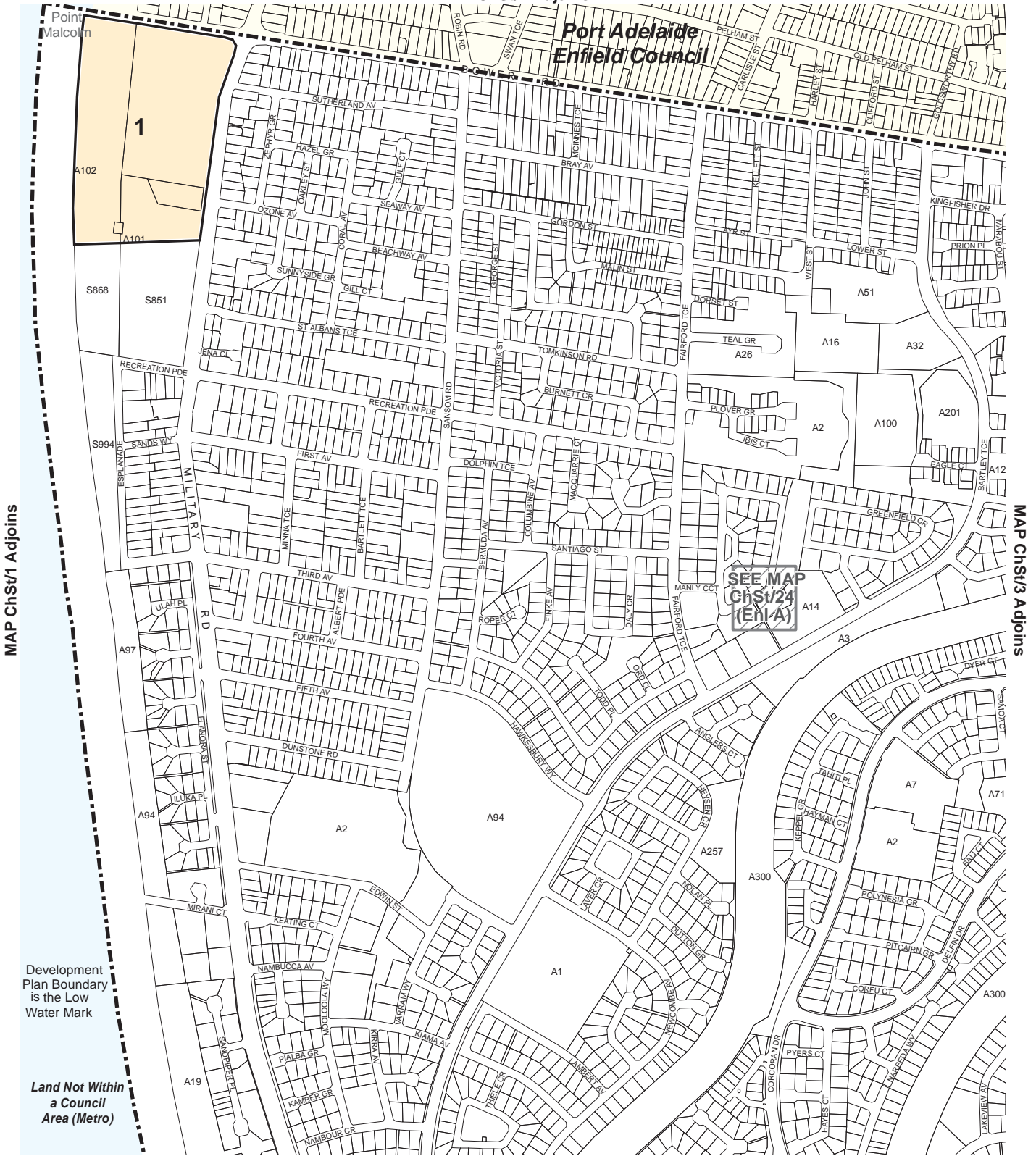


Policy Area Map ChSt/2

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/6 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Precinct
1 Fort Glanville

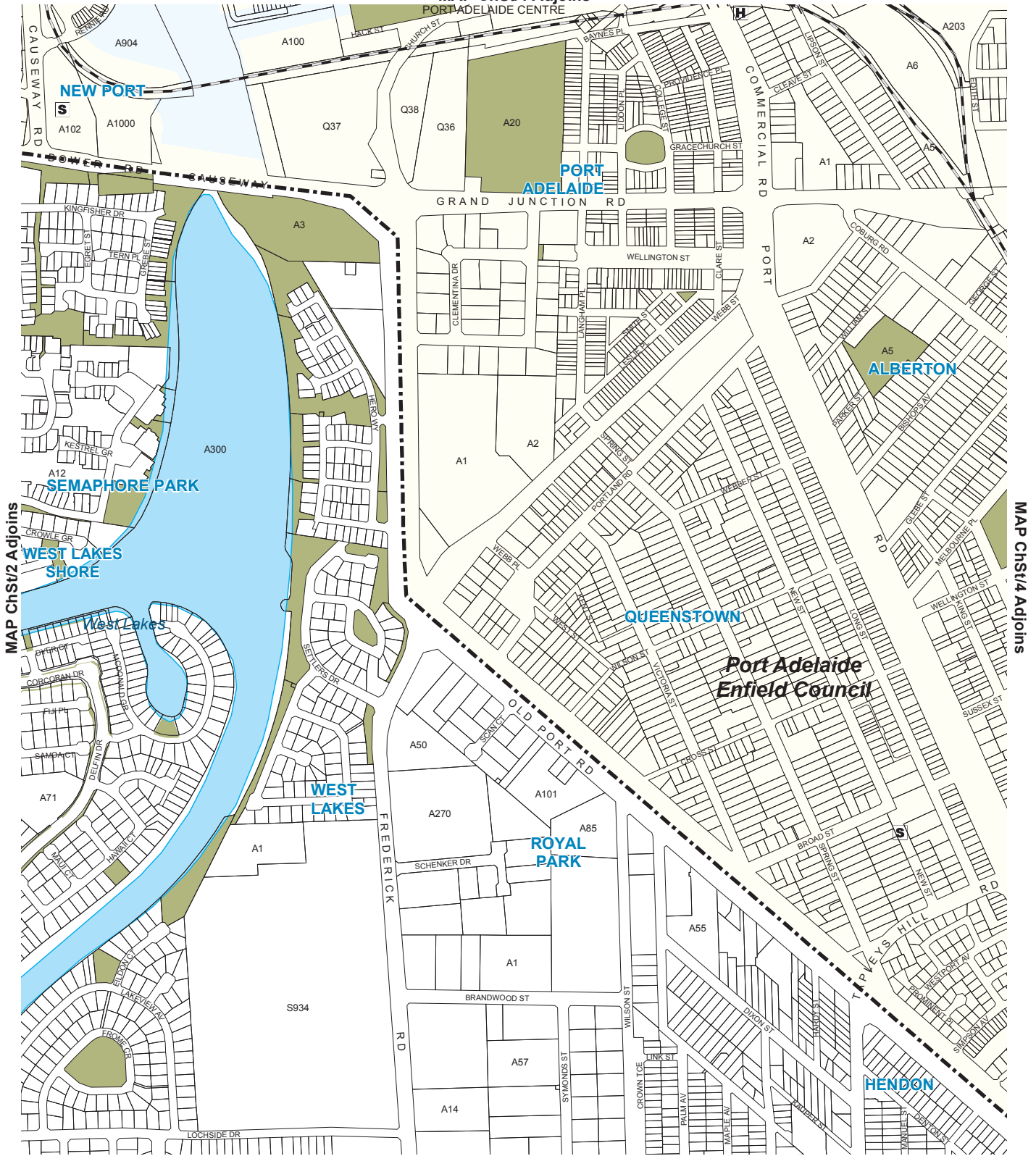


Precinct Map ChSt/2

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

PORT ADELAIDE CENTRE



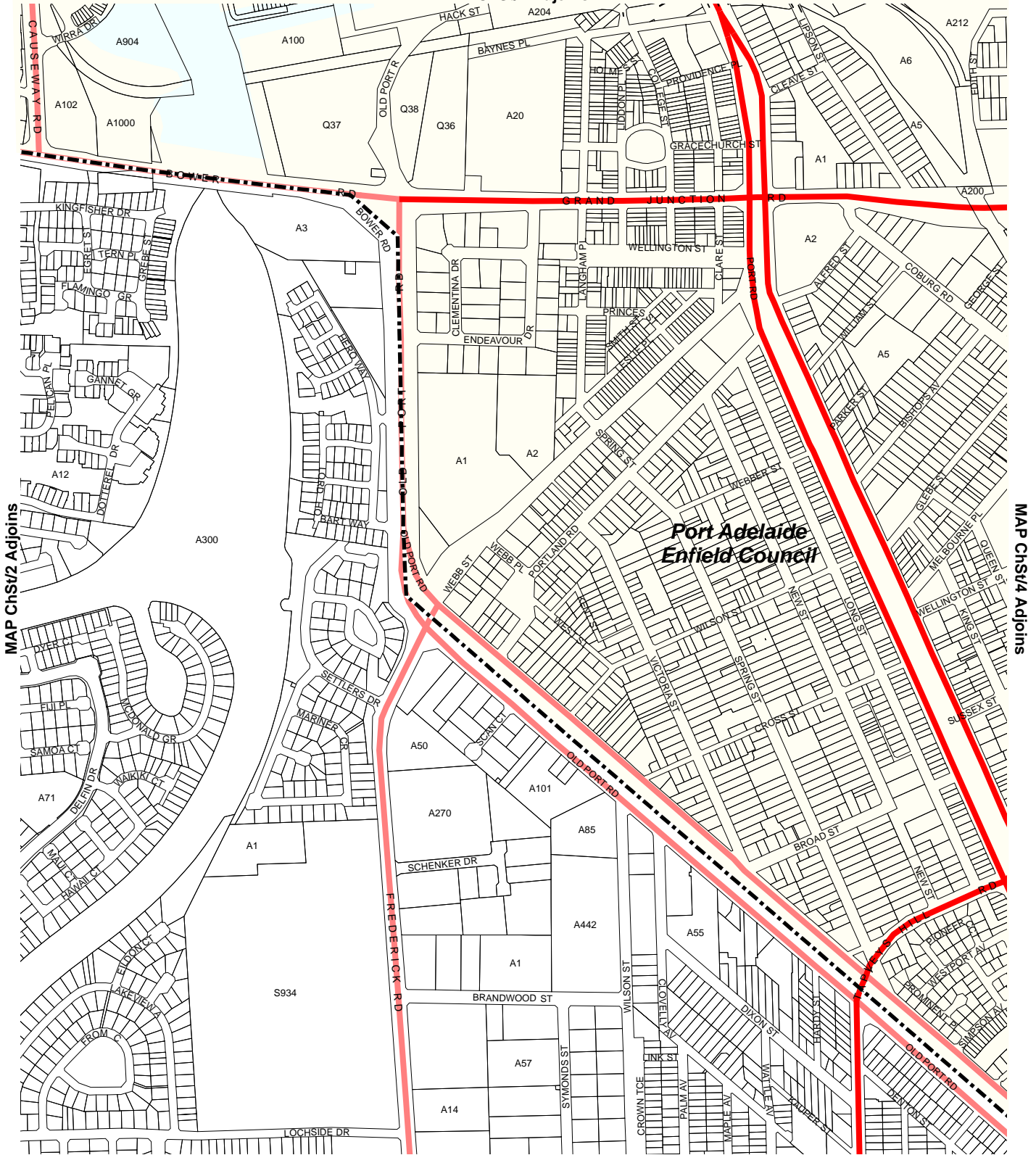
MAP ChSt/7 Adjoins

Location Map ChSt/3

- S** School
- H** Other Health Services
- Railways
- Local Reserves
- Waterbodies
- Development Plan Boundary



MAP ChSt/1 Adjoins



MAP ChSt/2 Adjoins

MAP ChSt/4 Adjoins

MAP ChSt/7 Adjoins

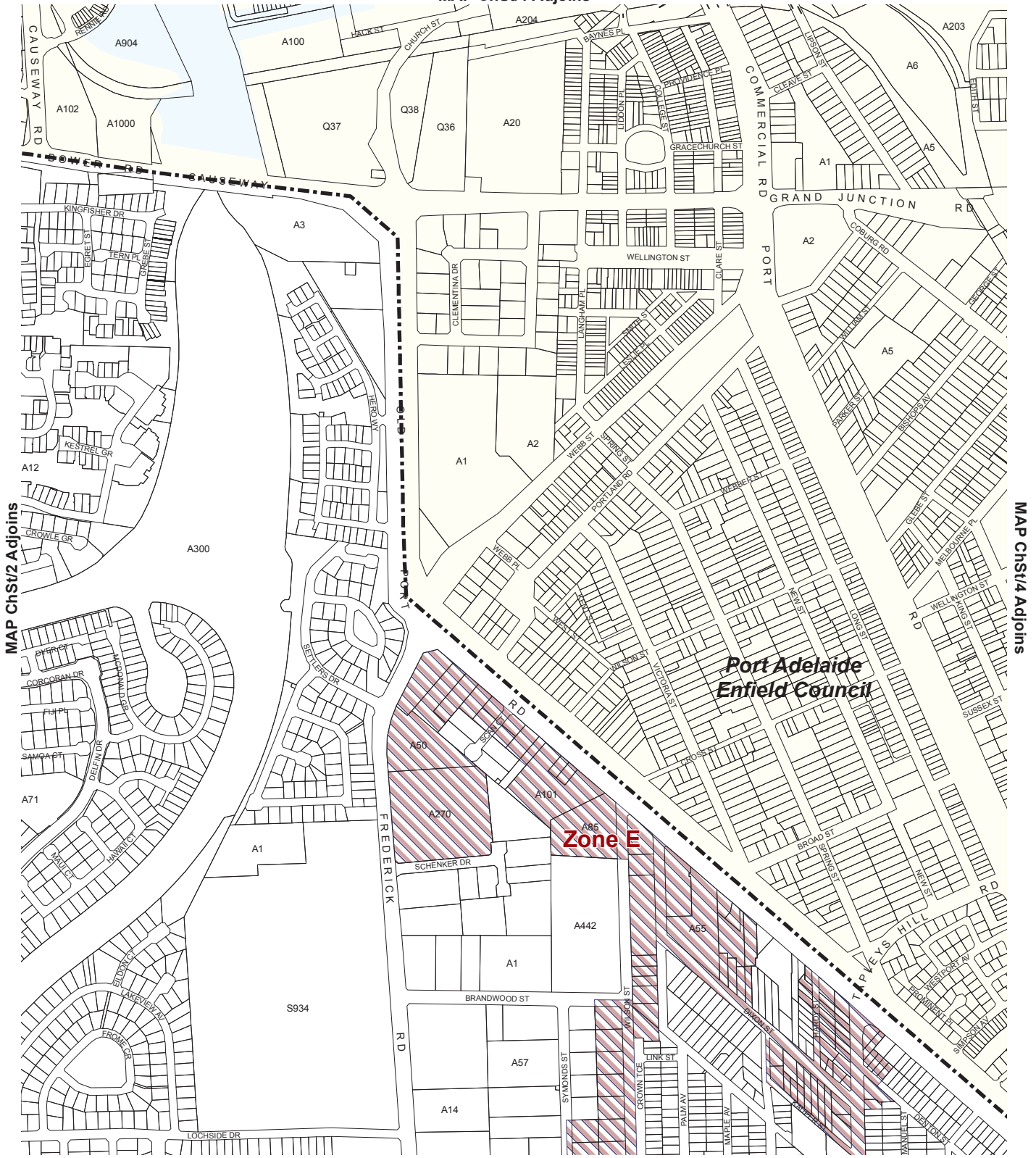


Overlay Map ChSt/3

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Development Plan Boundary




MAP ChSt/1 Adjoins



MAP ChSt/7 Adjoins

Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

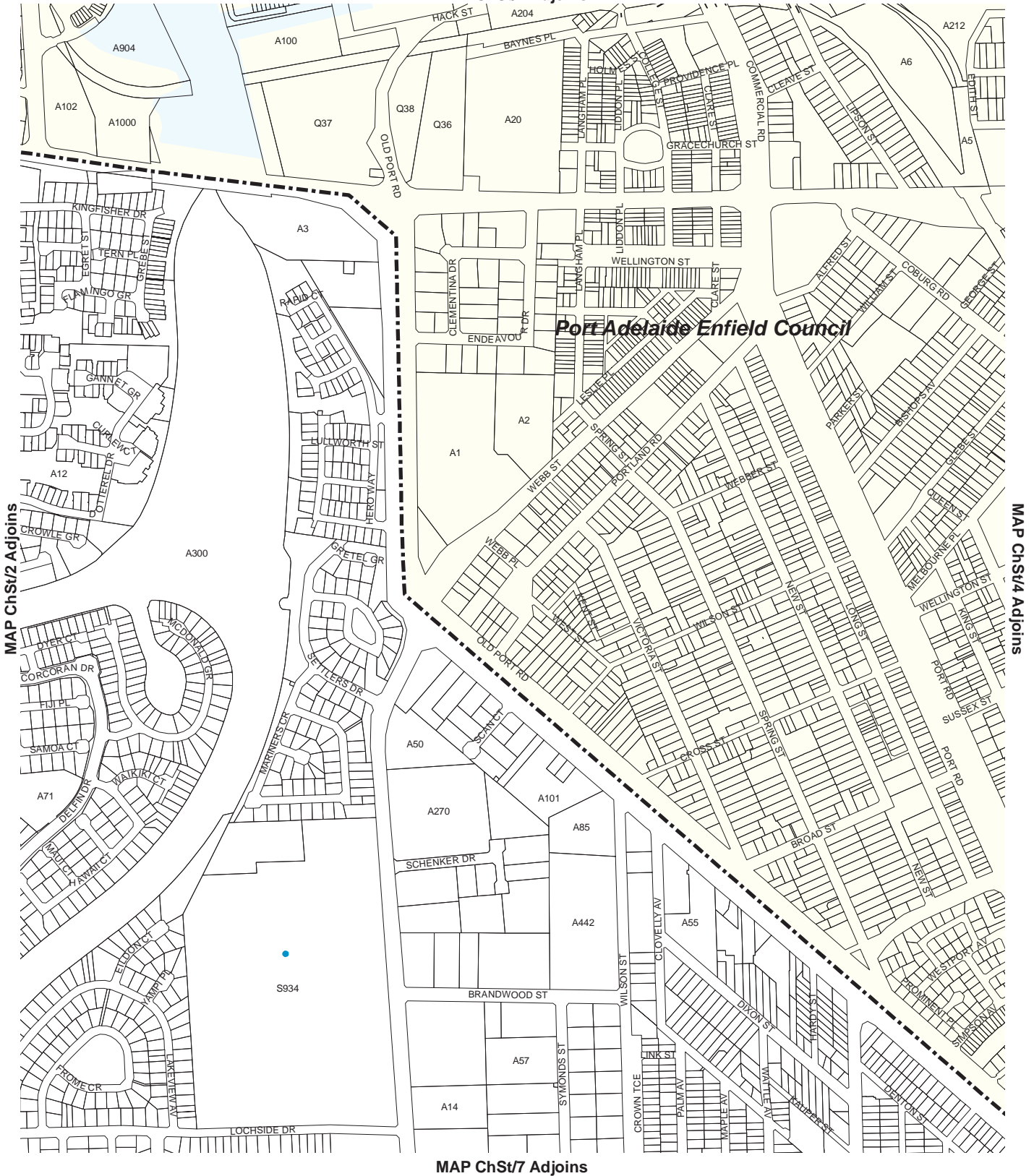


-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary

Overlay Map ChSt/3

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



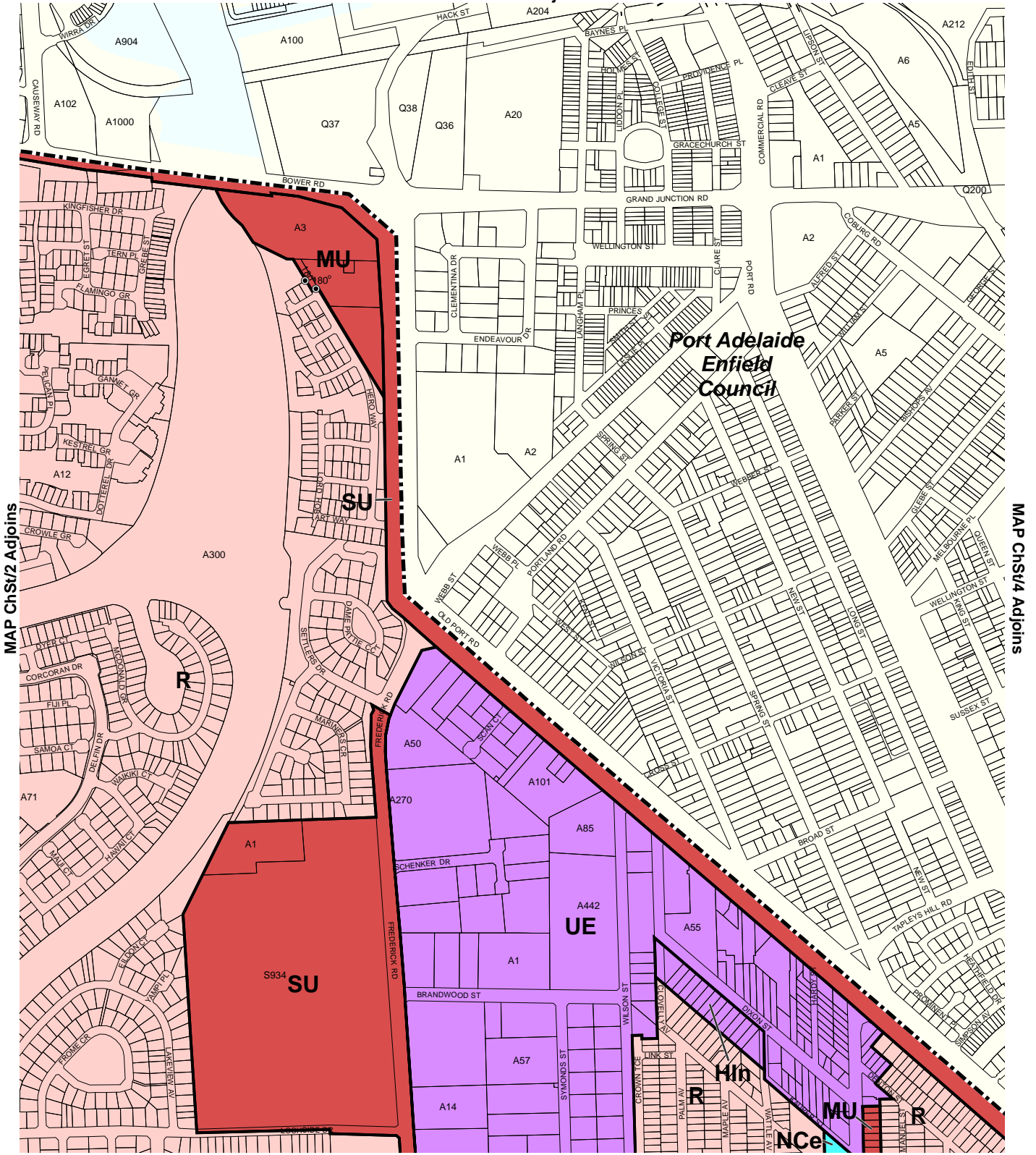
Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.



Overlay Map ChSt/3 HERITAGE

- Local heritage place
- Development Plan Boundary

MAP ChSt/1 Adjoins



MAP ChSt/7 Adjoins

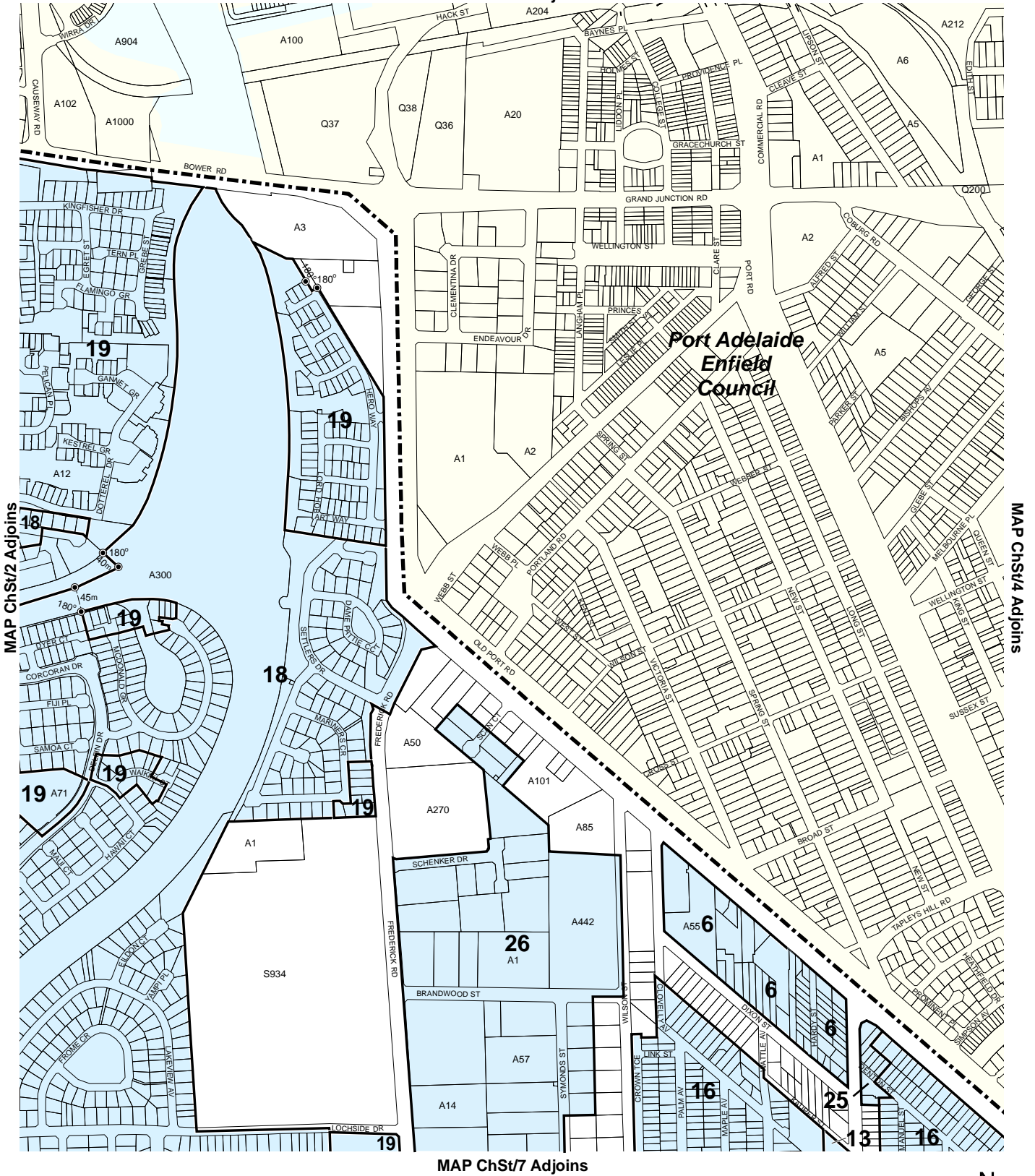
Lamberts Conformal Conic Projection, GDA94

- Zones**
- HIn Home Industry
 - MU Mixed Use
 - NCe Neighbourhood Centre
 - R Residential
 - SU Special Use
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary



Zone Map ChSt/3

MAP ChSt/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

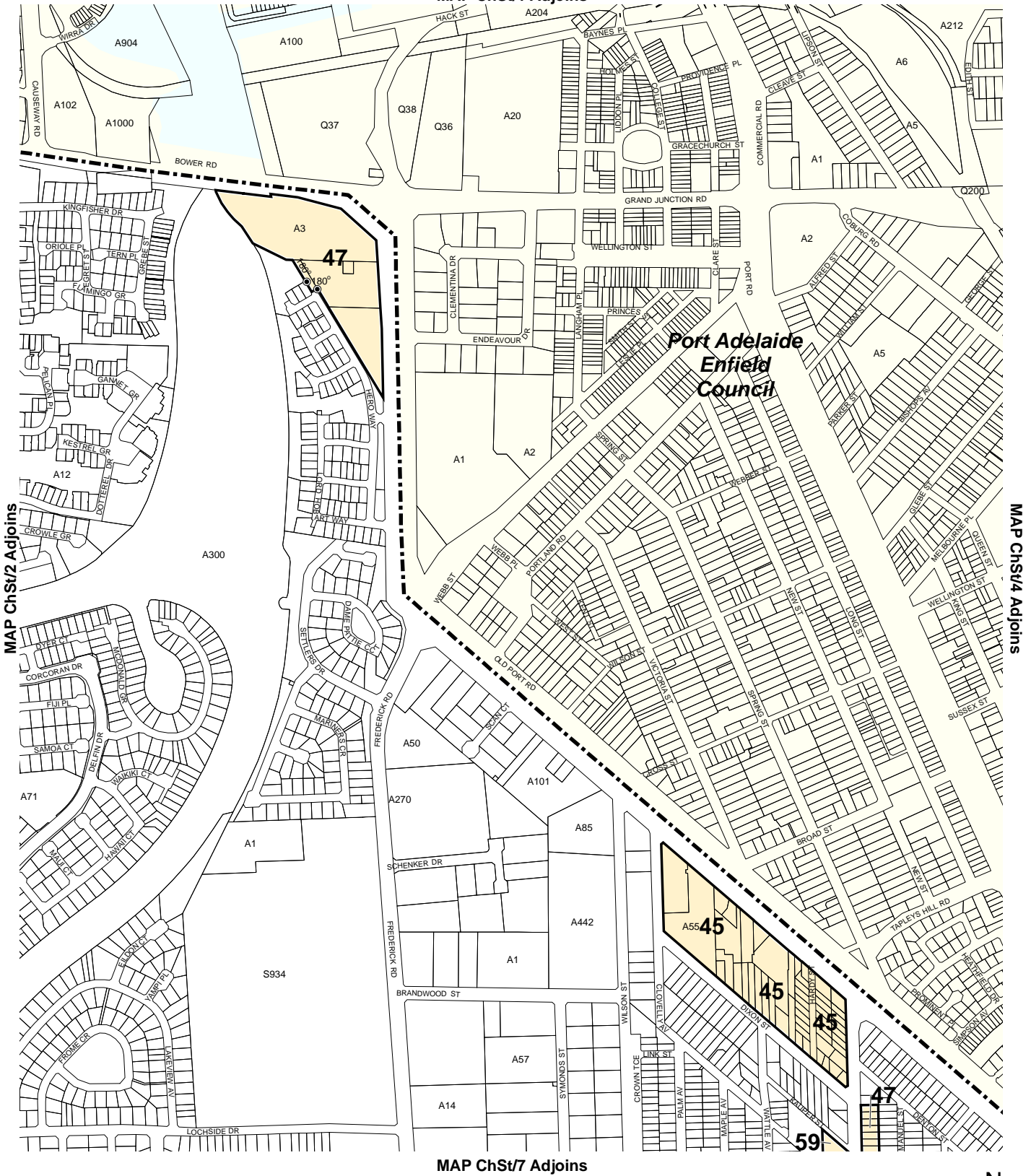
Policy Area

- 6 Bulky Goods
- 13 Royal Park
- 16 Mid Suburban
- 18 West Lakes General
- 19 West Lakes Medium Density
- 25 Main Road Commercial
- 26 Core Industry

- Policy Area Boundary
- Development Plan Boundary

Policy Area Map ChSt/3

MAP ChSt/1 Adjoins



MAP ChSt/2 Adjoins

MAP ChSt/4 Adjoins

MAP ChSt/7 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

- 45 Old Port Road
- 47 Mixed Use
- 59 Retail Core Royal Park

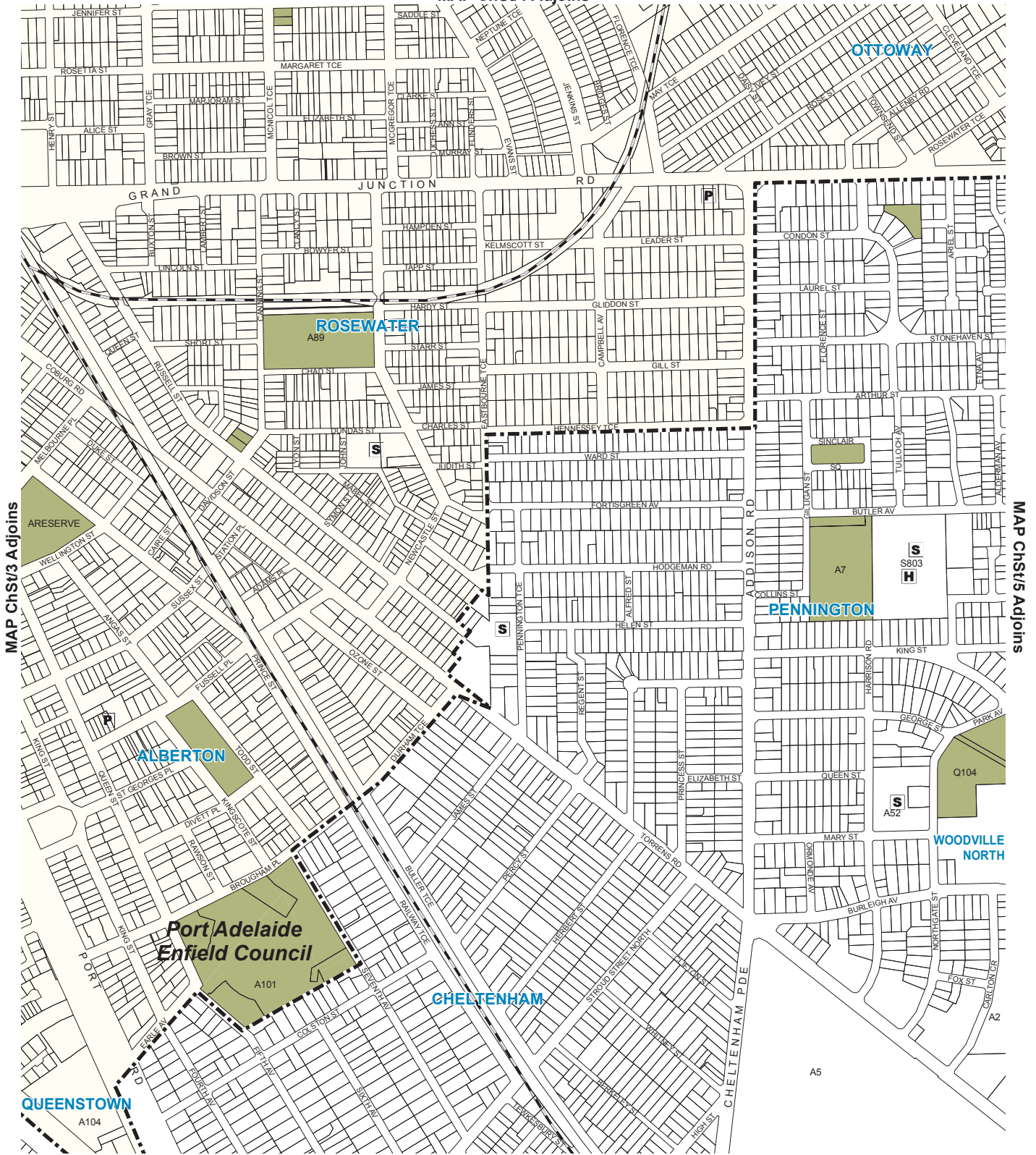


Precinct Map ChSt/3

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

OTTOWAY



MAP ChSt/8 Adjoins

MAP ChSt/5 Adjoins

MAP ChSt/3 Adjoins

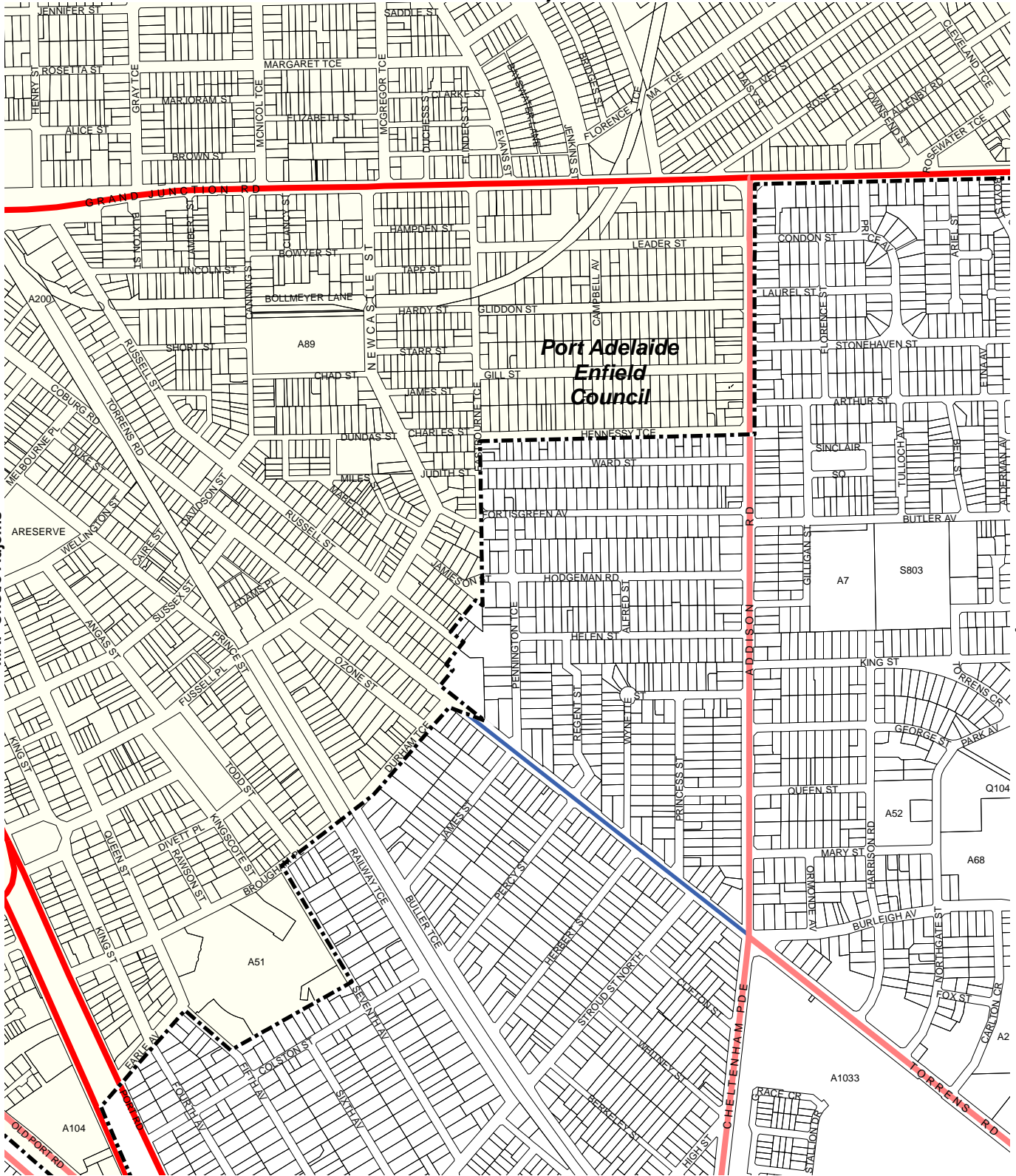


0 500 m

- S School
- P Post Office
- H Other Health Services
- Railways
- Local Reserves
- Development Plan Boundary

Location Map ChSt/4

MAP ChSt/1 Adjoins



MAP ChSt/8 Adjoins

MAP ChSt/5 Adjoins

MAP ChSt/3 Adjoins

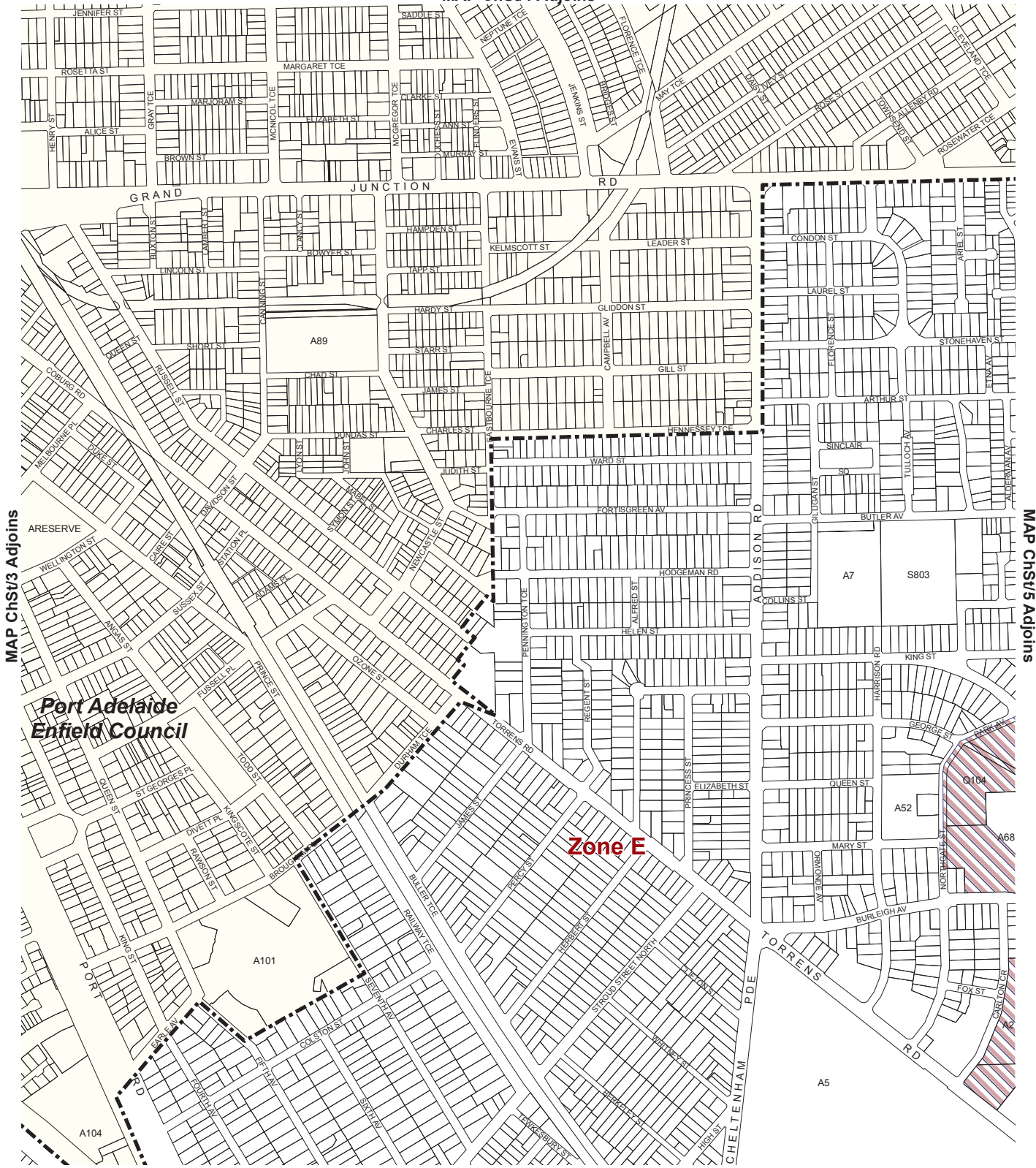


Overlay Map ChSt/4

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/1 Adjoins



Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

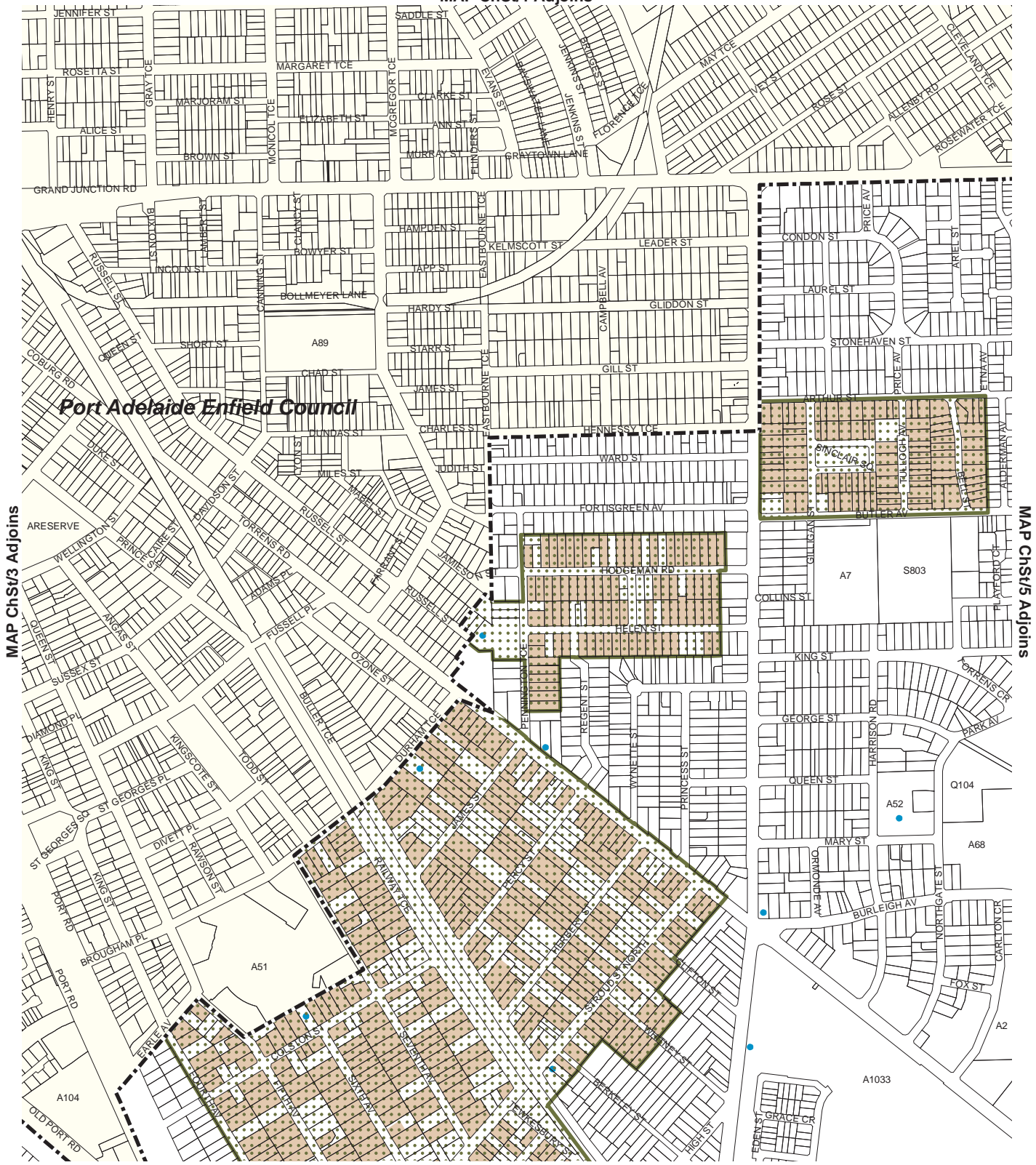


- Airport Building Heights
- Industry Interface Area
- Development Plan Boundary

Overlay Map ChSt/4

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.

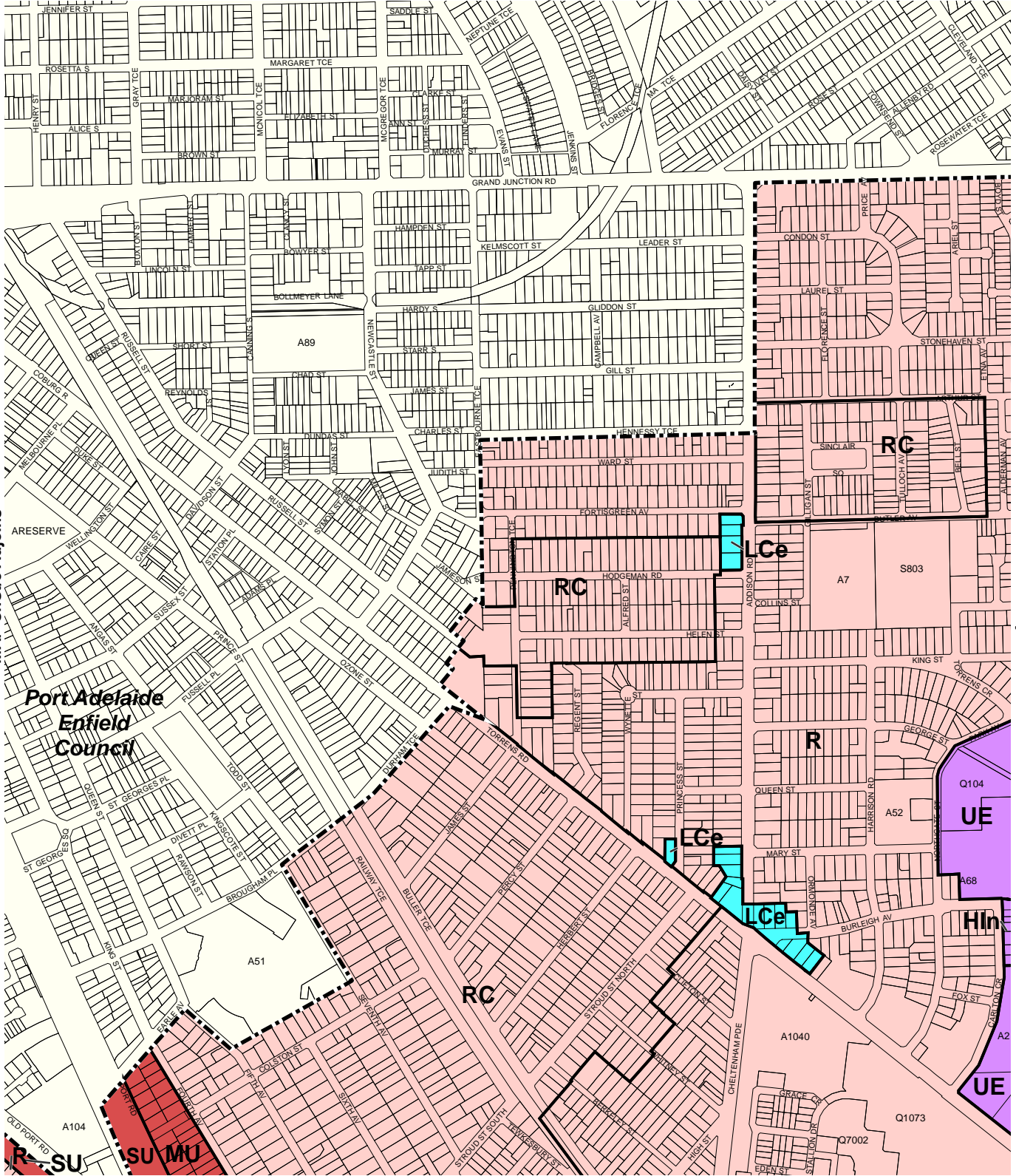


- Local heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/4

HERITAGE

MAP ChSt/1 Adjoins



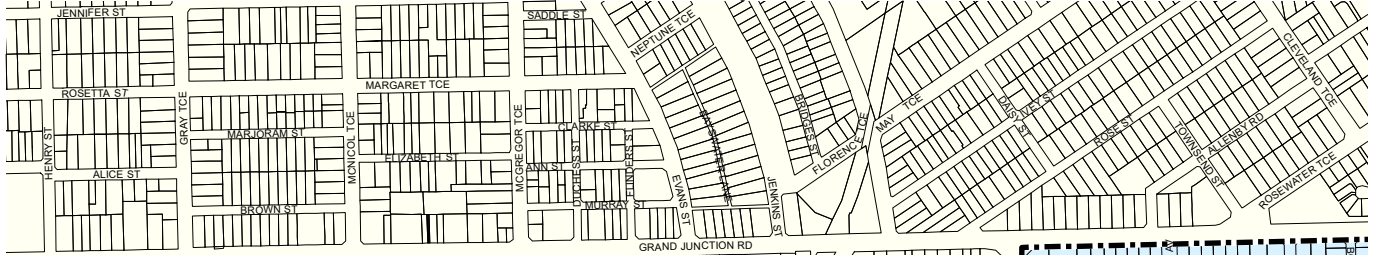
Lamberts Conformal Conic Projection, GDA94

- Zones**
- HIn Home Industry
 - LCe Local Centre
 - MU Mixed Use
 - R Residential
 - RC Residential Character
 - SU Special Use
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary

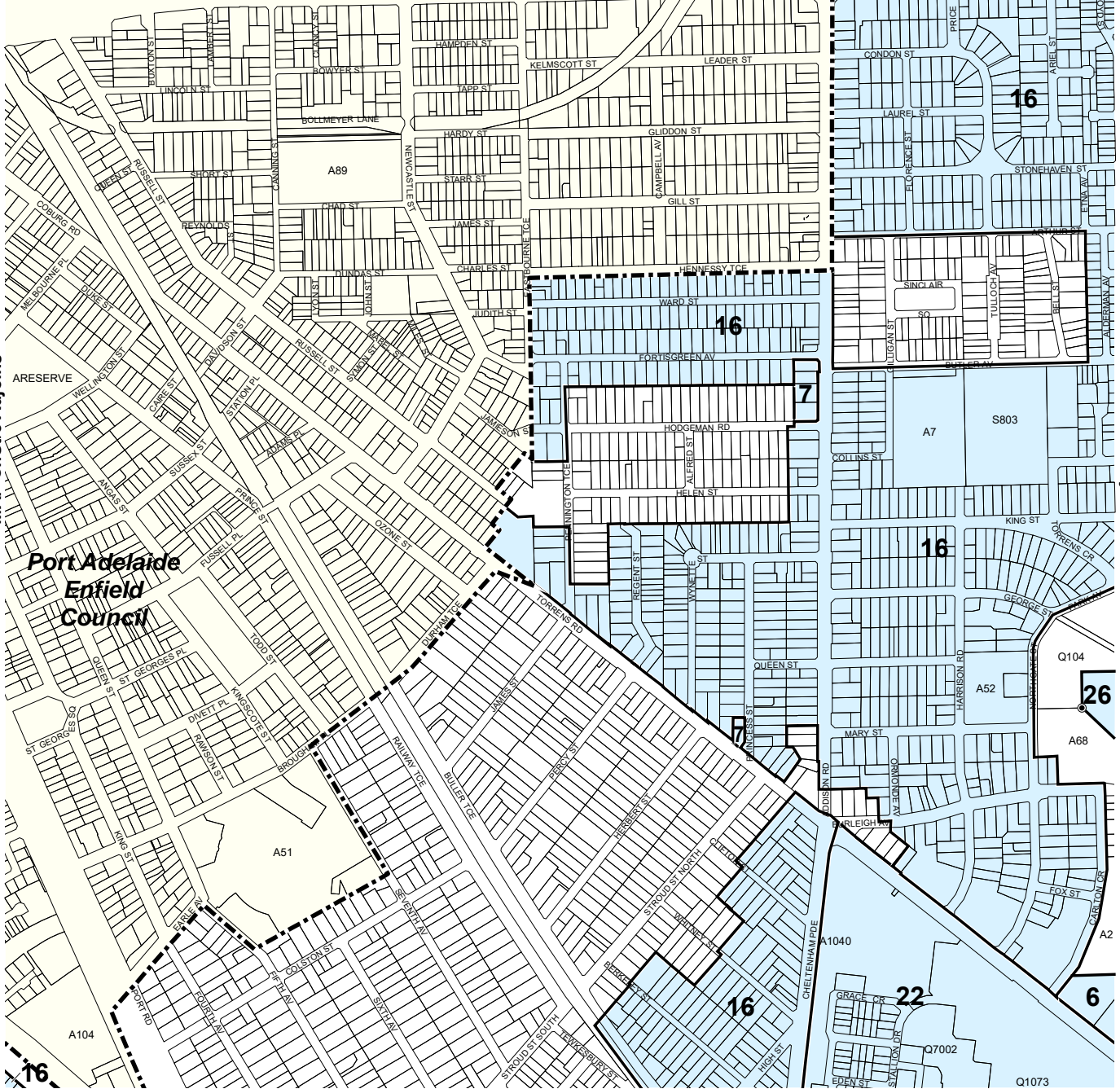


Zone Map ChSt/4

MAP ChSt/1 Adjoins



MAP ChSt/8 Adjoins



MAP ChSt/3 Adjoins

MAP ChSt/5 Adjoins

Port Adelaide
Enfield
Council

Lamberts Conformal Conic Projection, GDA94

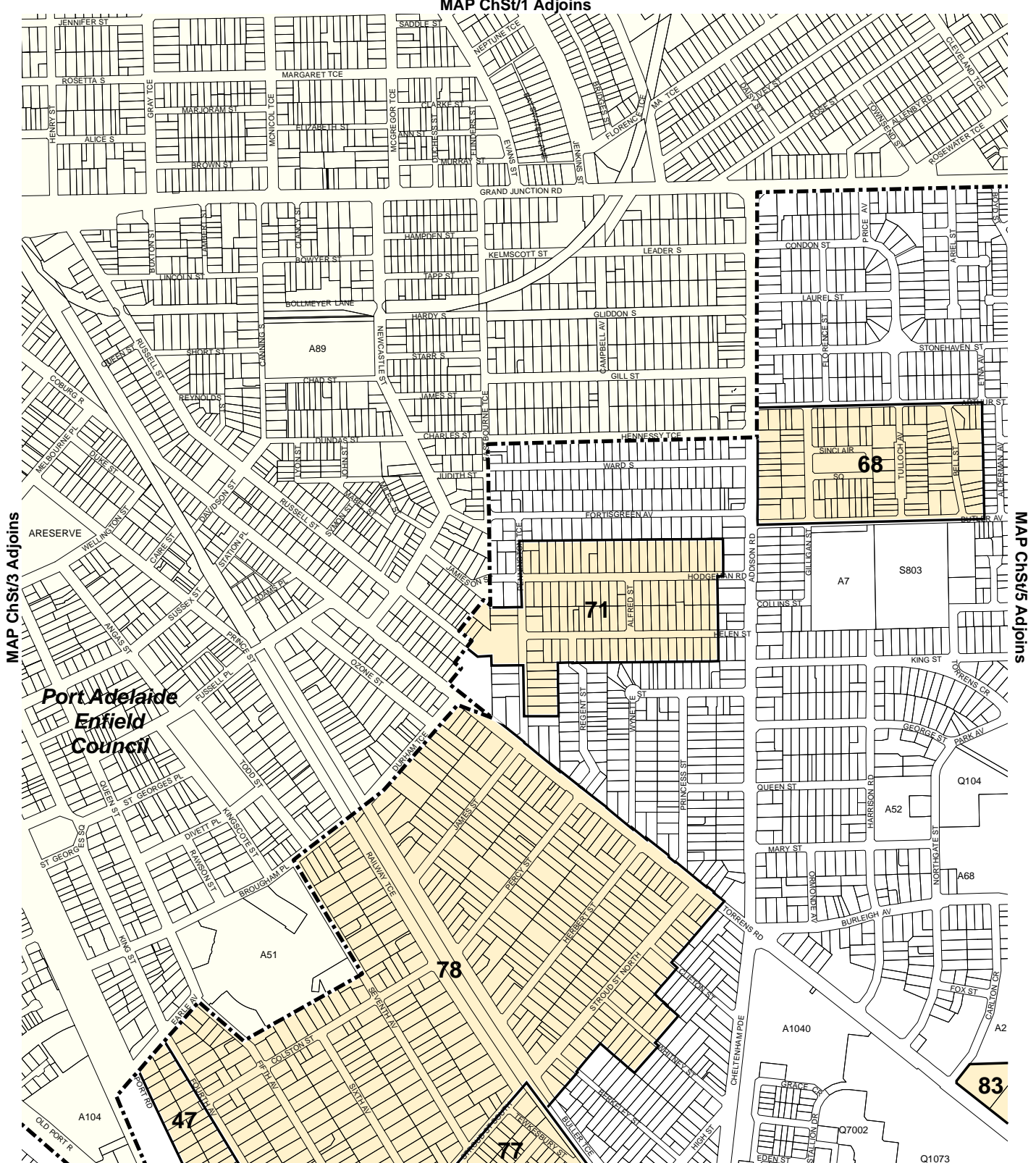
- Policy Area**
- 6 Bulky Goods
 - 7 Local Shopping
 - 16 Mid Suburban
 - 22 Cheltenham Park
 - 26 Core Industry



Policy Area Map ChSt/4

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins



MAP ChSt/8 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

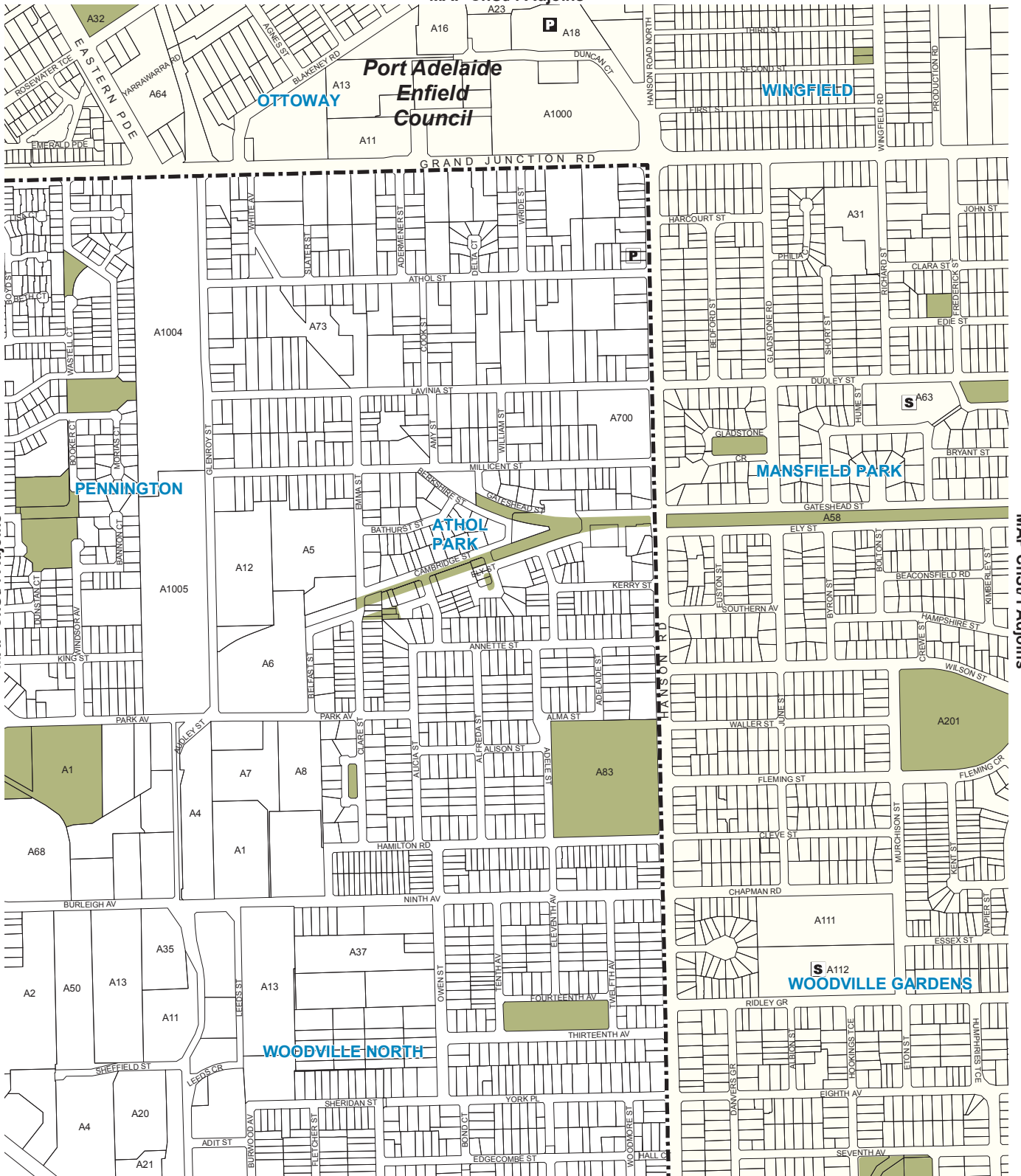
- 47 Mixed Use
- 68 Early SA Housing Trust
- 71 Pre World War One
- 77 Cheltenham East
- 78 Cheltenham West
- 83 Woodville North



Precinct Map ChSt/4

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins



MAP ChSt/9 Adjoins

MAP ChSt/1 Adjoins

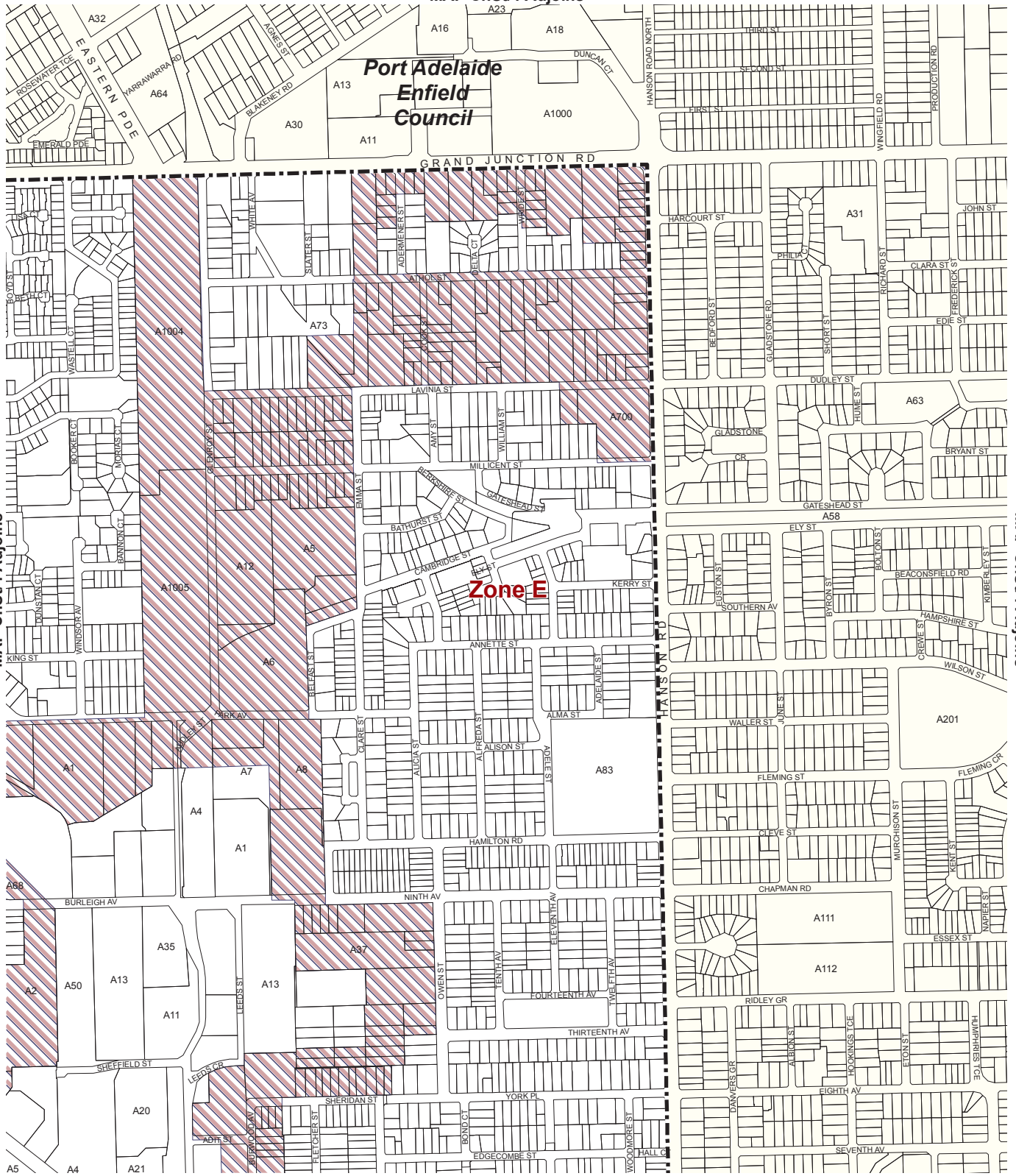
MAP ChSt/4 Adjoins




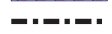
- S** School
- P** Post Office
- P** Police Station
- Local Reserves
- Development Plan Boundary

Location Map ChSt/5

MAP ChSt/1 Adjoins



Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

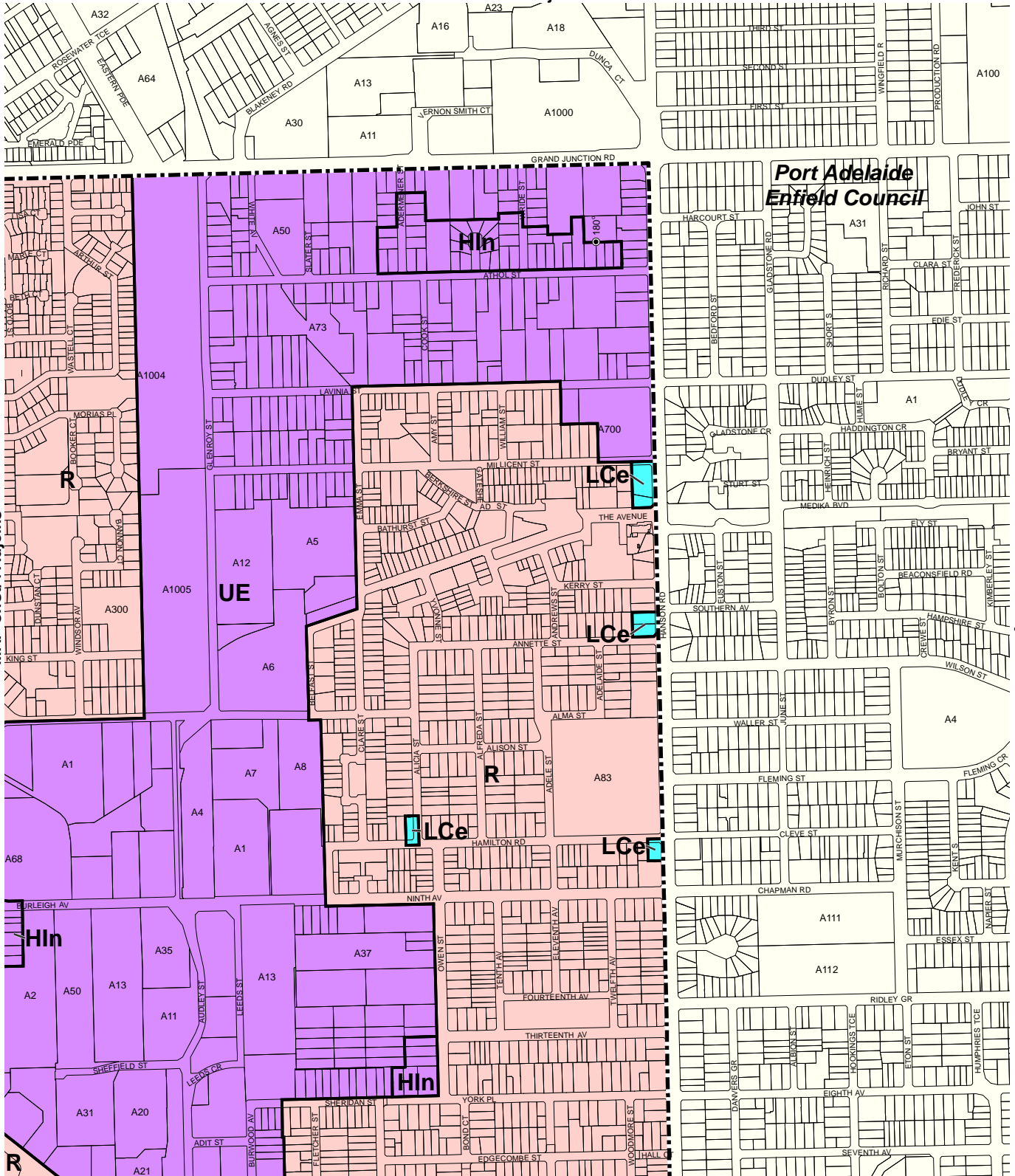
-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary



Overlay Map ChSt/5

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



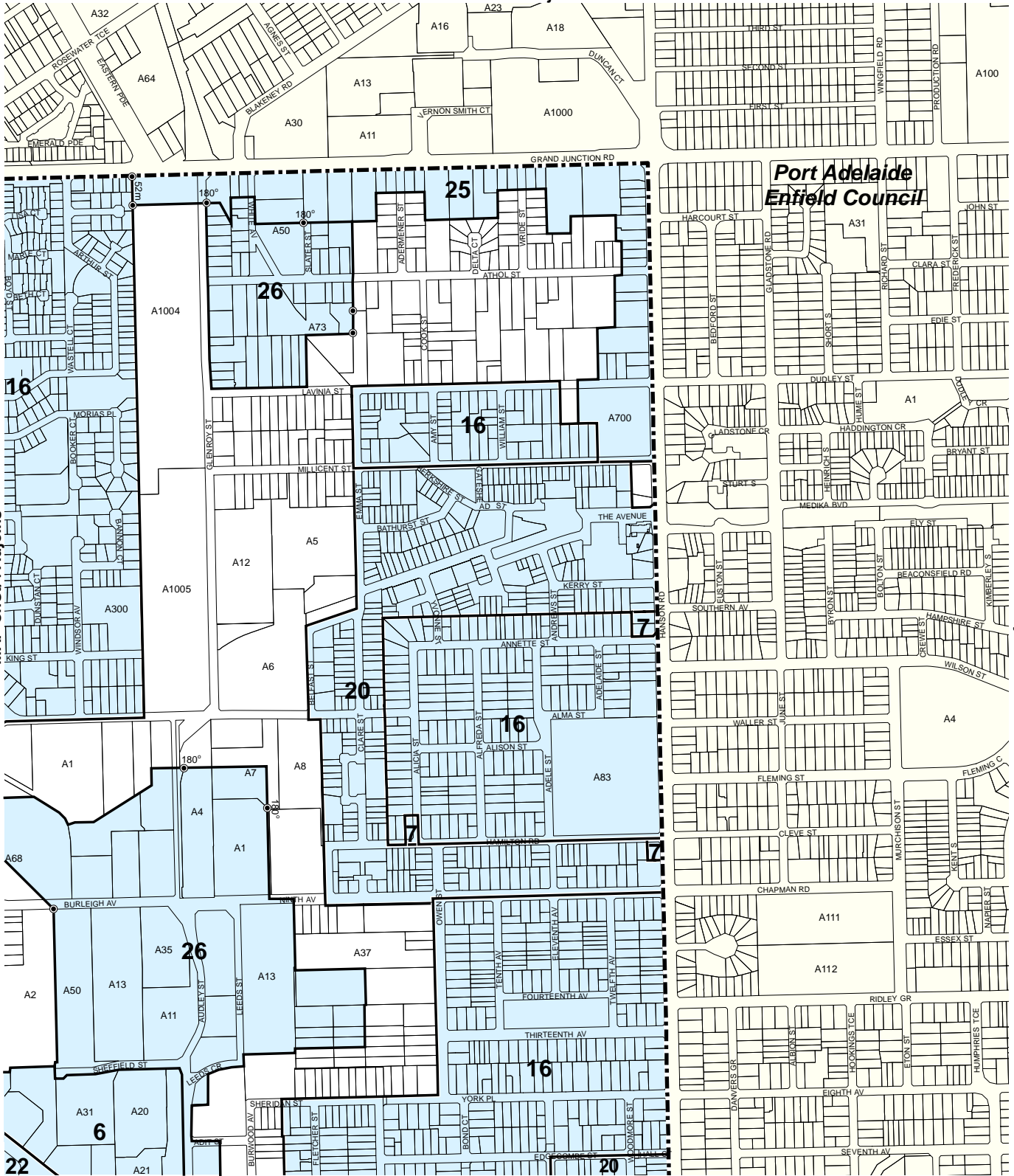
Lamberts Conformal Conic Projection, GDA94



- Zones**
- HIn Home Industry
 - LCe Local Centre
 - R Residential
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary

Zone Map ChSt/5

MAP ChSt/1 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 6 Bulky Goods
- 7 Local Shopping
- 16 Mid Suburban
- 20 Integrated Medium Density
- 22 Cheltenham Park
- 25 Main Road Commercial
- 26 Core Industry

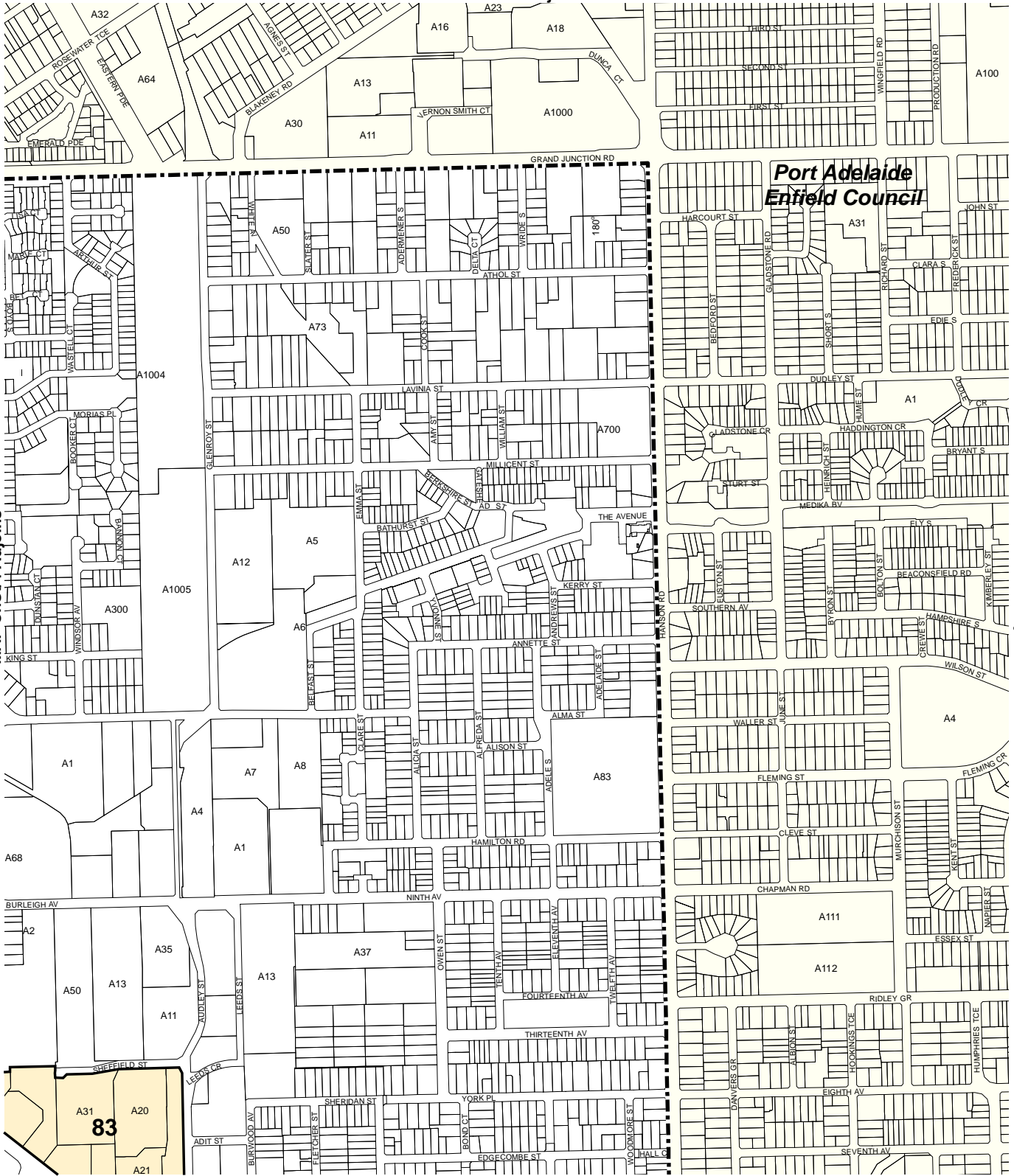


0 500m

- Policy Area Boundary
- Development Plan Boundary

Policy Area Map ChSt/5

MAP ChSt/1 Adjoins



Port Adelaide
Enfield Council

MAP ChSt/4 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/9 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

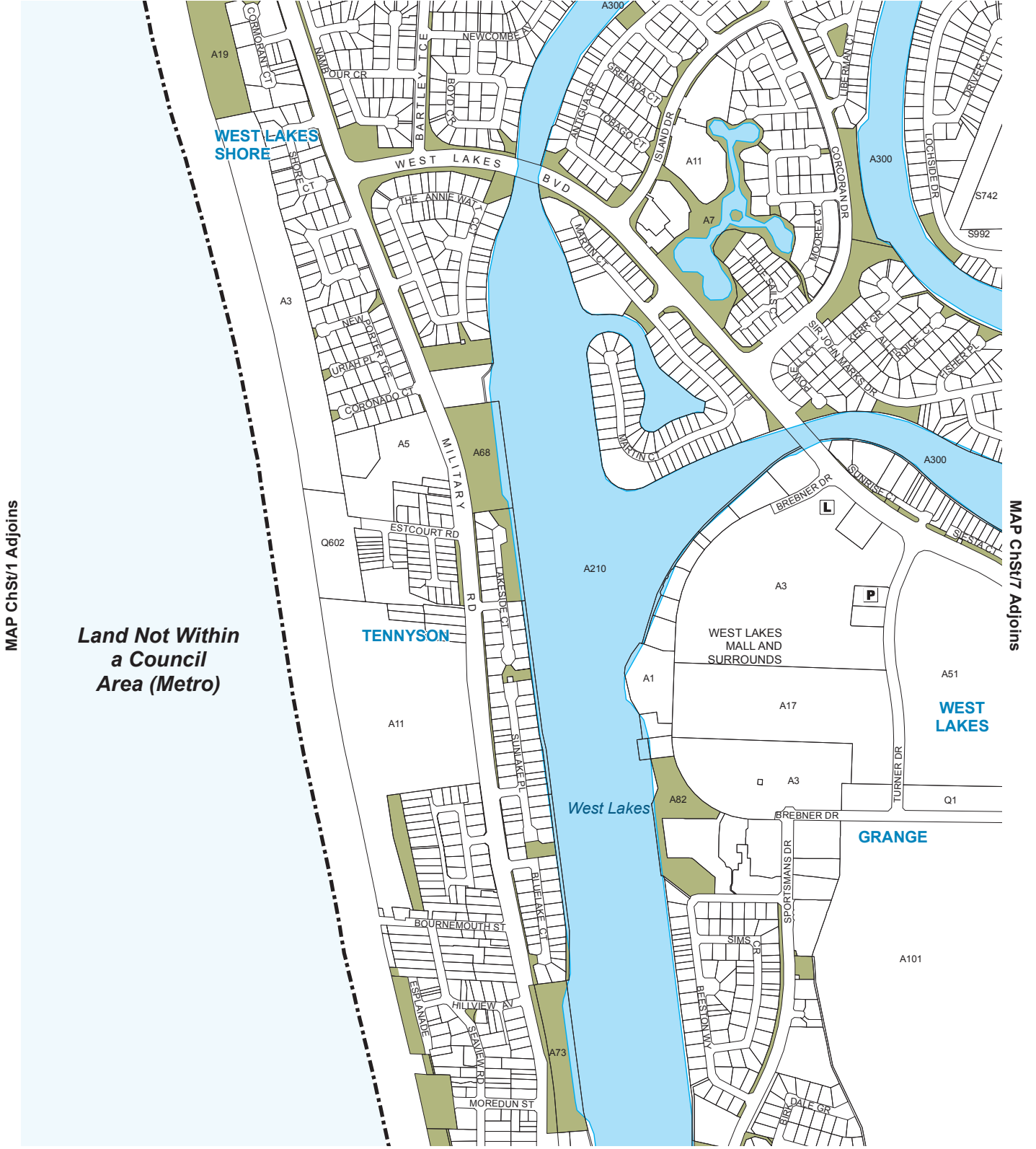
83 Woodville North



Precinct Map ChSt/5

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/2 Adjoins



*Land Not Within
a Council
Area (Metro)*

MAP ChSt/1 Adjoins

MAP ChSt/7 Adjoins

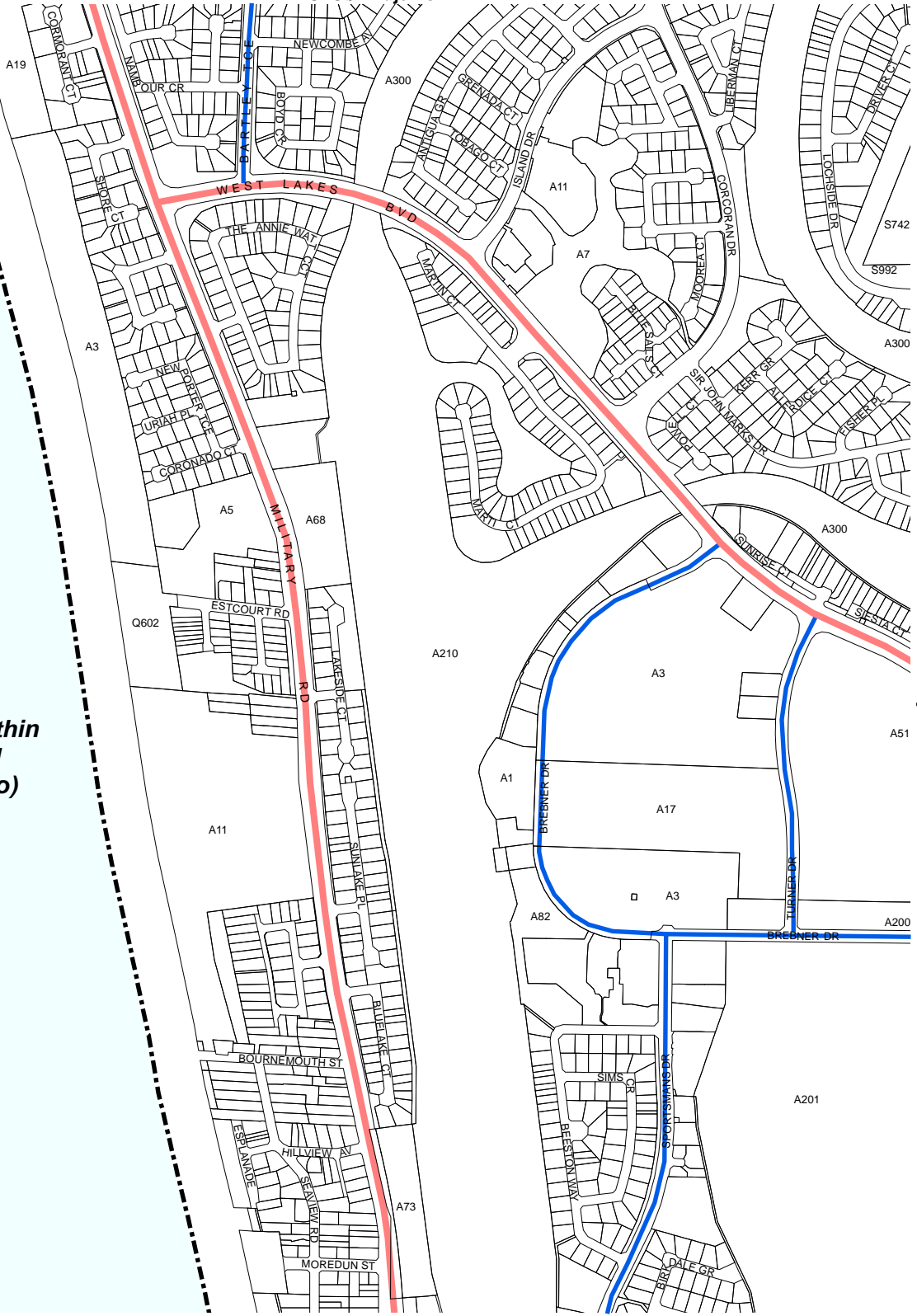
MAP ChSt/11 Adjoins



- L Public Library
- P Post Office
- Local Reserves
- Waterbodies
- Development Plan Boundary

Location Map ChSt/6

MAP ChSt/2 Adjoins



MAP ChSt/1 Adjoins

*Land Not Within
a Council
Area (Metro)*

MAP ChSt/11 Adjoins

MAP ChSt/7 Adjoins



Overlay Map ChSt/6 TRANSPORT

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/2 Adjoins

MAP ChSt/1 Adjoins

*Land Not Within
a Council
Area (Metro)*



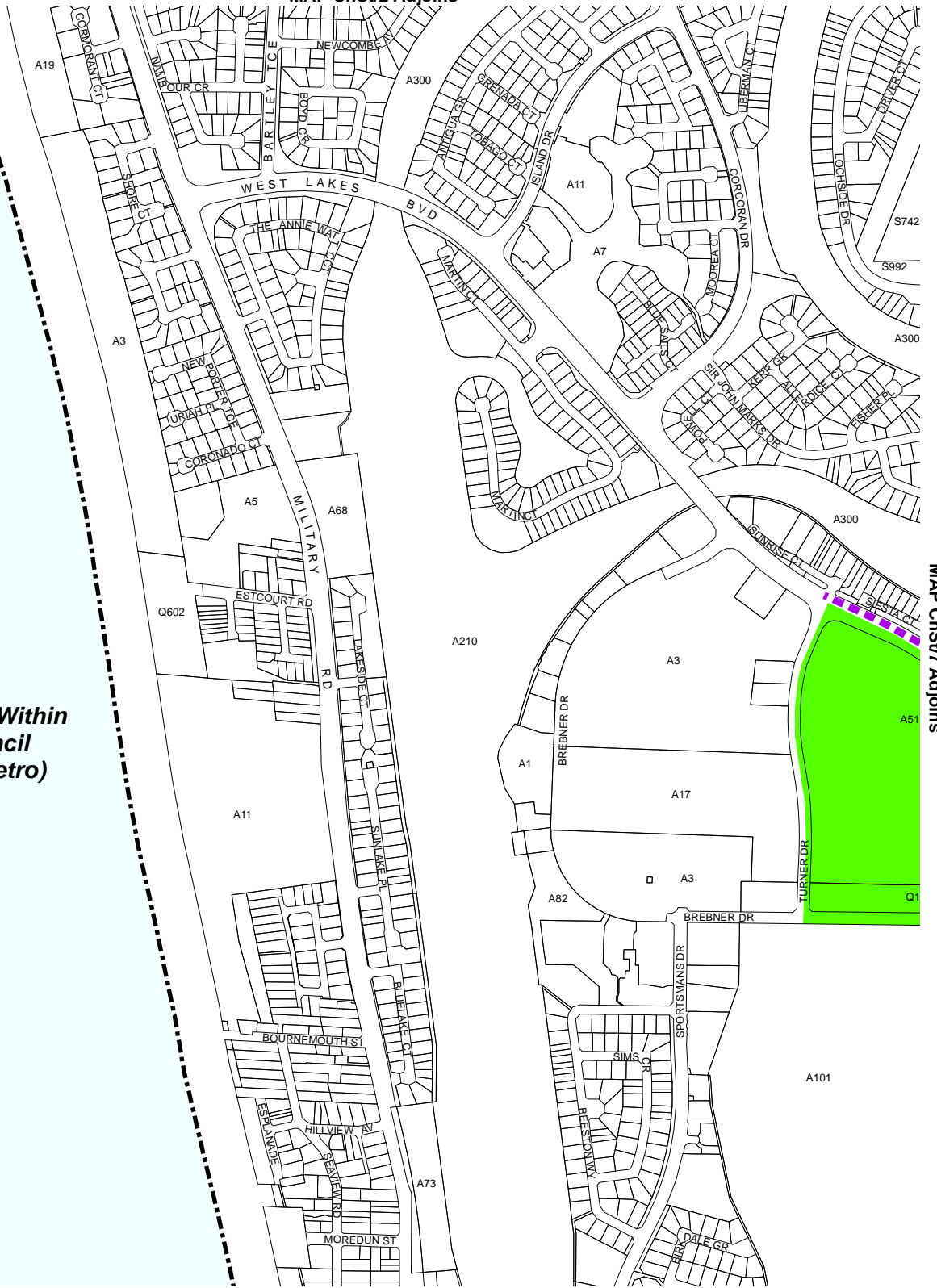
Heritage points are indicative only.
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Items please refer to the relevant tables within this document.



Overlay Map ChSt/6 HERITAGE

- State heritage place
- Development Plan Boundary

MAP ChSt/2 Adjoins



MAP ChSt/11 Adjoins




*Land Not Within
a Council
Area (Metro)*

MAP ChSt/1 Adjoins

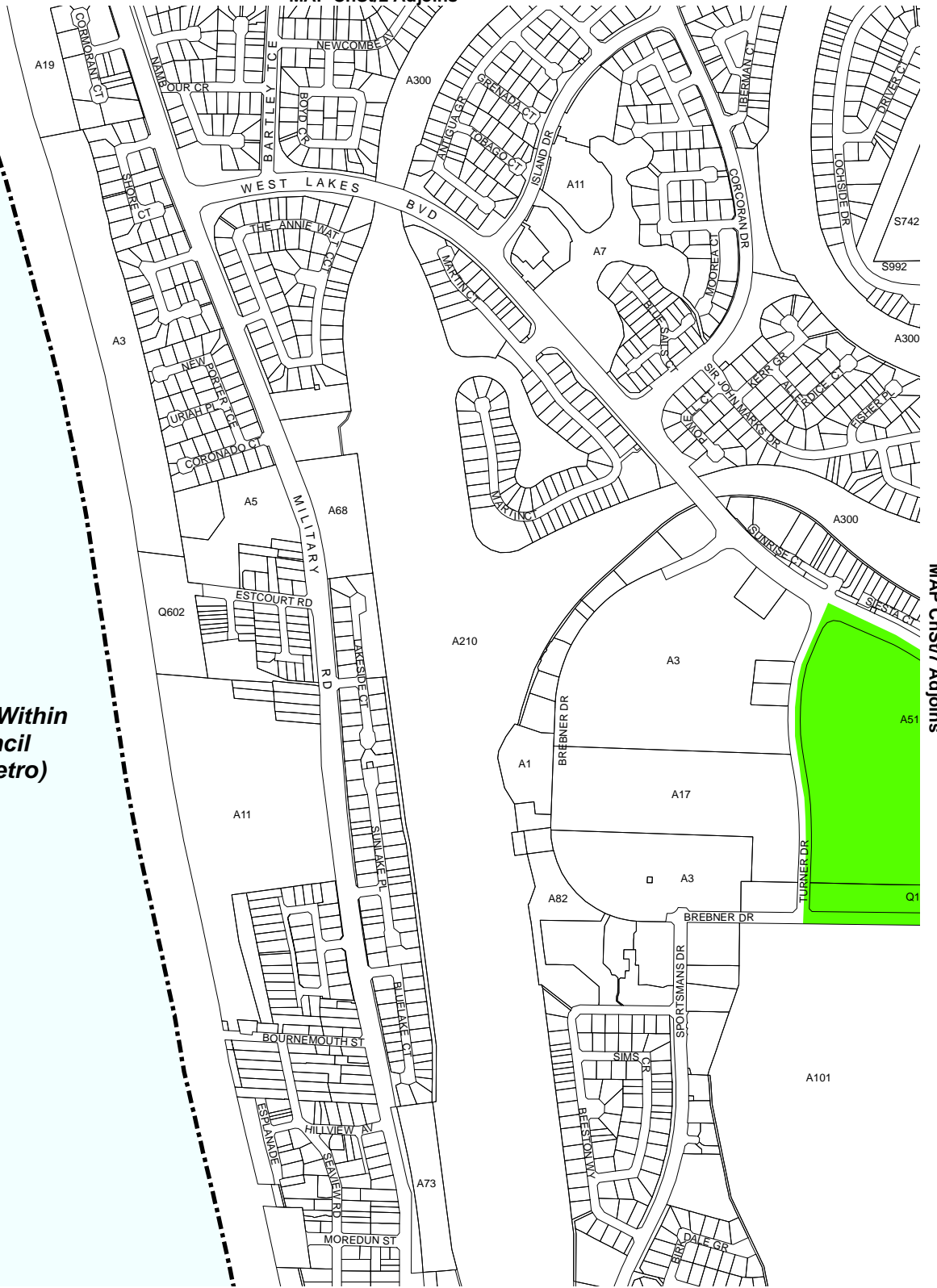
MAP ChSt/7 Adjoins



Overlay Map ChSt/6 NOISE AND AIR EMISSIONS

-  Proposed Tram Line
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary

MAP ChSt/2 Adjoins



*Land Not Within
a Council
Area (Metro)*

MAP ChSt/11 Adjoins

MAP ChSt/7 Adjoins

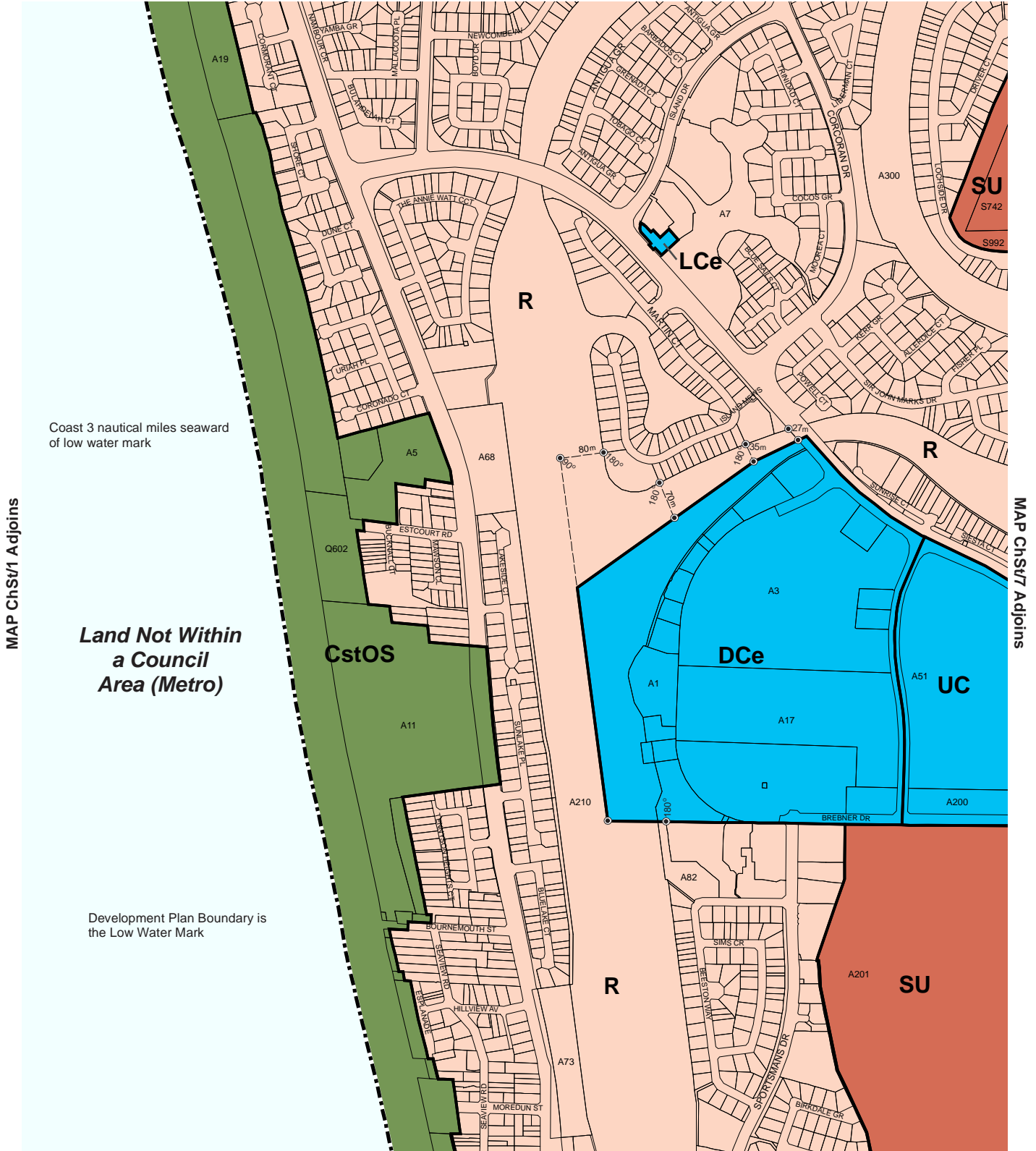


0 500m

Overlay Map ChSt/6 AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP ChSt/2 Adjoins



Coast 3 nautical miles seaward of low water mark

Land Not Within a Council Area (Metro)

Development Plan Boundary is the Low Water Mark

MAP ChSt/11 Adjoins

Lamberts Conformal Conic Projection, GDA94

Zones

- CstOS Coastal Open Space
- DCe District Centre
- LCe Local Centre
- R Residential
- SU Special Use
- UC Urban Core
- Zone Boundary
- Development Plan Boundary



Zone Map ChSt/6

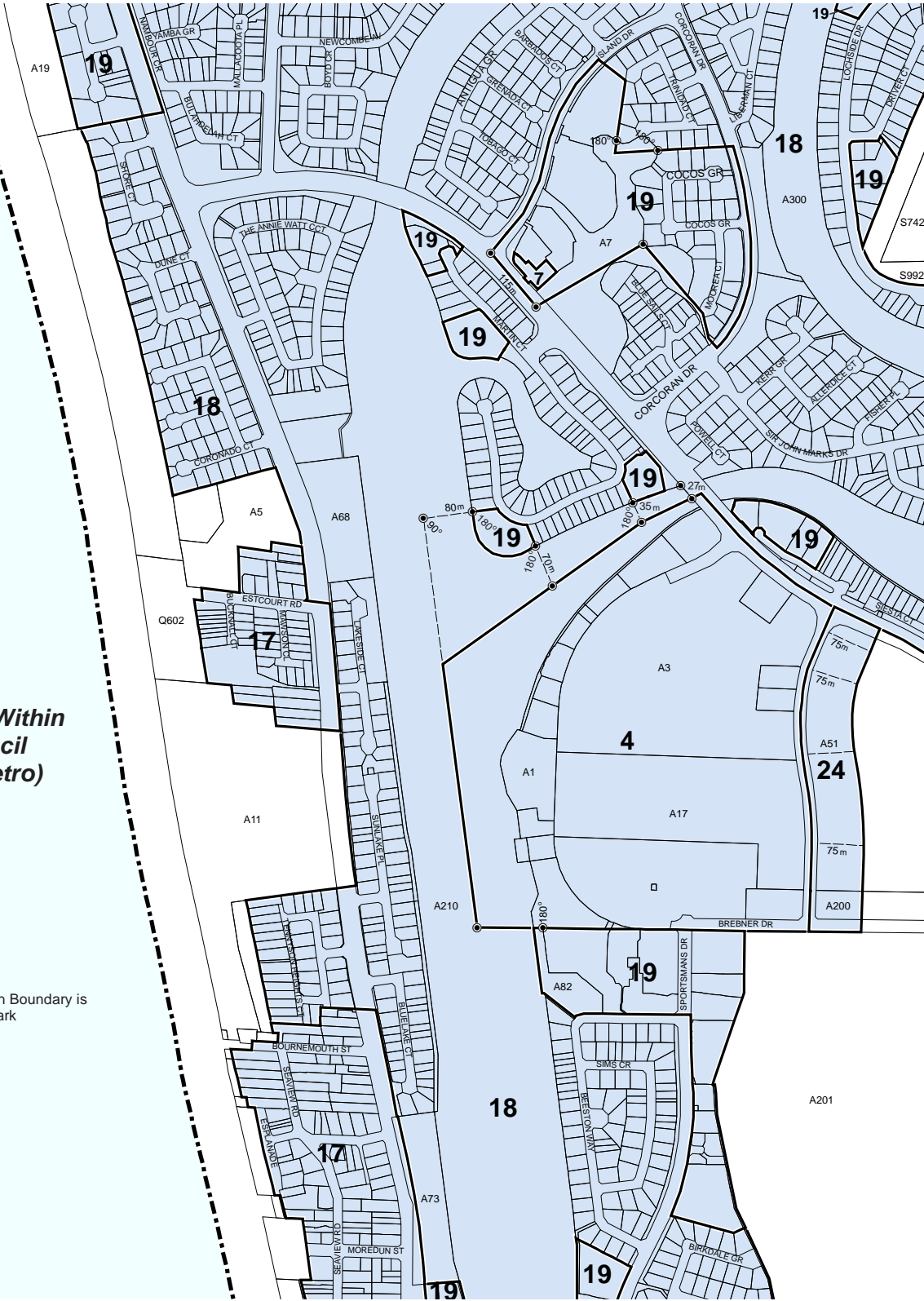
MAP ChSt/2 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/7 Adjoins

**Land Not Within
a Council
Area (Metro)**

Development Plan Boundary is
the Low Water Mark



MAP ChSt/11 Adjoins

Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 17 Western Edge
 - 18 West Lakes General
 - 19 West Lakes Medium Density
 - 24 Main Street
 - 4 West Lakes
 - 7 Local Shopping



Policy Area Map ChSt/6

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/2 Adjoins

MAP ChSt/1 Adjoins

Land Not Within
a Council
Area (Metro)

Development Plan Boundary is
the Low Water Mark

MAP ChSt/11 Adjoins

MAP ChSt/7 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

16 Retail Core West Lakes

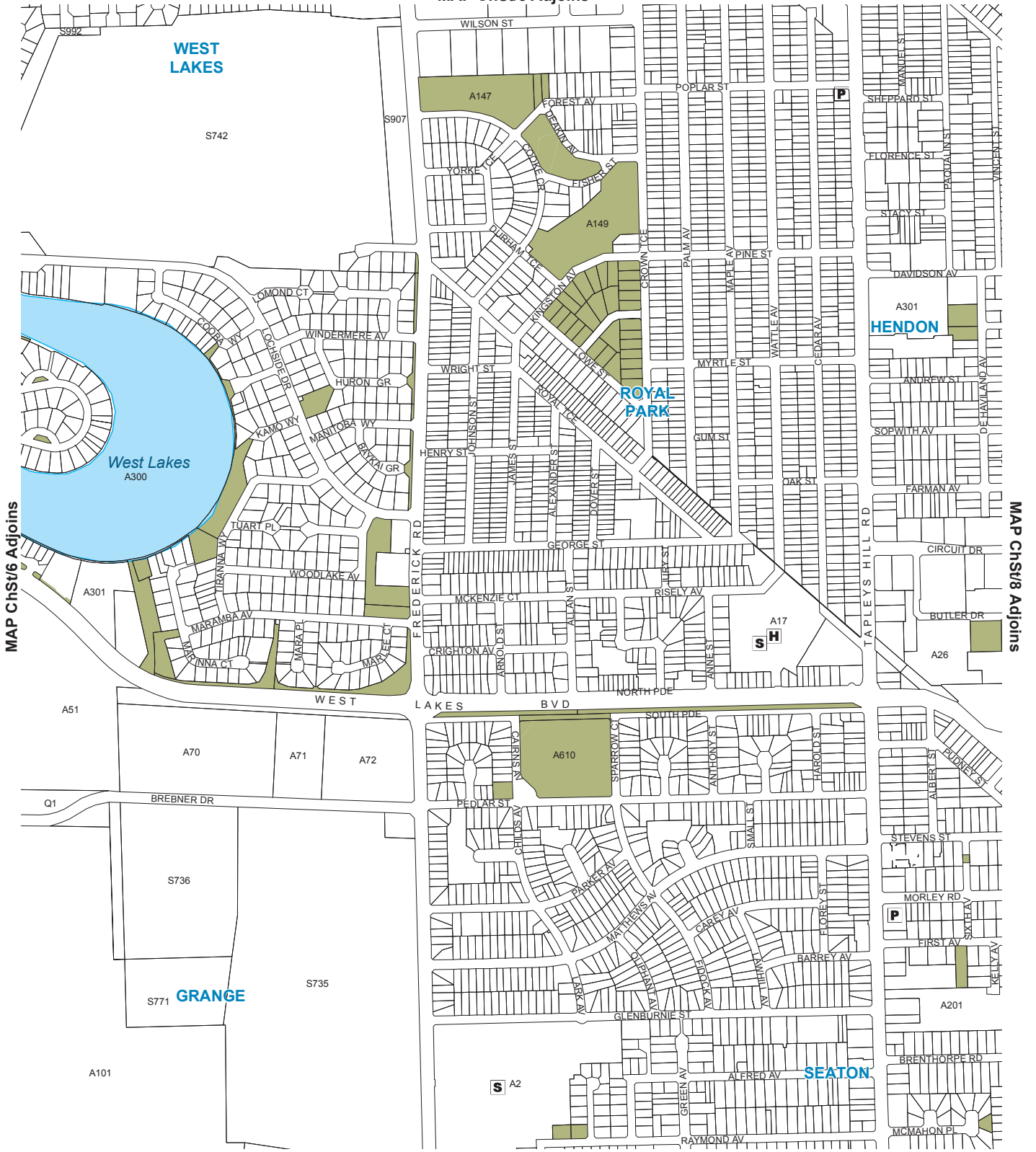
17 Lakefront



Precinct Map ChSt/6

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/3 Adjoins

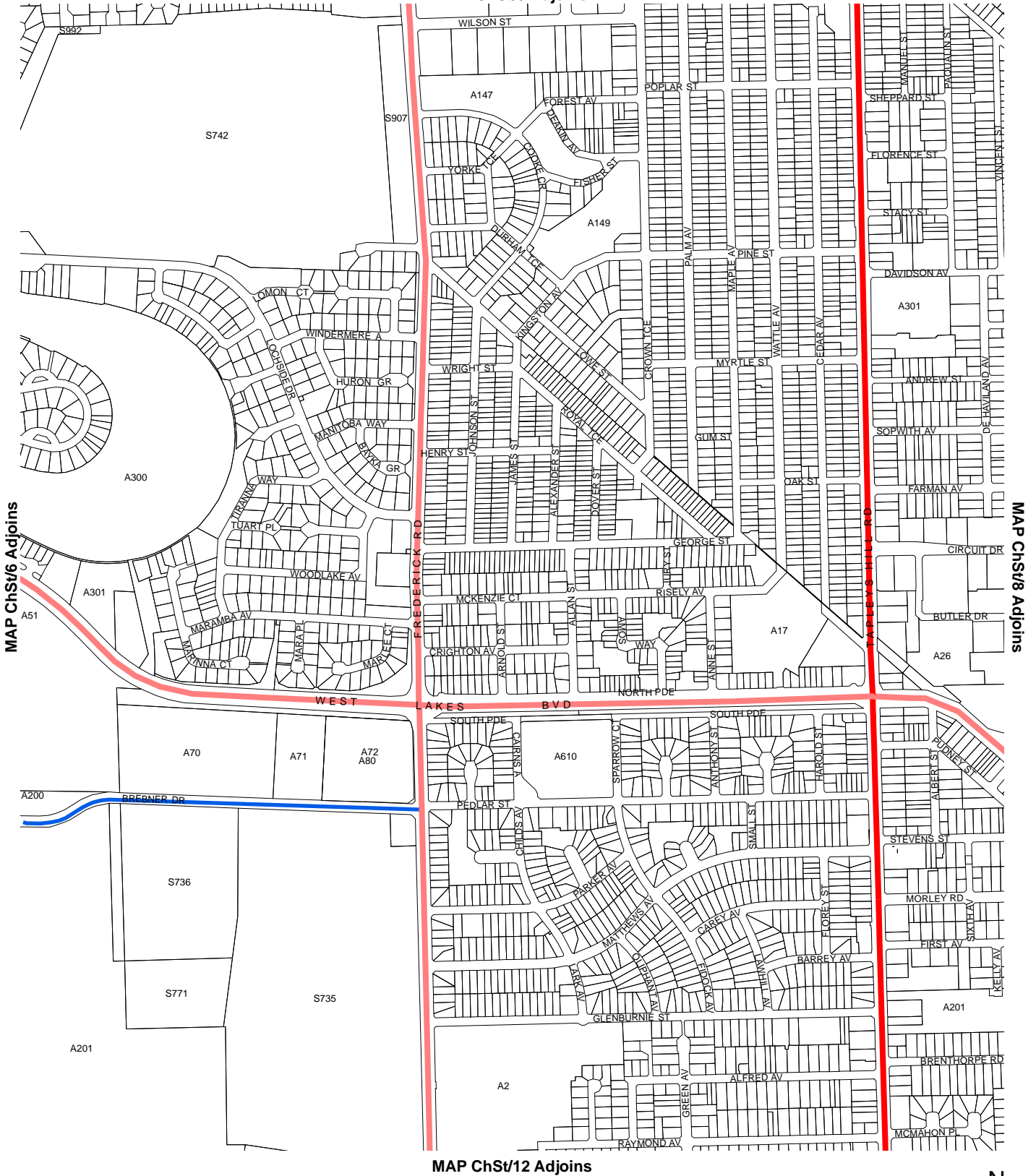


MAP ChSt/12 Adjoins

- S** School
- P** Post Office
- H** Other Health Services
- Local Reserves
- Waterbodies

Location Map ChSt/7

MAP ChSt/3 Adjoins

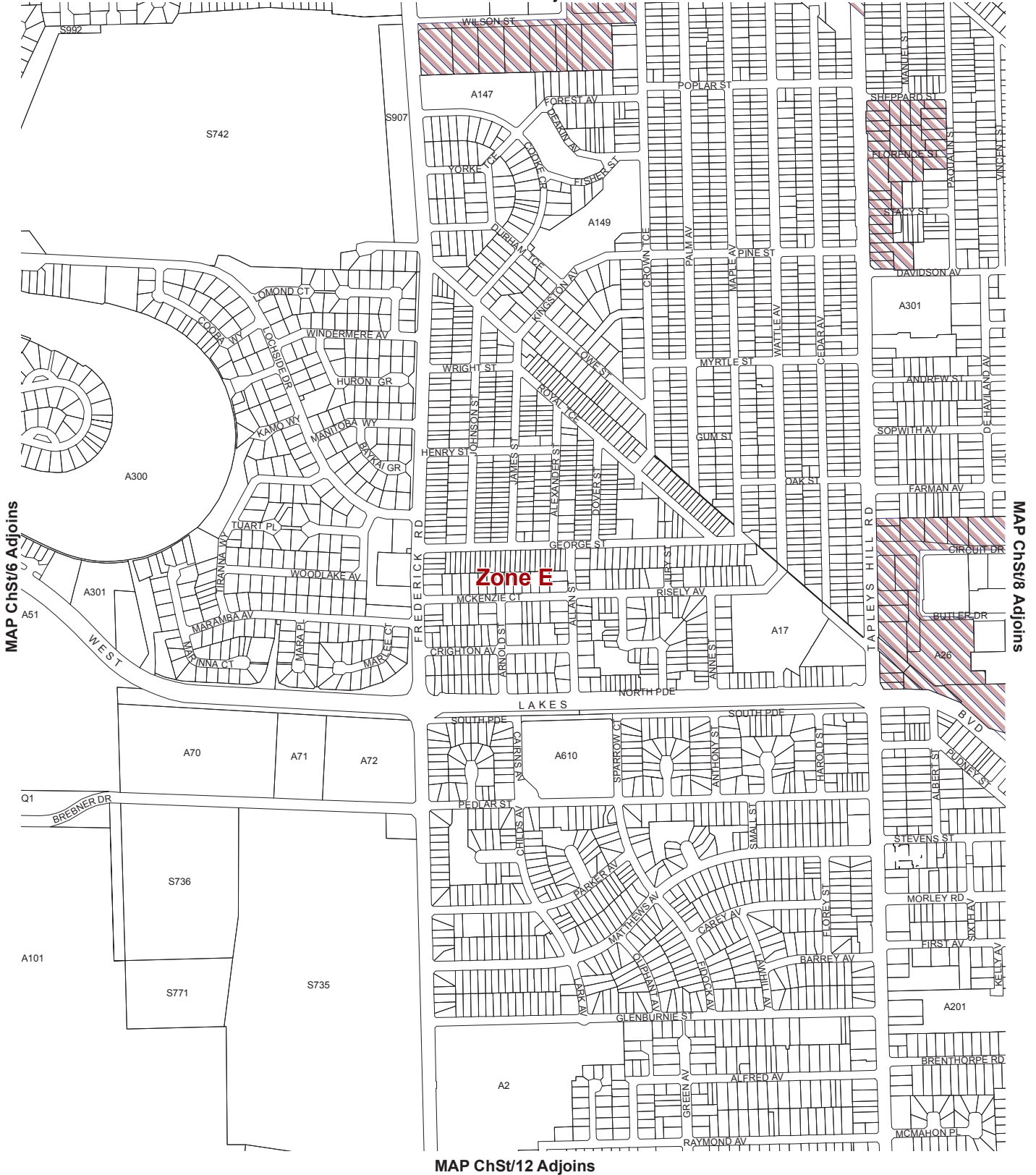


- ▬ Primary Arterial Roads
- ▬ Secondary Arterial Roads
- ▬ Distributor Roads

Overlay Map ChSt/7

TRANSPORT

MAP ChSt/3 Adjoins



Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

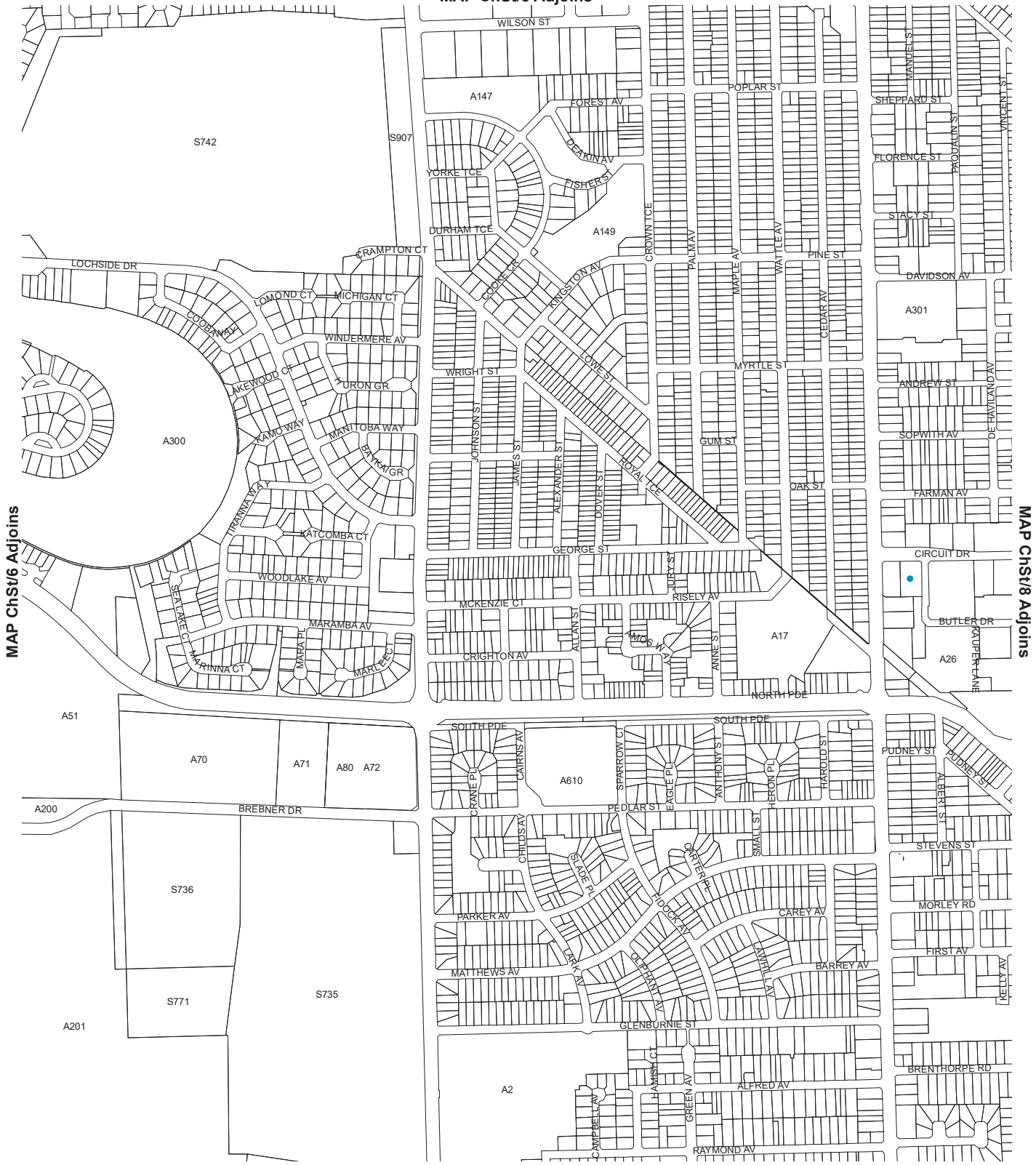


Overlay Map ChSt/7

DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Industry Interface Area

MAP ChSt/3 Adjoins



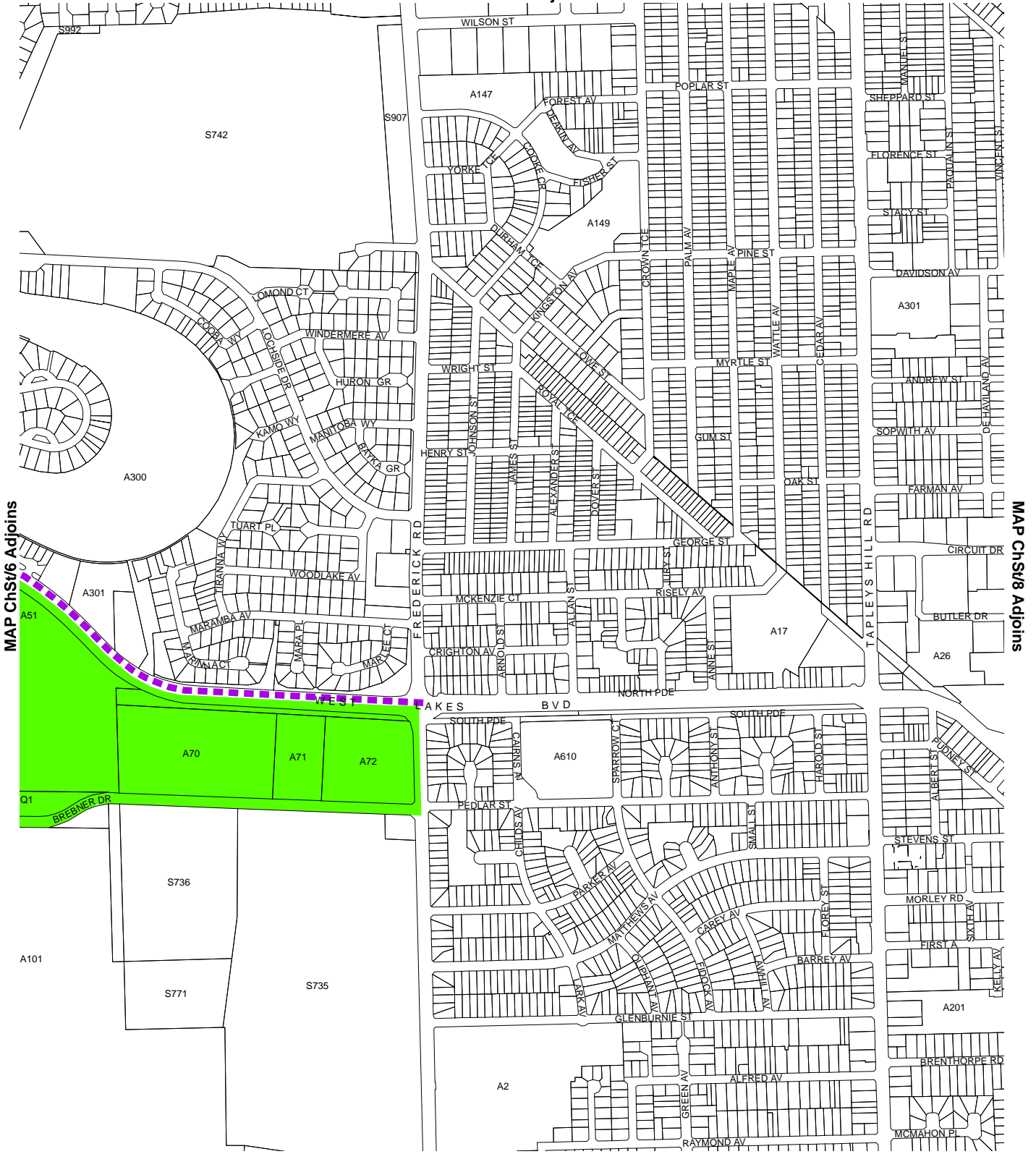
Heritage points are indicative only.
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 Items please refer to the relevant tables within this document.



Overlay Map ChSt/7 HERITAGE

• Local heritage place

MAP ChSt/3 Adjoins



MAP ChSt/12 Adjoins

- - - - - Proposed Tram Line
- Noise and Air Emissions Designated Area



Overlay Map ChSt/7

NOISE AND AIR EMISSIONS

MAP ChSt/3 Adjoins



MAP ChSt/6 Adjoins


MAP ChSt/8 Adjoins

MAP ChSt/12 Adjoins



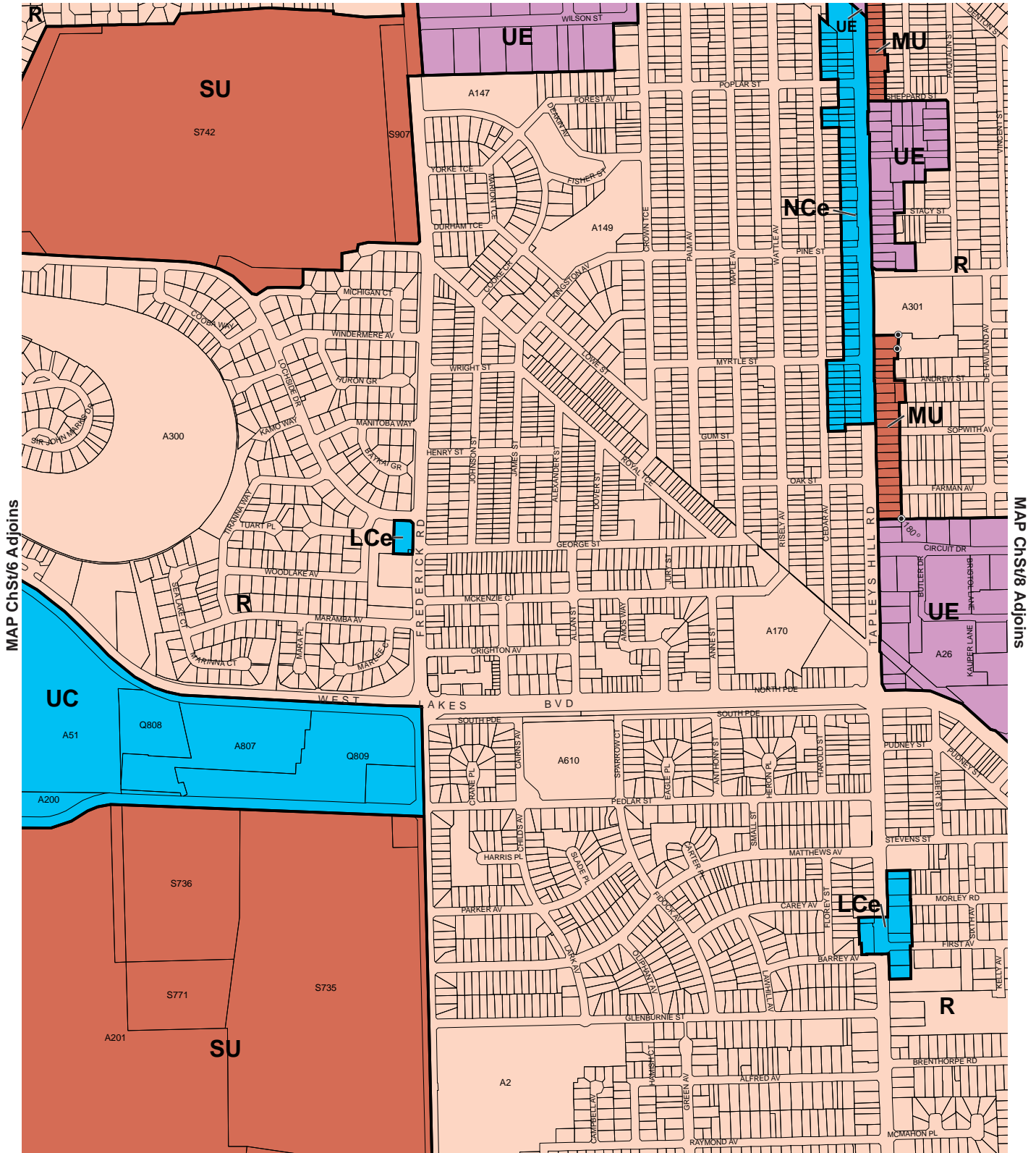
Overlay Map ChSt/7

AFFORDABLE HOUSING

 Affordable Housing Designated Area

CHARLES STURT COUNCIL
Consolidated - 14 January 2021

MAP ChSt/3 Adjoins



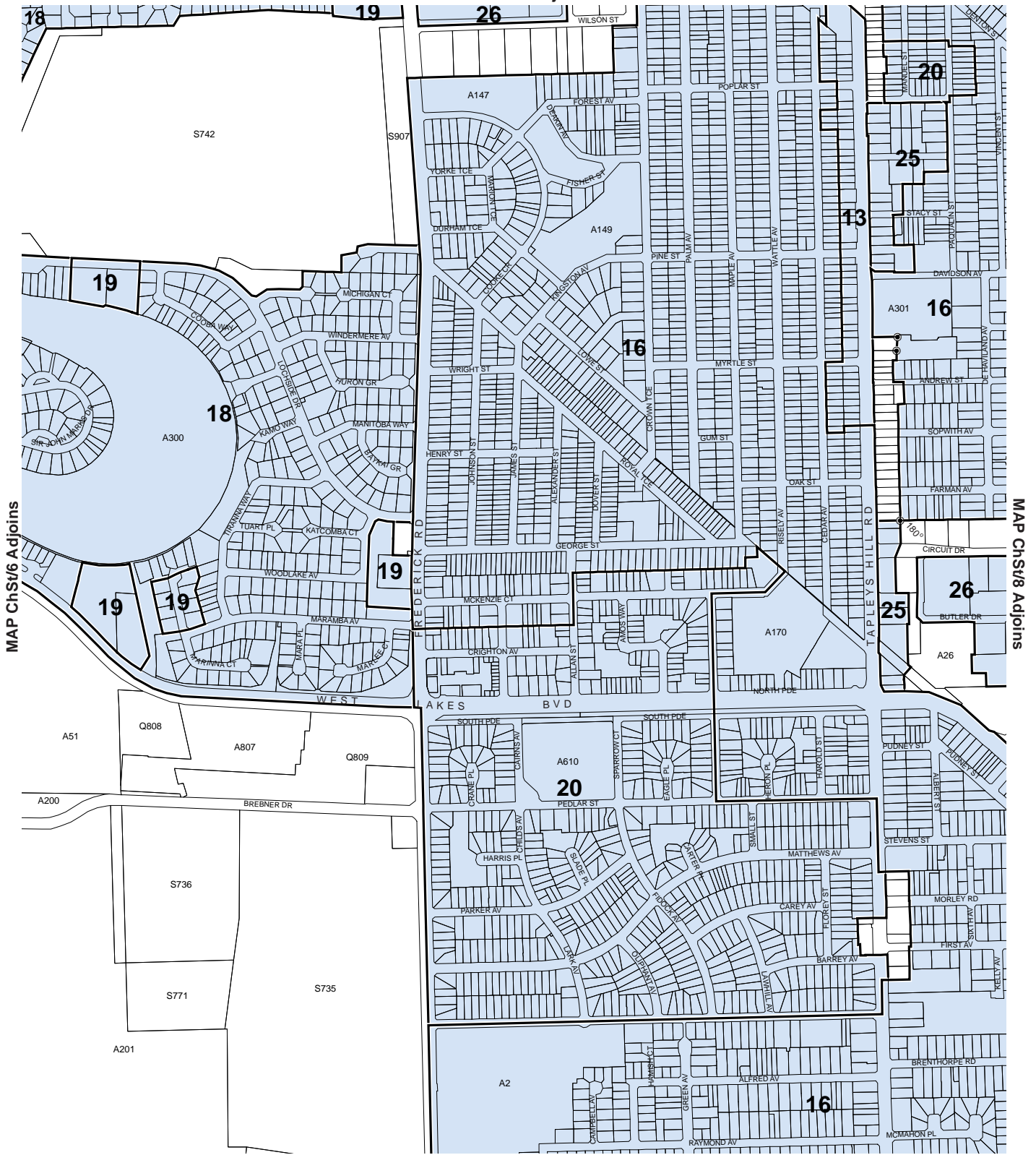
Lamberts Conformal Conic Projection, GDA94

- Zones**
- LCe Local Centre
 - MU Mixed Use
 - NCe Neighbourhood Centre
 - R Residential
 - SU Special Use
 - UC Urban Core
 - UE Urban Employment
 - Zone Boundary



Zone Map ChSt/7

MAP ChSt/3 Adjoins



MAP ChSt/12 Adjoins

Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 13 Royal Park
 - 16 Mid Suburban
 - 18 West Lakes General
 - 19 West Lakes Medium Density
 - 20 Integrated Medium Density
 - 25 Main Road Commercial
 - 26 Core Industry



Policy Area Map ChSt/7

 Policy Area Boundary

MAP ChSt/3 Adjoins



MAP ChSt/6 Adjoins

MAP ChSt/8 Adjoins

MAP ChSt/12 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

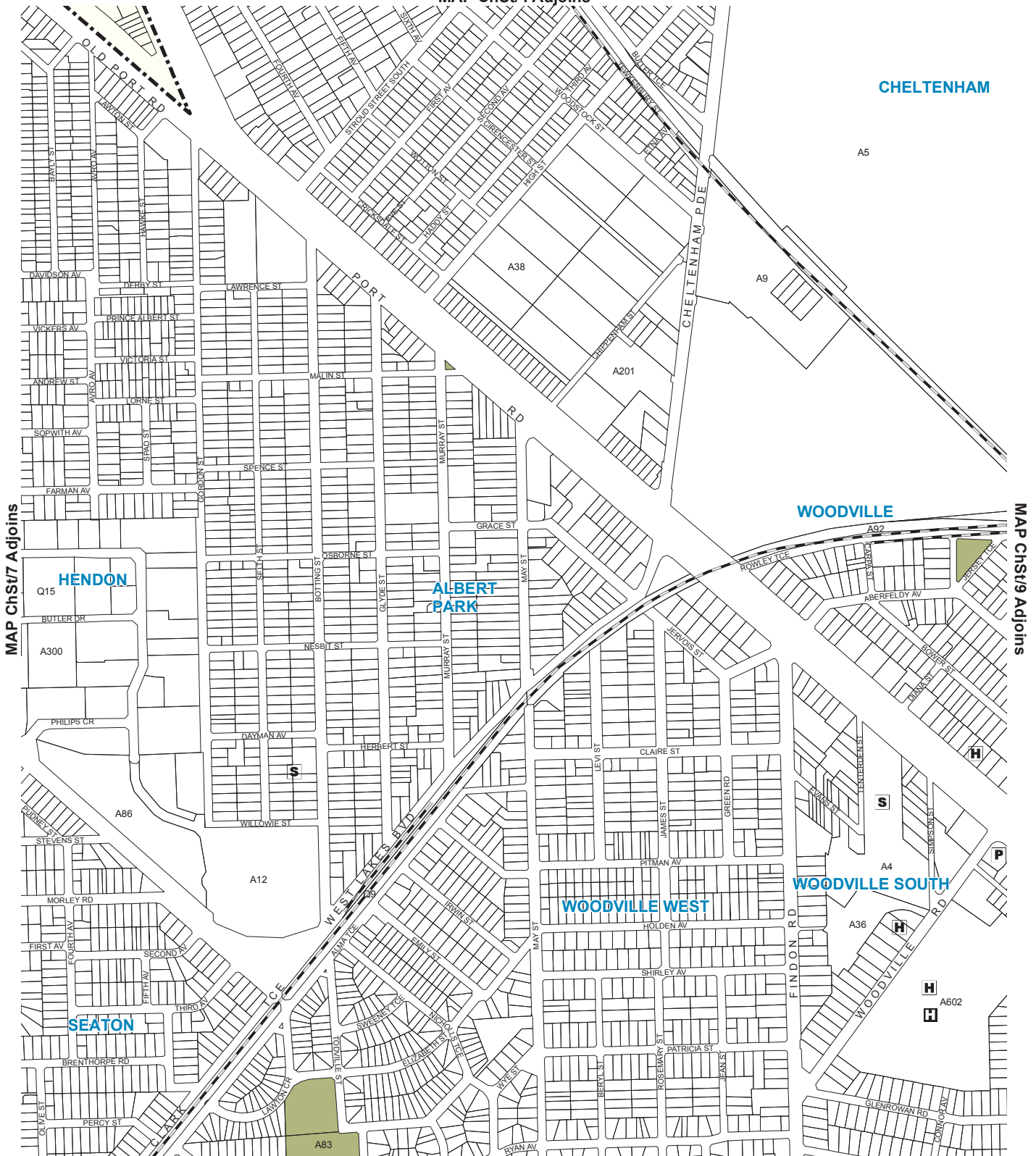
- 47 Mixed Use
- 59 Retail Core Royal Park
- 60 Commercial Fringe Royal Park

 Precinct Boundary








Precinct Map ChSt/7

MAP ChSt/4 Adjoins

CHELTENHAM



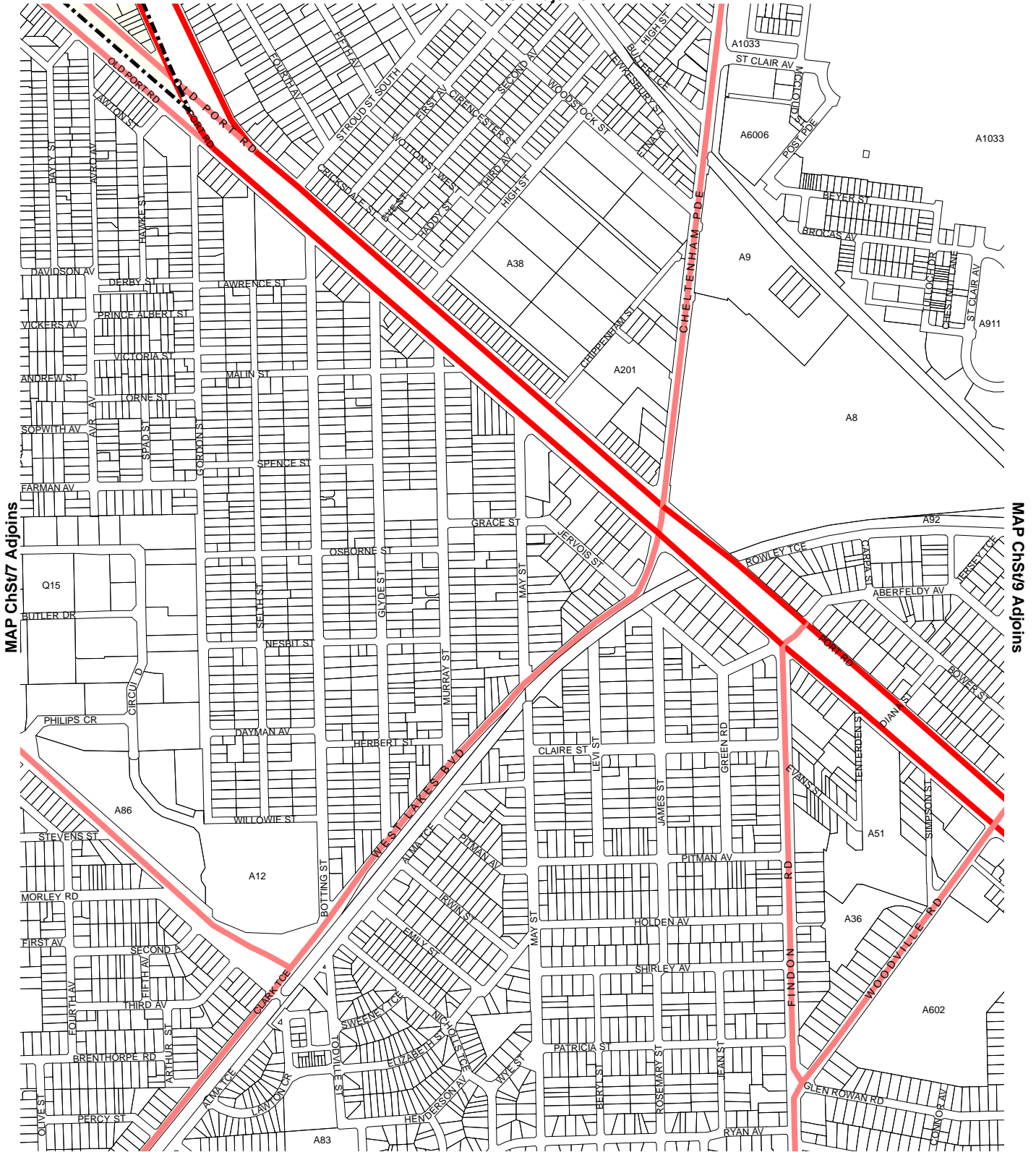
MAP ChSt/13 Adjoins

-  School
-  Post Office
-  Other Health Services
-  Hospital
-  Railways
-  Local Reserves
-  Development Plan Boundary



Location Map ChSt/8

MAP ChSt/4 Adjoins



MAP ChSt/13 Adjoins

MAP ChSt/7 Adjoins

MAP ChSt/9 Adjoins

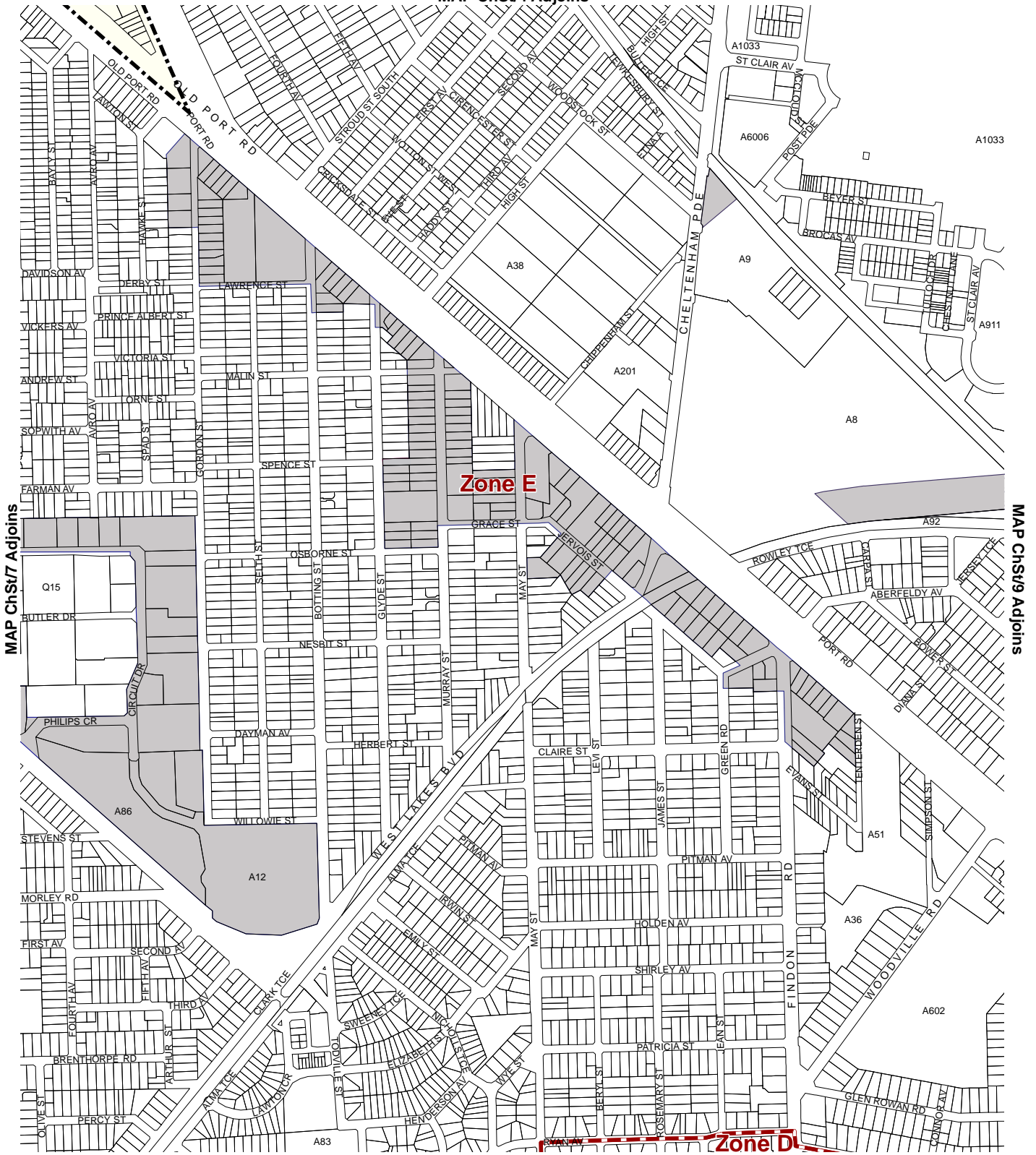


Overlay Map ChSt/8

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Development Plan Boundary

MAP ChSt/4 Adjoins



Airport Building Heights

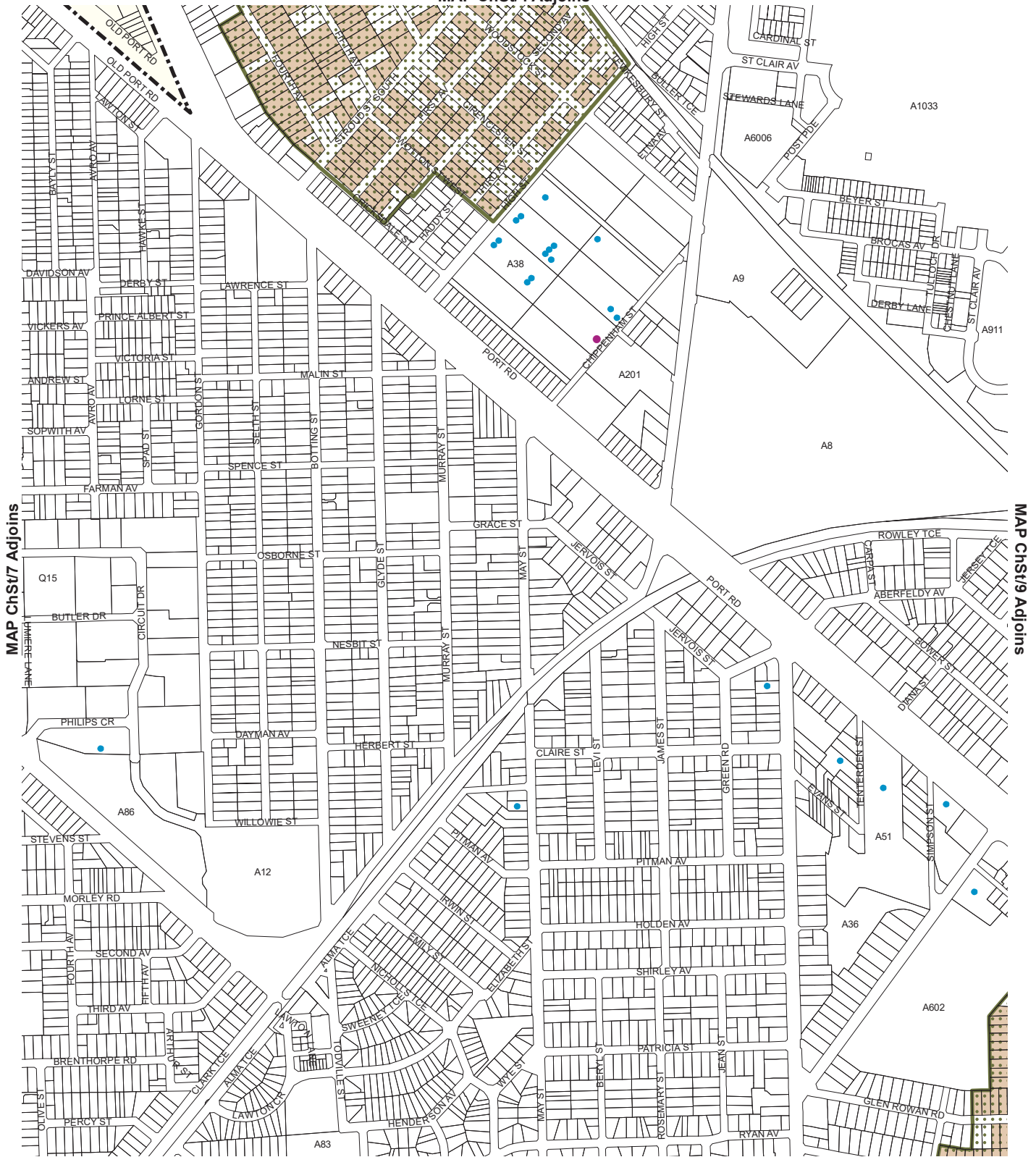
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level

- Airport Building Heights
- Industry Interface Area
- Development Plan Boundary

Overlay Map ChSt/8

DEVELOPMENT CONSTRAINTS

MAP ChSt/4 Adjoins



Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.

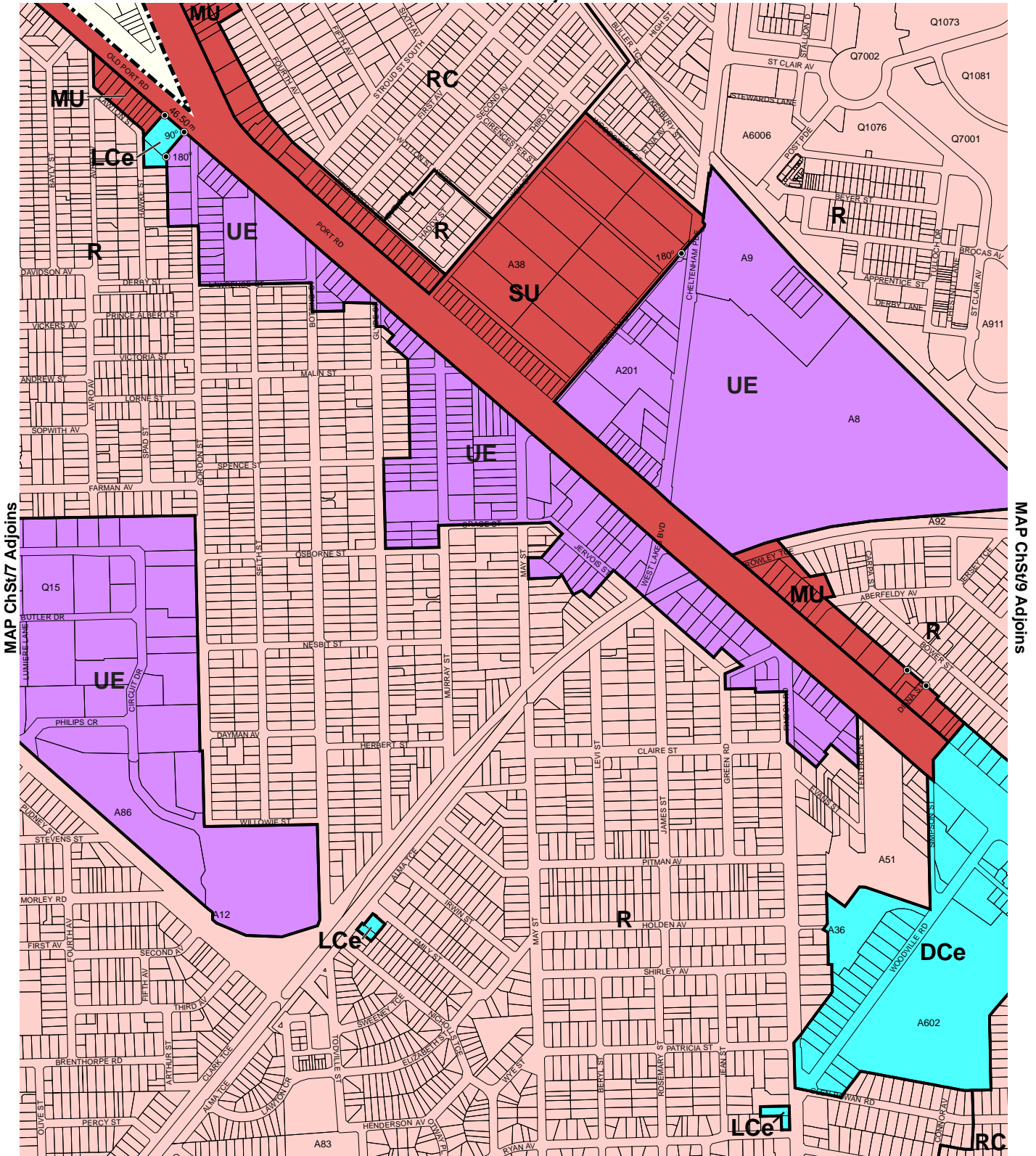


- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/8

HERITAGE

MAP ChSt/4 Adjoins



MAP ChSt/13 Adjoins

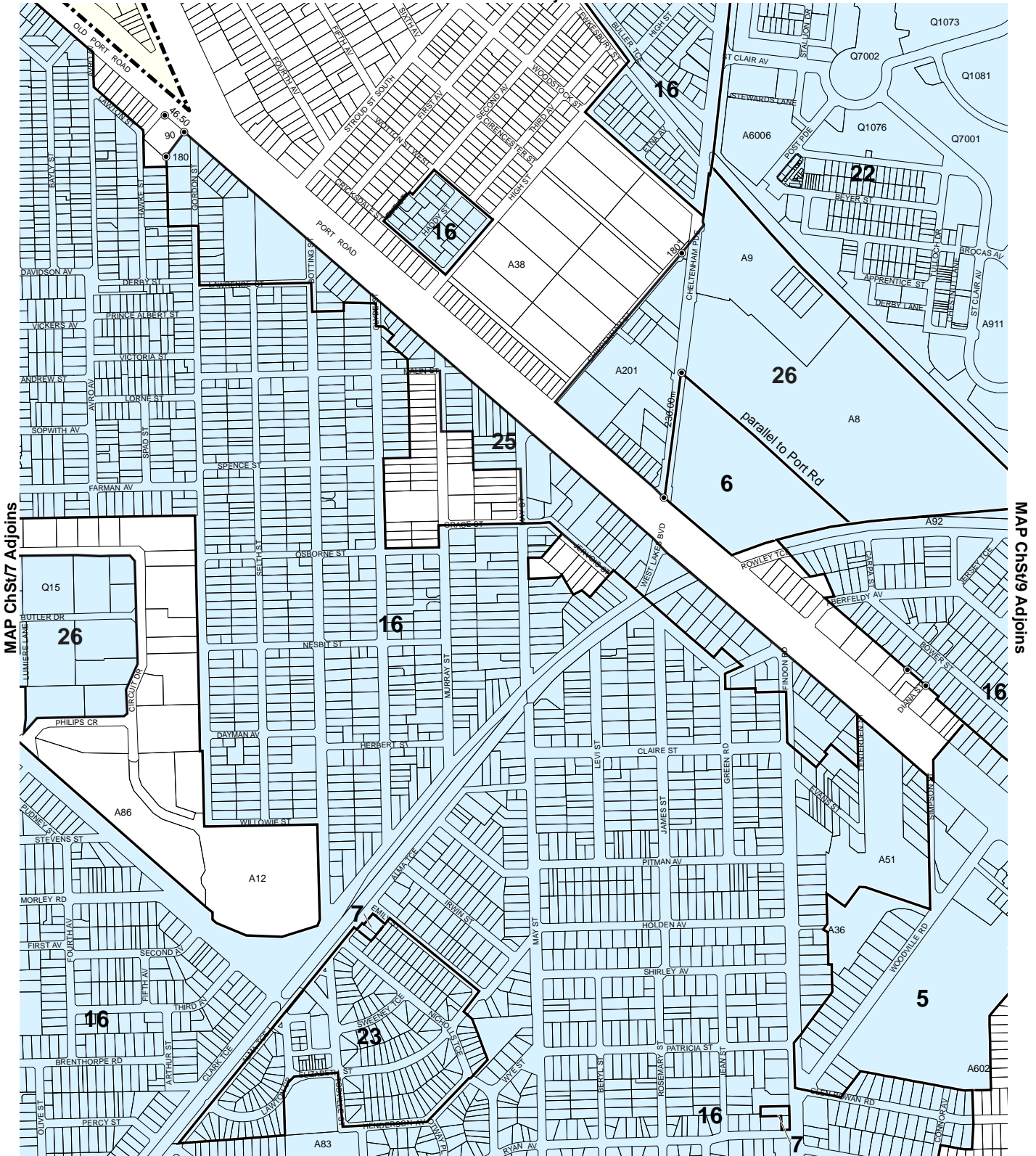
Lamberts Conformal Conic Projection, GDA94

- Zones**
- DCe District Centre
 - LCe Local Centre
 - MU Mixed Use
 - R Residential
 - RC Residential Character
 - SU Special Use
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary



Zone Map ChSt/8

MAP ChSt/4 Adjoins



MAP ChSt/13 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

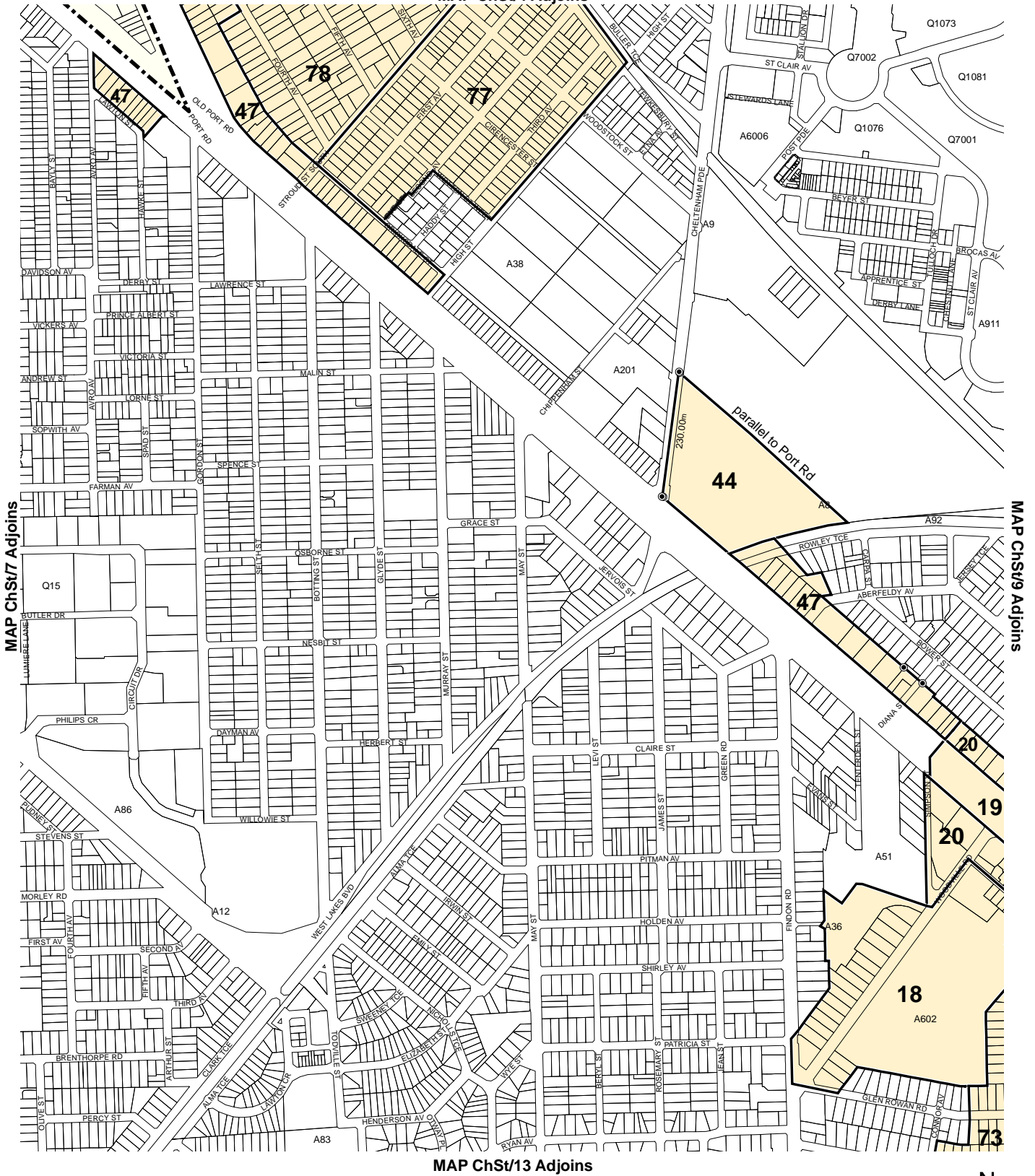
- 5 Woodville
- 6 Bulky Goods
- 7 Local Shopping
- 16 Mid Suburban
- 22 Cheltenham Park
- 23 Woodville West
- 25 Main Road Commercial
- 26 Core Industry



Policy Area Map ChSt/8

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/4 Adjoins



MAP ChSt/13 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

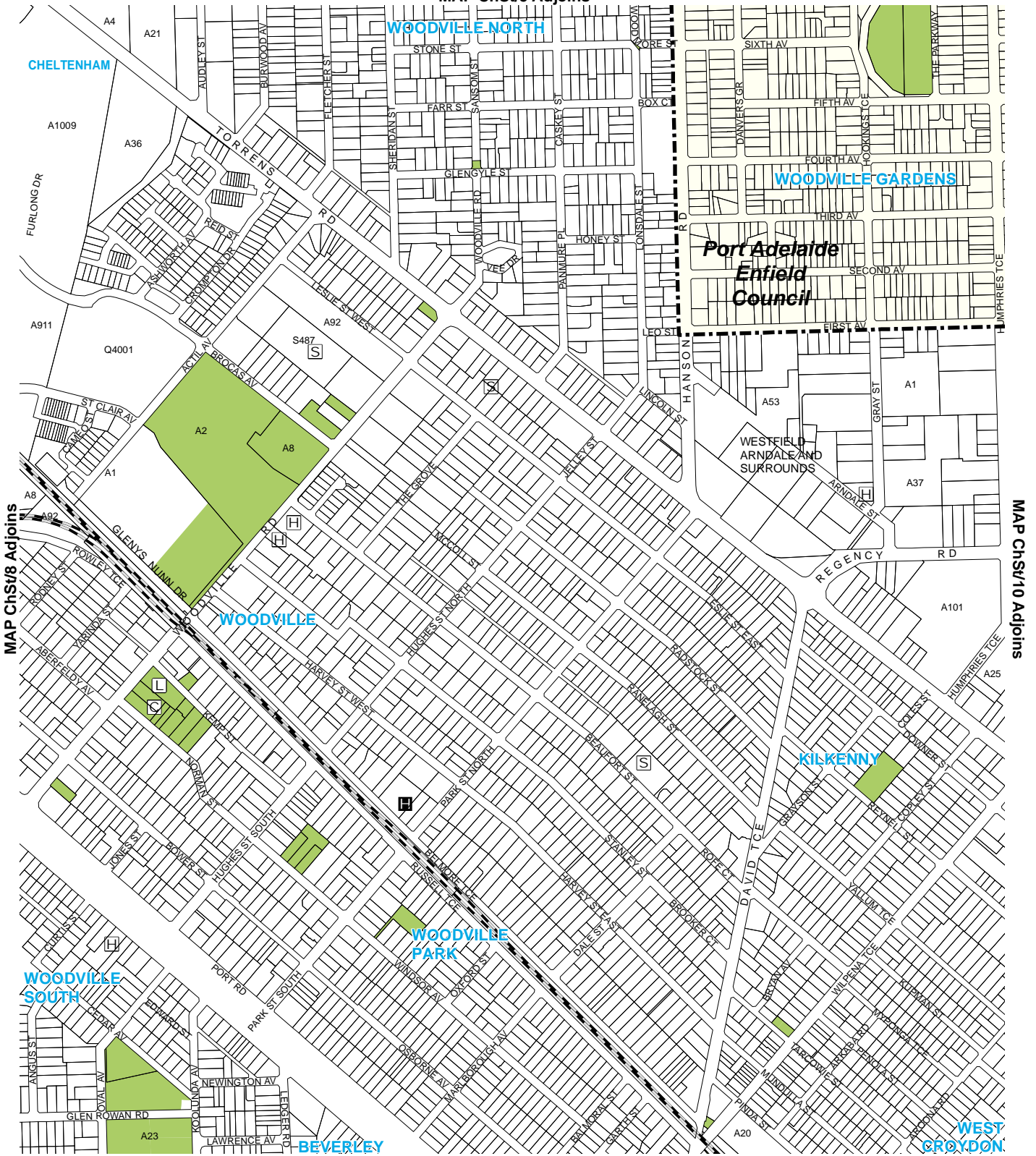
- 18 Woodville Road Medical
- 19 Woodville Road Gardens
- 20 Civic
- 44 Woodville Bulky Goods
- 47 Mixed Use
- 73 Woodville South Character
- 77 Cheltenham East
- 78 Cheltenham West

- Precinct Boundary
- Development Plan Boundary











Precinct Map ChSt/8

MAP ChSt/5 Adjoins



MAP ChSt/14 Adjoins

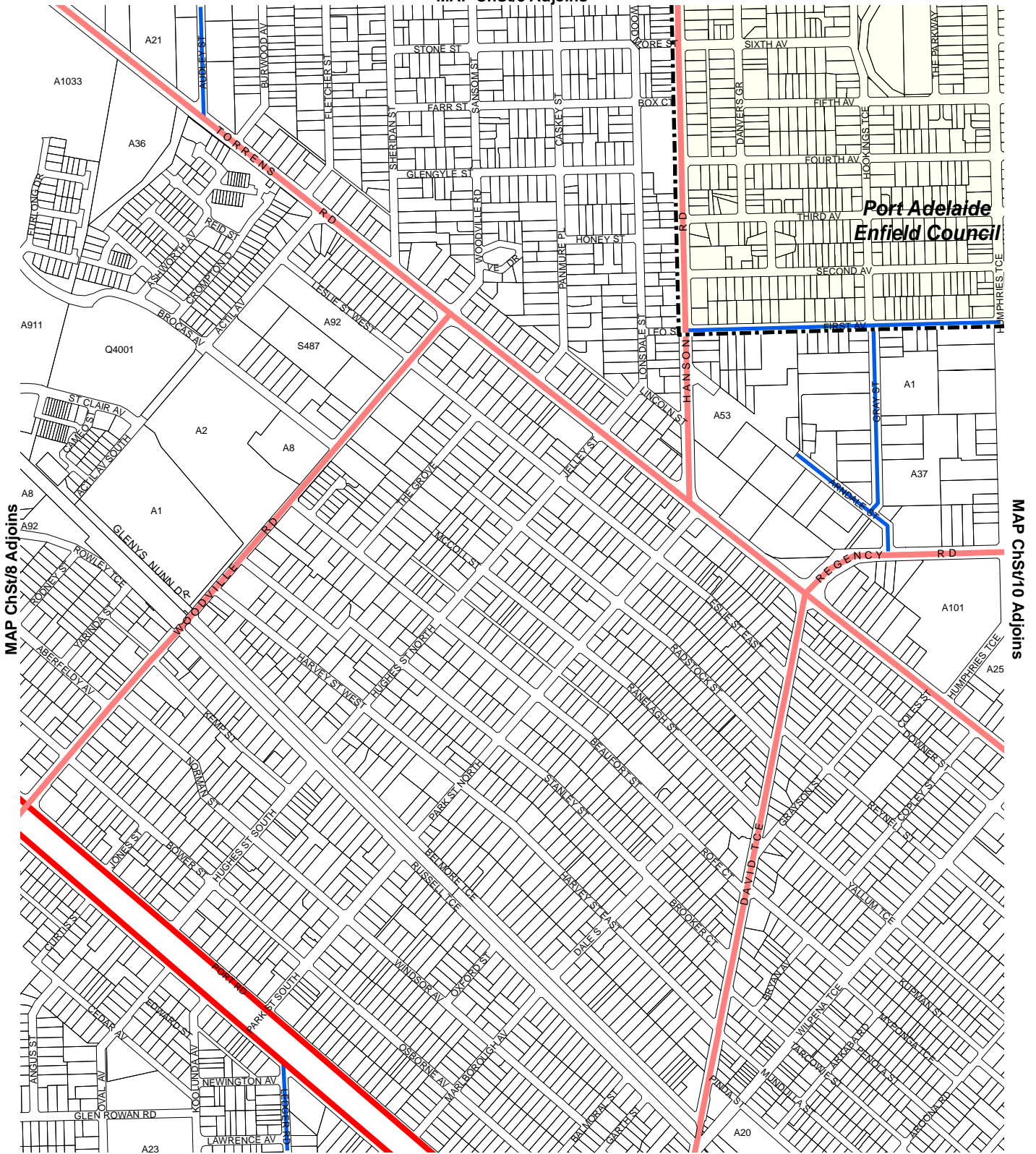


-  School
-  Public Library
-  Council Office
-  Other Health Services
-  Hospital
-  Railways
-  Local Reserves
-  Development Plan Boundary

Location Map ChSt/9

MAP ChSt/5 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins



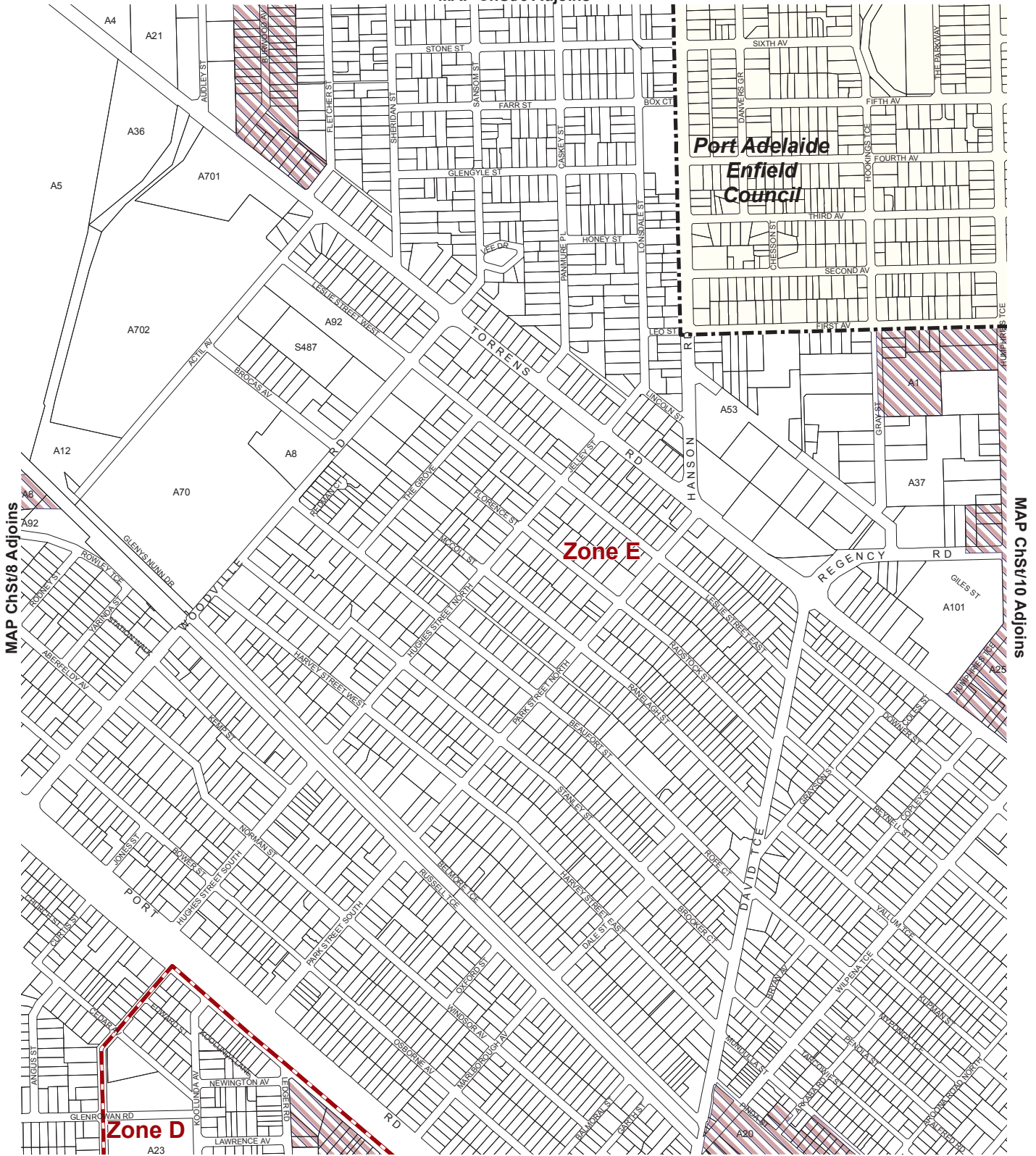
0 500m

Overlay Map ChSt/9

TRANSPORT

- ▬ Primary Arterial Roads
- ▬ Secondary Arterial Roads
- ▬ Distributor Roads
- Development Plan Boundary

MAP ChSt/5 Adjoins




- Airport Building Heights**
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level

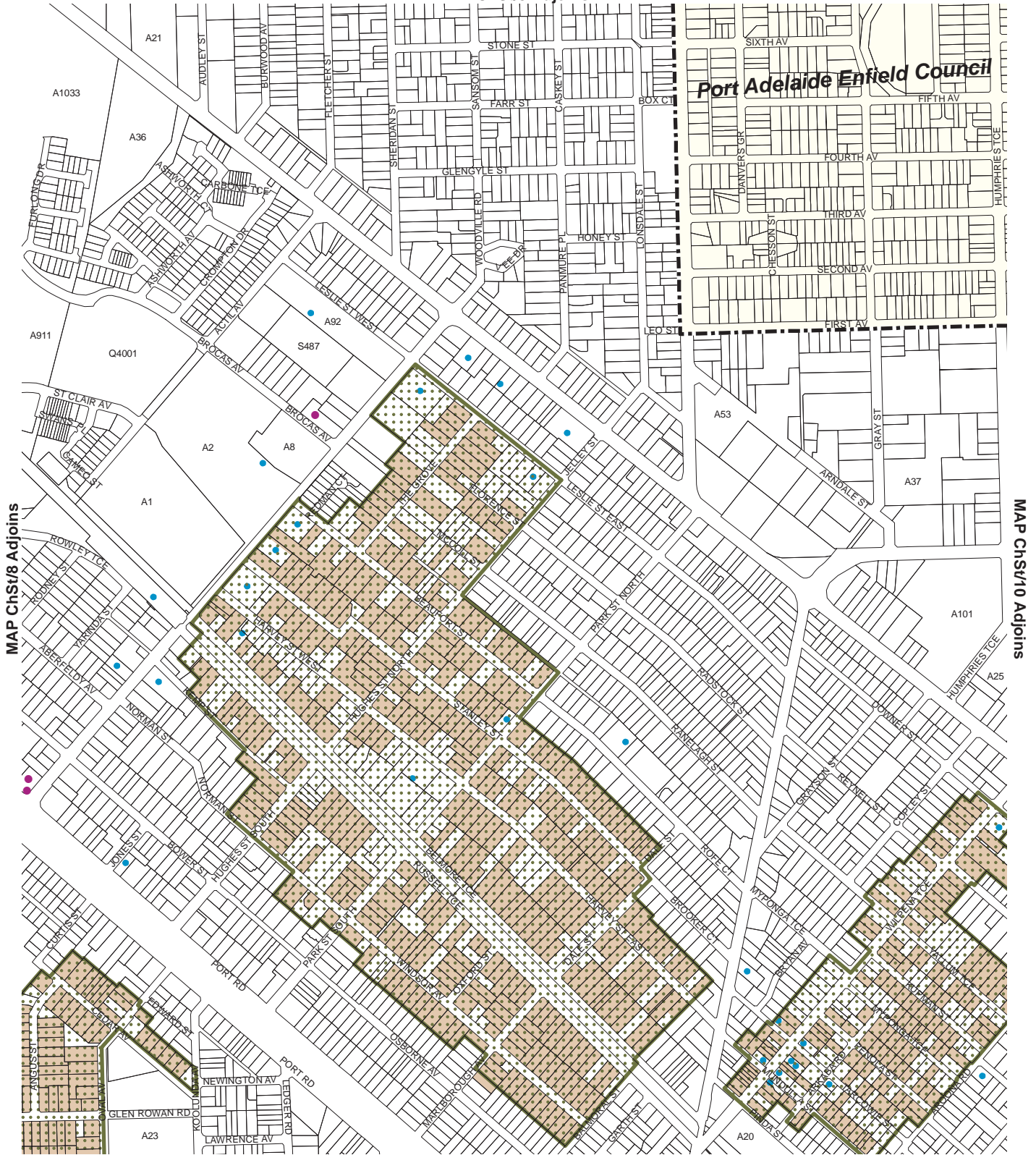


Overlay Map ChSt/9

DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary

MAP ChSt/5 Adjoins



Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.



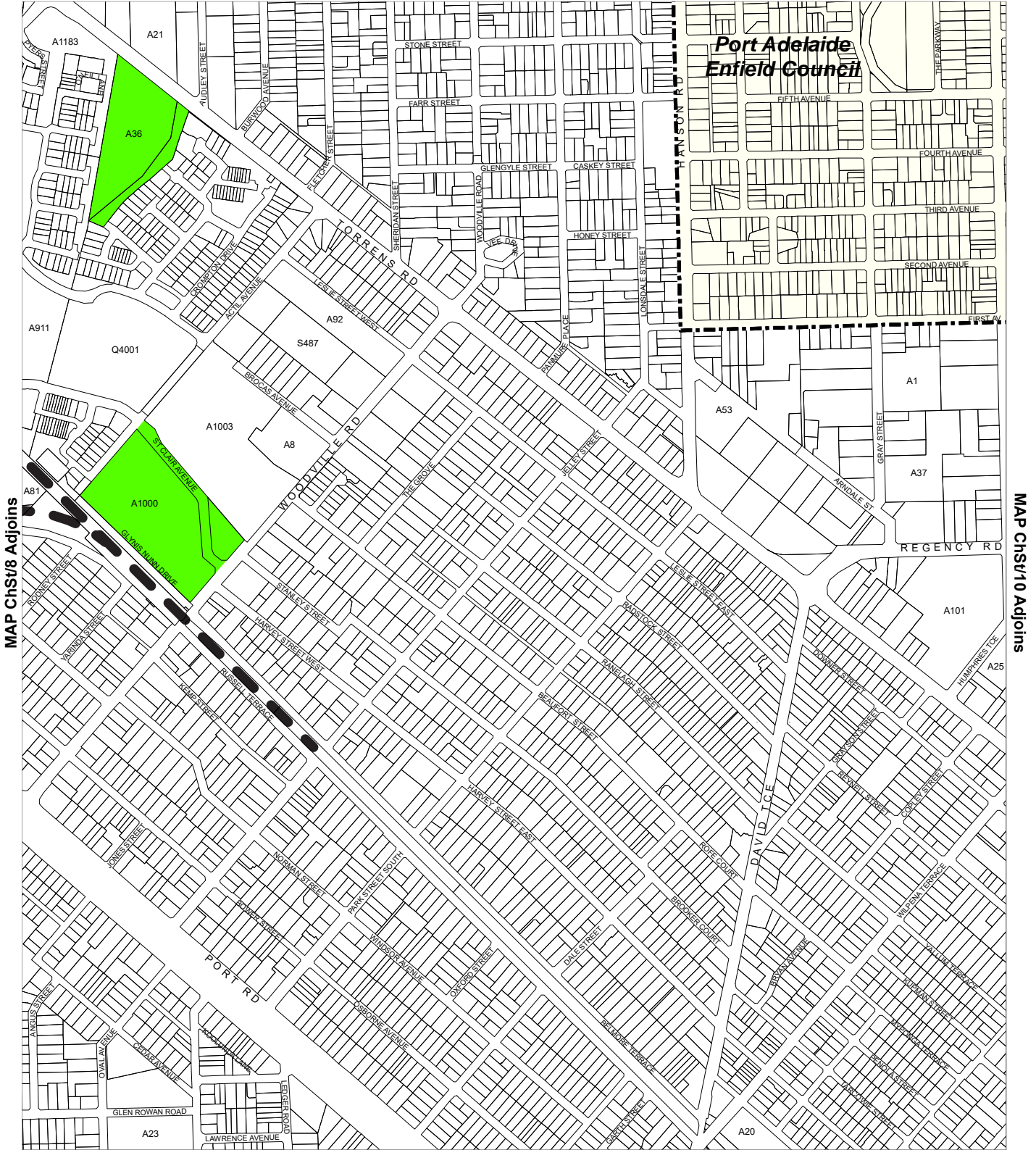
- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/9




HERITAGE

MAP ChSt/5 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

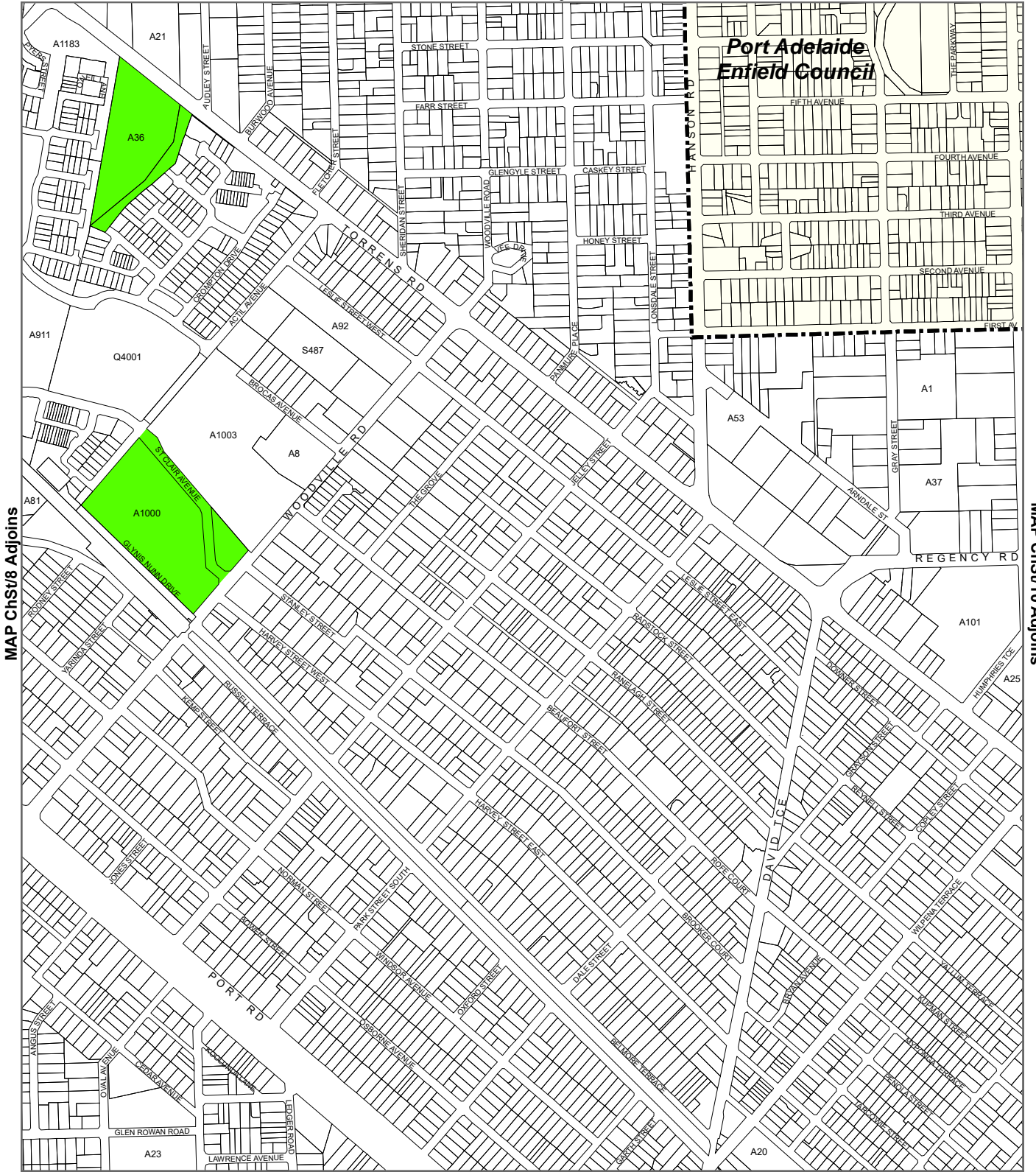
-  Train Line
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary

Overlay Map ChSt/9

NOISE AND AIR EMISSIONS

MAP ChSt/5 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

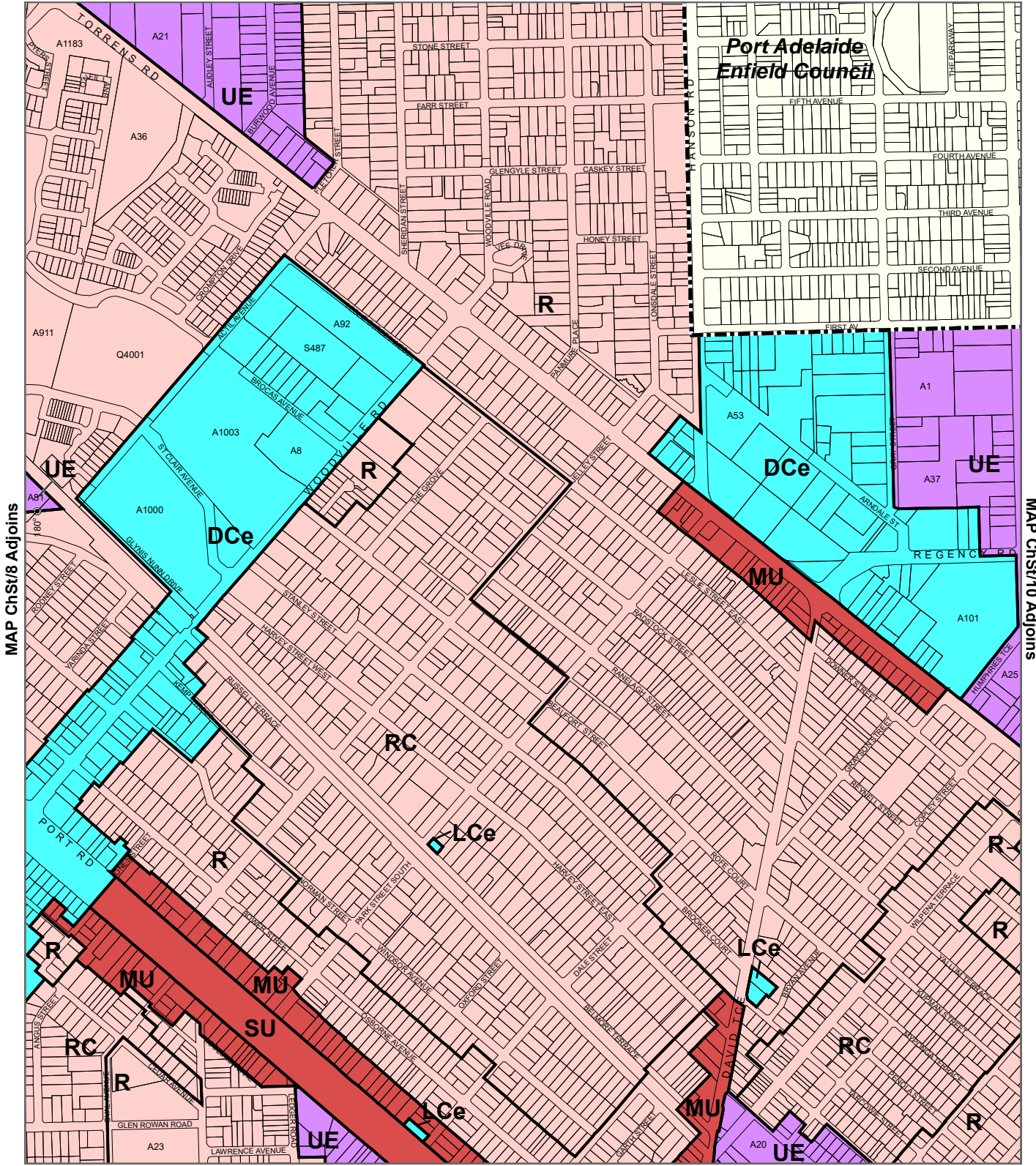


Overlay Map ChSt/9 AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP ChSt/5 Adjoins

Port Adelaide
Enfield Council



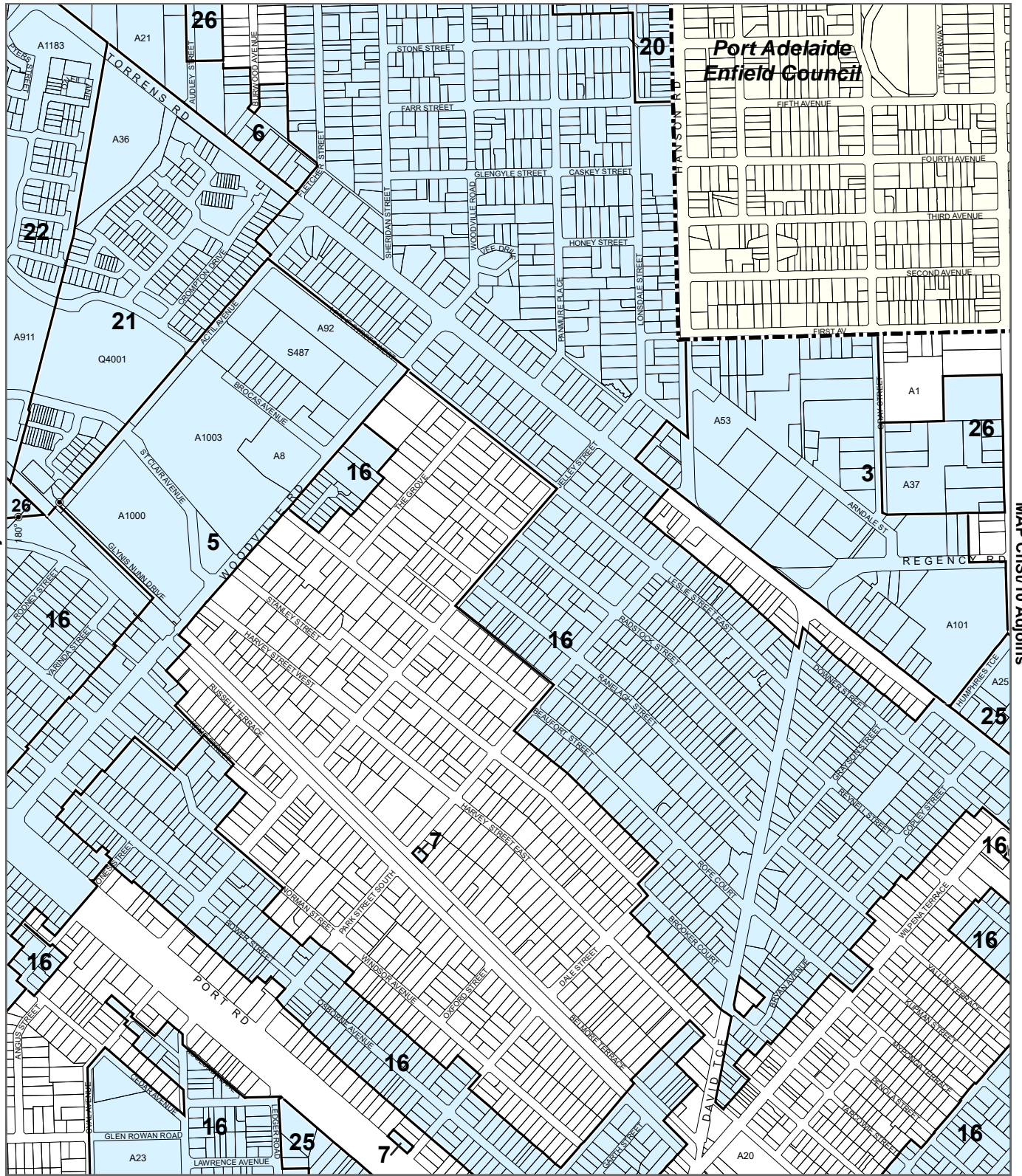
Lamberts Conformal Conic Projection, GDA94

- Zones**
- District Centre
 - Local Centre
 - Mixed Use
 - Residential
 - Residential Character
 - Special Use
 - Urban Employment
 - Zone Boundary
 - Development Plan Boundary



Zone Map ChSt/9

MAP ChSt/5 Adjoins



Port Adelaide
Enfield Council

MAP ChSt/14 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

- 3 Kilkeny
- 5 Woodville
- 6 Bulky Goods
- 7 Local Shopping
- 16 Mid Suburban
- 20 Integrated Medium Density
- 21 Woodville Medium Density
- 22 Cheltenham Park
- 25 Main Road Commercial
- 26 Core Industry

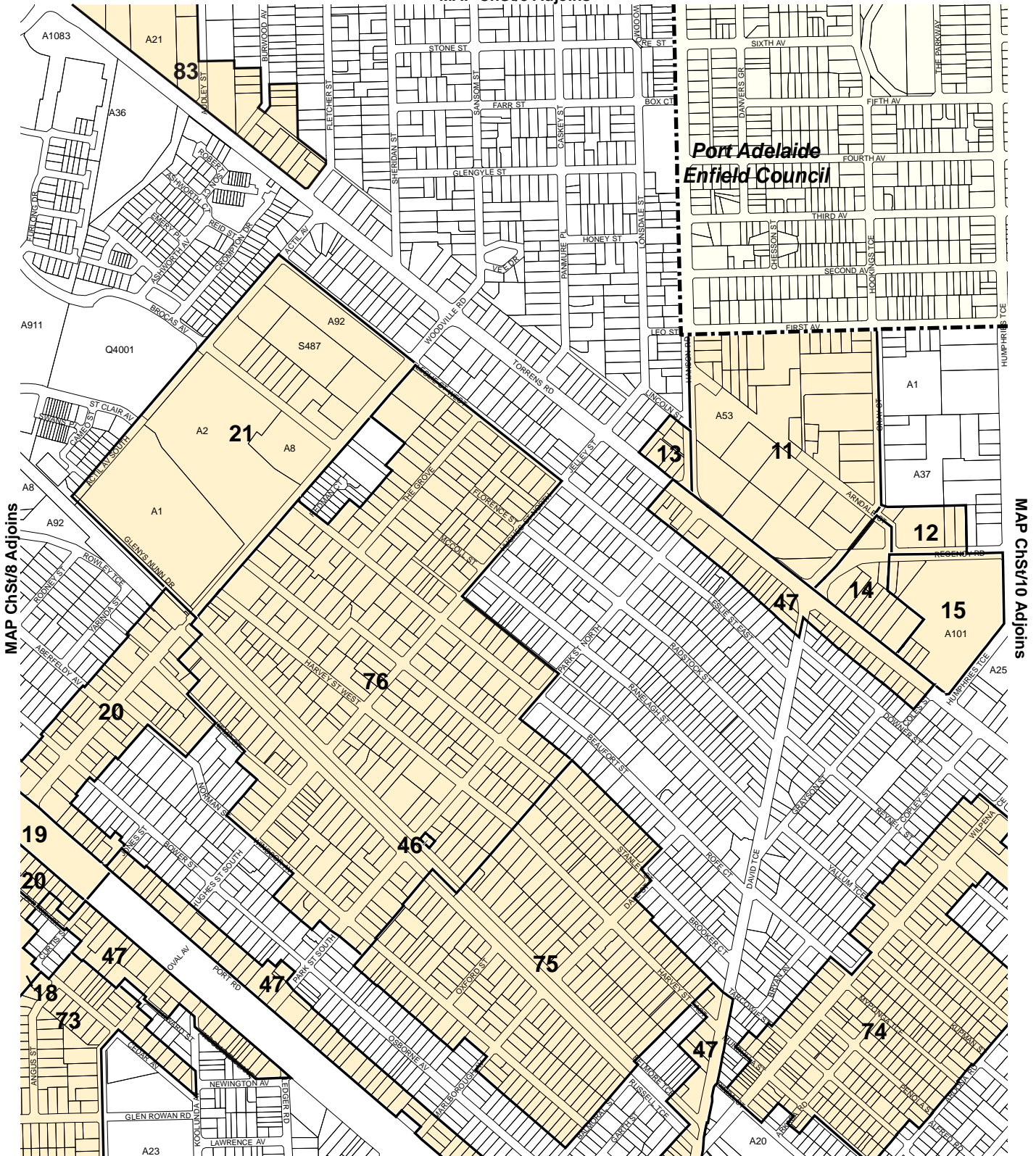


Policy Area Map ChSt/9

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/5 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

- 11 Retail Core Kilkenny
- 12 Regency Road
- 13 Hanson Road
- 14 Commercial Fringe Kilkenny
- 15 Residential
- 18 Woodville Road Medical
- 19 Woodville Road Gardens
- 20 Civic
- 21 Recreation/Education
- 46 Stations

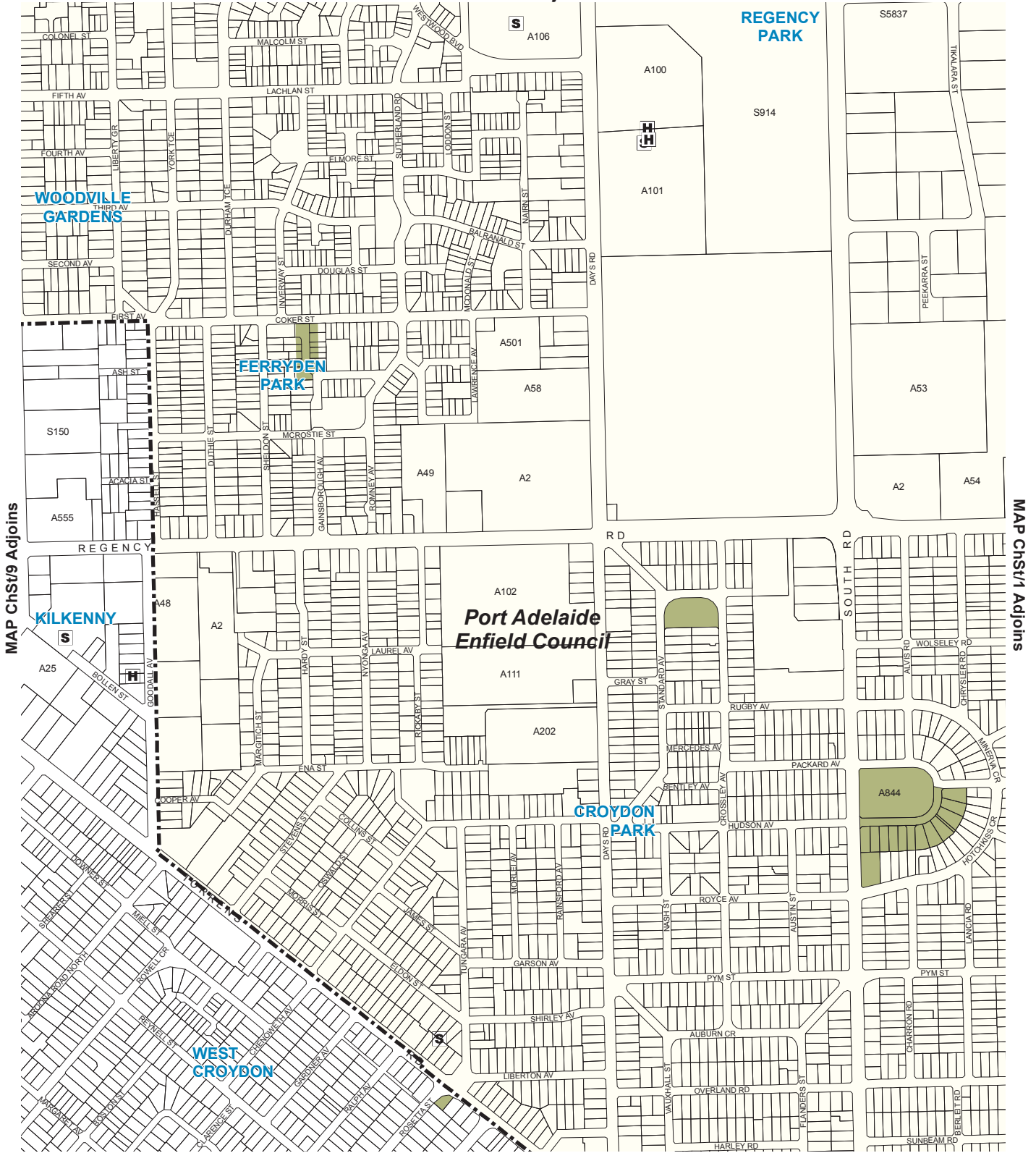
- 47 Mixed Use
- 73 Woodville South Character
- 74 Kilkenny Character
- 75 Woodville Park
- 76 Woodville Character
- 83 Woodville North



Precinct Map ChSt/9

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

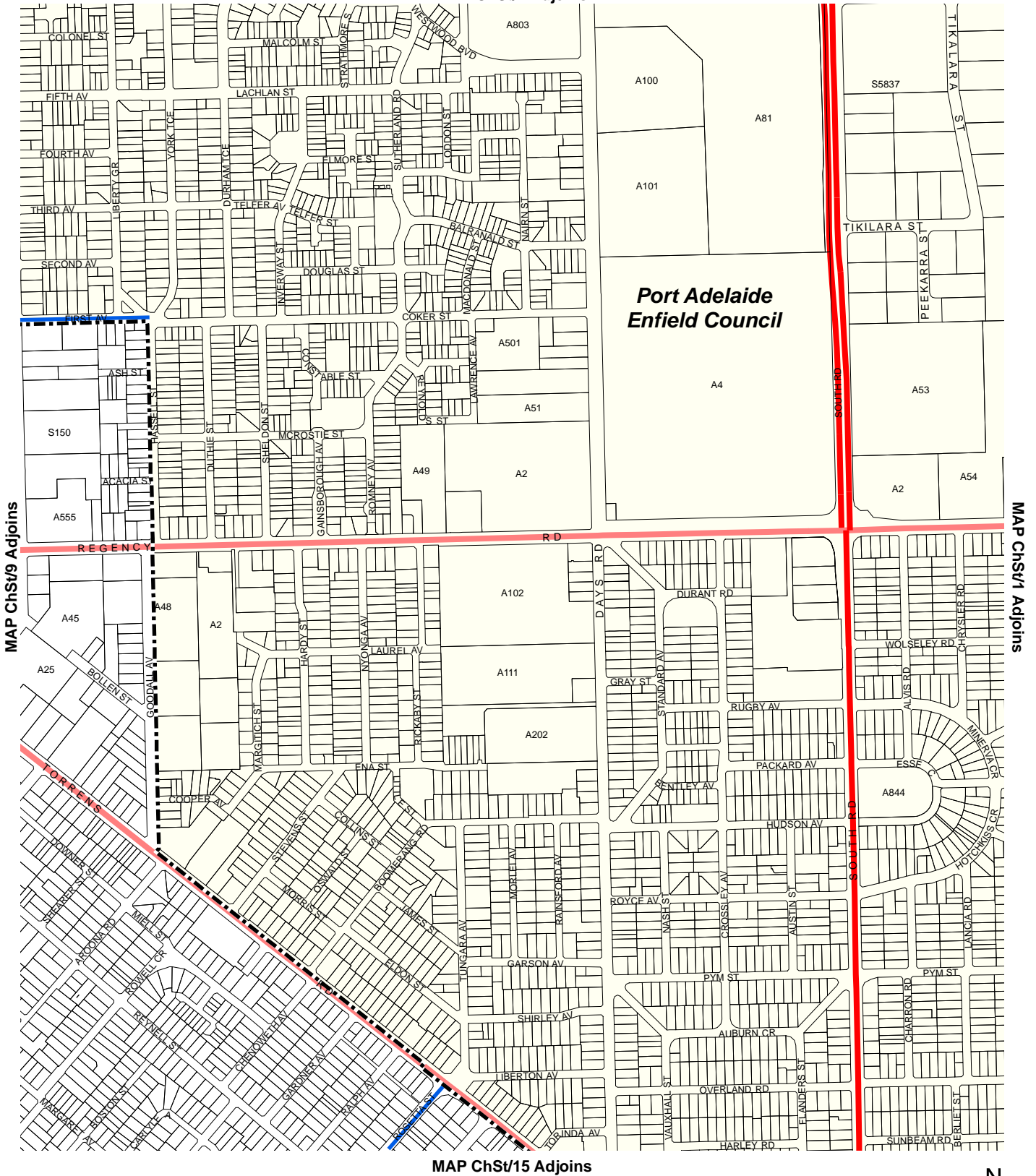


MAP ChSt/15 Adjoins

Location Map ChSt/10

-  School
-  Other Health Services
-  Local Reserves
-  Development Plan Boundary

MAP ChSt/1 Adjoins



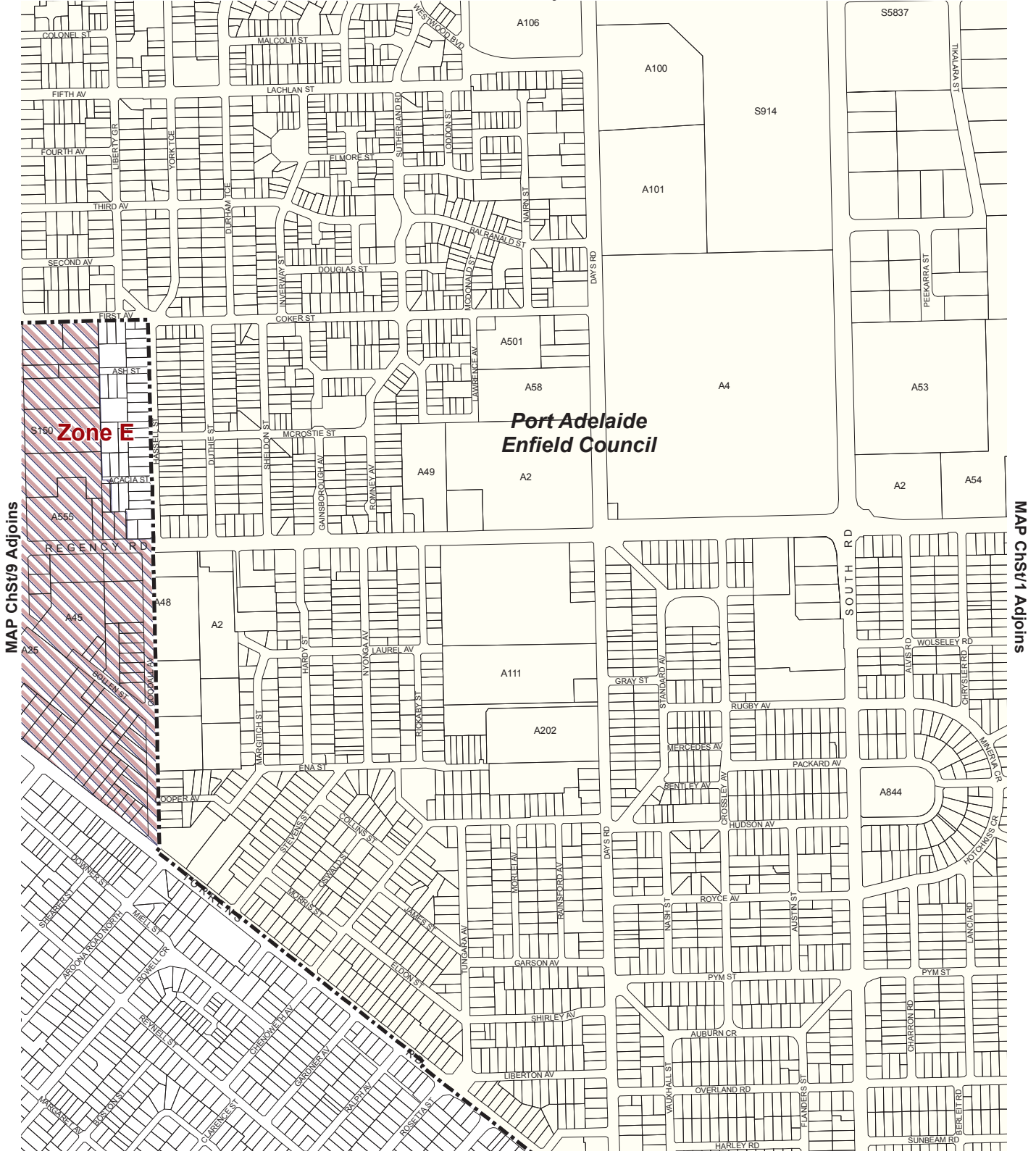
MAP ChSt/15 Adjoins

- Primary Arterial Roads
- Secondary Arterial Roads
- Distributor Roads
- - - - Development Plan Boundary

Overlay Map ChSt/10



TRANSPORT

MAP ChSt/1 Adjoins



MAP ChSt/15 Adjoins

Airport Building Heights
Zone E All Structures Exceeding 100 metres above existing ground level

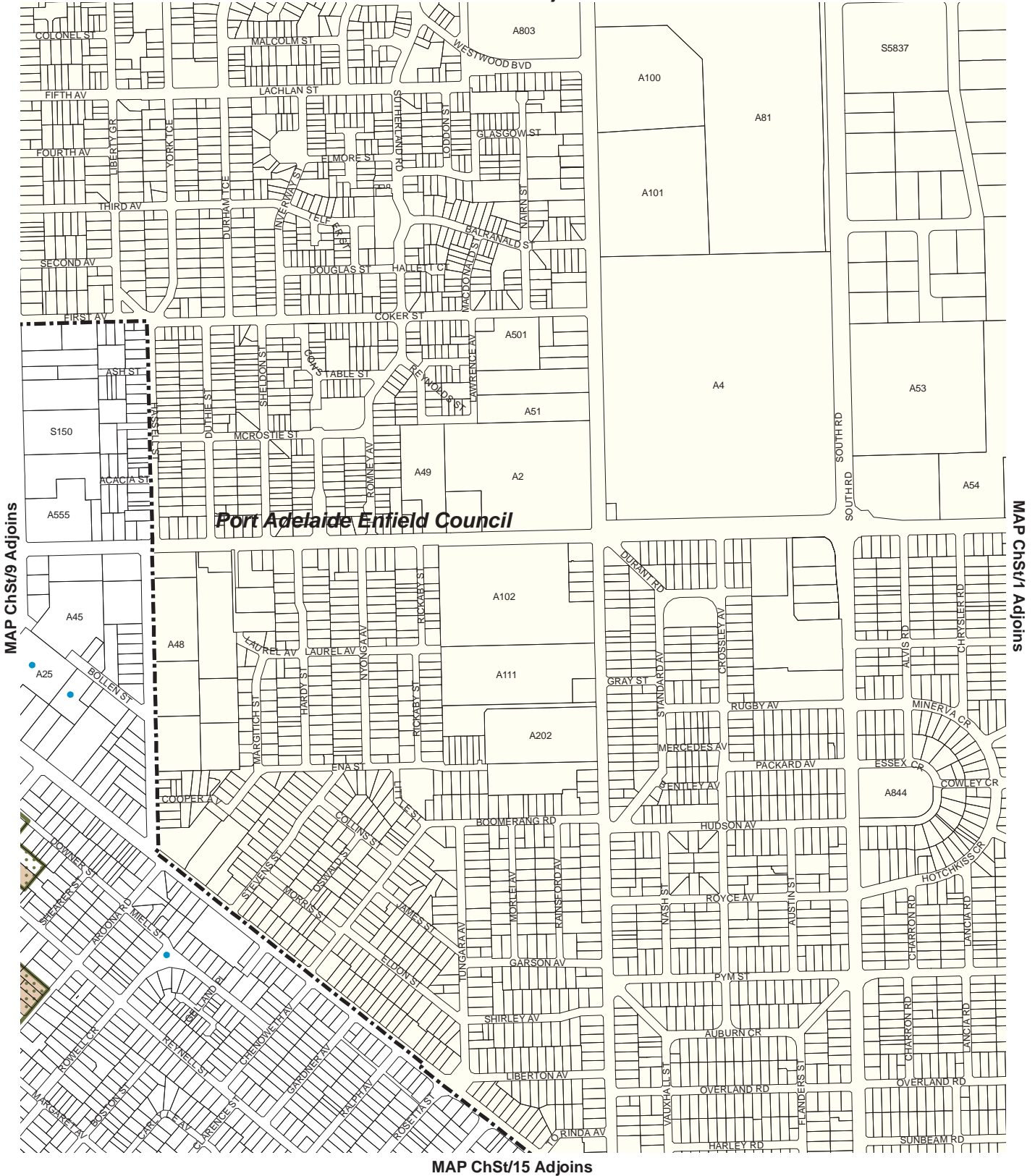
-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary



Overlay Map ChSt/10

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

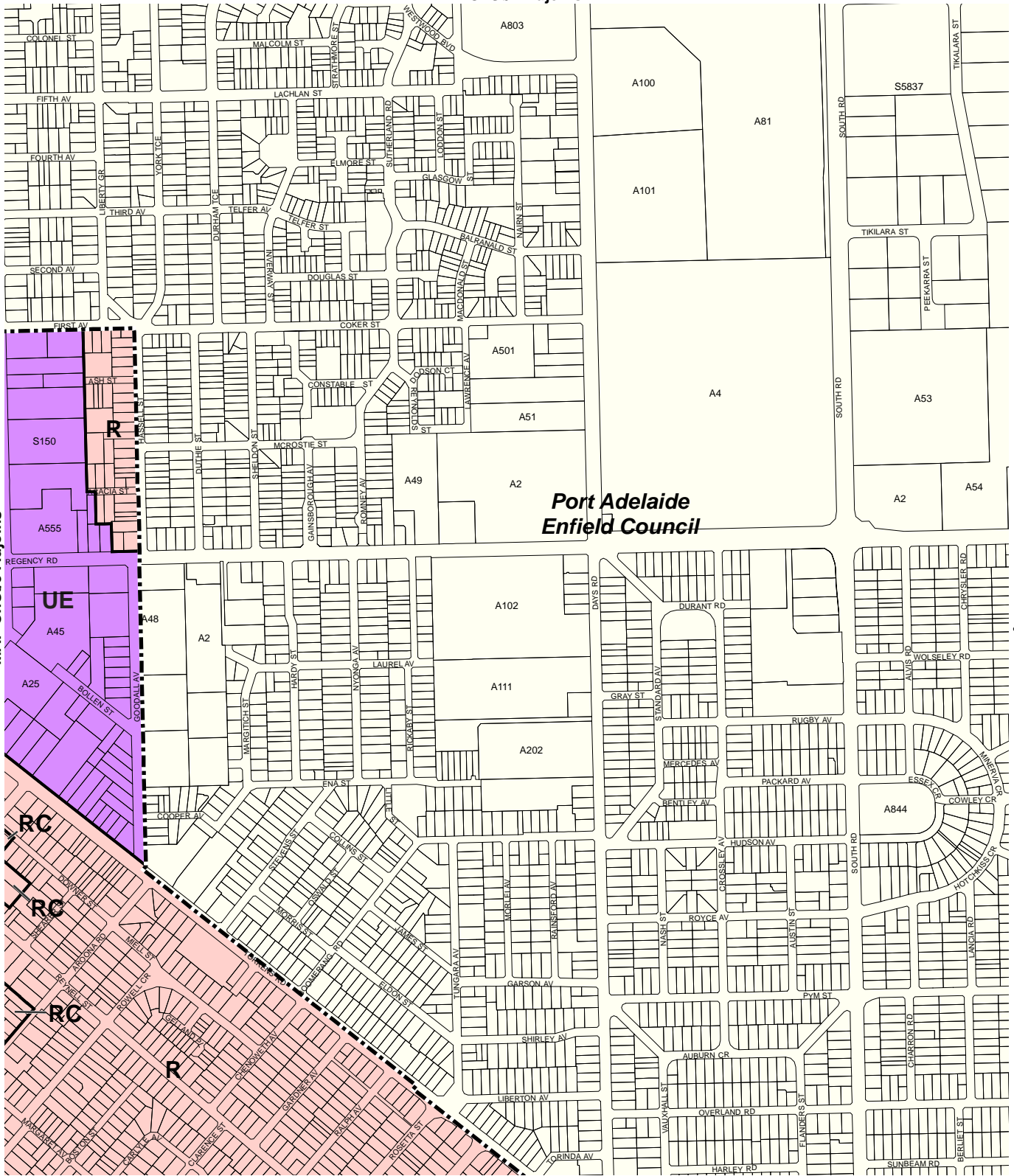


Overlay Map ChSt/10

HERITAGE

- Local heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

MAP ChSt/1 Adjoins



Port Adelaide
Enfield Council

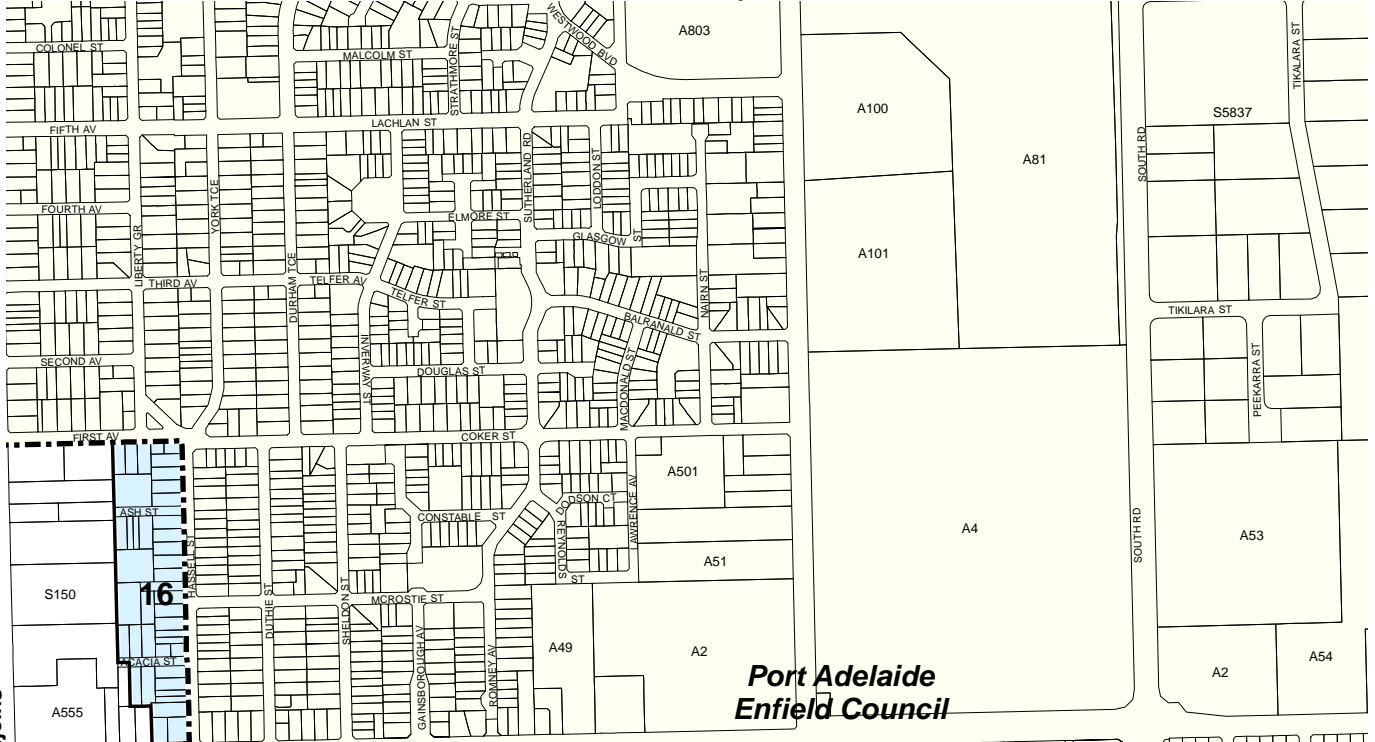
Lamberts Conformal Conic Projection, GDA94



- Zones**
- R Residential
 - RC Residential Character
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary

Zone Map ChSt/10

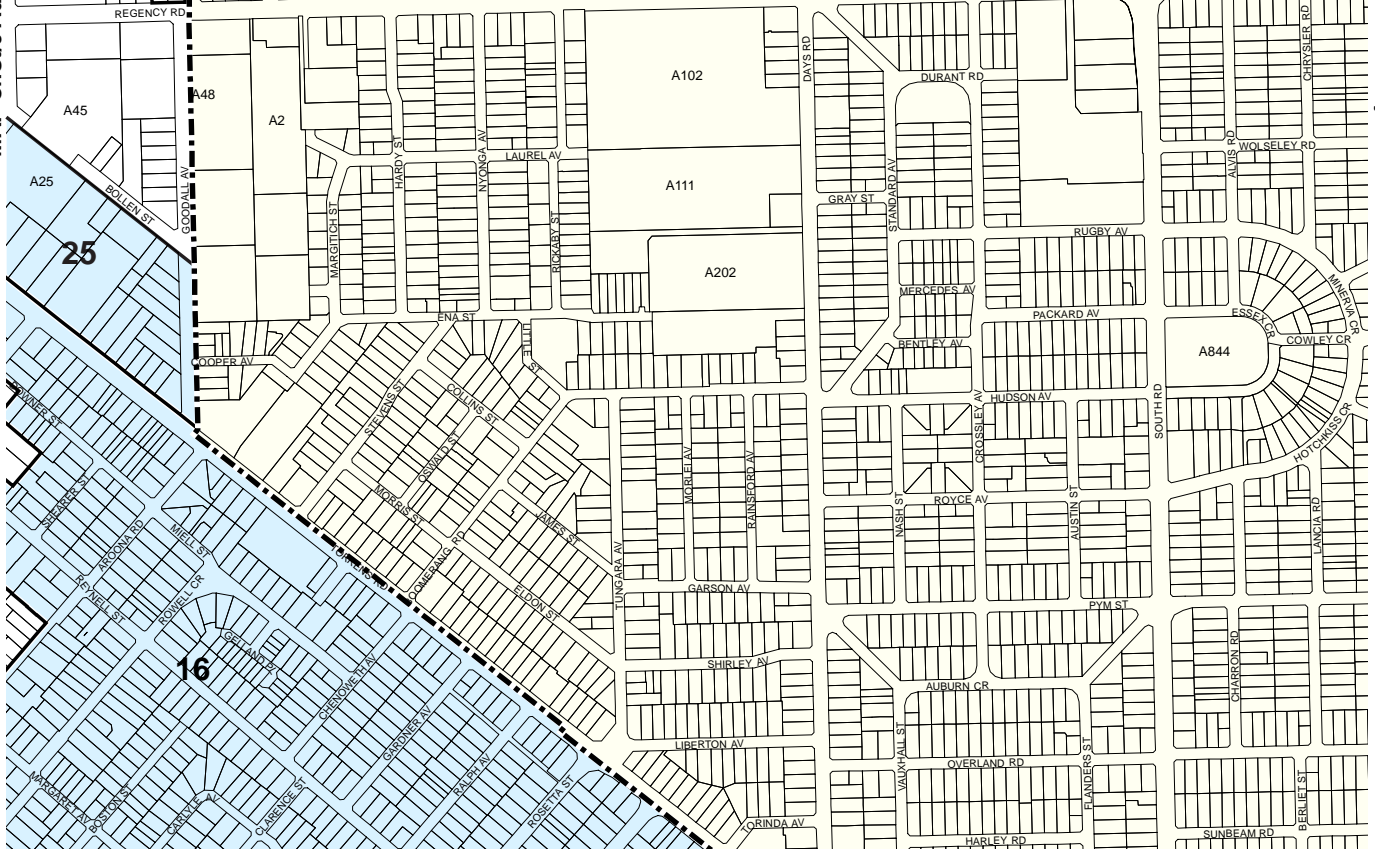
MAP ChSt/1 Adjoins



Port Adelaide
Enfield Council

MAP ChSt/9 Adjoins

MAP ChSt/1 Adjoins



MAP ChSt/15 Adjoins

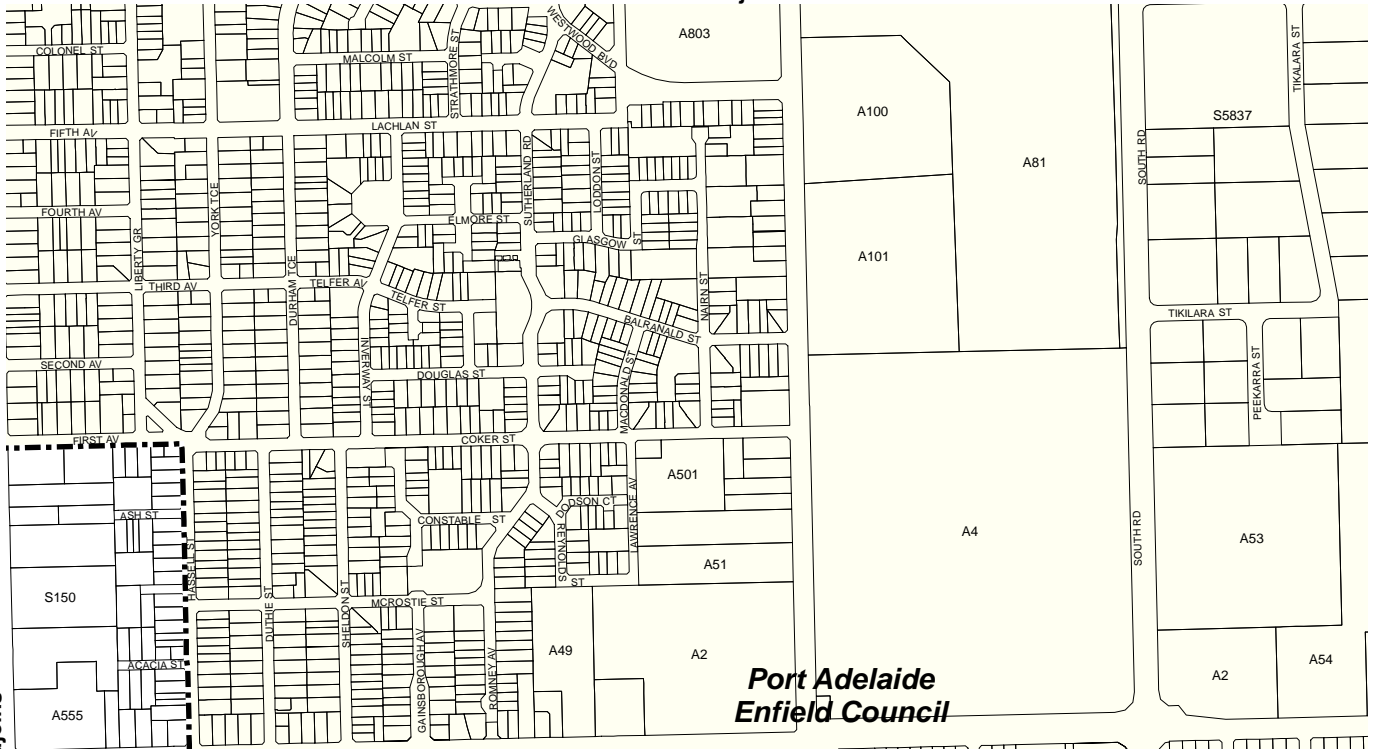
Lamberts Conformal Conic Projection, GDA94
Policy Area
 16 Mid Suburban
 25 Main Road Commercial



Policy Area Map ChSt/10

- Policy Area Boundary
- Development Plan Boundary

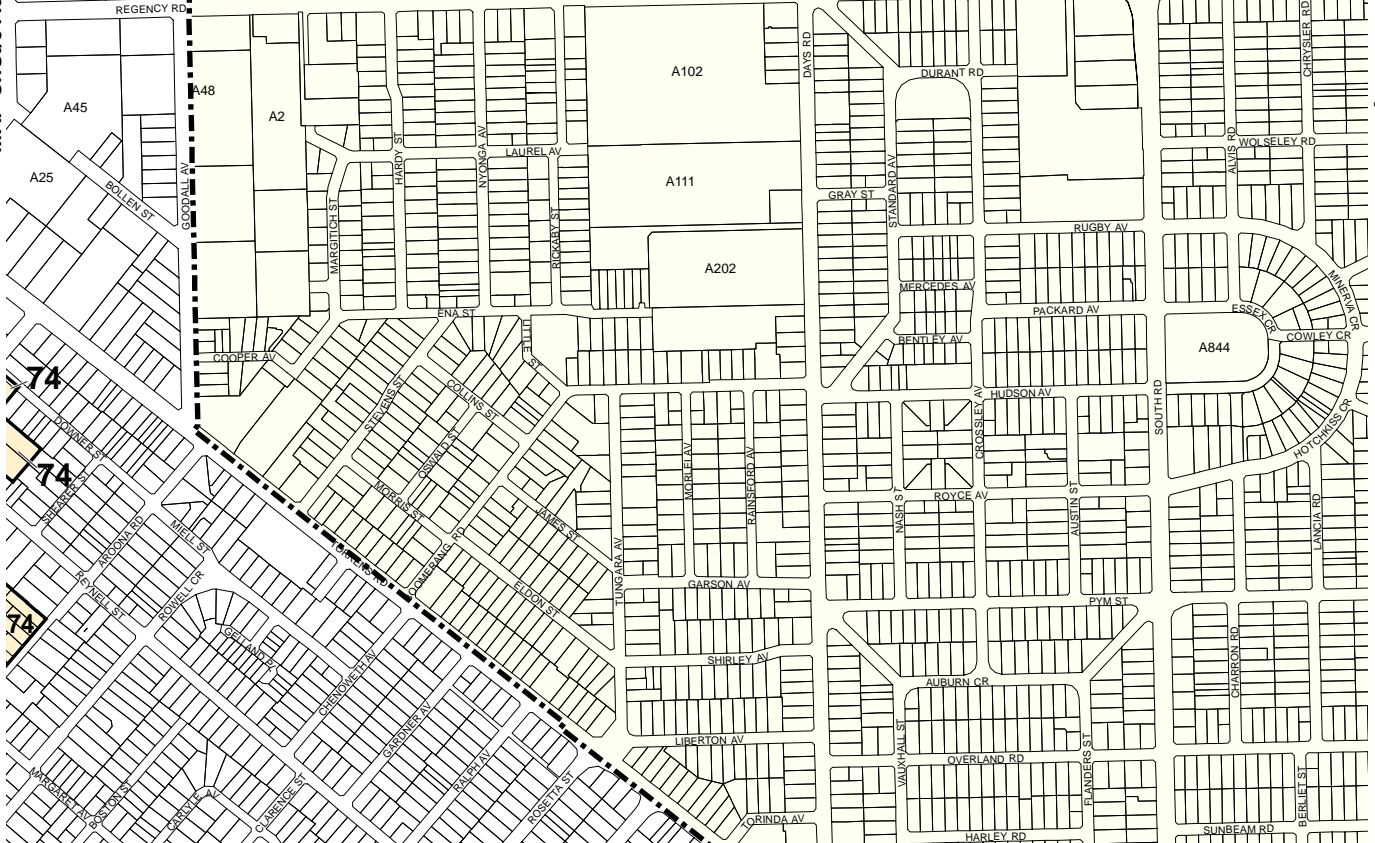
MAP ChSt/1 Adjoins



Port Adelaide
Enfield Council

MAP ChSt/9 Adjoins

MAP ChSt/1 Adjoins



MAP ChSt/15 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct
74 Kilkenny Character



Precinct Map ChSt/10

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

MAP ChSt/6 Adjoins

MAP ChSt/12 Adjoins

Land Not Within
a Council
Area (Metro)

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

Grange Jetty

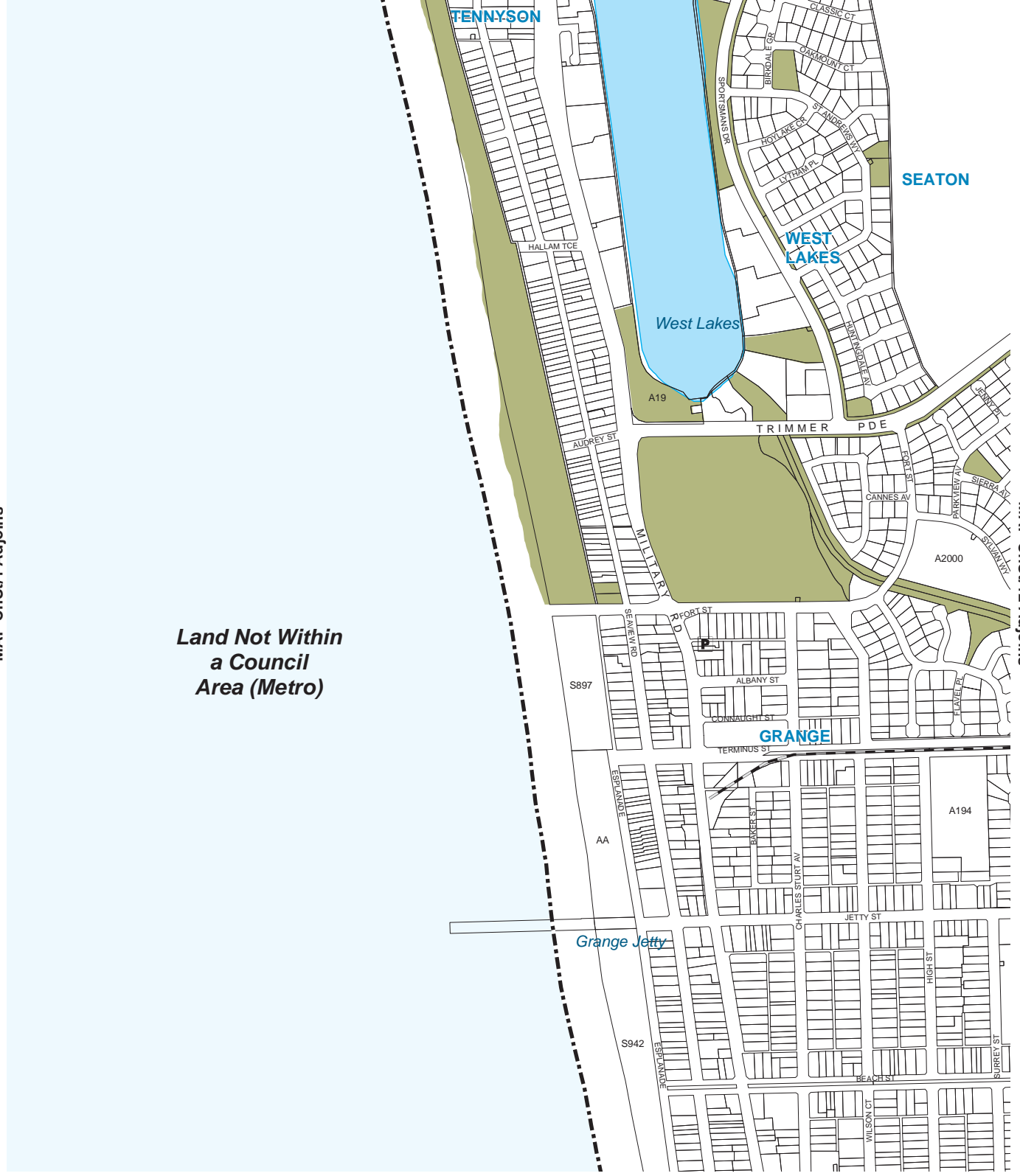
TENNYSON






SEATON

WEST LAKES

West Lakes

GRANGE



-  Post Office
-  Railways
-  Local Reserves
-  Waterbodies
-  Development Plan Boundary

Location Map ChSt/11

MAP ChSt/6 Adjoins

MAP ChSt/1 Adjoins

Land Not Within
a Council
Area (Metro)

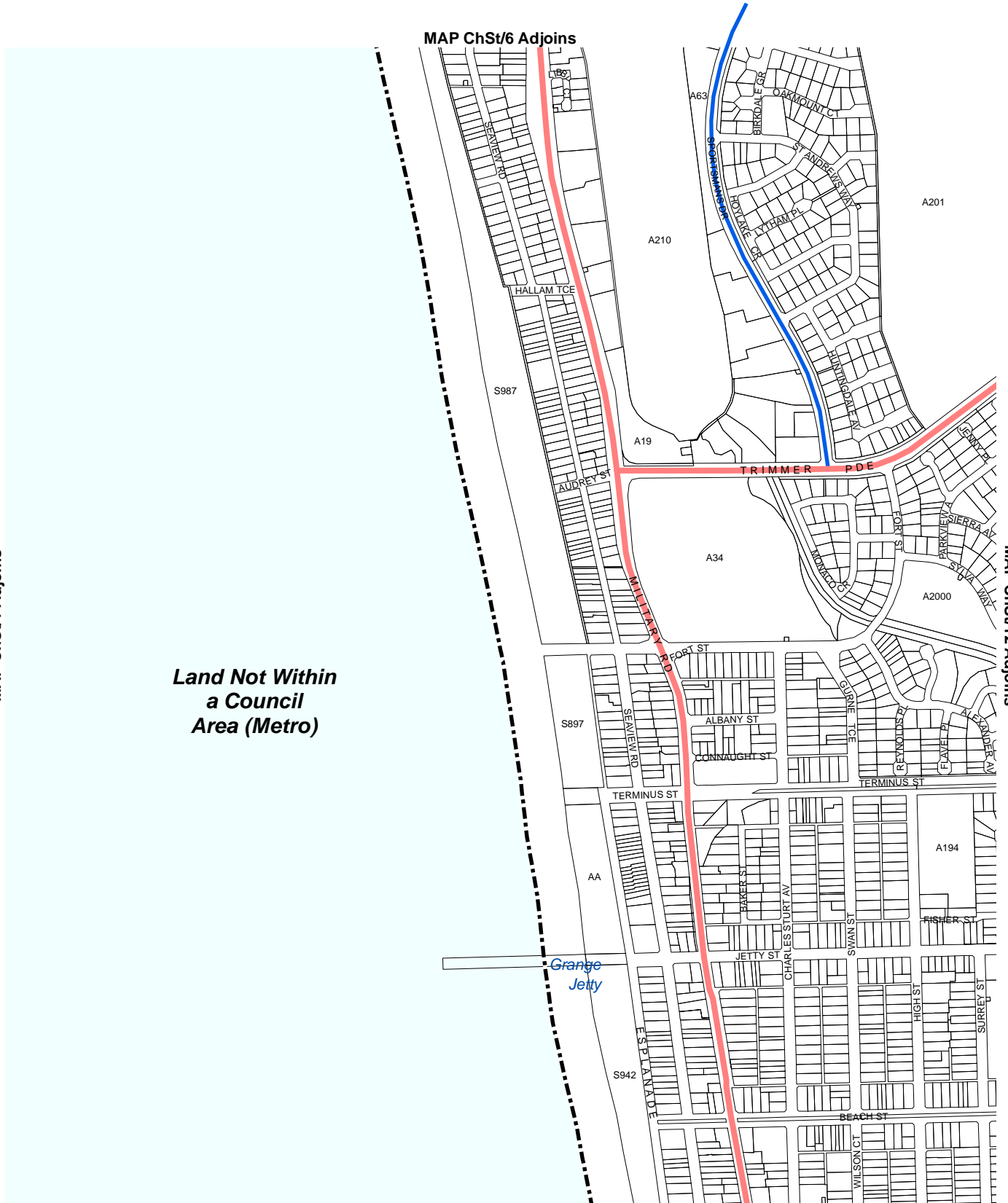
MAP ChSt/2 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

Overlay Map ChSt/11 TRANSPORT



MAP ChSt/1 Adjoins

MAP ChSt/6 Adjoins

MAP ChSt/12 Adjoins

Land Not Within
a Council
Area (Metro)

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

- Airport Building Heights**
- Zone C** All Structures Exceeding 15 metres above existing ground level
 - Zone D** All Structures Exceeding 45 metres above existing ground level
 - Zone E** All Structures Exceeding 100 metres above existing ground level



Overlay Map ChSt/11 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP ChSt/6 Adjoins

MAP ChSt/1 Adjoins

Land Not Within a Council Area (Metro)

MAP ChSt/12 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

Heritage points are indicative only. For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

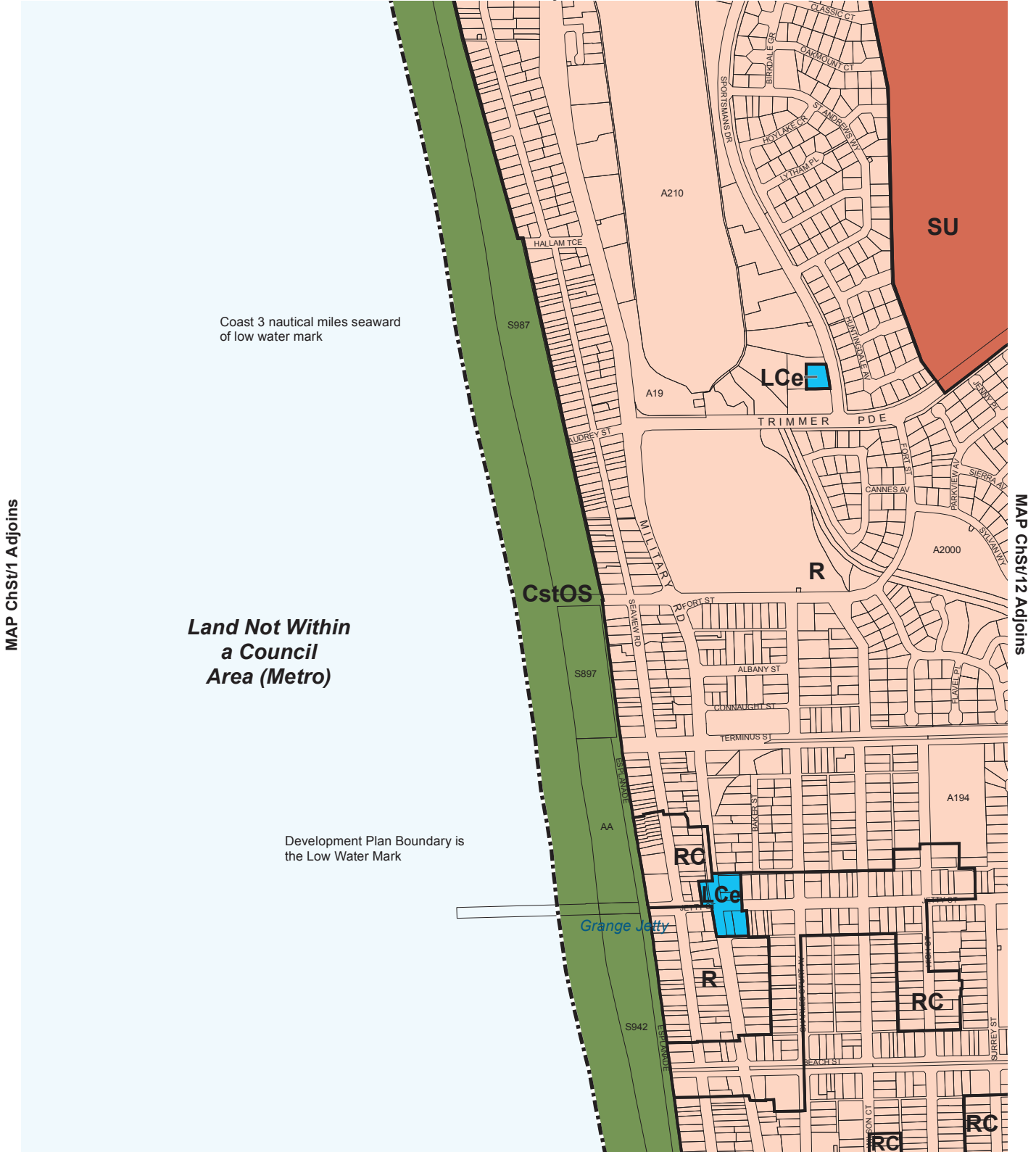


- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/11

HERITAGE

MAP ChSt/6 Adjoins



Land Not Within
a Council
Area (Metro)

Development Plan Boundary is
the Low Water Mark

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

Lamberts Conformal Conic Projection, GDA94



Zones

- CstOS Coastal Open Space
- LCe Local Centre
- R Residential
- RC Residential Character
- SU Special Use
- Zone Boundary
- Development Plan Boundary

Zone Map ChSt/11

MAP ChSt/6 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/12 Adjoins

Land Not Within
a Council
Area (Metro)

Development Plan Boundary is
the Low Water Mark

Grange
Jetty

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

- 16 Mid Suburban
- 17 Western Edge
- 18 West Lakes General
- 19 West Lakes Medium Density
- 7 Local Shopping



0 500 m

- Policy Area Boundary
- Development Plan Boundary

Policy Area Map ChSt/11

MAP ChSt/6 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/12 Adjoins

Land Not Within
a Council
Area (Metro)

Development Plan Boundary is
the Low Water Mark

Grange Jetty

MAP ChSt/1 Adjoins

MAP ChSt/17 Adjoins



Lamberts Conformal Conic Projection, GDA94

Precinct
80 Grange

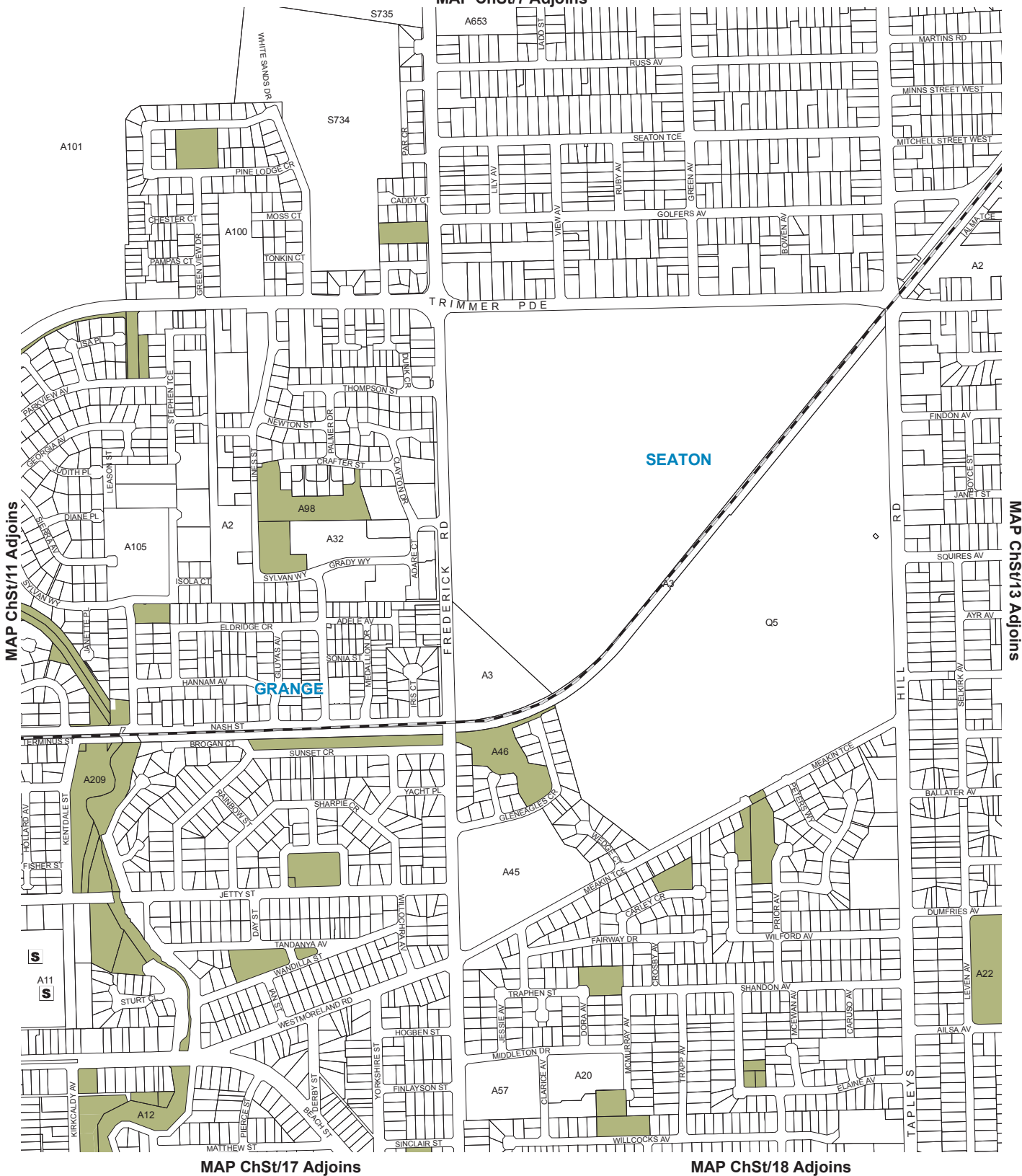


0 500 m

Precinct Map ChSt/11

-  Precinct Boundary
-  Development Plan Boundary

MAP ChSt/7 Adjoins






SEATON

GRANGE

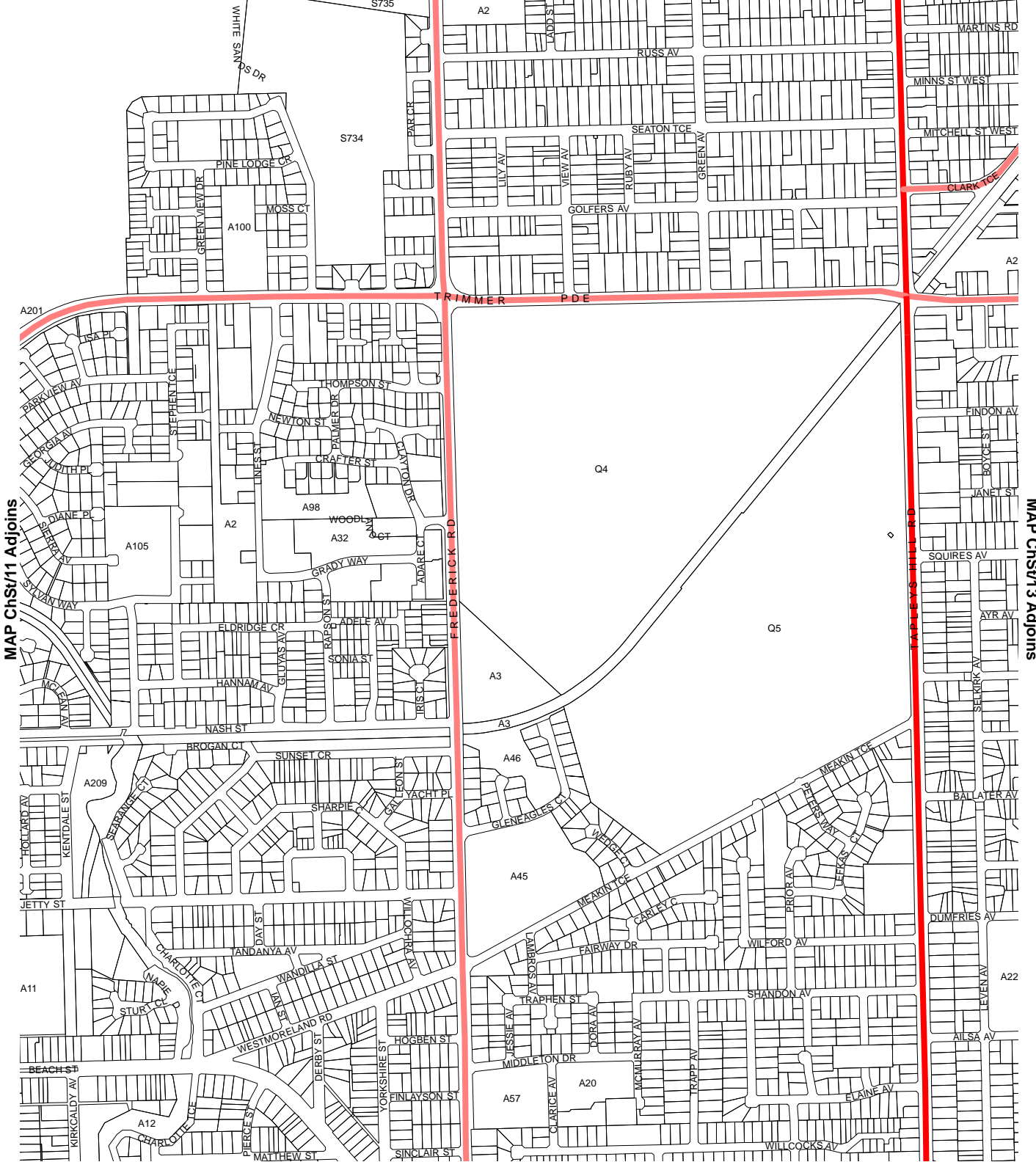


0 500 m

Location Map ChSt/12

-  School
-  Railways
-  Local Reserves

MAP ChSt/7 Adjoins



MAP ChSt/17 Adjoins

MAP ChSt/18 Adjoins

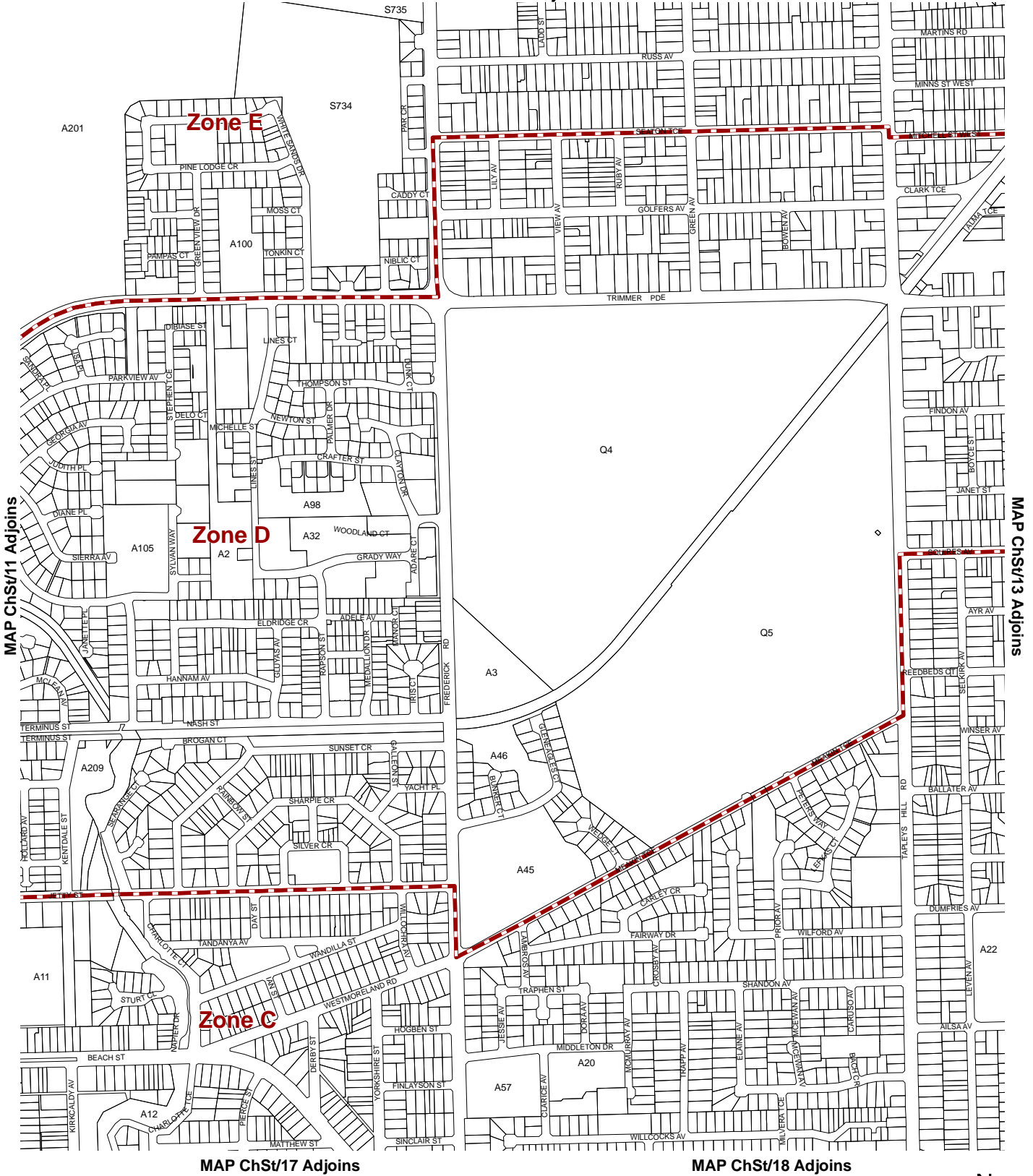


Overlay Map ChSt/12

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads

MAP ChSt/7 Adjoins



Airport Building Heights

- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level

 Airport Building Heights



0  500m

Overlay Map ChSt/12 DEVELOPMENT CONSTRAINTS

MAP ChSt/7 Adjoins



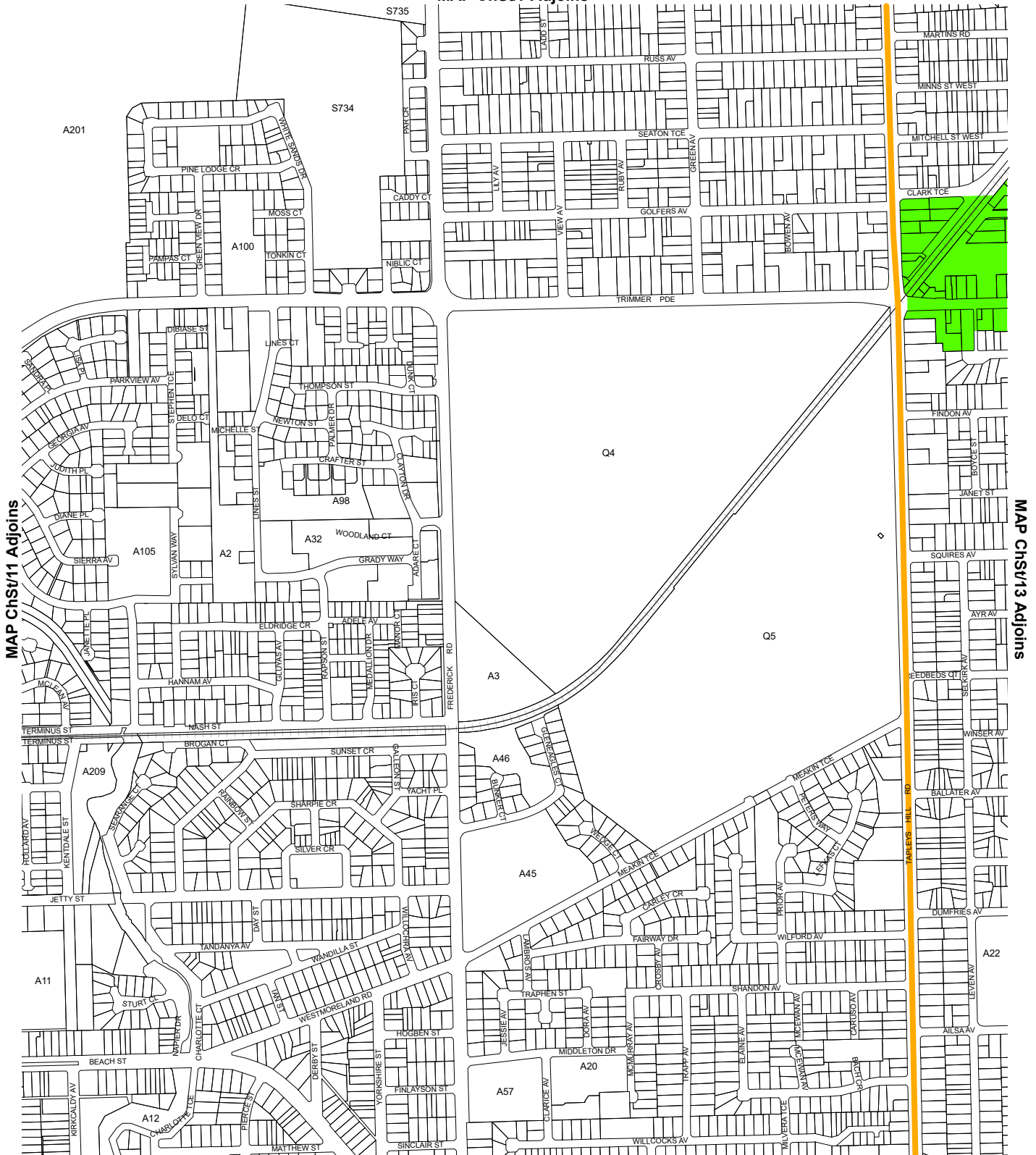
Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.

- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area

Overlay Map ChSt/12

HERITAGE

MAP ChSt/7 Adjoins






MAP ChSt/11 Adjoins

MAP ChSt/13 Adjoins

MAP ChSt/17 Adjoins

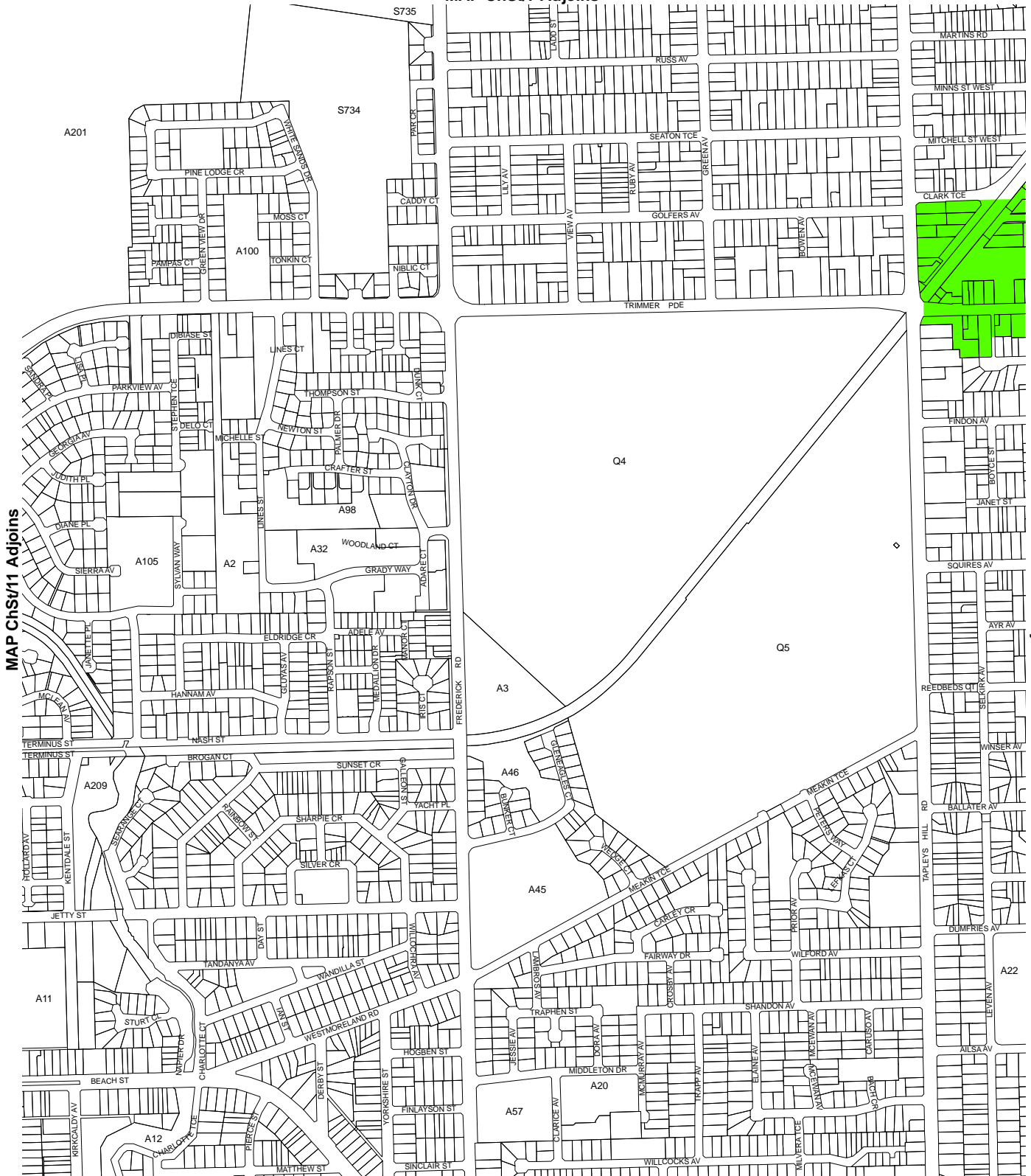
MAP ChSt/18 Adjoins

-  Type B
-  Railway
-  Noise and Air Emissions Designated Area



Overlay Map ChSt/12 NOISE AND AIR EMISSIONS

MAP ChSt/7 Adjoins

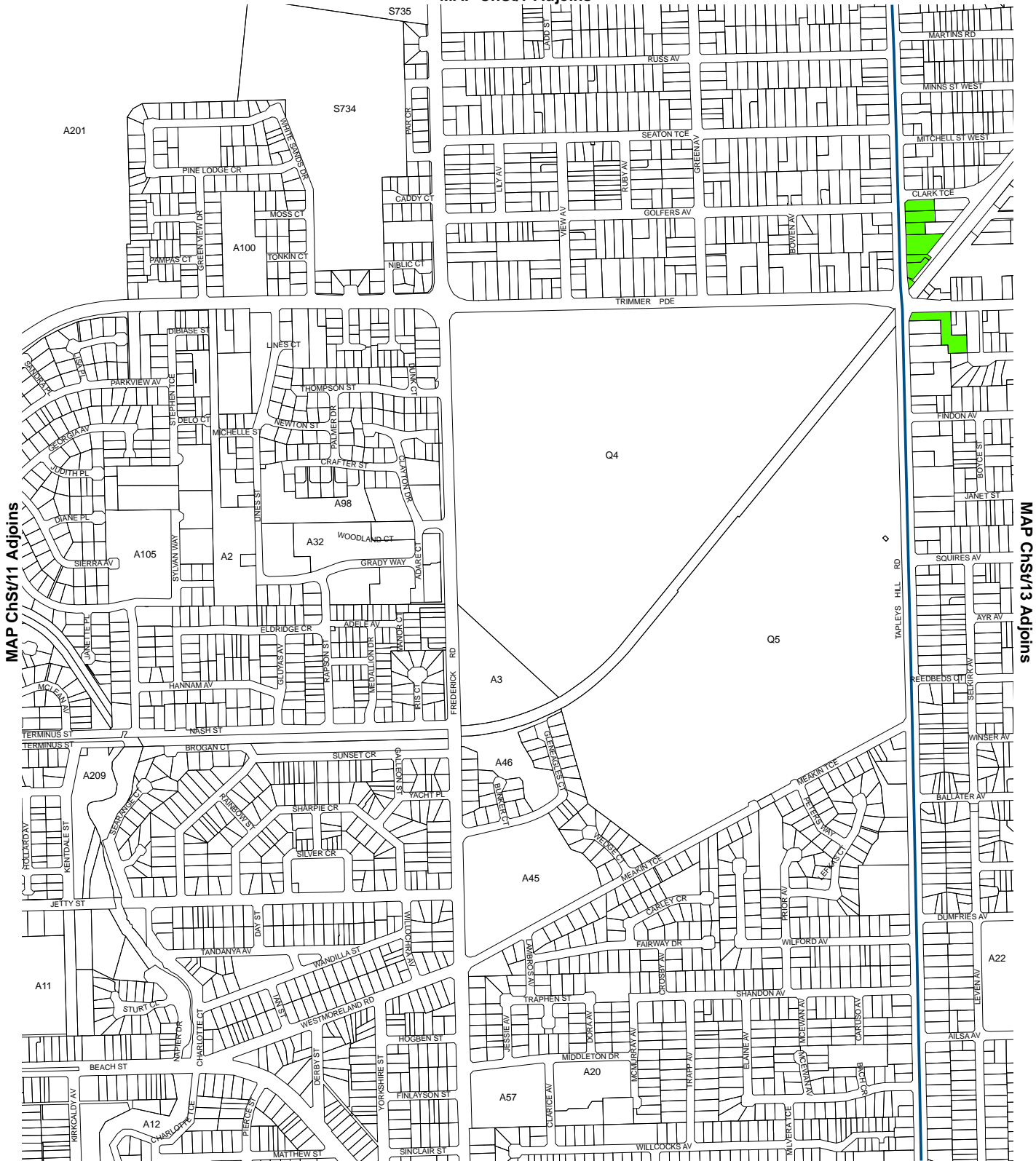


Overlay Map ChSt/12

AFFORDABLE HOUSING

 Urban Affordable Designated Area

MAP ChSt/7 Adjoins



MAP ChSt/11 Adjoins

MAP ChSt/13 Adjoins



MAP ChSt/17 Adjoins

MAP ChSt/18 Adjoins

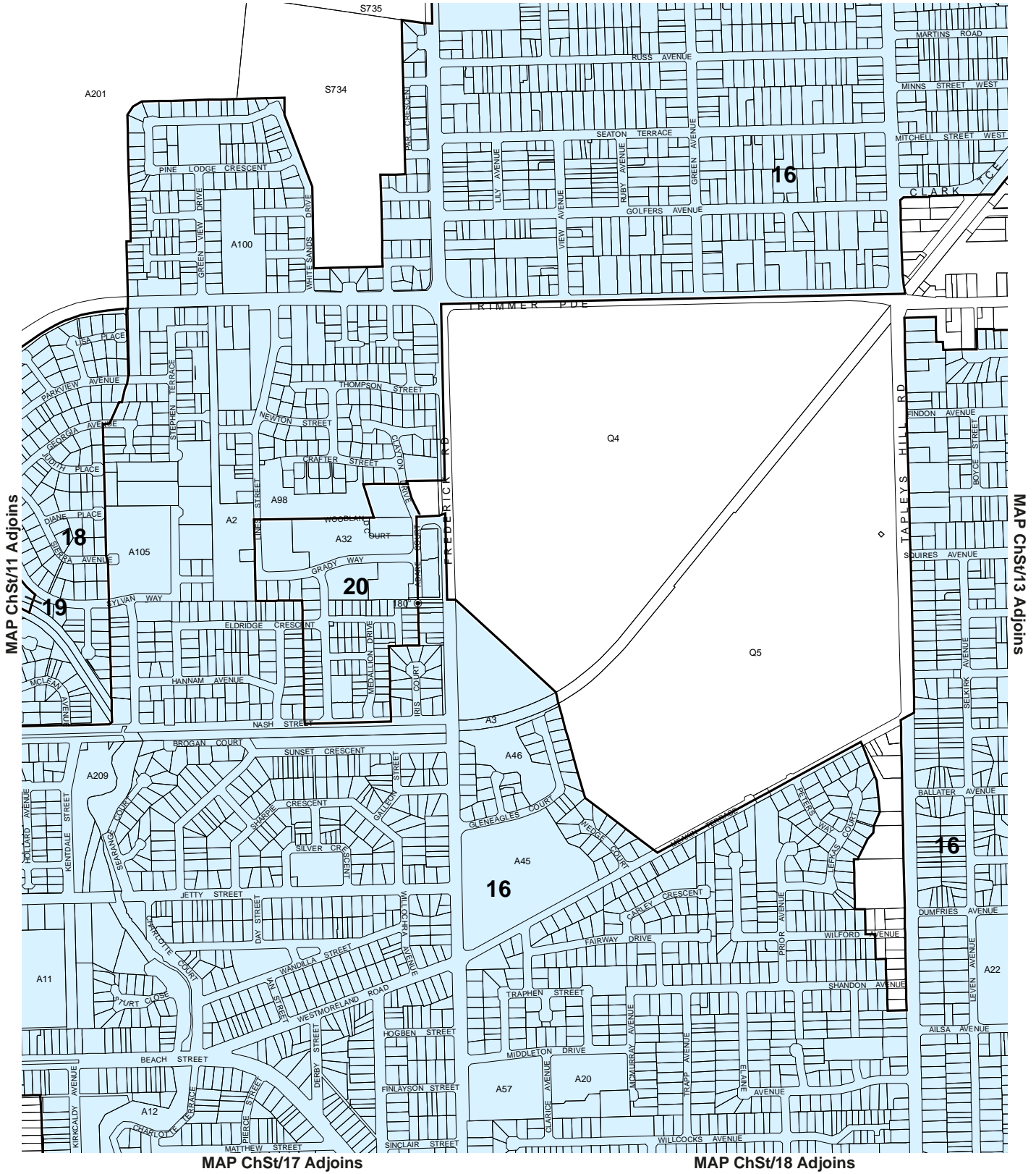


Overlay Map ChSt/12

STRATEGIC TRANSPORT ROUTES

-  Strategic Transport Route
-  Strategic Transport Routes Designated Area

MAP ChSt/7 Adjoins



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 16 Mid Suburban
- 18 West Lakes General
- 19 West Lakes Medium Density
- 20 Integrated Medium Density



Policy Area Map ChSt/12

 Policy Area Boundary

MAP ChSt/7 Adjoins



- Lamberts Conformal Conic Projection, GDA94
- Precinct**
- 47 Mixed Use
 - 80 Grange

 Precinct Boundary

Precinct Map ChSt/12

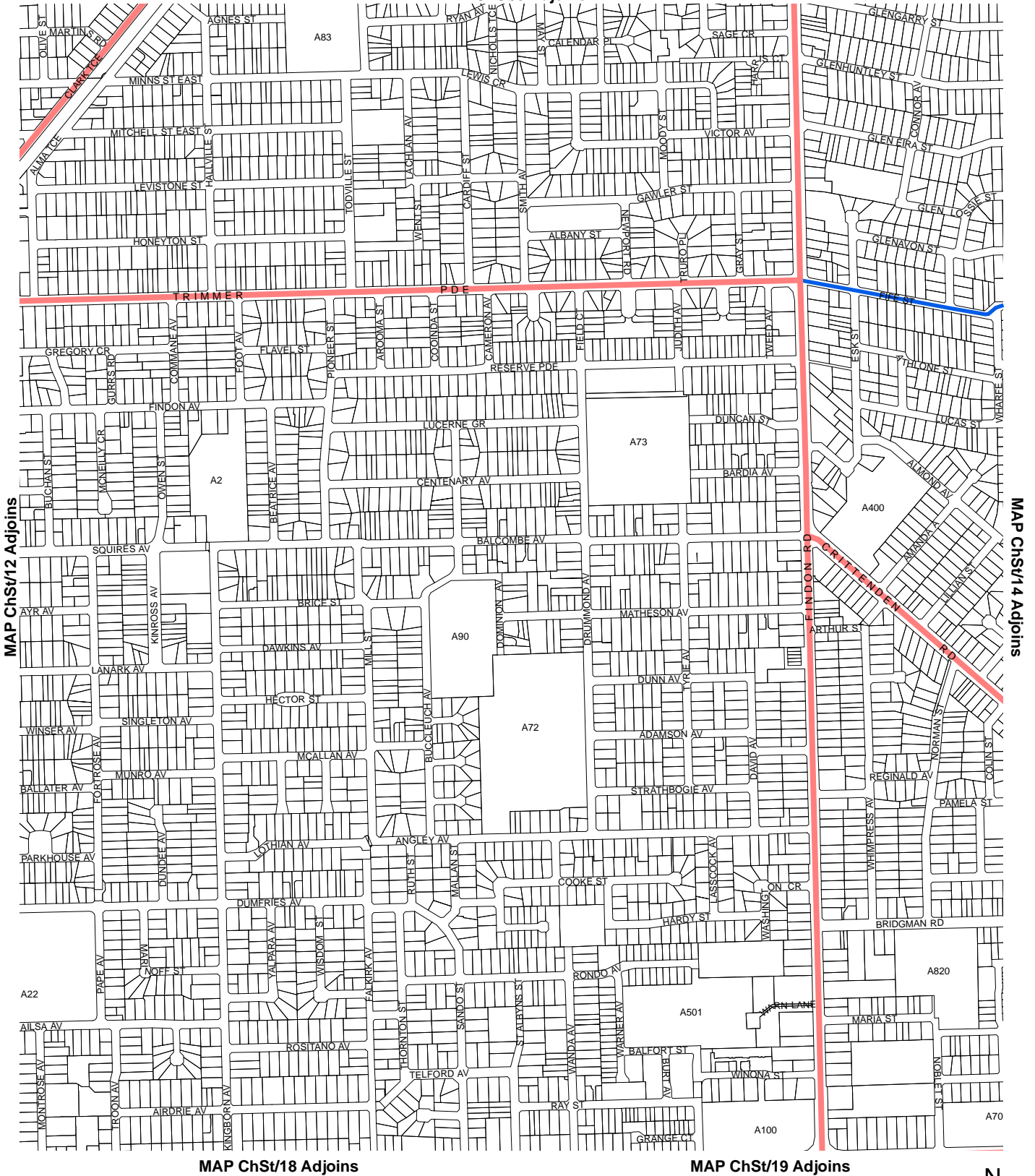
MAP ChSt/8 Adjoins



Location Map ChSt/13

- S** School
- L** Public Library
- P** Post Office
- H** Other Health Services
- Railways
- Local Reserves

MAP ChSt/8 Adjoins



MAP ChSt/12 Adjoins

MAP ChSt/14 Adjoins

MAP ChSt/18 Adjoins

MAP ChSt/19 Adjoins

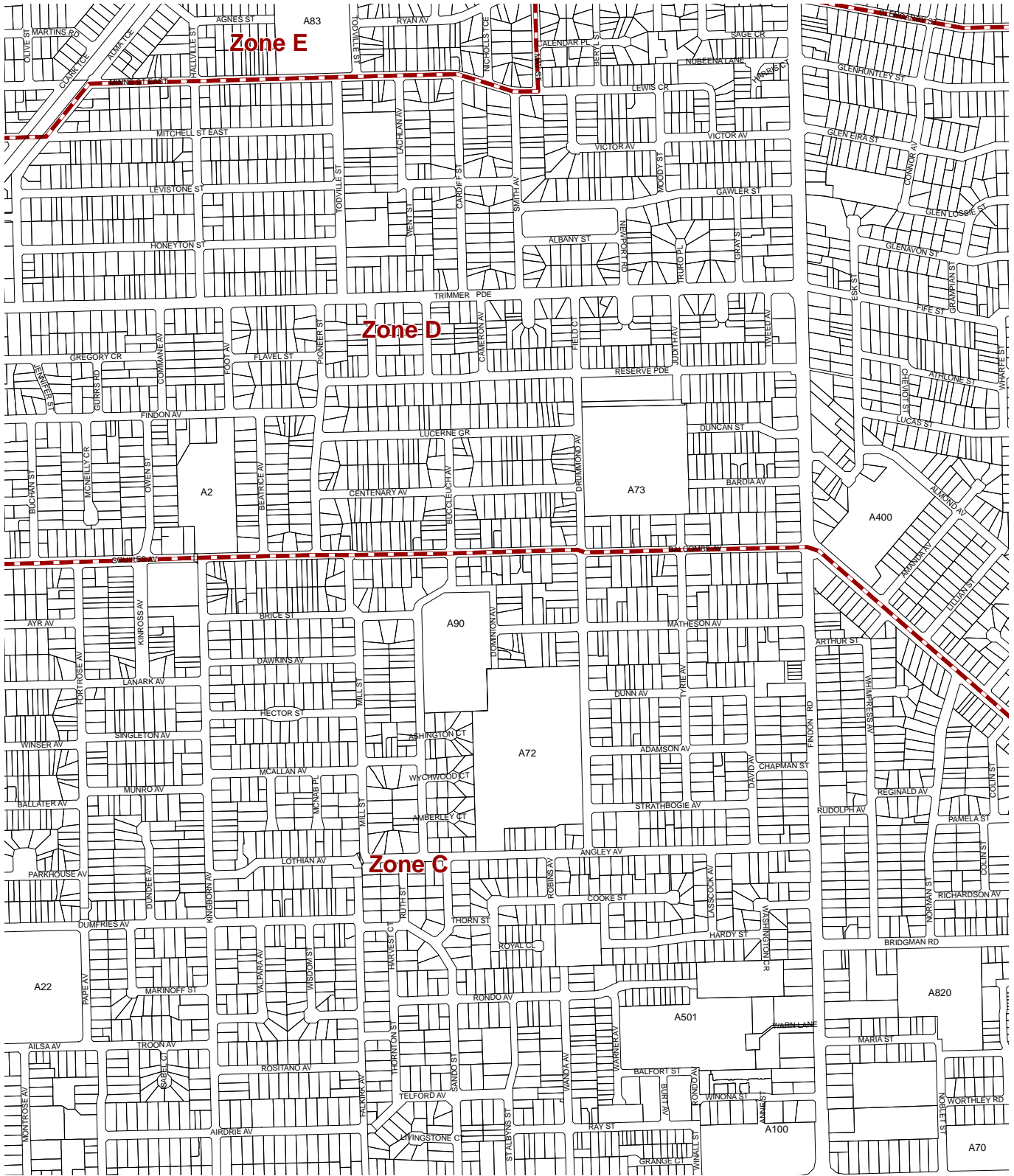


0 500m

Overlay Map ChSt/13

TRANSPORT

- Secondary Arterial Roads
- Distributor Roads



Airport Building Heights

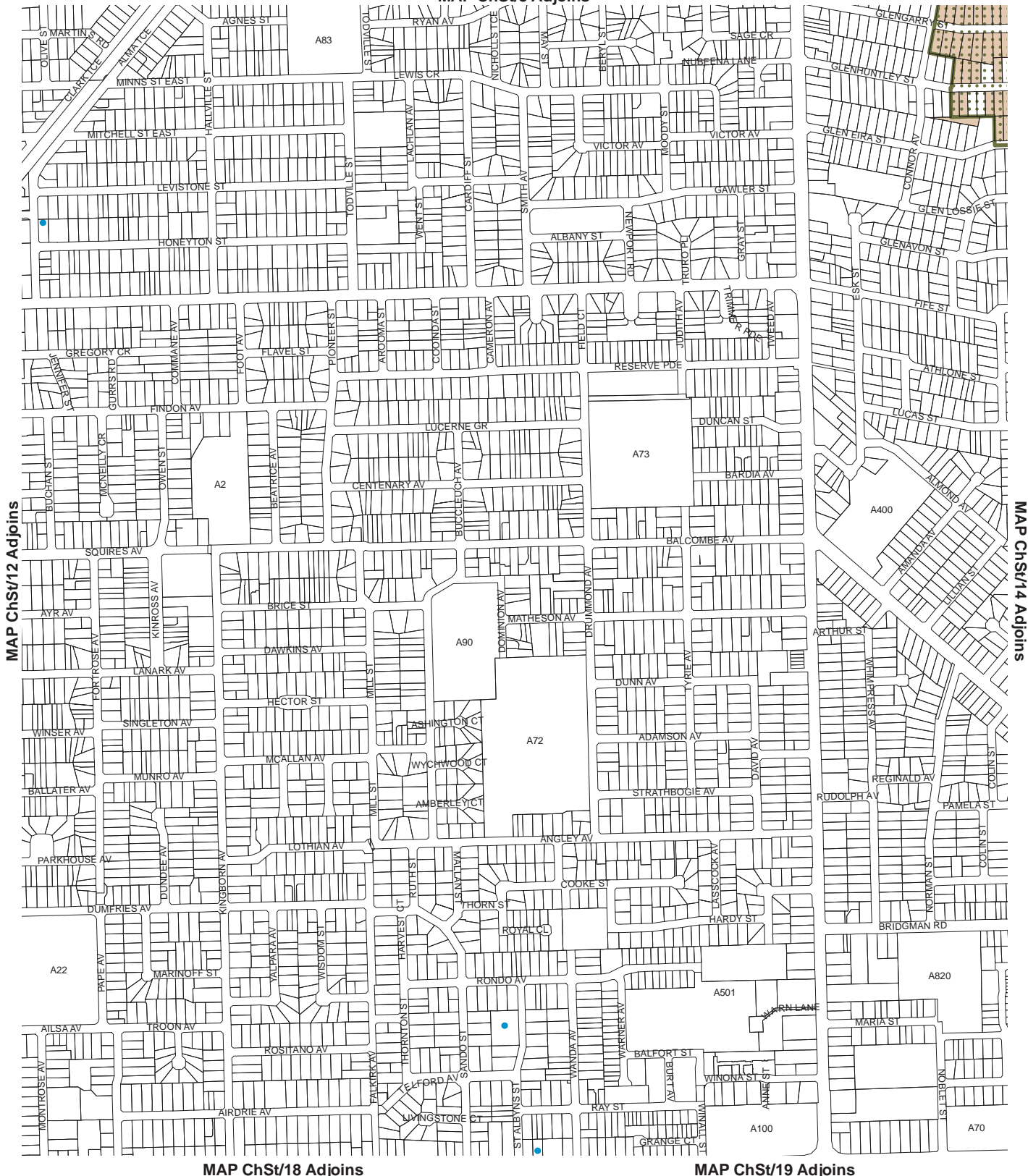
- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level



Overlay Map ChSt/13 DEVELOPMENT CONSTRAINTS

Airport Building Heights

MAP ChSt/8 Adjoins



MAP ChSt/12 Adjoins

MAP ChSt/14 Adjoins

MAP ChSt/18 Adjoins

MAP ChSt/19 Adjoins

Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

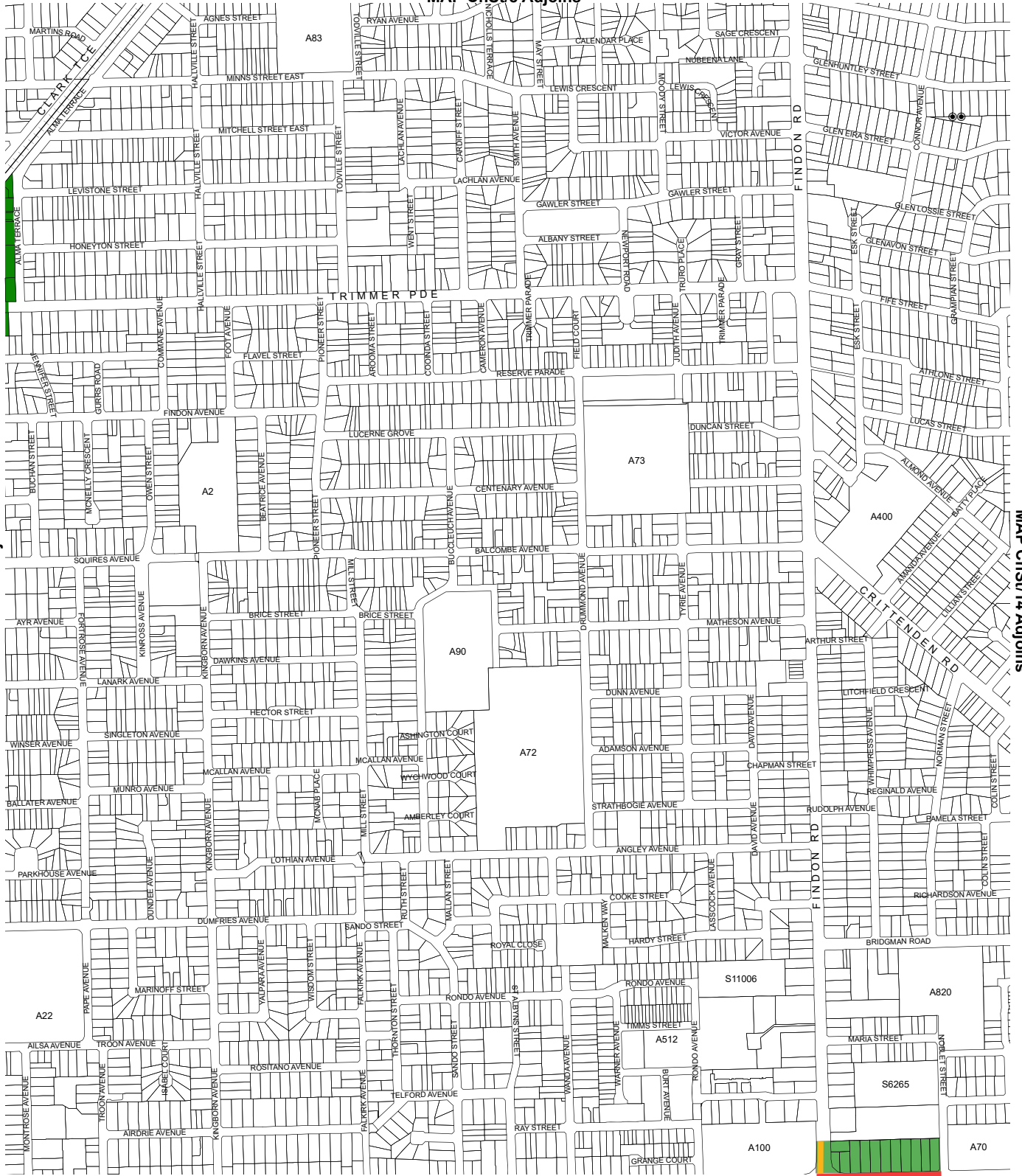


Overlay Map ChSt/13

HERITAGE

- Local heritage place
- Contributory item
- Historic Conservation Area

MAP ChSt/8 Adjoins







MAP ChSt/12 Adjoins

MAP ChSt/14 Adjoins

MAP ChSt/18 Adjoins

MAP ChSt/19 Adjoins

-  Type A Road
-  Type B Road
-  Railway
-  Noise and Air Emissions Designated Area



Overlay Map ChSt/13

NOISE AND AIR EMISSIONS

MAP ChSt/8 Adjoins



MAP ChSt/12 Adjoins

MAP ChSt/14 Adjoins

MAP ChSt/18 Adjoins

MAP ChSt/19 Adjoins

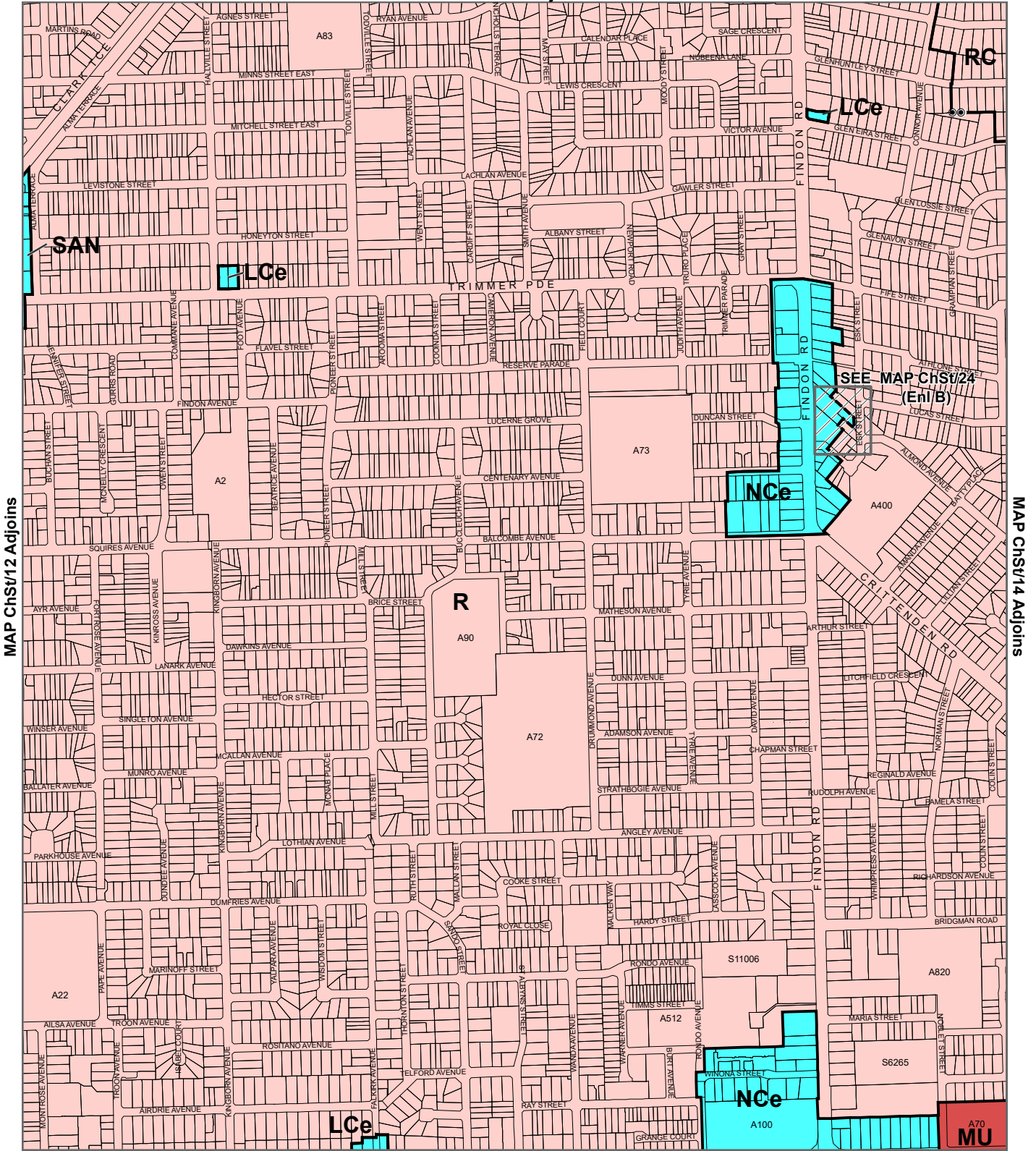


Overlay Map ChSt/13

AFFORDABLE HOUSING

 Urban Affordable Designated Area

MAP ChSt/8 Adjoins



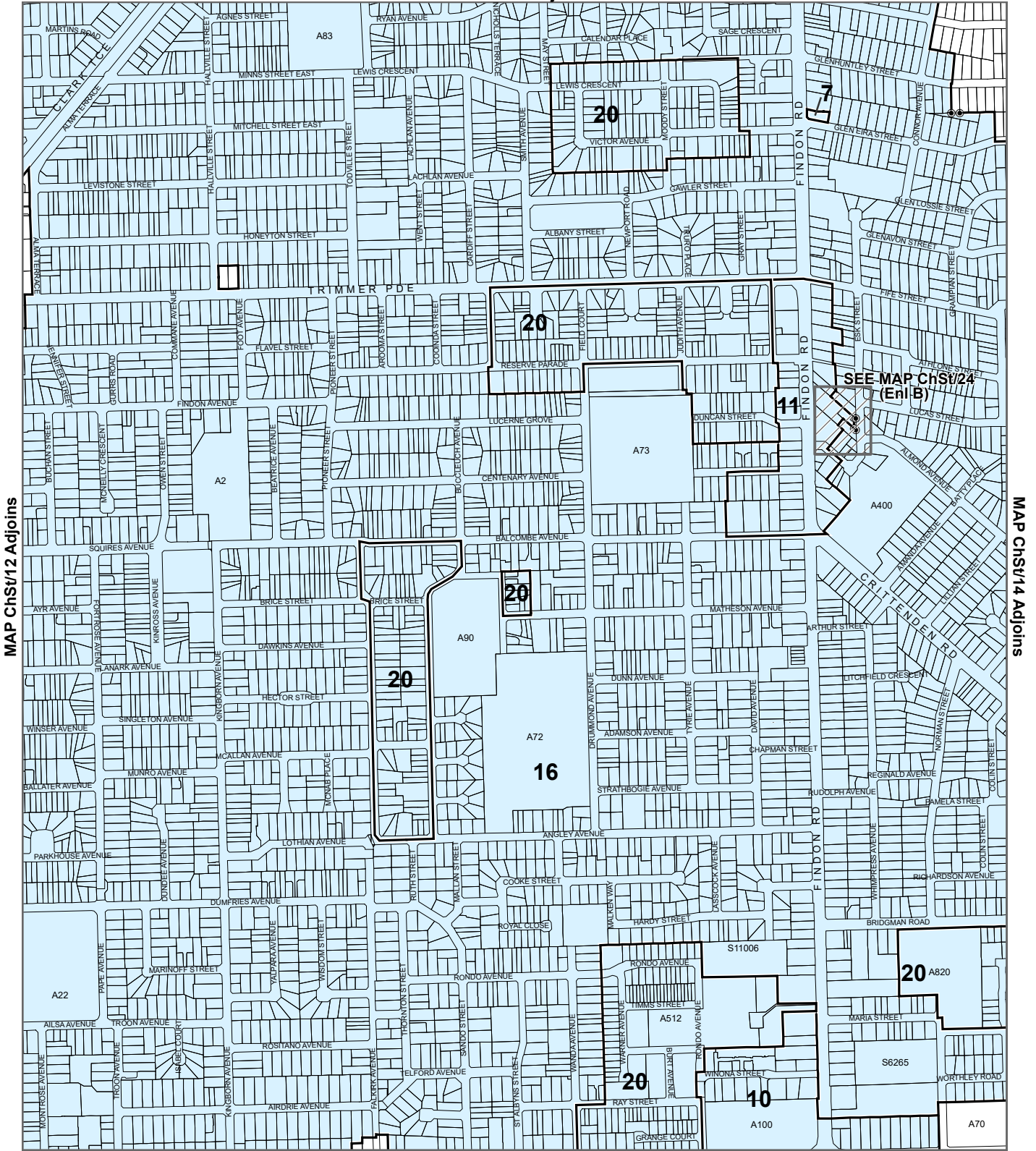
See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Zones

- LCe Local Centre
- MU Mixed Use
- NCe Neighbourhood Centre
- R Residential
- RC Residential Character
- SAN Suburban Activity Node
- Zone Boundary

Zone Map ChSt/13

MAP ChSt/8 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Policy Area

- 7 Local Shopping
- 10 Findon
- 11 Findon Road
- 16 Mid Suburban
- 20 Integrated Medium Density

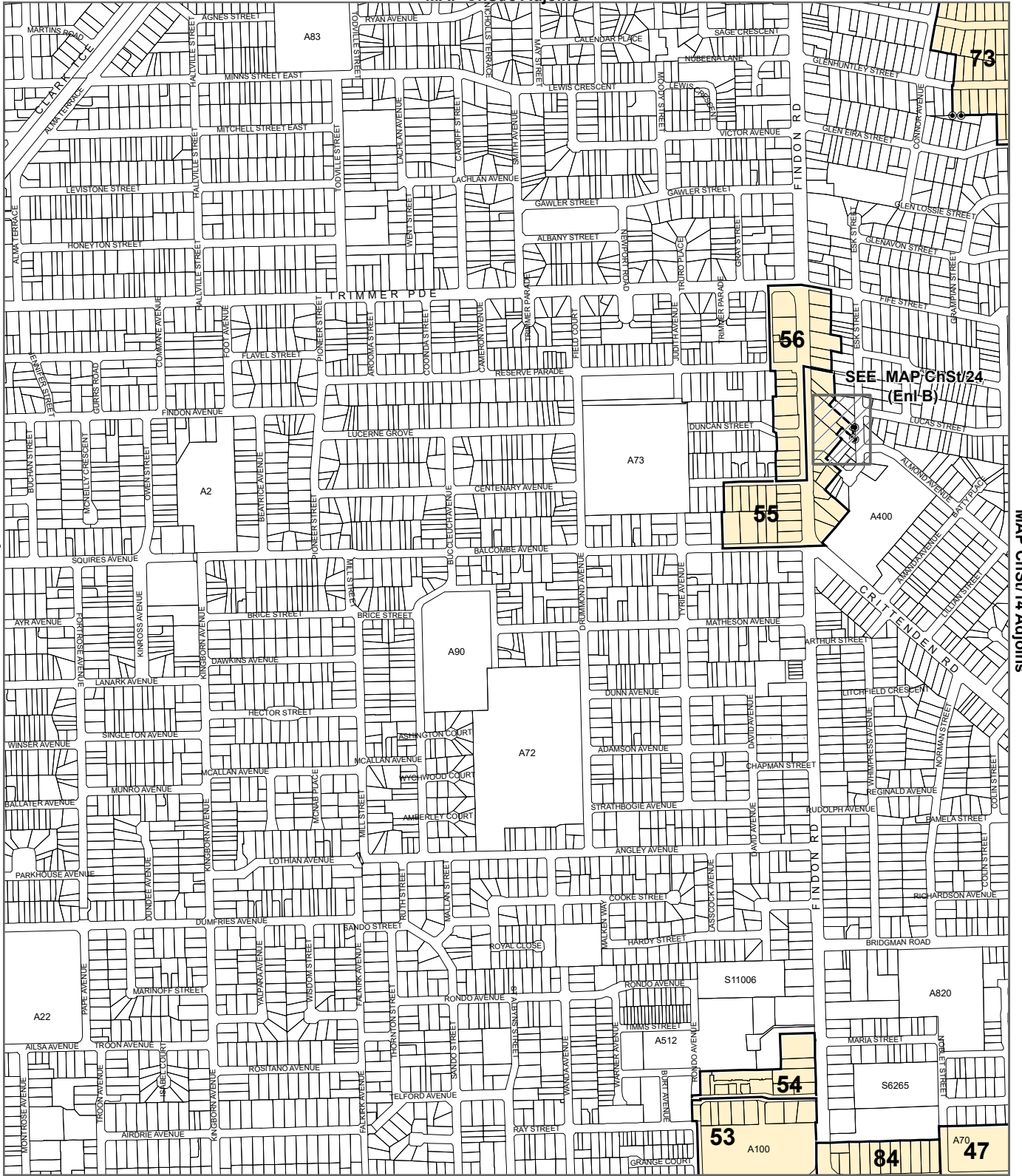
MAP ChSt/19 Adjoins



Policy Area Map ChSt/13

Policy Area Boundary

MAP ChSt/8 Adjoins



MAP ChSt/18 Adjoins

MAP ChSt/19 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Precinct

- 47 Mixed Use
- 53 Retail Core Findon
- 54 Winona Street
- 55 Retail Core Findon Road
- 56 Commercial Fringe Findon Road
- 73 Woodville South Character
- 84 Findon Centre East

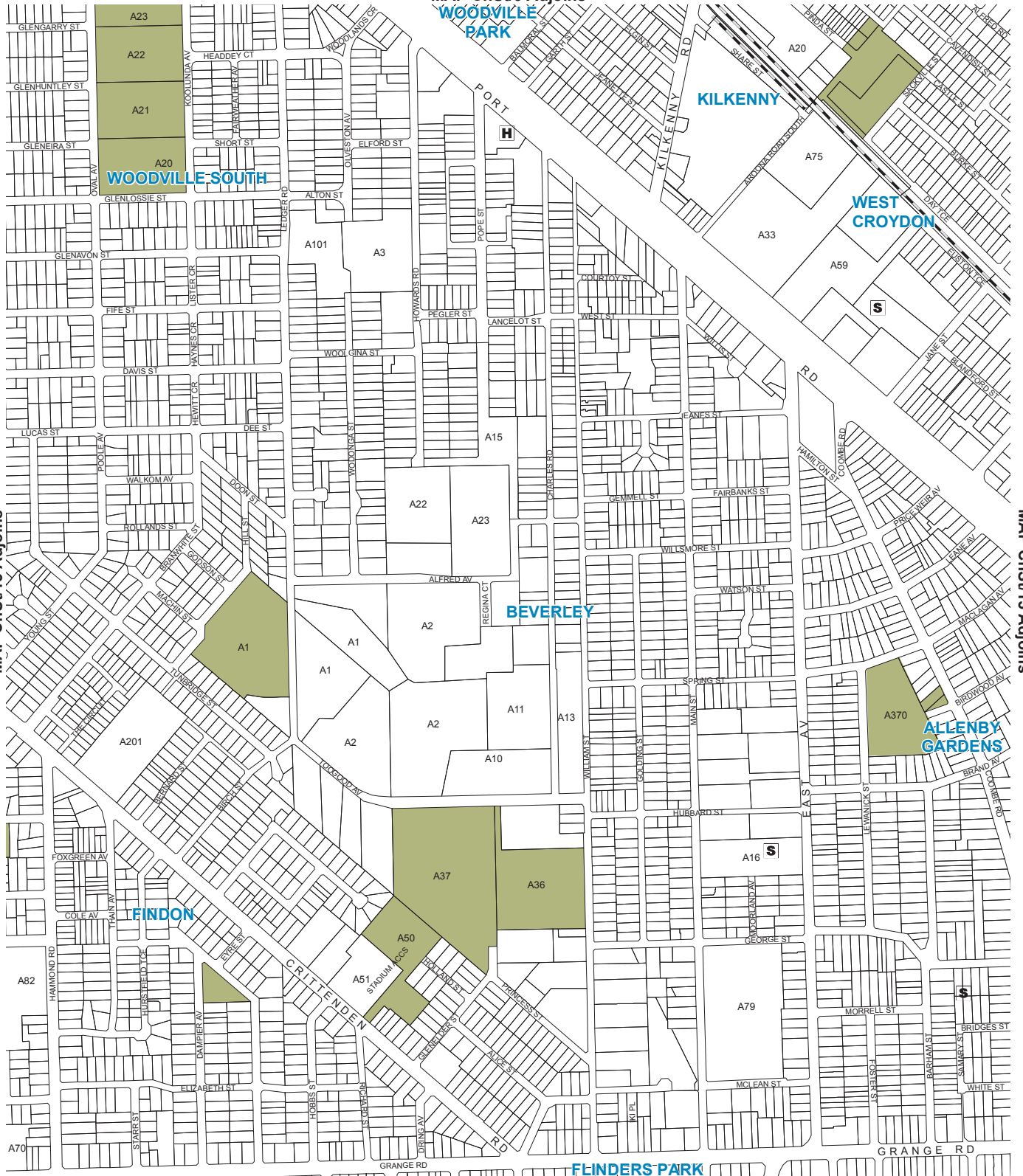


Precinct Map ChSt/13

Precinct Boundary

MAP ChSt9 Adjoins

WOODVILLE PARK



BEVERLEY

KILKENNY

WEST CROYDON

ALLENBY GARDENS

WOODVILLE SOUTH

FFINDON

FLINDERS PARK

MAP ChSt19 Adjoins

MAP ChSt20 Adjoins

MAP ChSt15 Adjoins

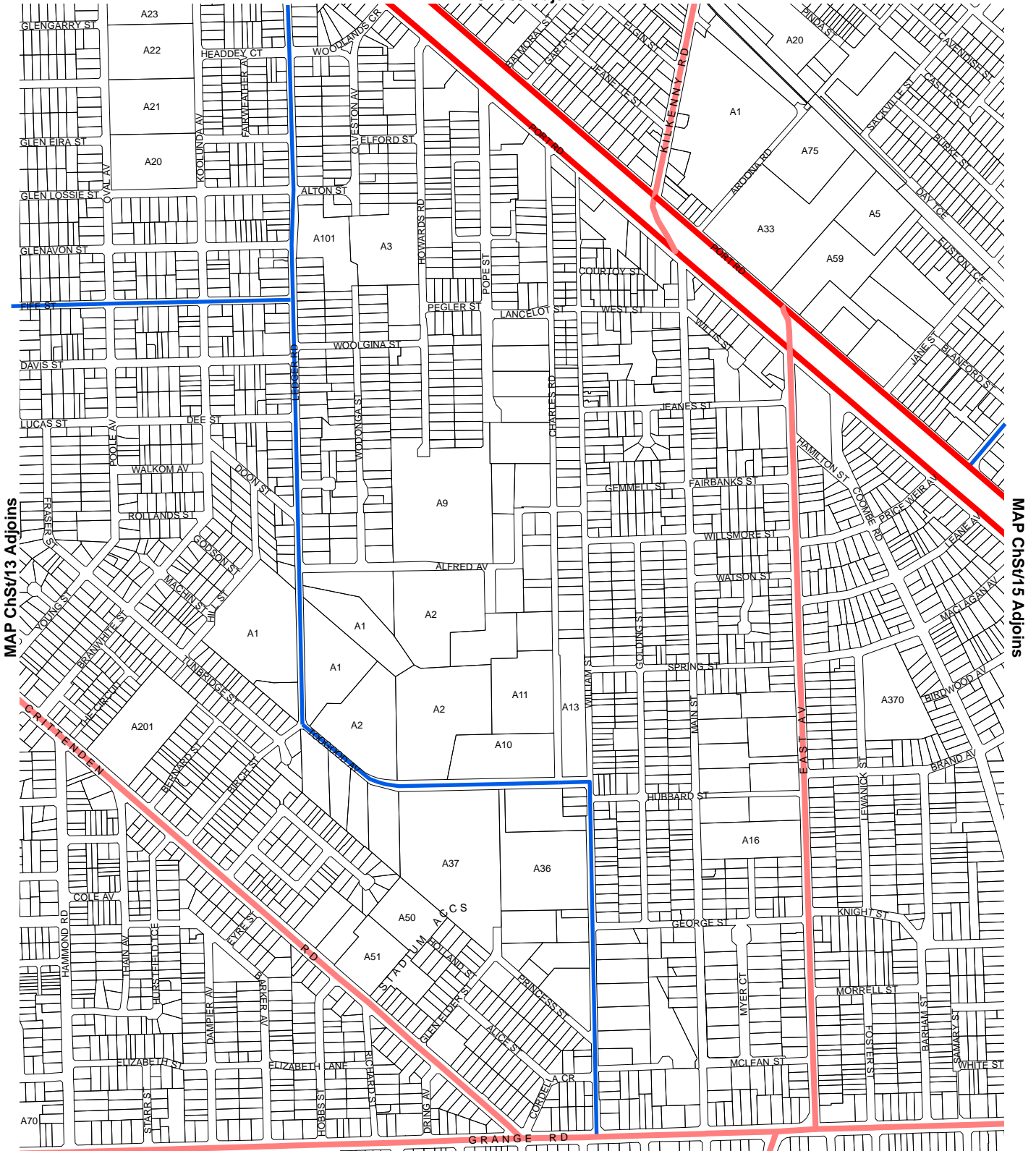
MAP ChSt13 Adjoins



Location Map ChSt/14

- S** School
- H** Other Health Services
- Railways
- Local Reserves

MAP ChSt/9 Adjoins



MAP ChSt/19 Adjoins

MAP ChSt/20 Adjoins



Overlay Map ChSt/14

TRANSPORT

- ▬ Primary Arterial Roads
- ▬ Secondary Arterial Roads
- ▬ Distributor Roads

MAP ChSt/9 Adjoins

Zone E

Zone E

Zone D

Zone C

MAP ChSt/13 Adjoins

MAP ChSt/15 Adjoins

MAP ChSt/19 Adjoins

MAP ChSt/20 Adjoins

Airport Building Heights

- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level

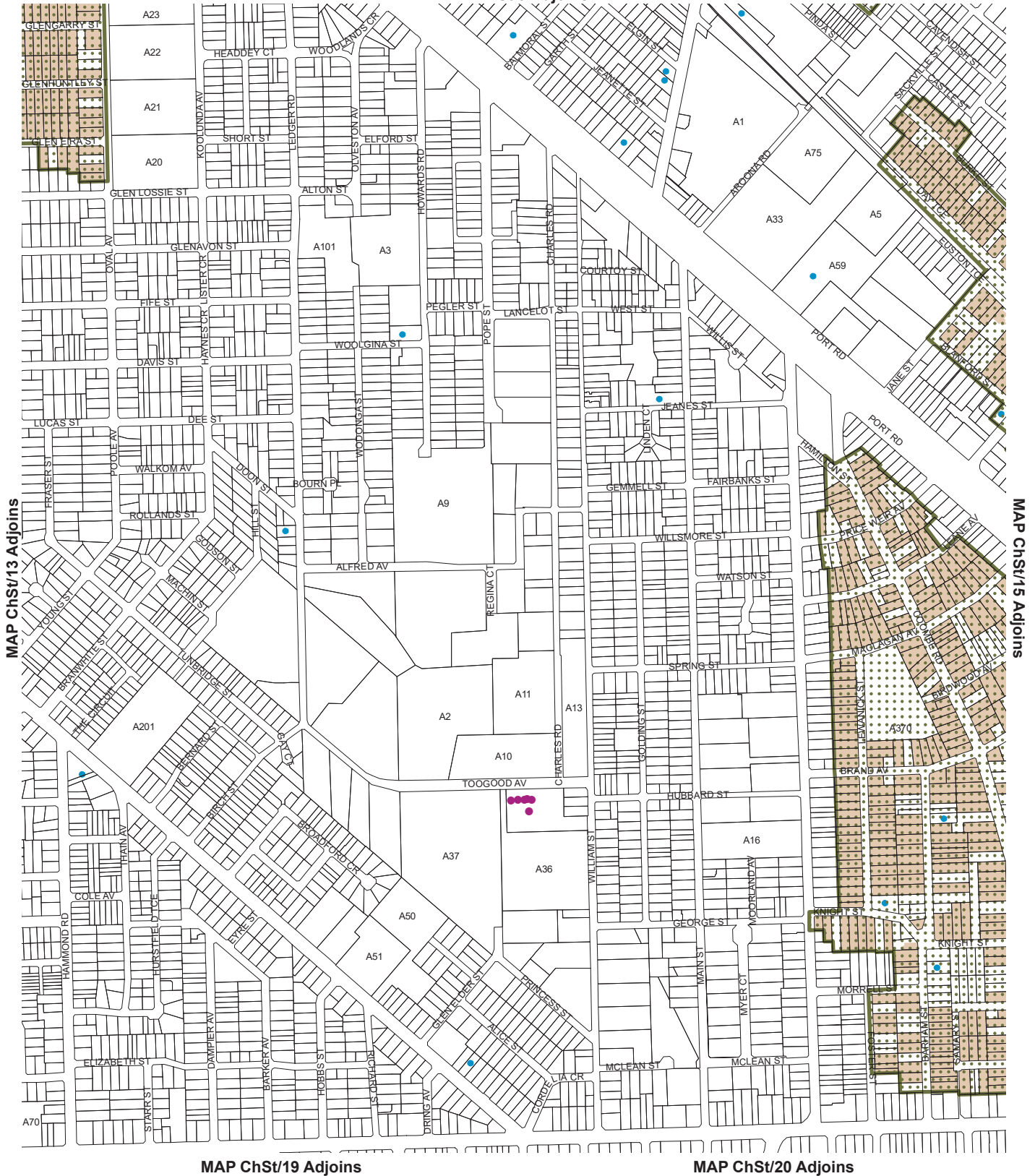


- Airport Building Heights
- Industry Interface Area

Overlay Map ChSt/14

DEVELOPMENT CONSTRAINTS

MAP ChSt/9 Adjoins



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 Items please refer to the relevant tables within this document.

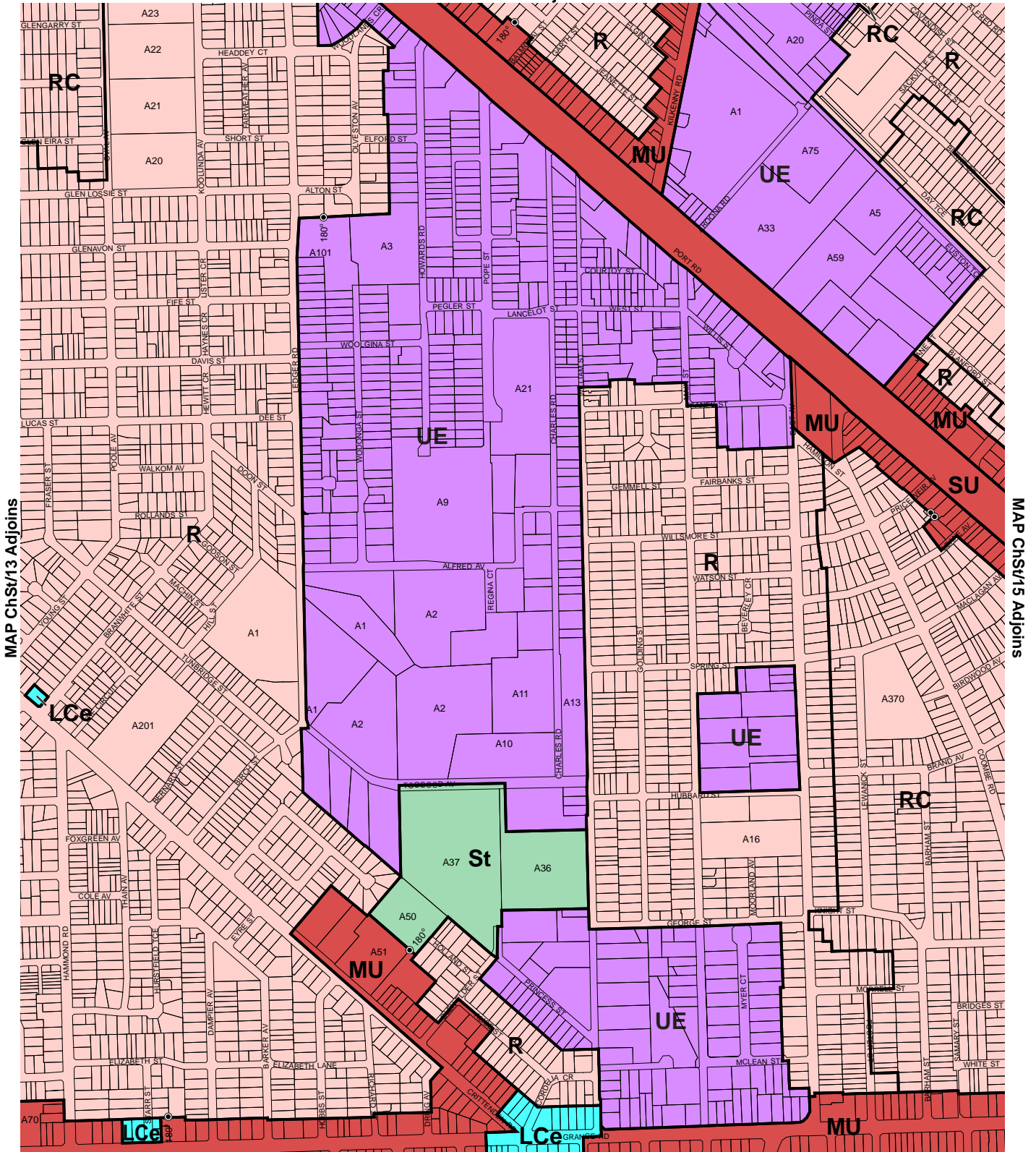


Overlay Map ChSt/14

HERITAGE

- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area

MAP ChSt/9 Adjoins



MAP ChSt/19 Adjoins

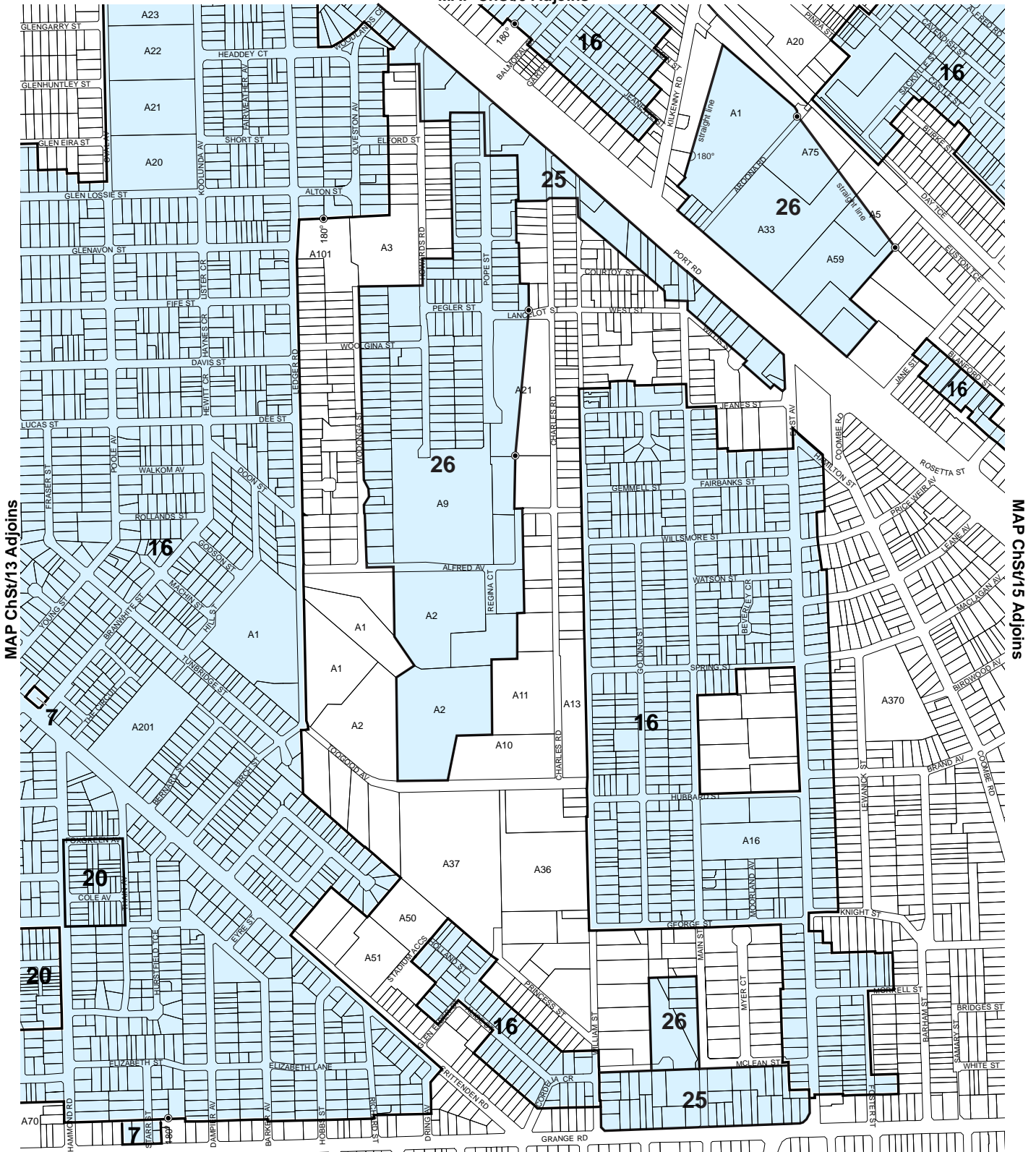
MAP ChSt/20 Adjoins

Lamberts Conformal Conic Projection, GDA94

- Zones**
- LCE Local Centre
 - MU Mixed Use
 - R Residential
 - RC Residential Character
 - SU Special Use
 - St Stadium
 - UE Urban Employment
 - Zone Boundary

Zone Map ChSt/14

MAP ChSt/9 Adjoins



MAP ChSt/19 Adjoins

MAP ChSt/20 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

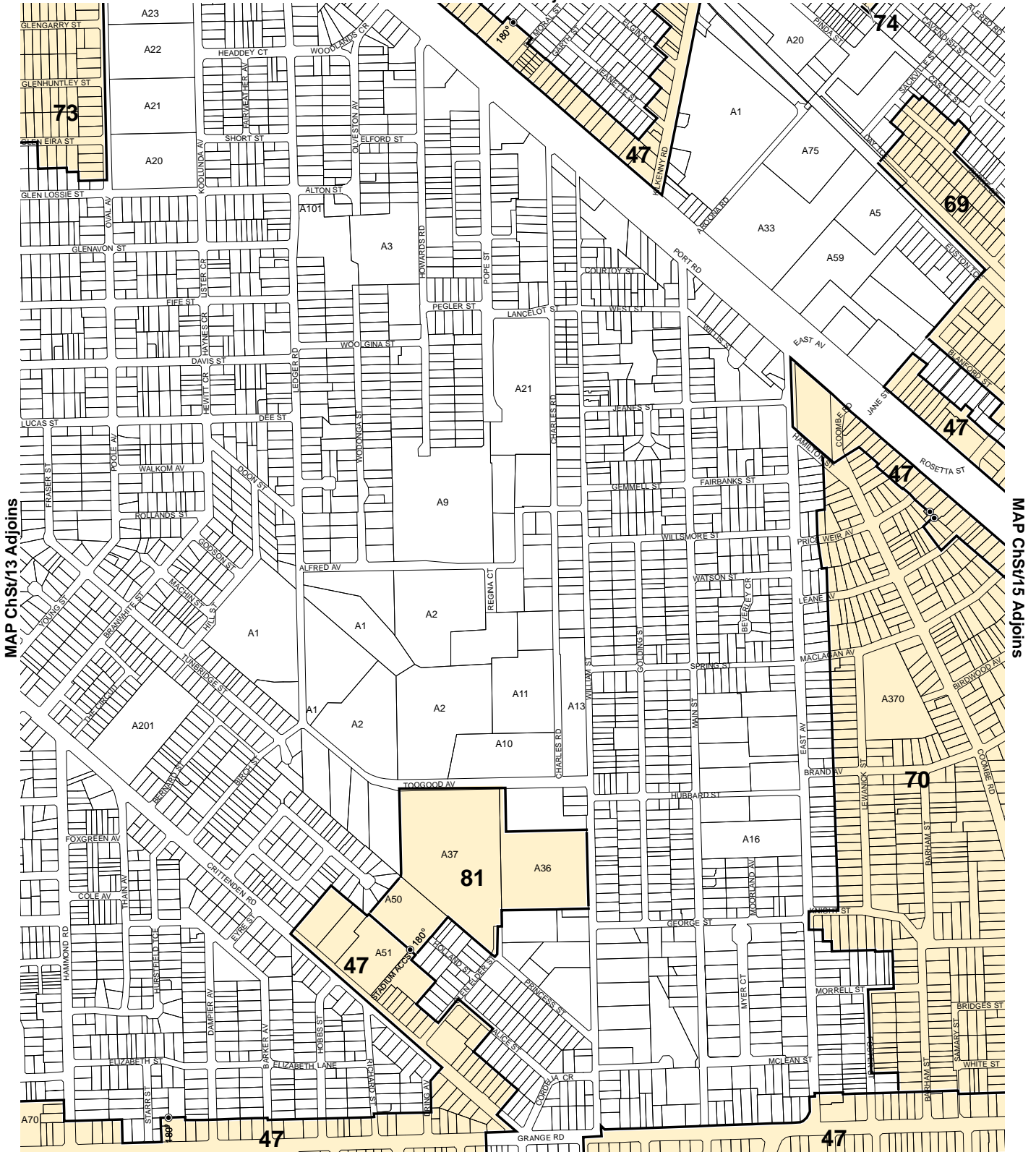
- 7 Local Shopping
- 16 Mid Suburban
- 20 Integrated Medium Density
- 25 Main Road Commercial
- 26 Core Industry



Policy Area Map ChSt/14

Policy Area Boundary

MAP ChSt/9 Adjoins



Lamberts Conformal Conic Projection, GDA94

Precinct

- 47 Mixed Use
- 69 Croydon/West Croydon
- 70 Allenby Gardens
- 73 Woodville South Character
- 74 Kilkenny Character
- 81 Stadium Beverley

 Precinct Boundary

Precinct Map ChSt/14

MAP ChSt/10 Adjoins

CROYDON PARK
Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

MAP ChSt/16 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins

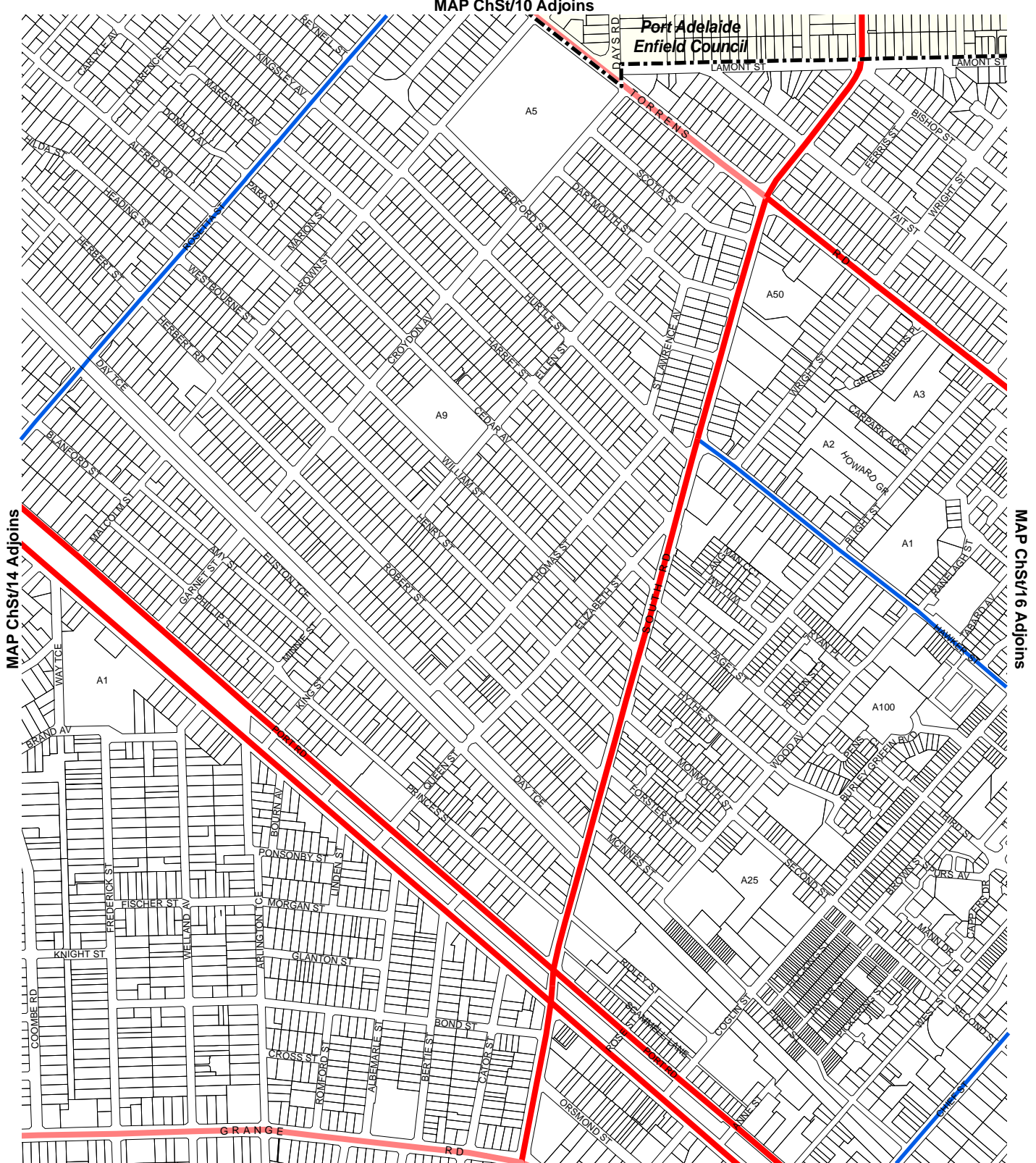


- S** School
- P** Post Office
- Railways
- Local Reserves
- Development Plan Boundary

Location Map ChSt/15

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

MAP ChSt/16 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins

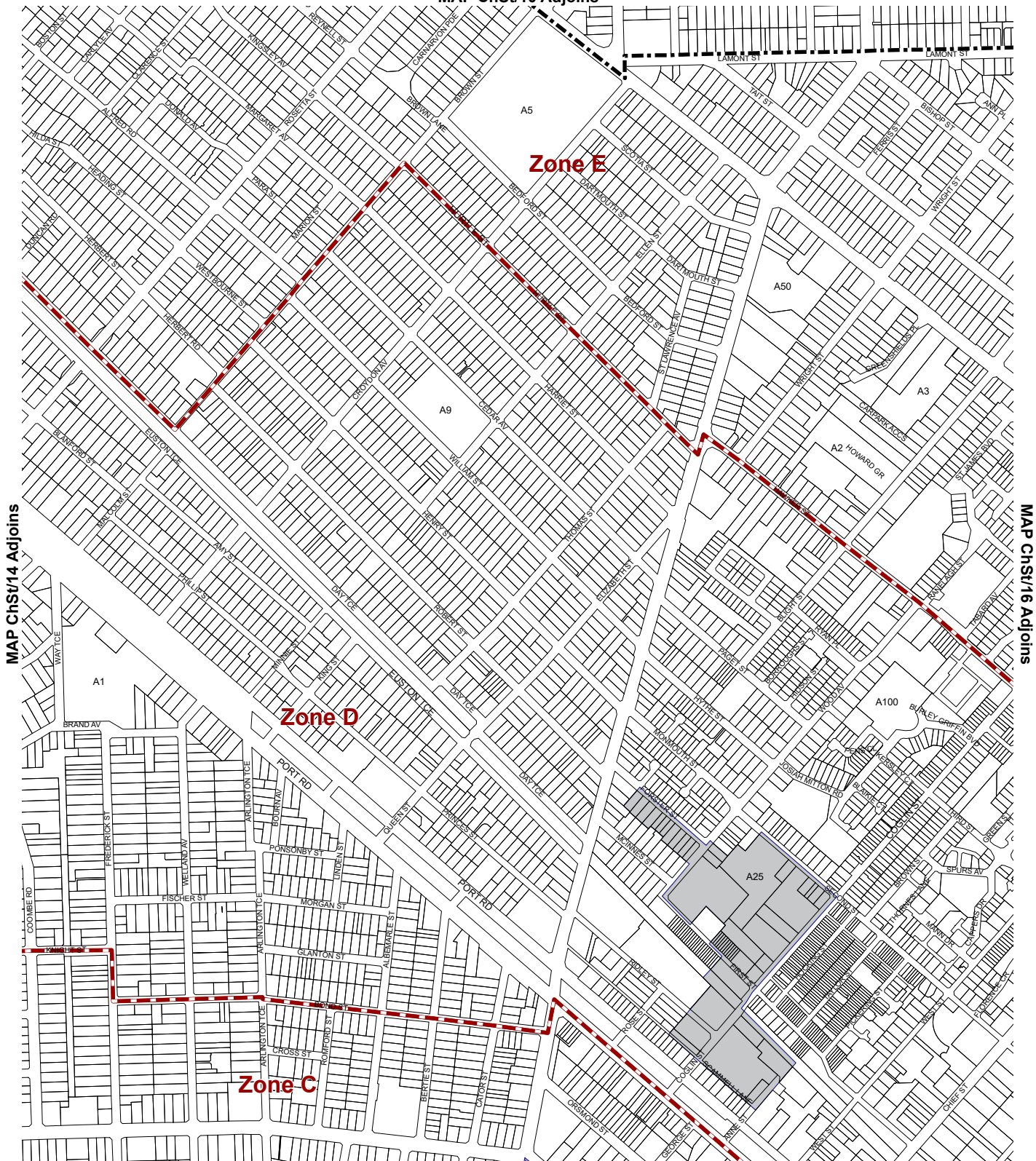


- ▬ Primary Arterial Roads
- ▬ Secondary Arterial Roads
- ▬ Distributor Roads
- Development Plan Boundary

Overlay Map ChSt/15



TRANSPORT

MAP ChSt/10 Adjoins



Airport Building Heights

- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level
- Zone E** All Structures Exceeding 100 metres above existing ground level

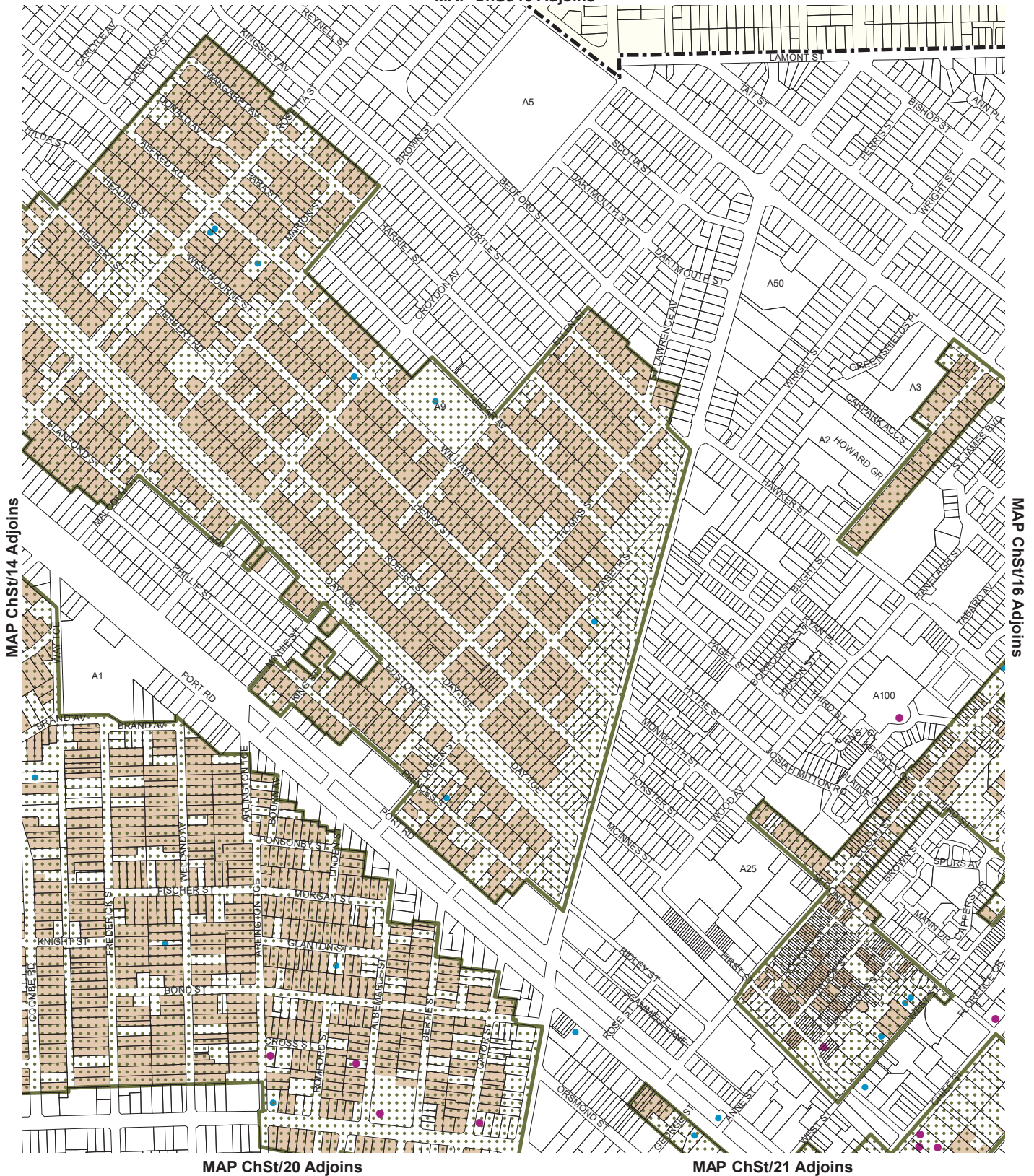
-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary



Overlay Map ChSt/15

DEVELOPMENT CONSTRAINTS

MAP ChSt/10 Adjoins



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For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.



- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/15

HERITAGE

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

MAP ChSt/16 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins



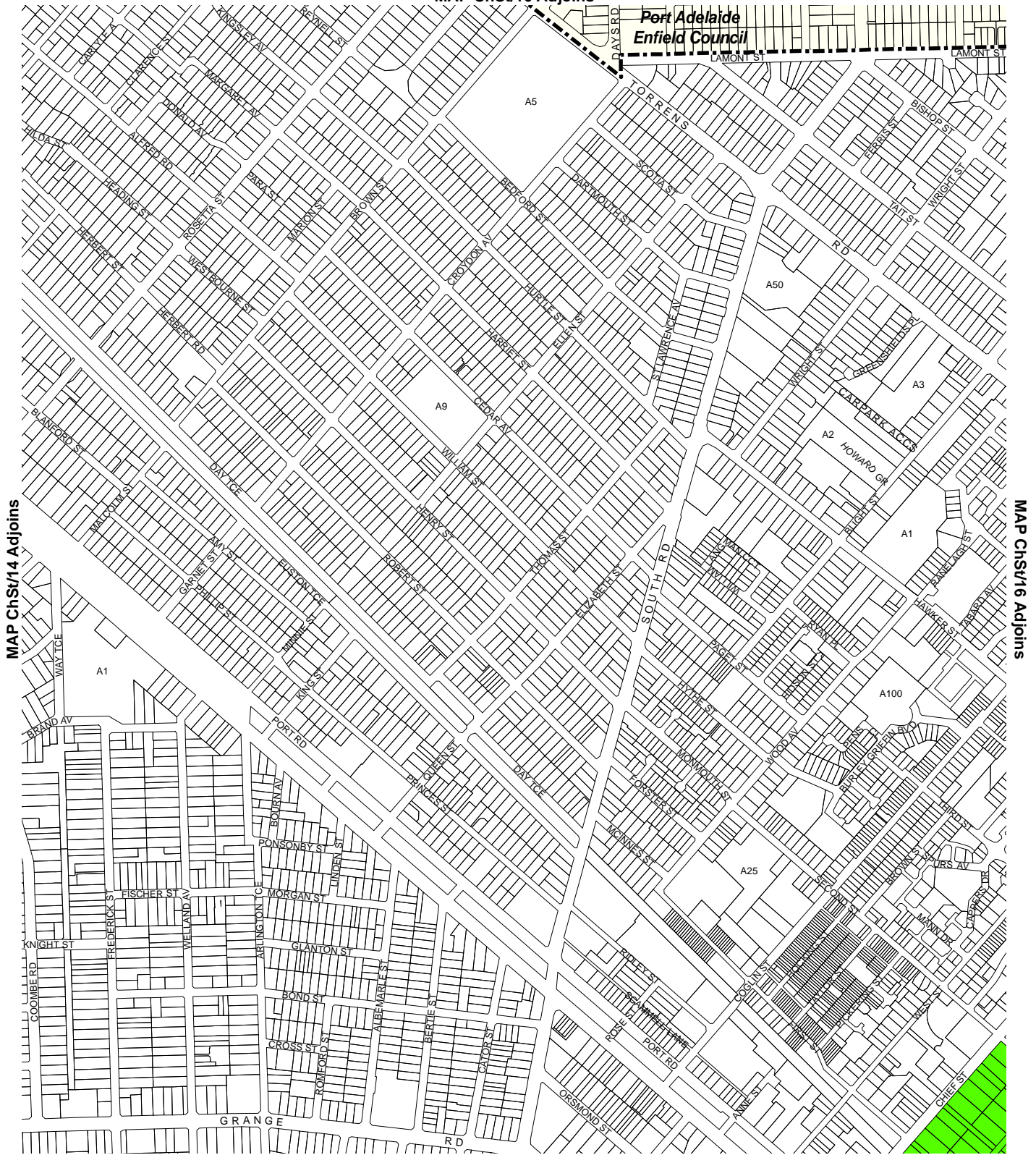
Overlay Map ChSt/15

NOISE AND AIR EMISSIONS

- Noise and Air Emissions Designated Area
- Development Plan Boundary

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/14 Adjoins

MAP ChSt/16 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins

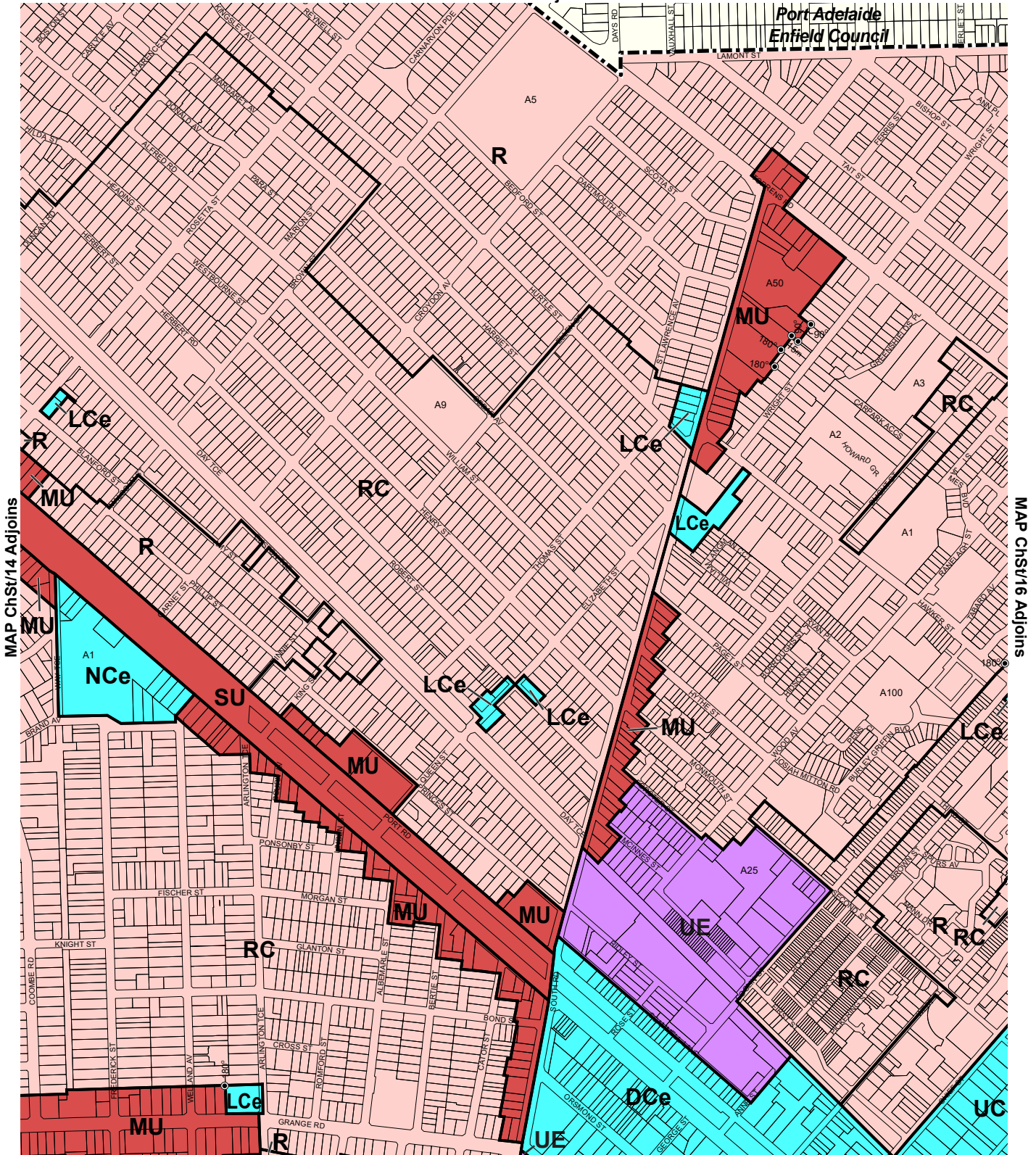
Overlay Map ChSt/15

AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins

Lamberts Conformal Conic Projection, GDA94

Zones

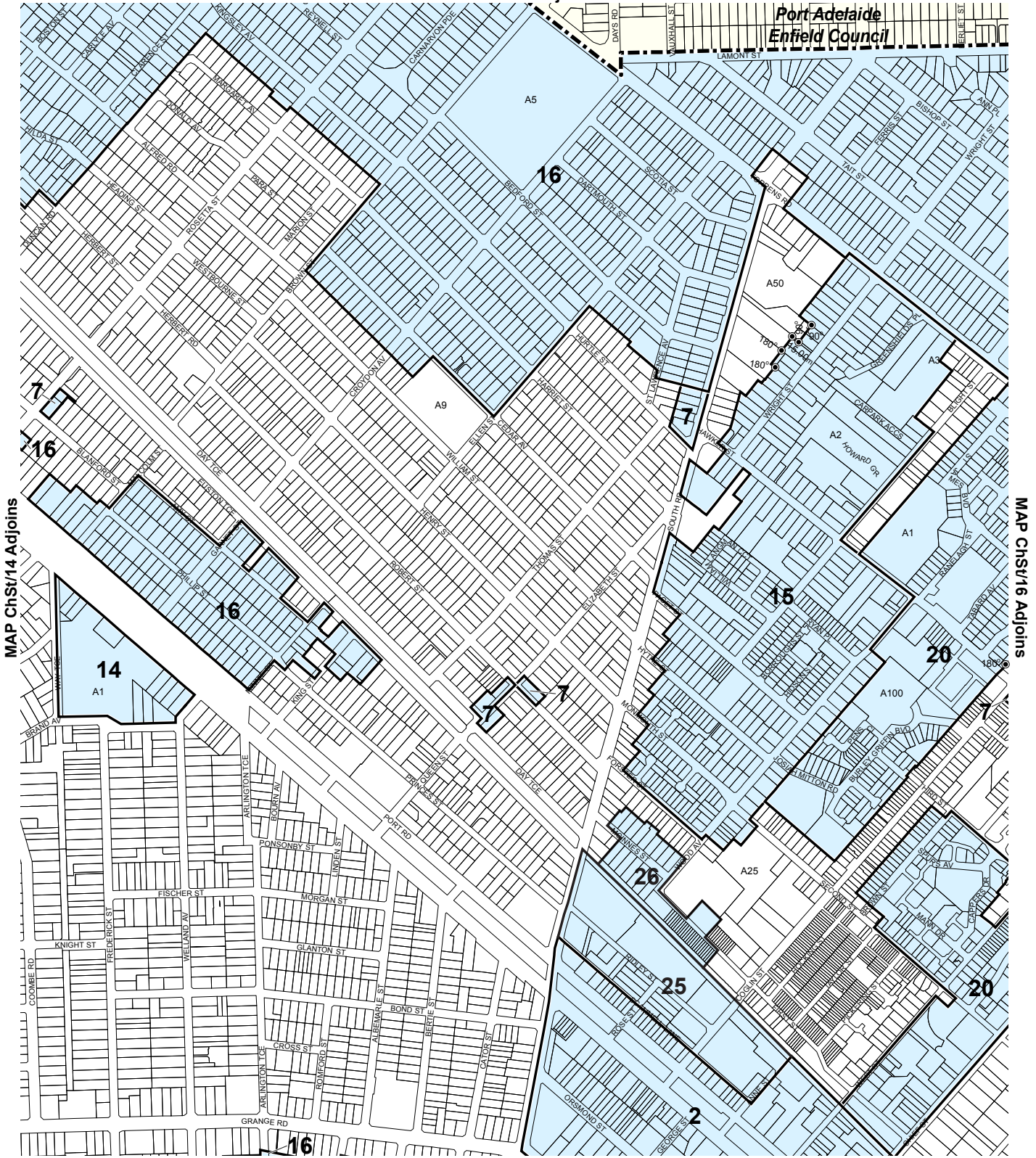
- DCe District Centre
- LCe Local Centre
- MU Mixed Use
- NCe Neighbourhood Centre
- R Residential
- RC Residential Character
- SU Special Use
- UC Urban Core
- UE Urban Employment
- Zone Boundary
- Development Plan Boundary



Zone Map ChSt/15

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



Lamberts Conformal Conic Projection, GDA94

Policy Area

- 2 Hindmarsh
- 7 Local Shopping
- 14 Welland
- 15 Inner Suburban
- 16 Mid Suburban
- 20 Integrated Medium Density
- 25 Main Road Commercial
- 26 Core Industry

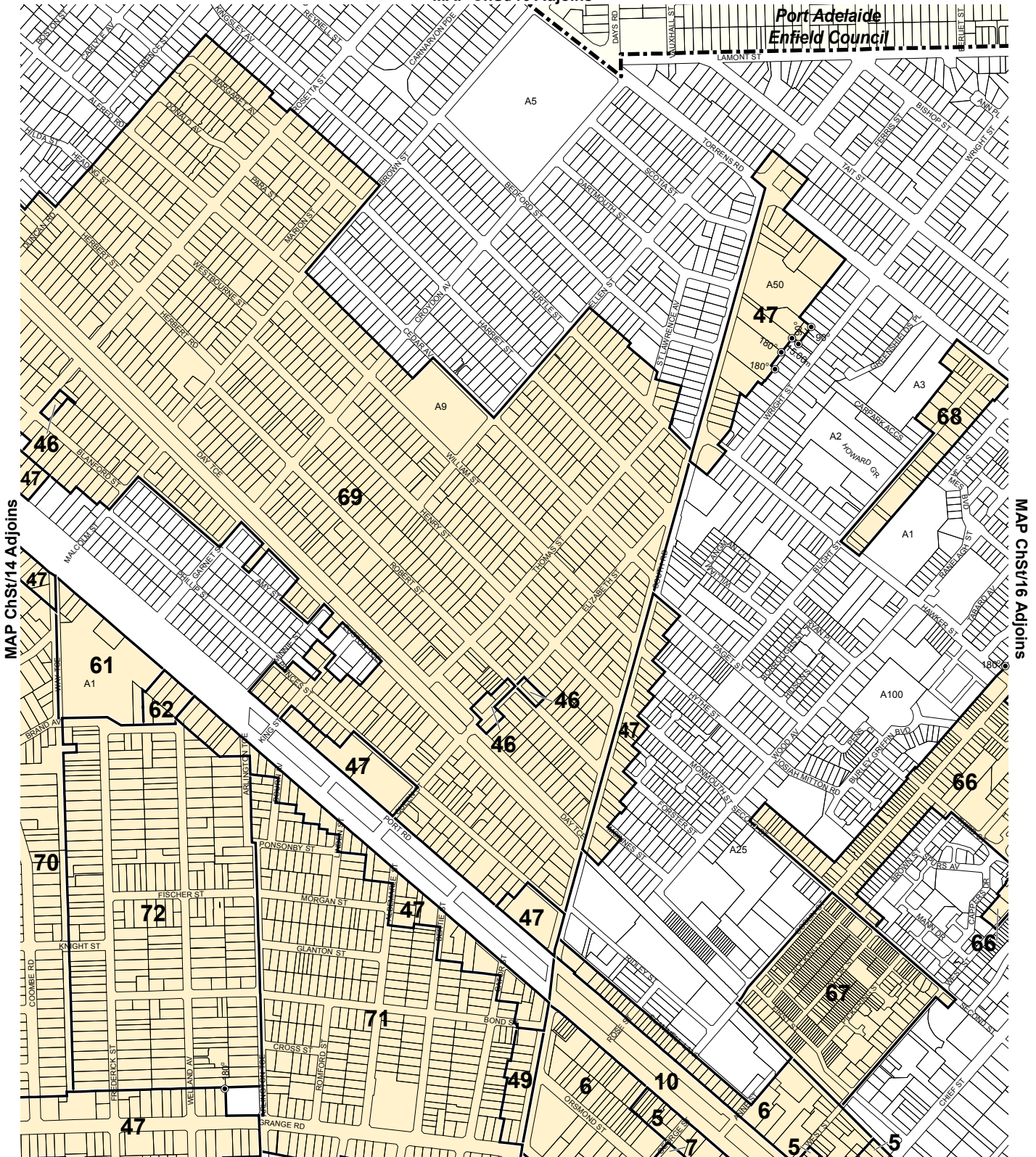


Policy Area Map ChSt/15

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/10 Adjoins

Port Adelaide
Enfield Council



MAP ChSt/20 Adjoins

MAP ChSt/21 Adjoins

Lamberts Conformal Conic Projection, GDA94

Precinct

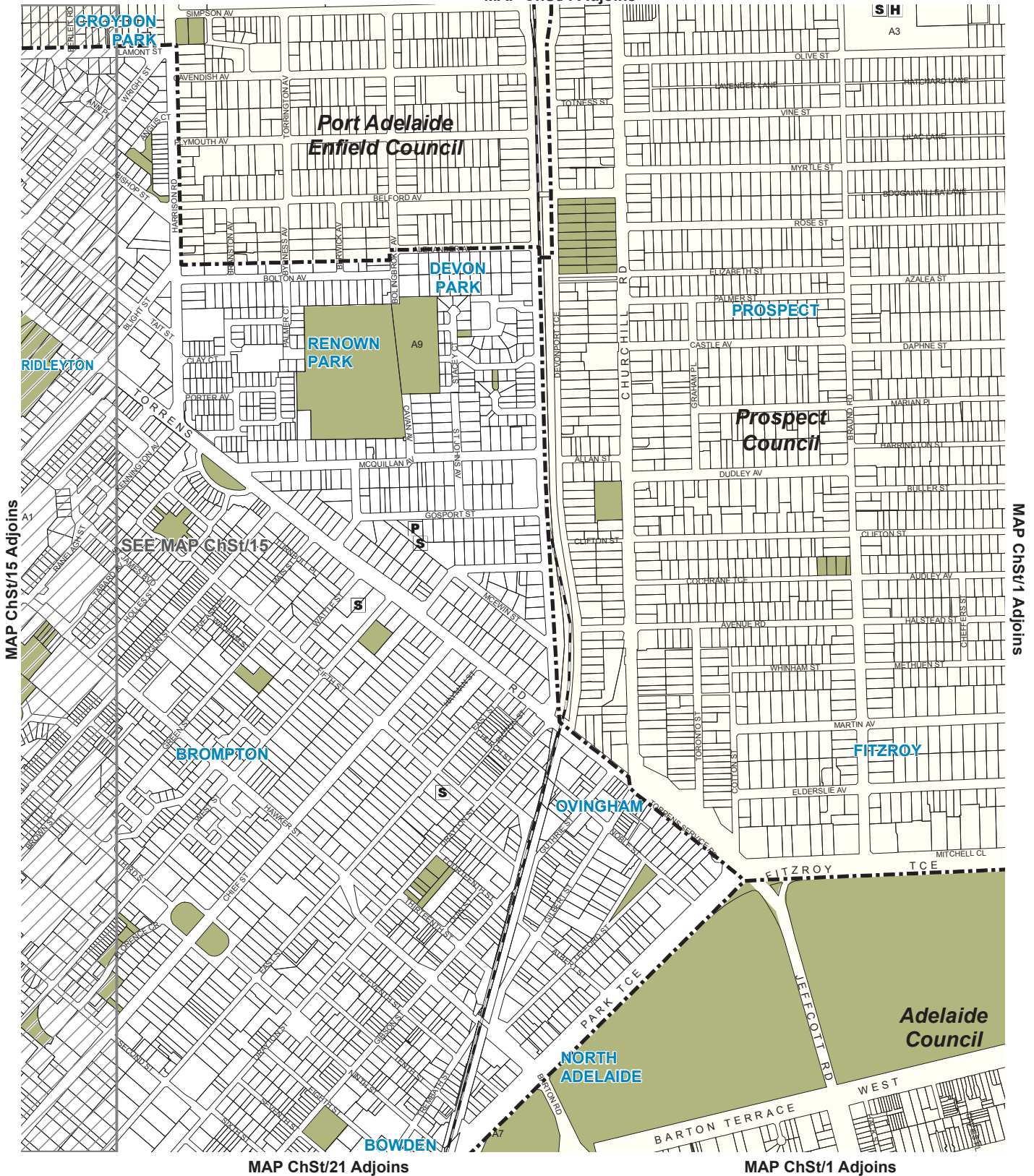
- 5 Historic Hub
- 6 Port Road Office
- 7 Manton Street
- 10 Port Road Gardens
- 46 Stations
- 47 Mixed Use
- 49 South Road
- 61 Retail Core Welland
- 62 Commercial Fringe Welland
- 66 Bowden/Brompton
- 67 Brompton Park
- 68 Early SA Housing Trust
- 69 Croydon/West Croydon
- 70 Allenby Gardens
- 71 Pre World War One
- 72 Welland Character



Precinct Map ChSt/15

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins



S H
A3

Port Adelaide
Enfield Council

DEVON PARK

RENOWN PARK
A9

PROSPECT

Prospect Council

SEE MAP ChSt/15

BROMPTON

OVINGHAM

FITZROY

NORTH
ADELAIDE

Adelaide Council

BOWDEN

MAP ChSt/21 Adjoins

MAP ChSt/1 Adjoins

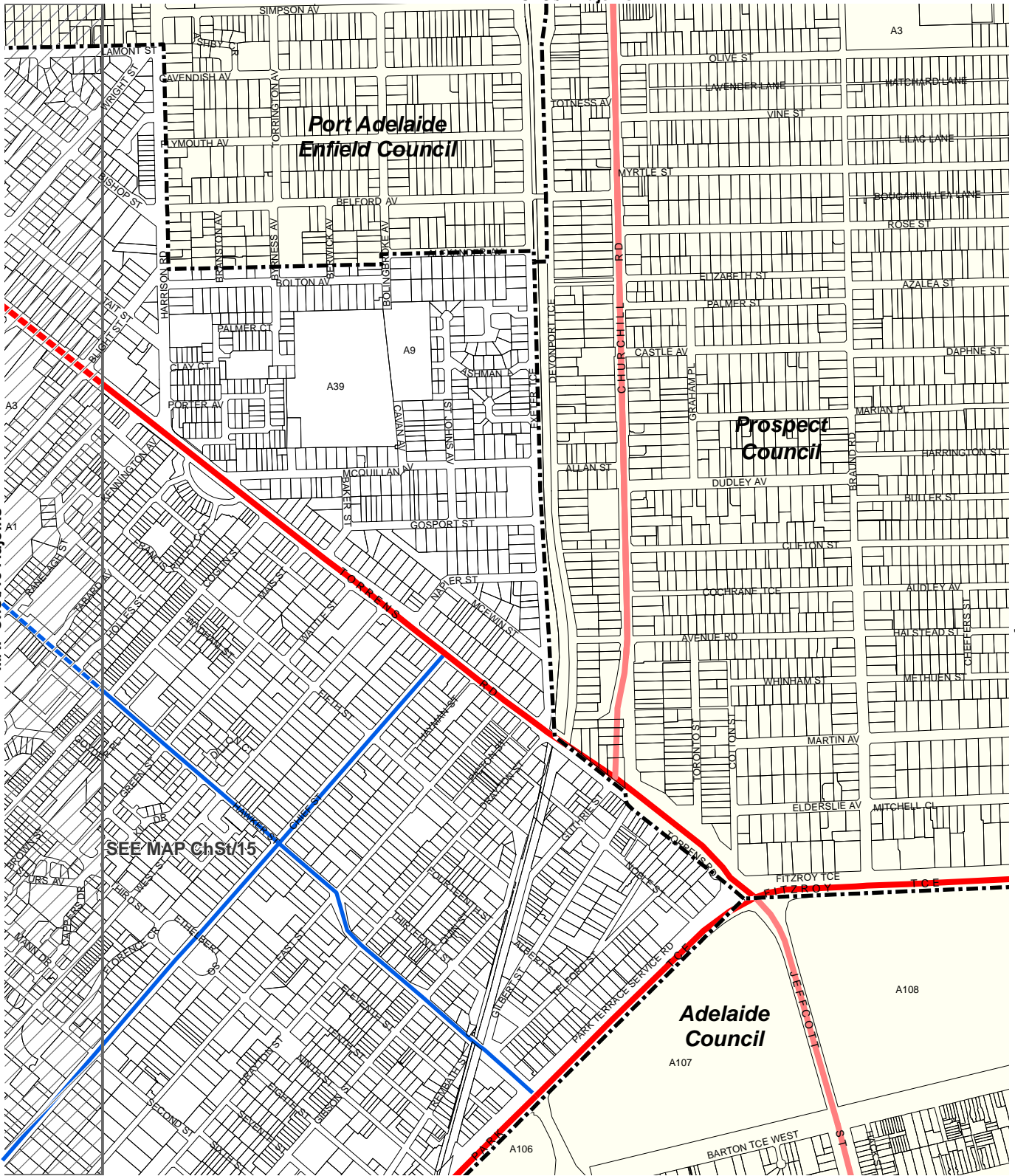
- S** School
- P** Post Office
- H** Other Health Services

- Railways
- Local Reserves
- Development Plan Boundary



Location Map ChSt/16

MAP ChSt/1 Adjoins



MAP ChSt/21 Adjoins

MAP ChSt/1 Adjoins

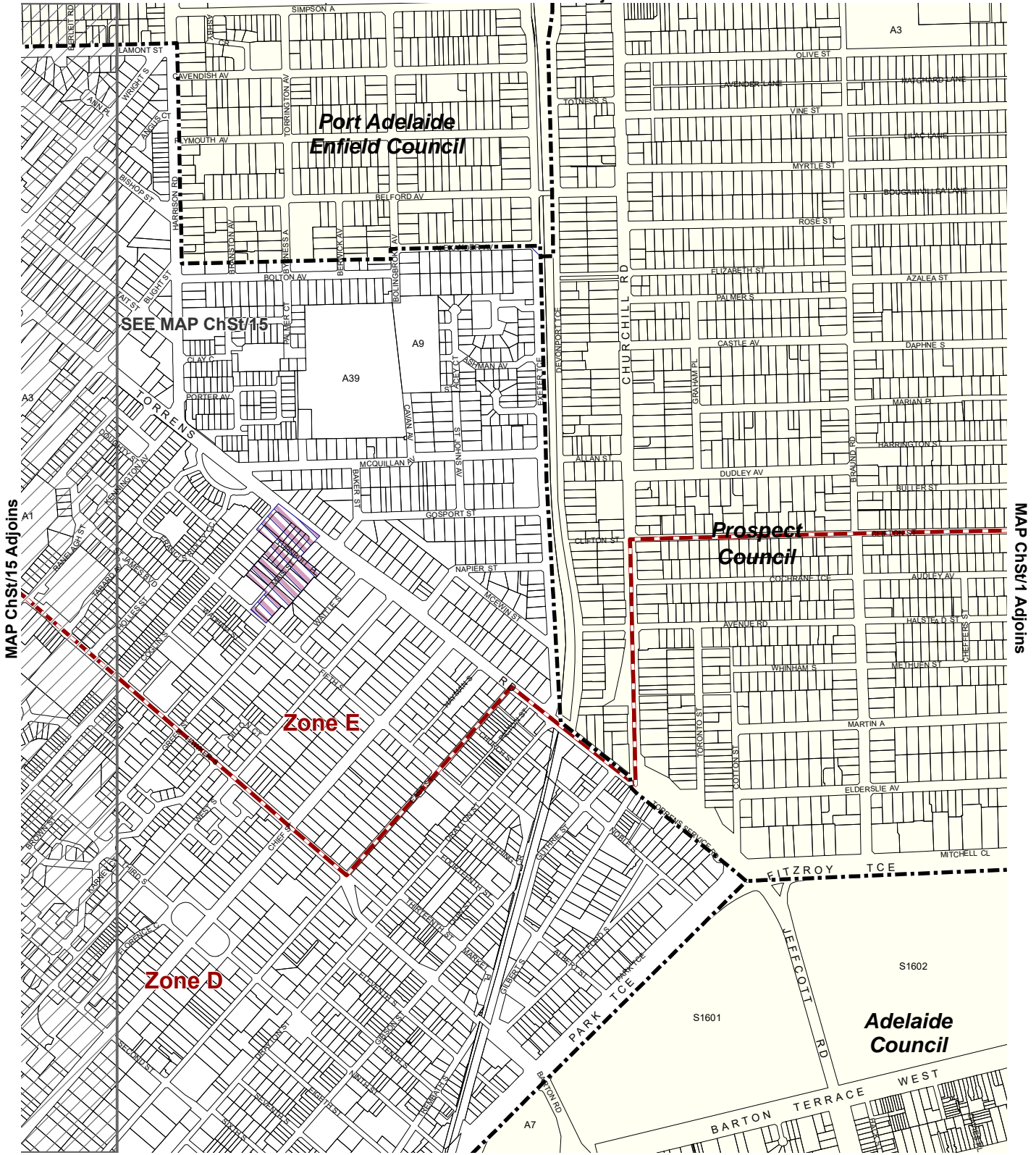


Overlay Map ChSt/16

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/1 Adjoins



Airport Building Heights
 Zone D All Structures Exceeding 45 metres above existing ground level
 Zone E All Structures Exceeding 100 metres above existing ground level

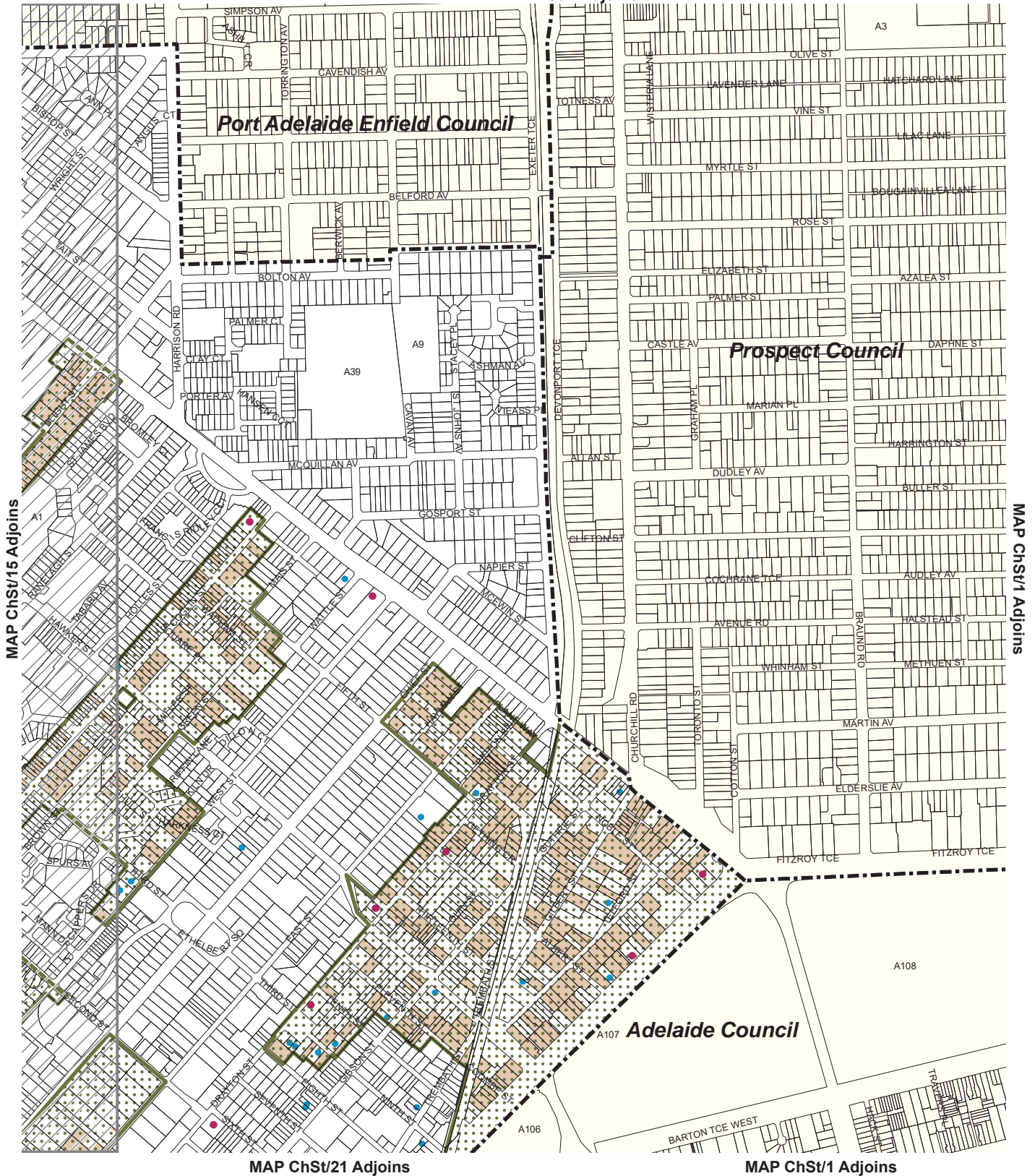


-  Airport Building Heights
-  Industry Interface Area
-  Development Plan Boundary

Overlay Map ChSt/16

DEVELOPMENT CONSTRAINTS

MAP ChSt/1 Adjoins



Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.

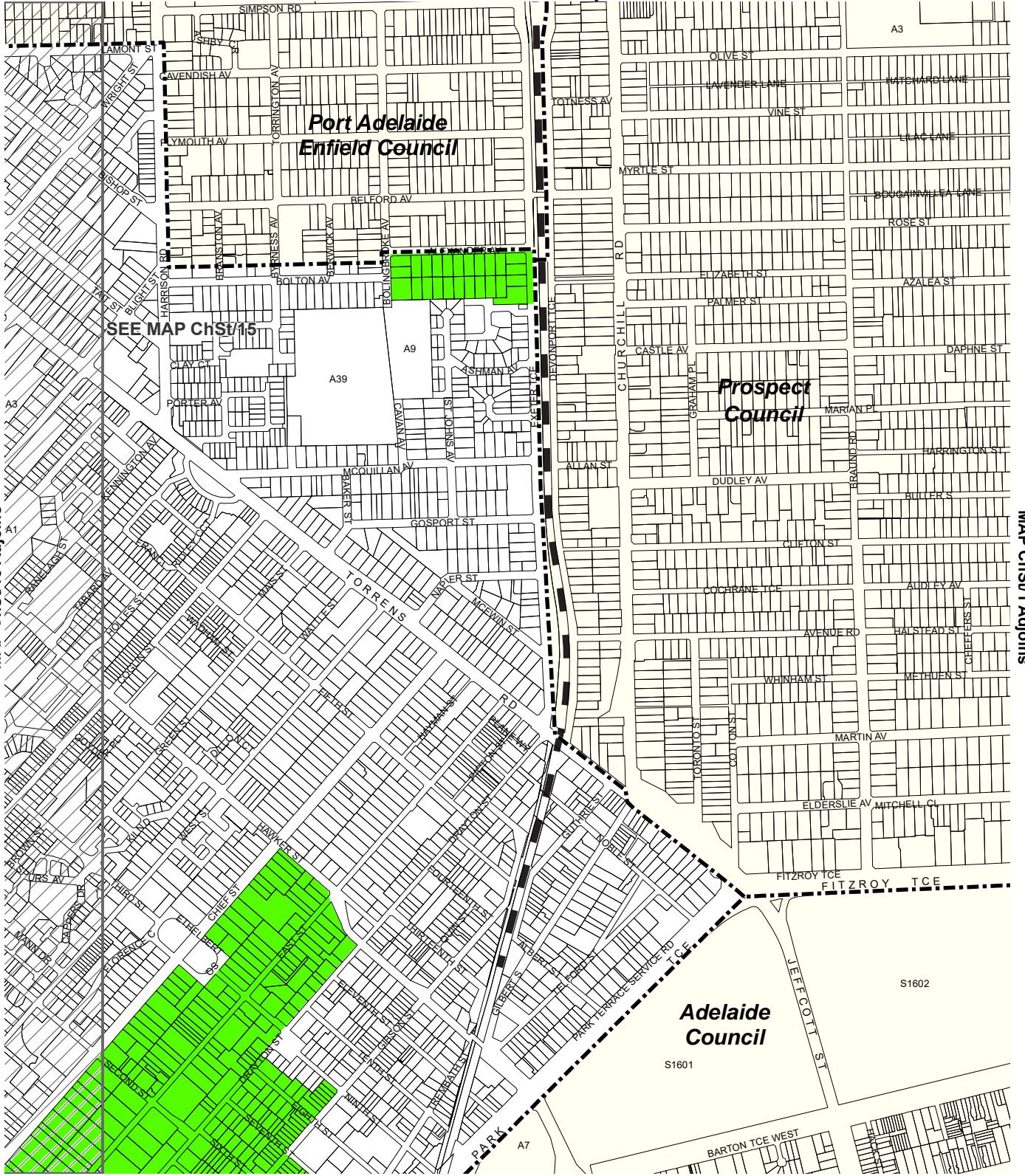


- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/16

HERITAGE

MAP ChSt/1 Adjoins



SEE MAP ChSt/15




Port Adelaide
Enfield Council

Prospect
Council

Adelaide
Council

MAP ChSt/21 Adjoins

MAP ChSt/1 Adjoins

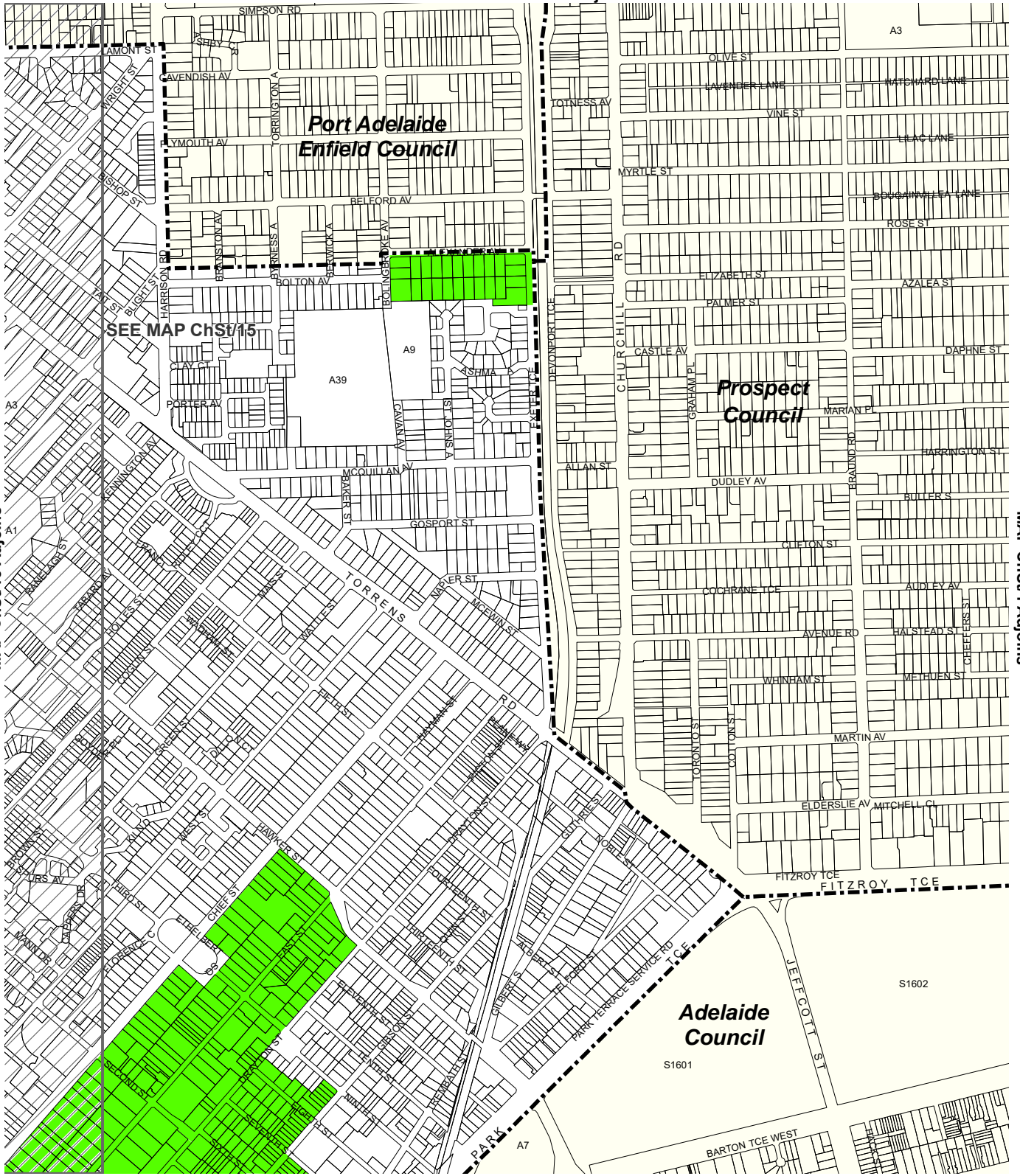
-  Train Line
-  Noise and Air Emissions Designated Area
-  Development Plan Boundary

Overlay Map ChSt/16

NOISE AND AIR EMISSIONS



MAP ChSt/1 Adjoins



SEE MAP ChSt/15

MAP ChSt/21 Adjoins

MAP ChSt/1 Adjoins



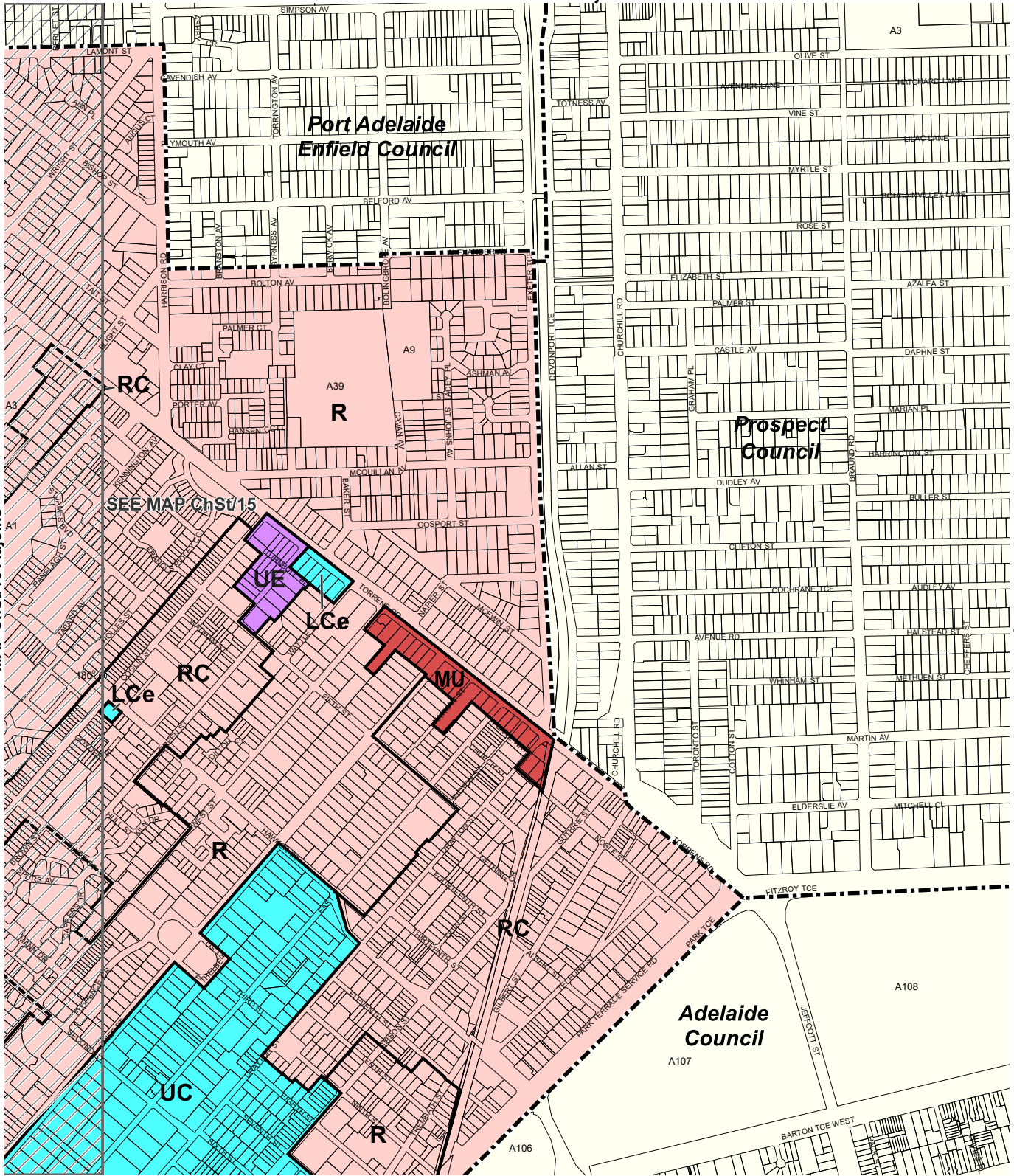
Overlay Map ChSt/16

AFFORDABLE HOUSING

Affordable Housing Designated Area

Development Plan Boundary

MAP ChSt/1 Adjoins



MAP ChSt/15 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/21 Adjoins

MAP ChSt/1 Adjoins

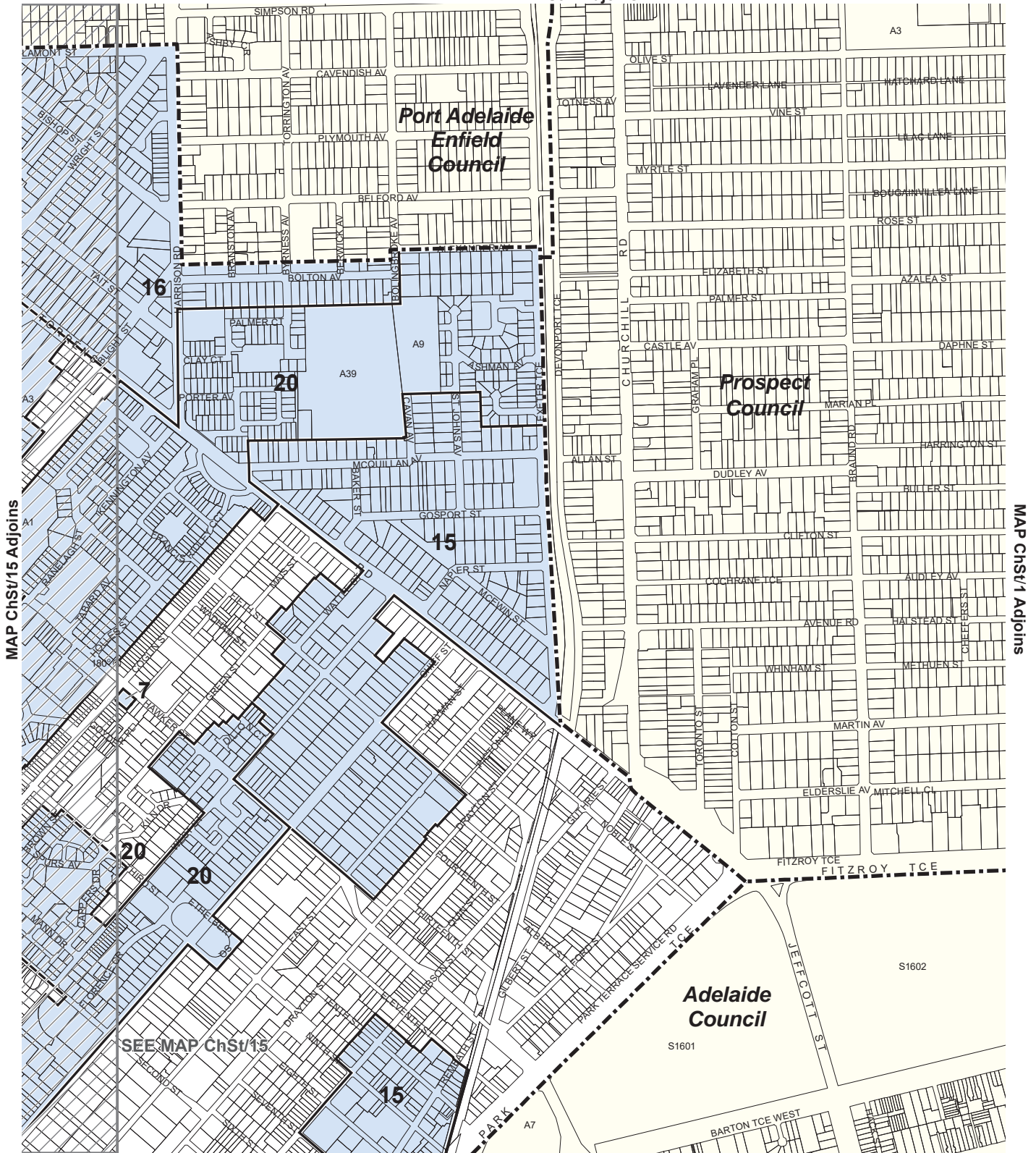
See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

- Zones**
- LCE Local Centre
 - MU Mixed Use
 - R Residential
 - RC Residential Character
 - UC Urban Core
 - UE Urban Employment
 - Zone Boundary
 - Development Plan Boundary



Zone Map ChSt/16

MAP ChSt/1 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

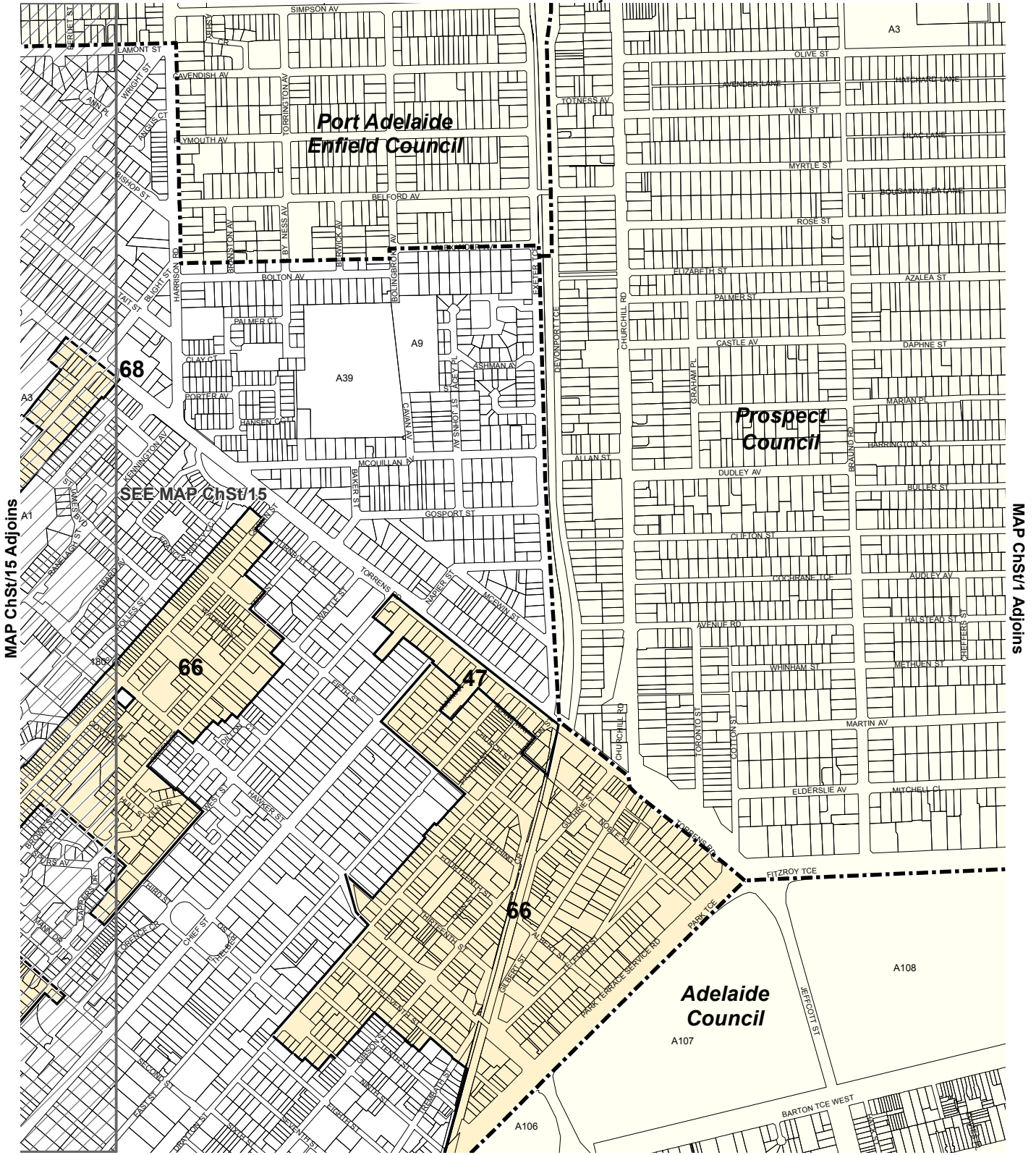
- Policy Area**
- 15 Inner Suburban
 - 16 Mid Suburban
 - 20 Integrated Medium Density
 - 7 Local Shopping



Policy Area Map ChSt/16

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/1 Adjoins

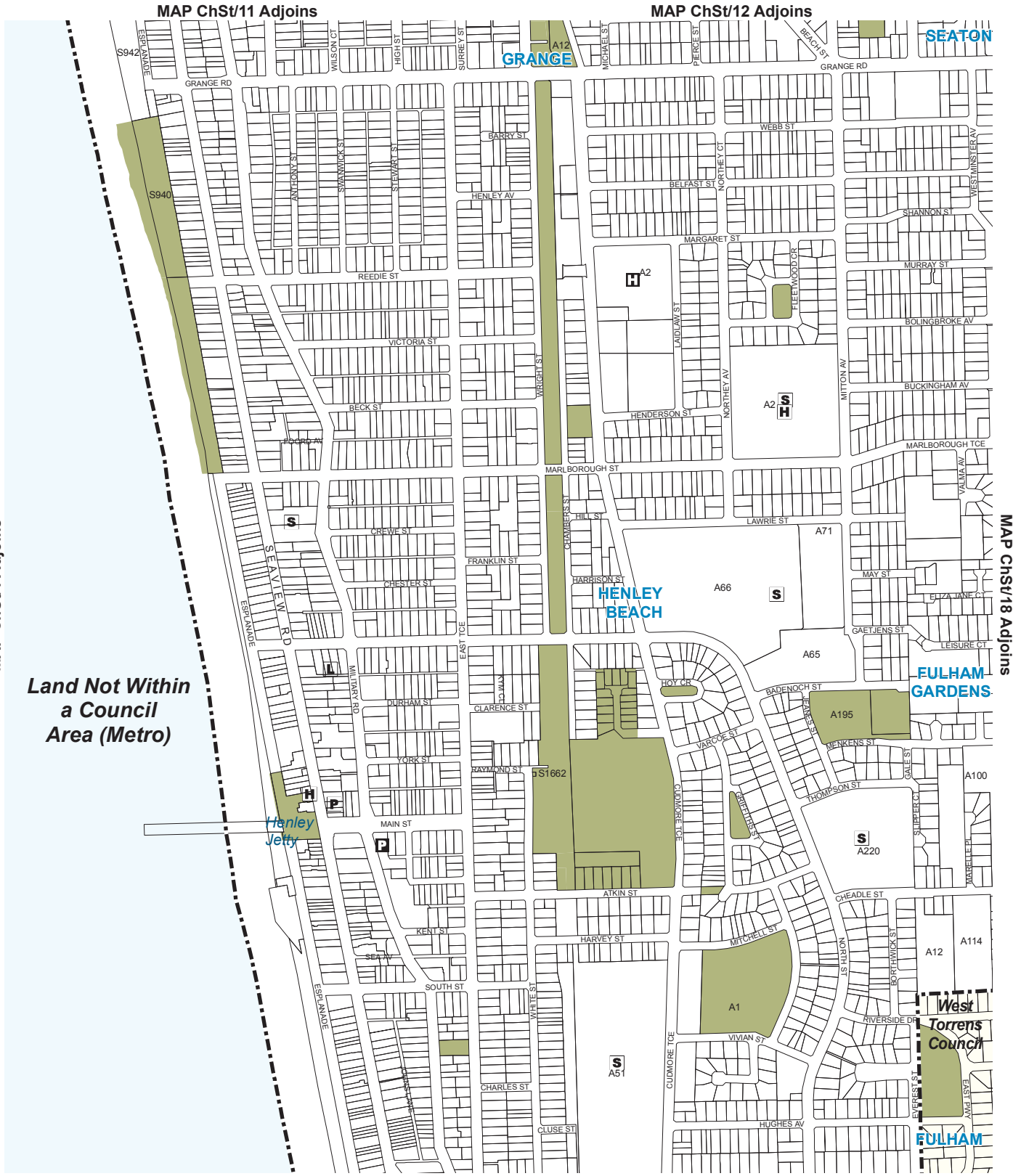


See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

- Precinct**
- 47 Mixed Use
 - 66 Bowden/Brompton
 - 68 Early SA Housing Trust

- Precinct Boundary
- Development Plan Boundary

Precinct Map ChSt/16



MAP ChSt/1 Adjoins

Land Not Within
a Council
Area (Metro)

Henley
Jetty

MAP ChSt/22 Adjoins

MAP ChSt/12 Adjoins

MAP ChSt/18 Adjoins



- School
- Public Library
- Post Office
- Other Health Services
- Hospital
- Police Station
- Local Reserves
- Development Plan Boundary

Location Map ChSt/17

MAP ChSt/11 Adjoins

MAP ChSt/12 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/8 Adjoins

Land Not Within a Council Area (Metro)

Henley Jetty

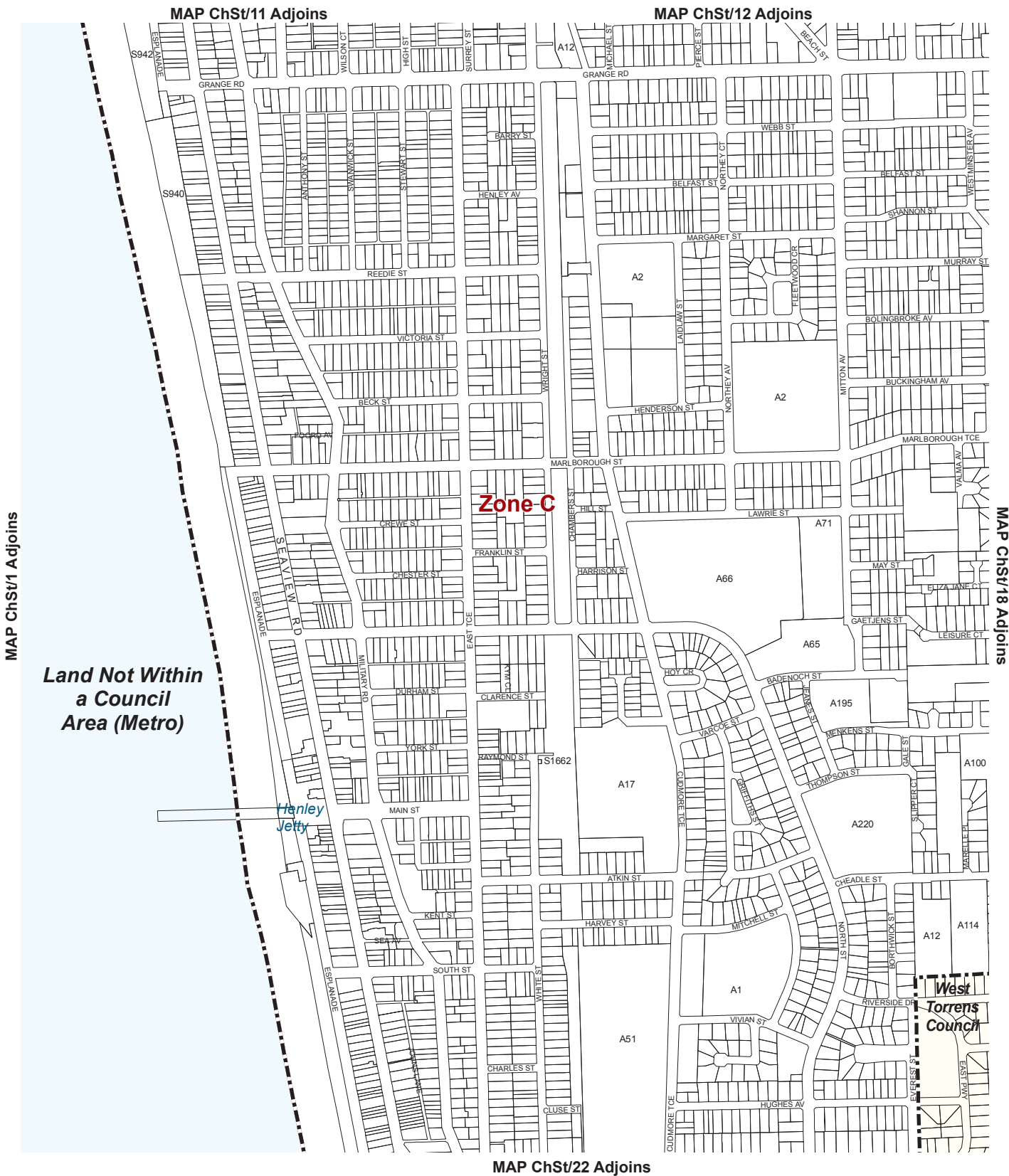
MAP ChSt/22 Adjoins



0 500m

Overlay Map ChSt/17 TRANSPORT

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

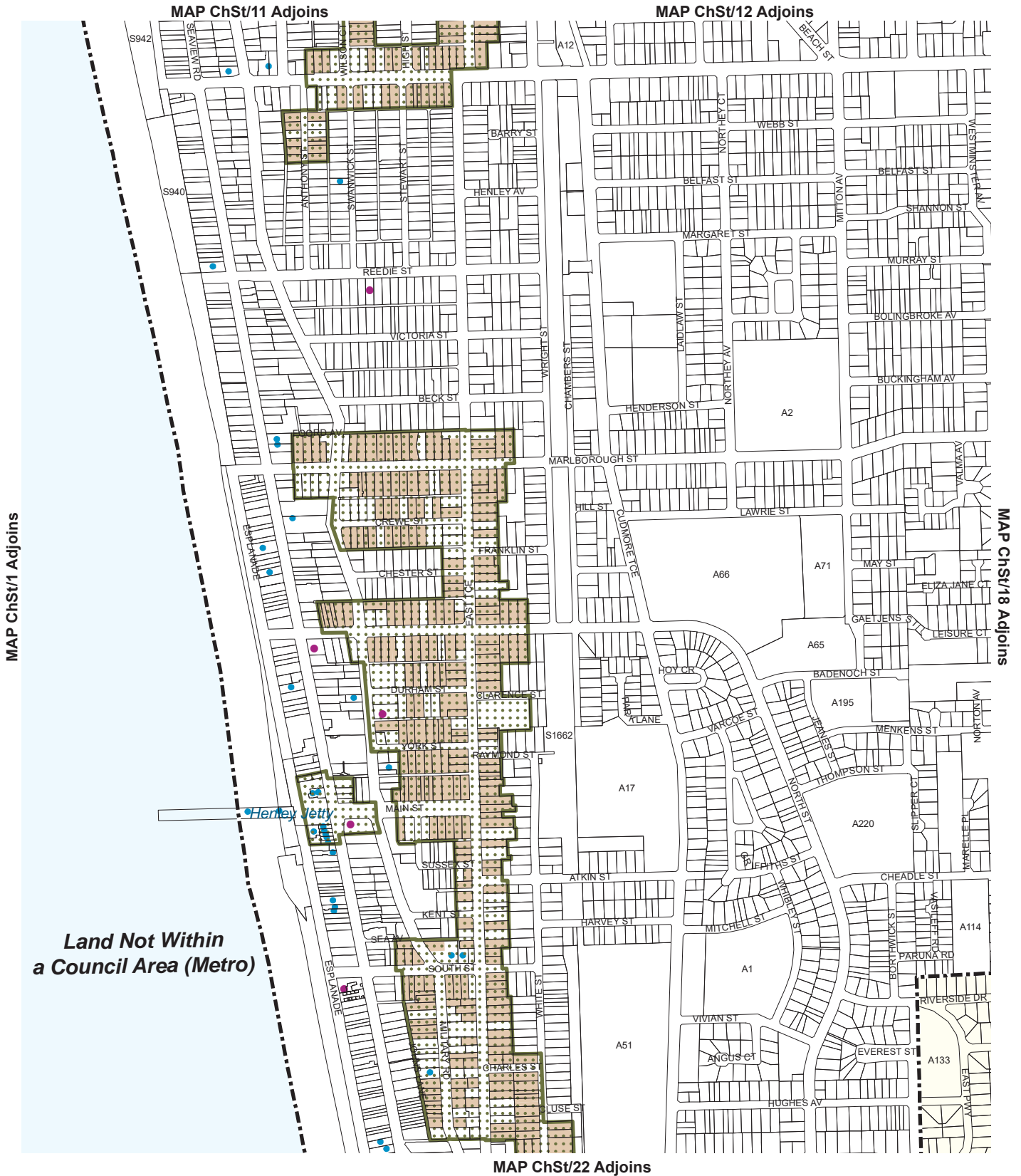


Airport Building Heights
Zone C All Structures Exceeding 15 metres above existing ground level



Overlay Map ChSt/17 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary



Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.

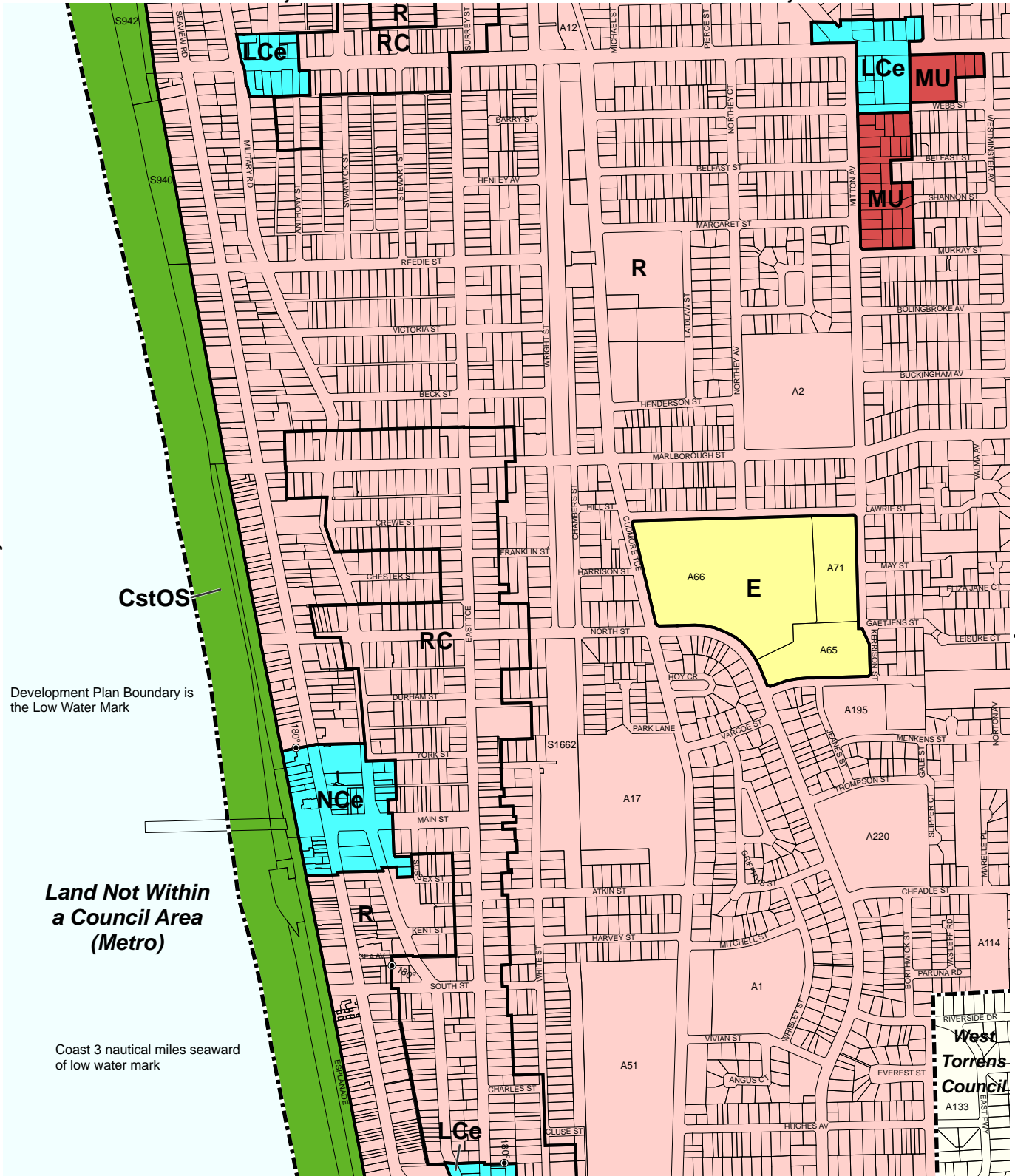


- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/17 HERITAGE

MAP ChSt/11 Adjoins

MAP ChSt/12 Adjoins

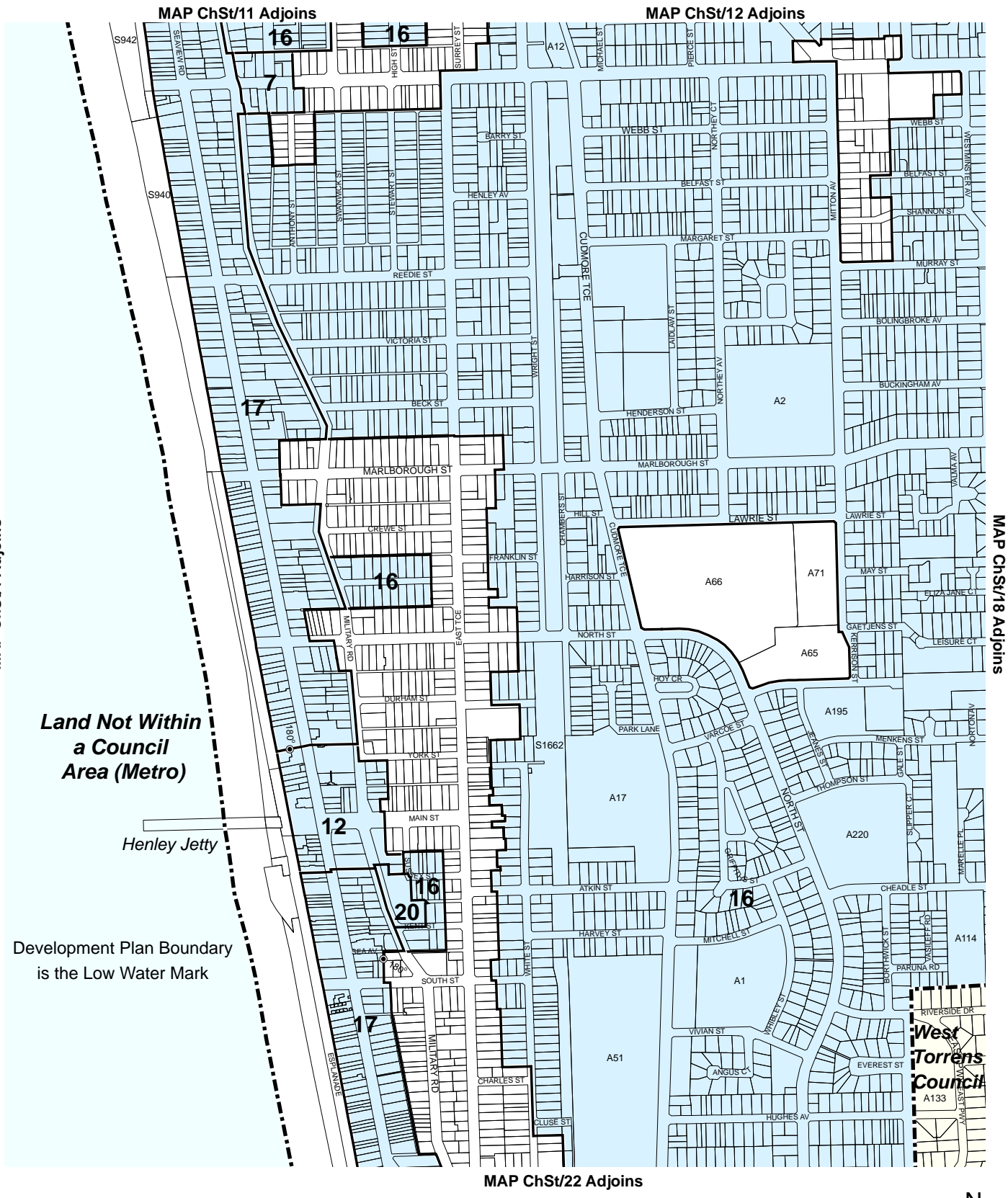


Lamberts Conformal Conic Projection, GDA94

- Zones**
- CstOS Coastal Open Space
 - E Education
 - LCe Local Centre
 - MU Mixed Use
 - NCe Neighbourhood Centre
 - R Residential
 - RC Residential Character
 - Zone Boundary
 - Development Plan Boundary



Zone Map ChSt/17



Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 7 Local Shopping
 - 12 Henley Square
 - 16 Mid Suburban
 - 17 Western Edge
 - 20 Integrated Medium Density

- Policy Area Boundary
- Development Plan Boundary



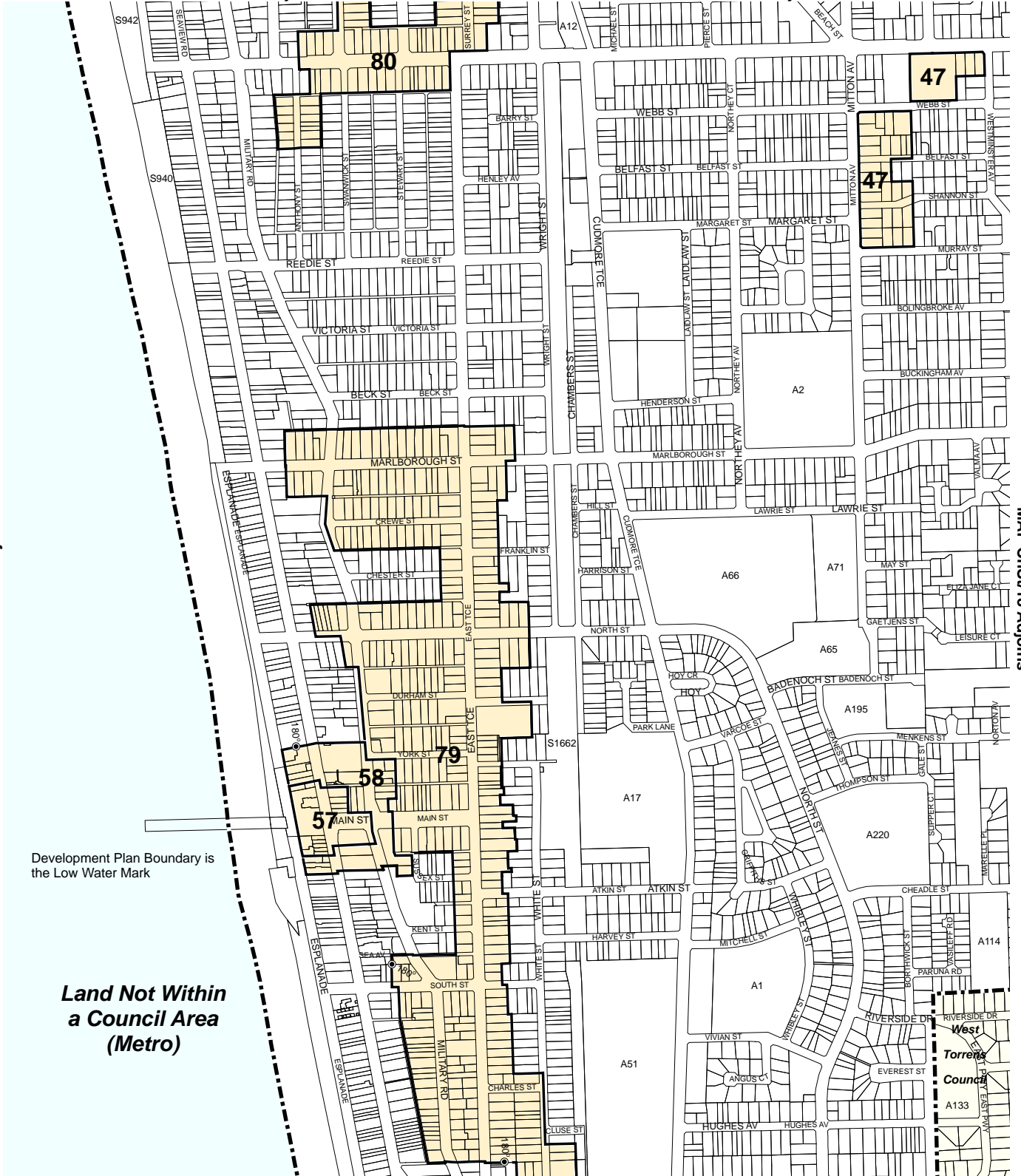
Policy Area Map ChSt/17

MAP ChSt/11 Adjoins

MAP ChSt/12 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/18 Adjoins



Development Plan Boundary is the Low Water Mark

Land Not Within a Council Area (Metro)

MAP ChSt/22 Adjoins

Lamberts Conformal Conic Projection, GDA94

- Precinct**
- 47 Mixed Use
 - 57 Henley Square
 - 58 Seaview Road
 - 79 Henley Beach
 - 80 Grange



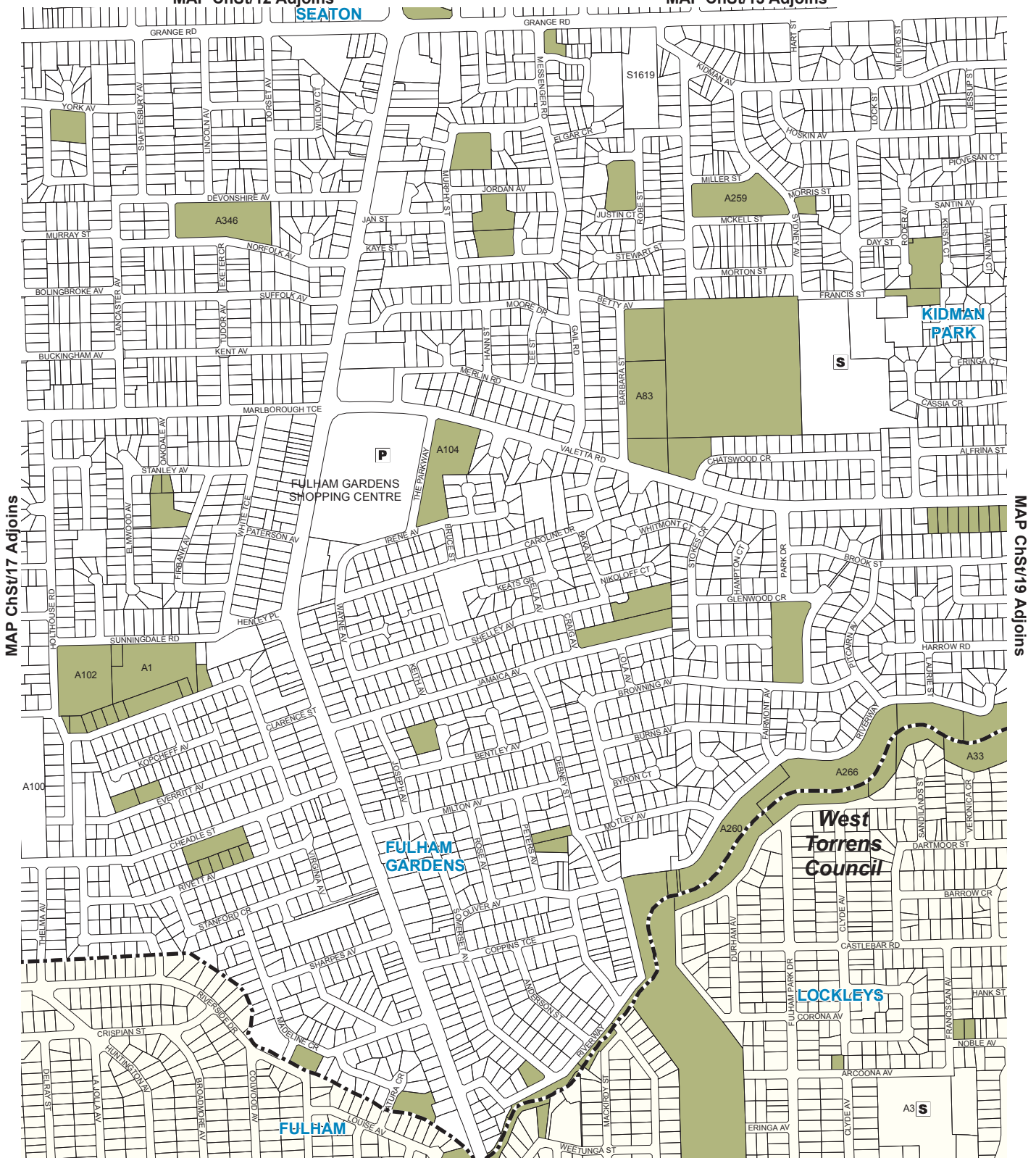
Precinct Map ChSt/17

- Precinct Boundary
- Development Plan Boundary

MAP ChSt/12 Adjoins

MAP ChSt/13 Adjoins

SEATON



MAP ChSt/17 Adjoins

MAP ChSt/19 Adjoins

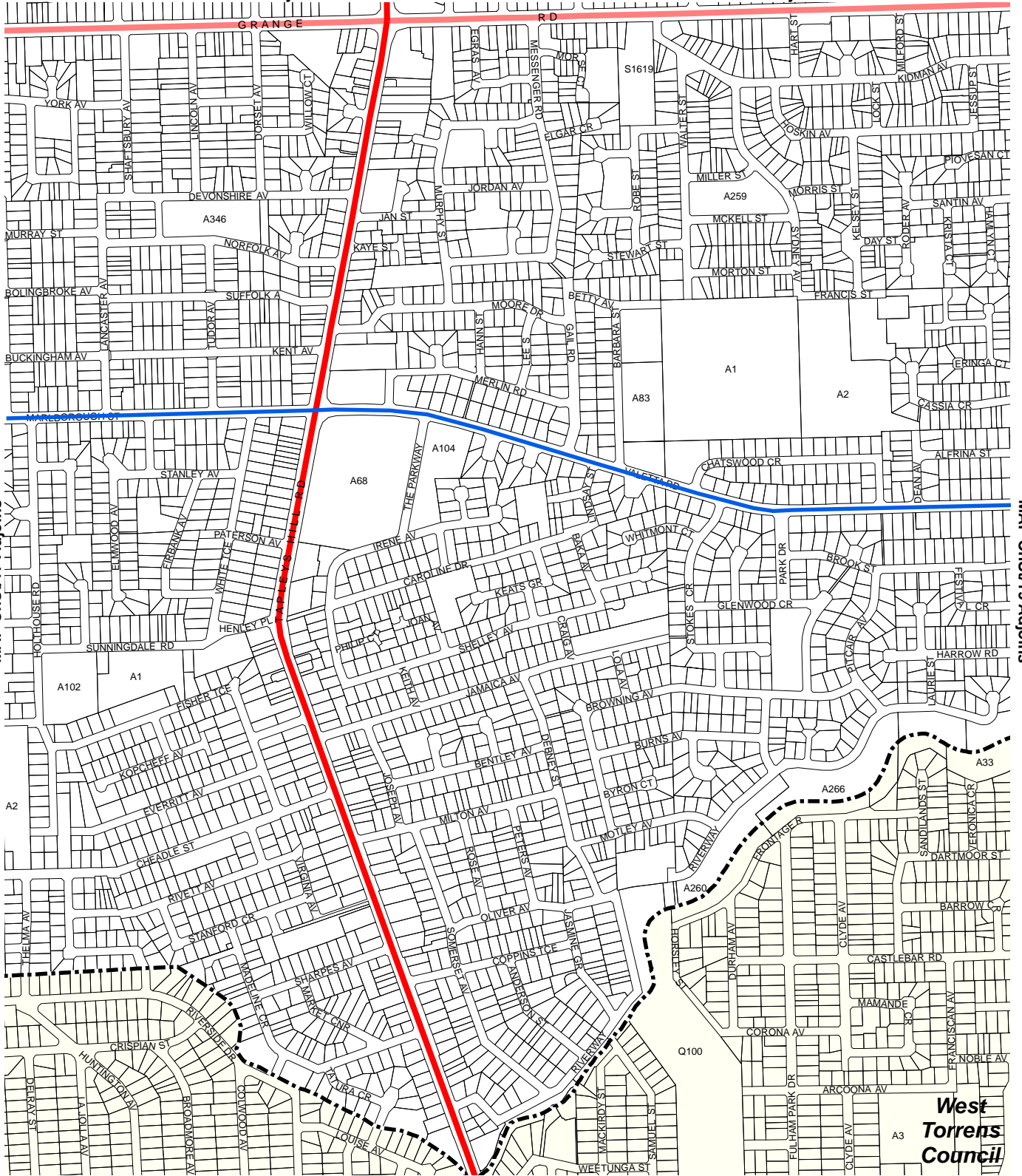
West Torrens Council

MAP ChSt/1 Adjoins



Location Map ChSt/18

- S School
- P Post Office
- Local Reserves
- Development Plan Boundary



West Torrens Council



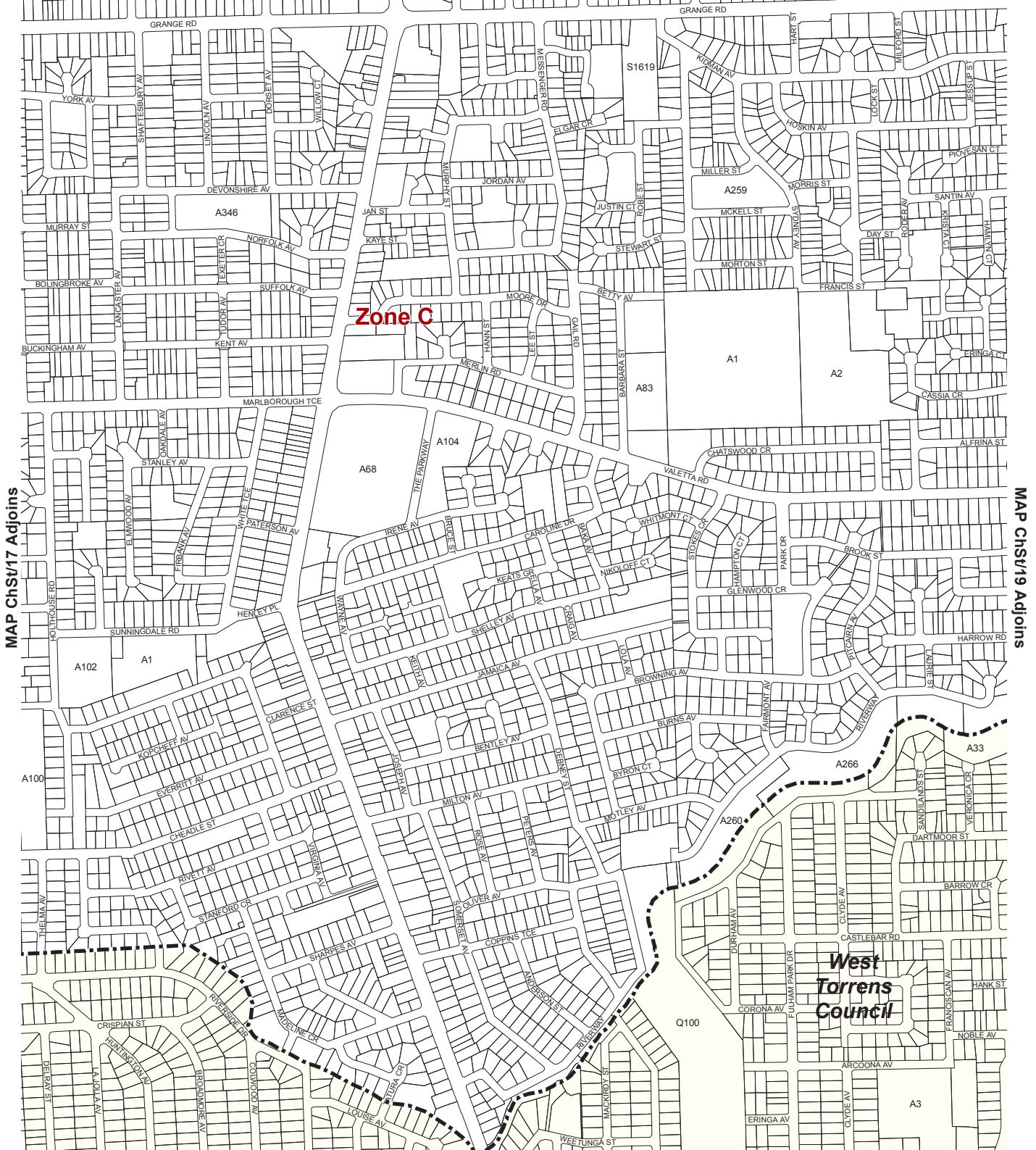
Overlay Map ChSt/18

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Distributor Roads
- - - - - Development Plan Boundary

MAP ChSt/12 Adjoins

MAP ChSt/13 Adjoins





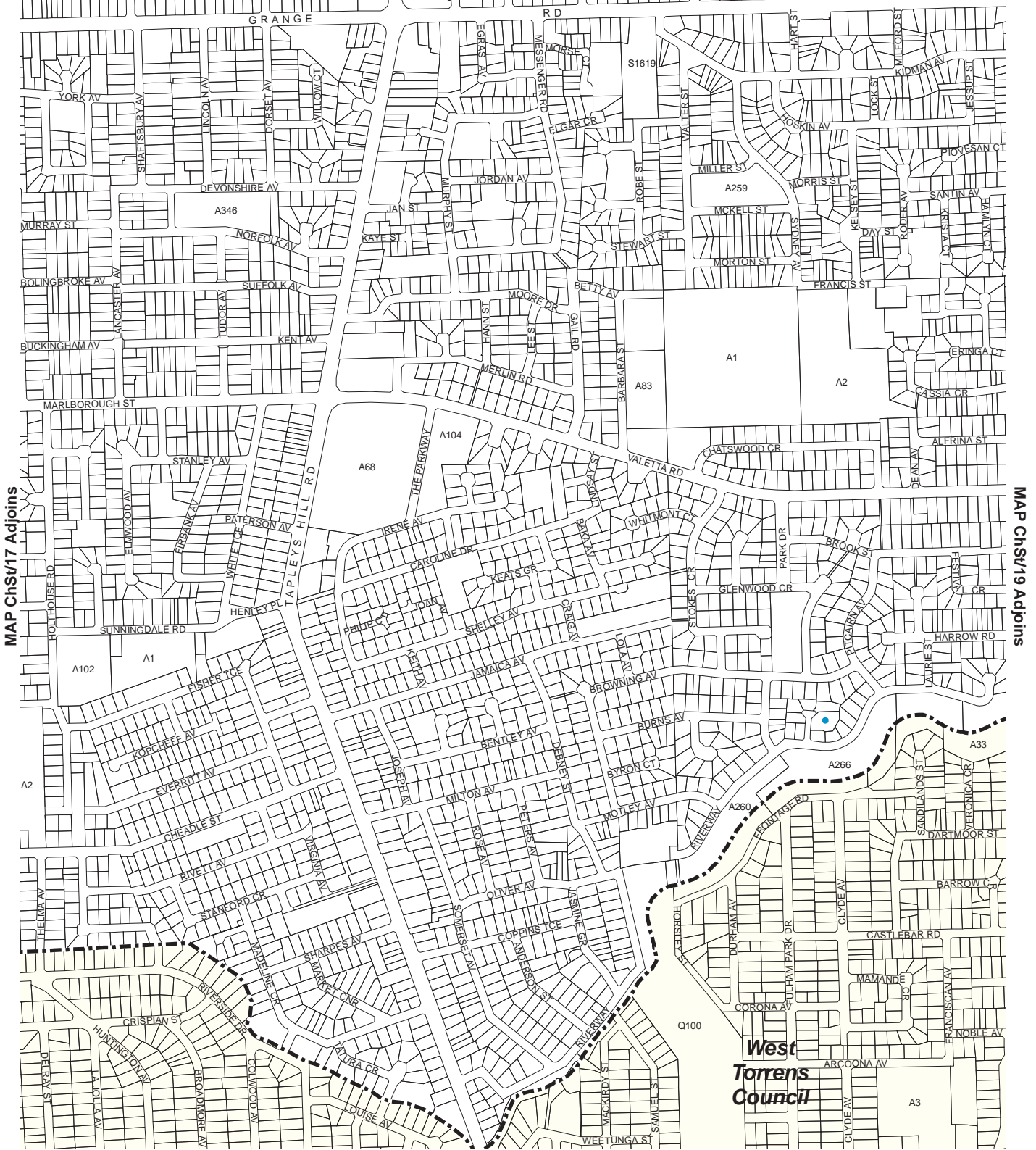
Airport Building Heights
Zone C All Structures Exceeding 15 metres above existing ground level



Overlay Map ChSt/18

DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Development Plan Boundary



MAP ChSt/17 Adjoins

MAP ChSt/19 Adjoins

Heritage points are indicative only. For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

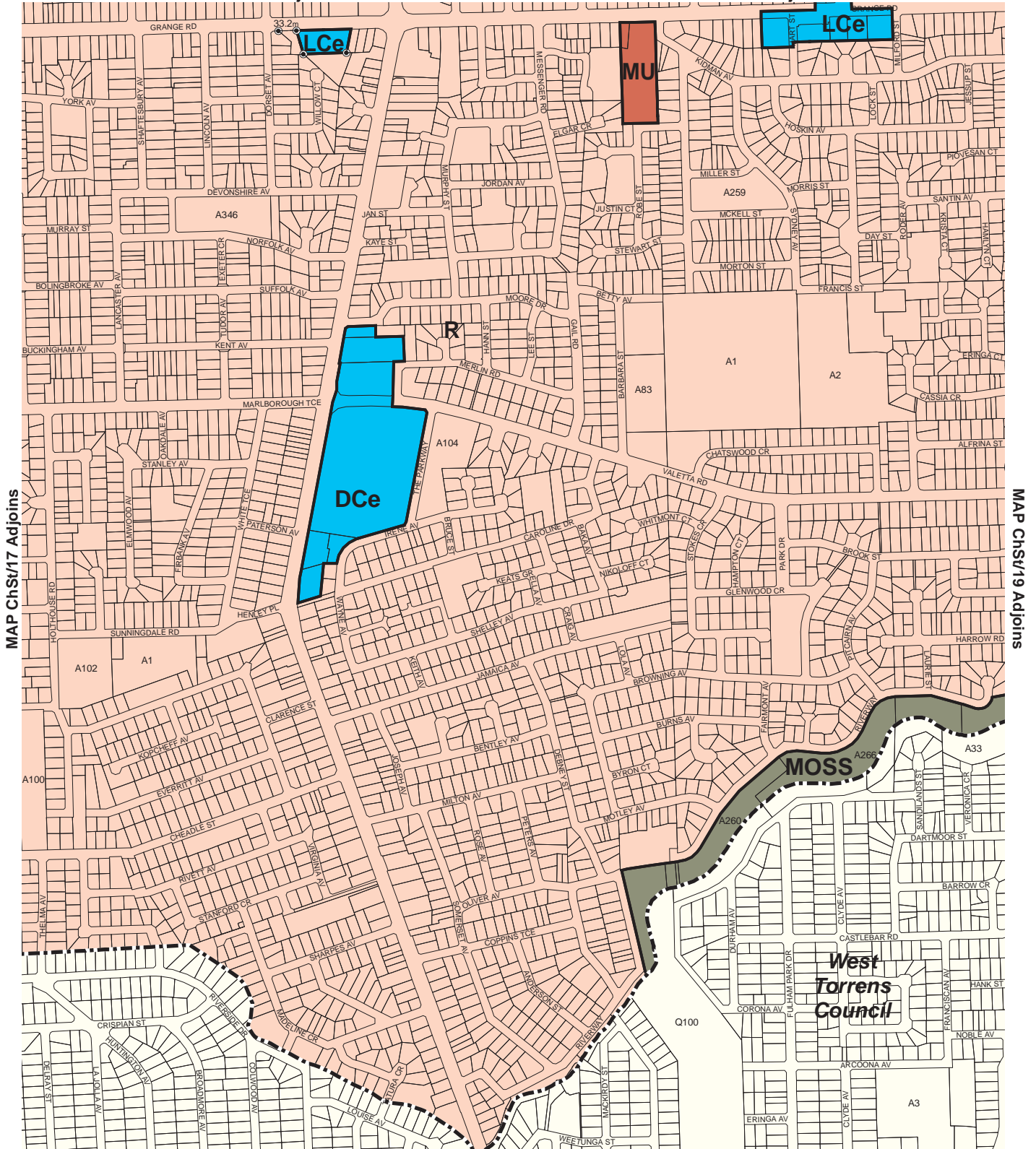


Overlay Map ChSt/18 HERITAGE

- Local heritage place
- Development Plan Boundary

MAP ChSt/12 Adjoins

MAP ChSt/13 Adjoins



MAP ChSt/1 Adjoins

Lamberts Conformal Conic Projection, GDA94



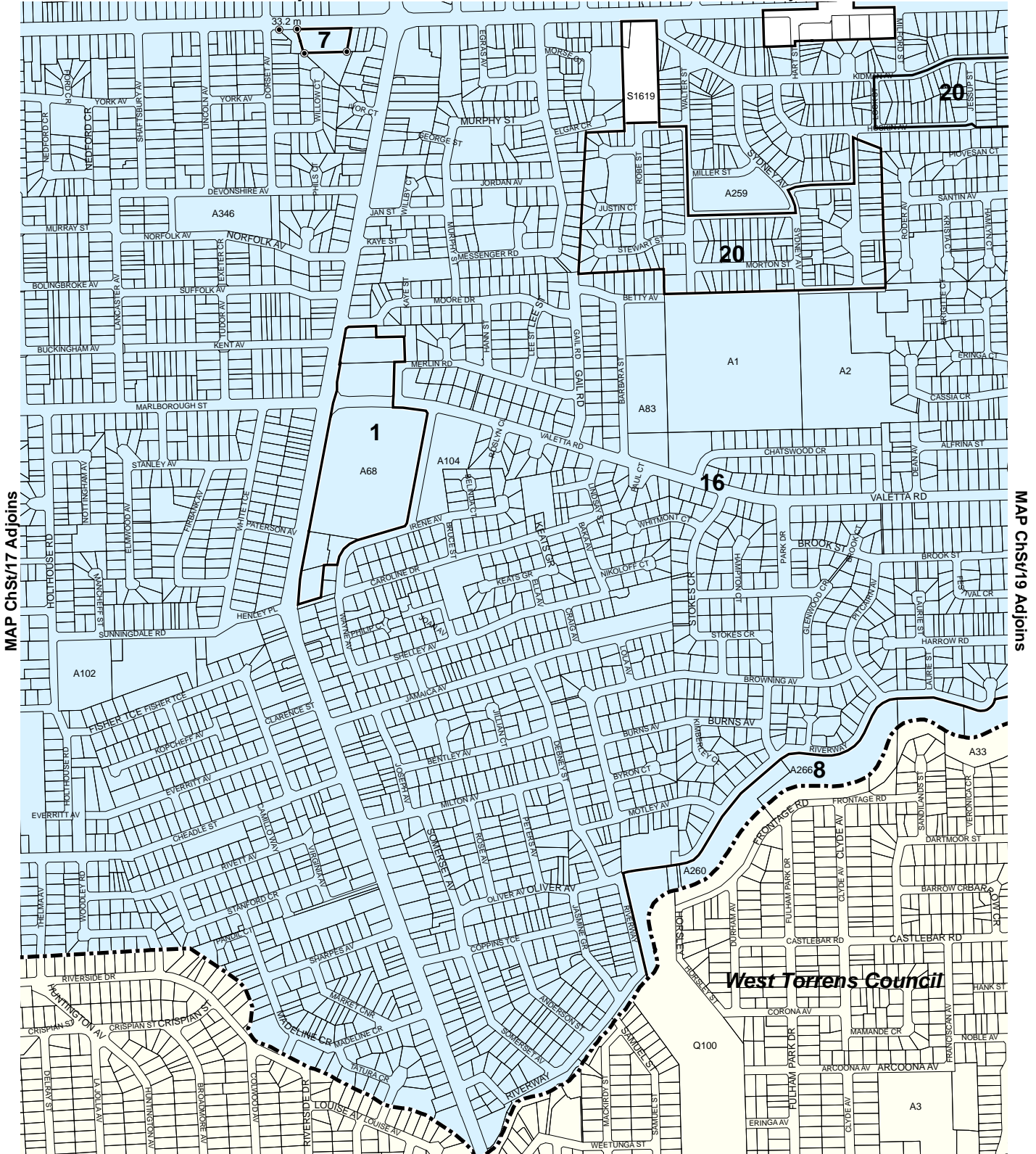
Zones

- DCe District Centre
- LCe Local Centre
- MOSS Metropolitan Open Space System
- MU Mixed Use
- R Residential
- Zone Boundary
- Development Plan Boundary

Zone Map ChSt/18

MAP ChSt/12 Adjoins

MAP ChSt/13 Adjoins



MAP ChSt/1 Adjoins

Lamberts Conformal Conic Projection, GDA94

Policy Area

- 1 Fulham Gardens
- 7 Local Shopping
- 8 Linear Park (River Torrens / Karrawirra Pari)
- 16 Mid Suburban
- 20 Integrated Medium Density

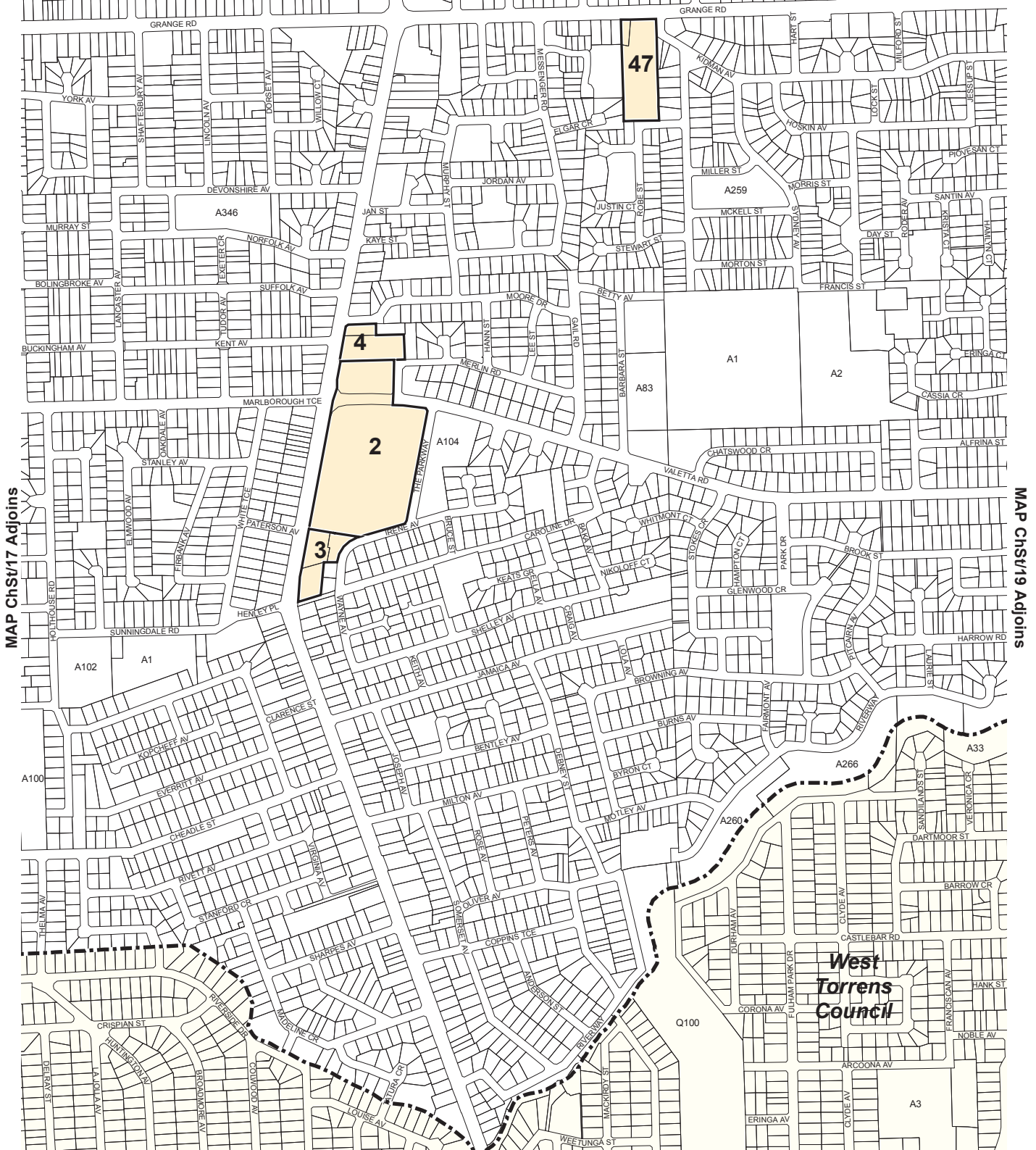


Policy Area Map ChSt/18

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/12 Adjoins

MAP ChSt/13 Adjoins



Lamberts Conformal Conic Projection, GDA94

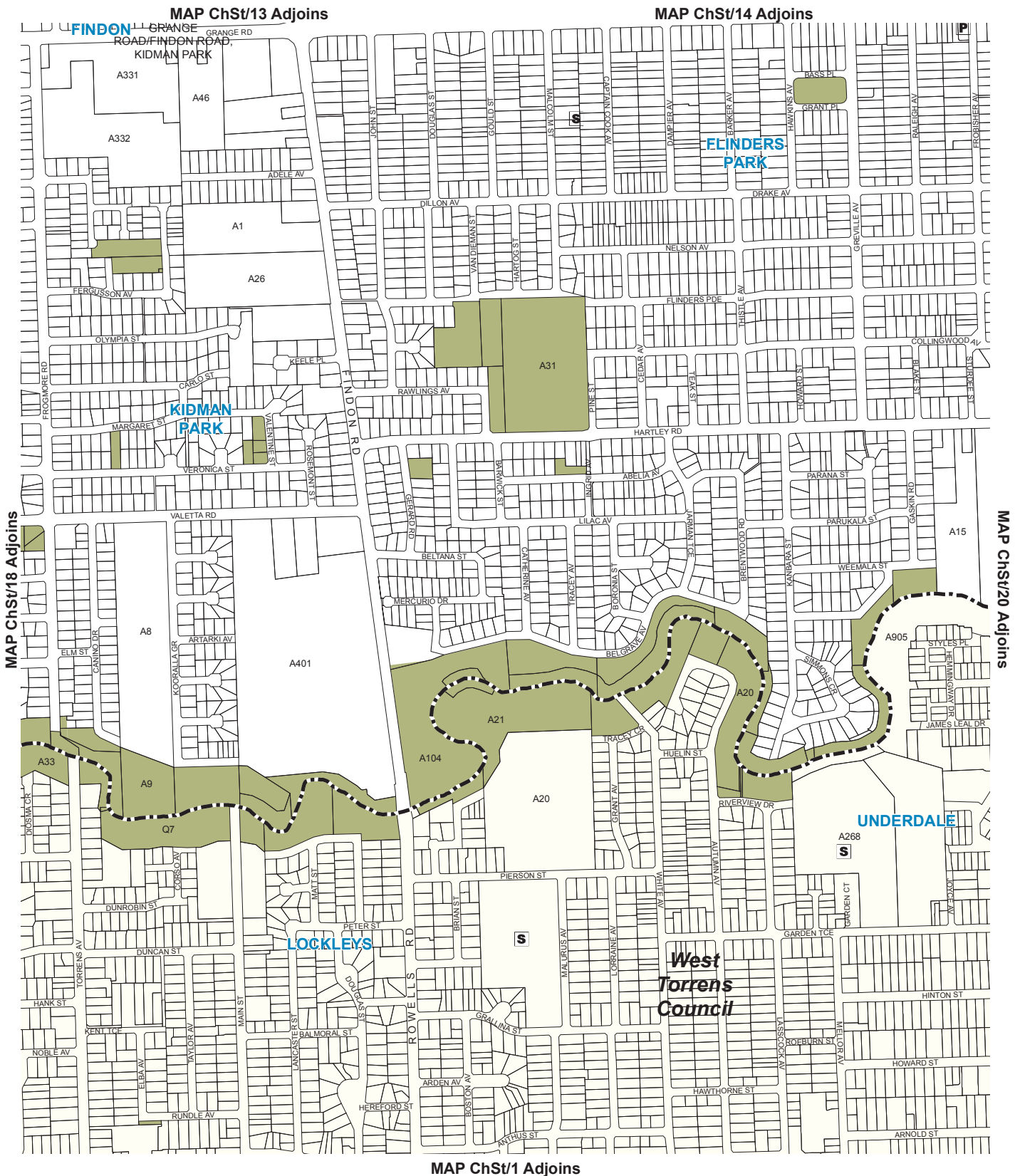
Precinct

- 2 Retail Core Fulham Gardens
- 3 Commercial Fringe Fulham Gardens
- 4 Recreation Fulham Gardens
- 47 Mixed Use



Precinct Map ChSt/18

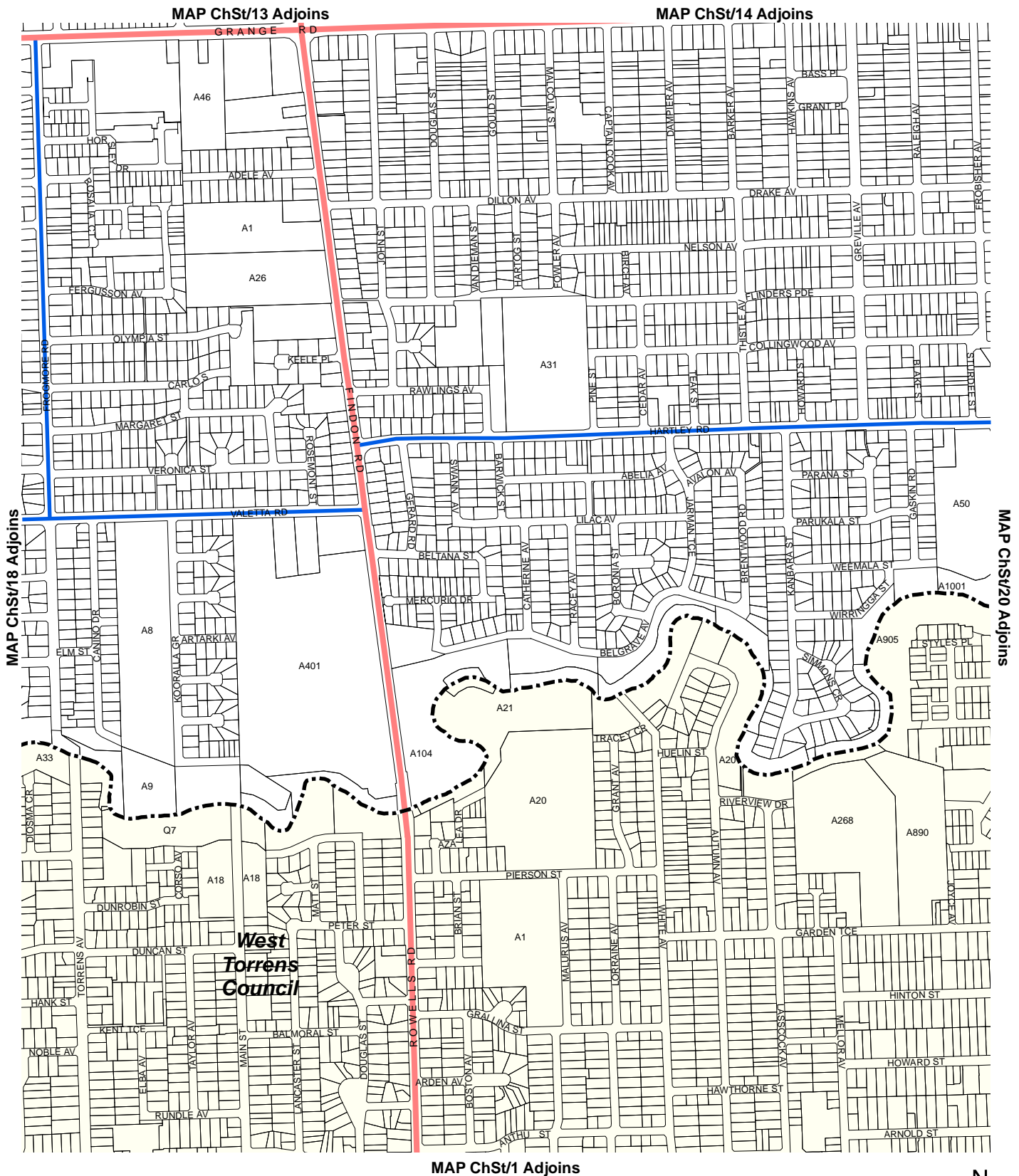
- Precinct Boundary
- Development Plan Boundary



Location Map ChSt/19

- S** School
- P** Post Office
- Local Reserves
- Development Plan Boundary





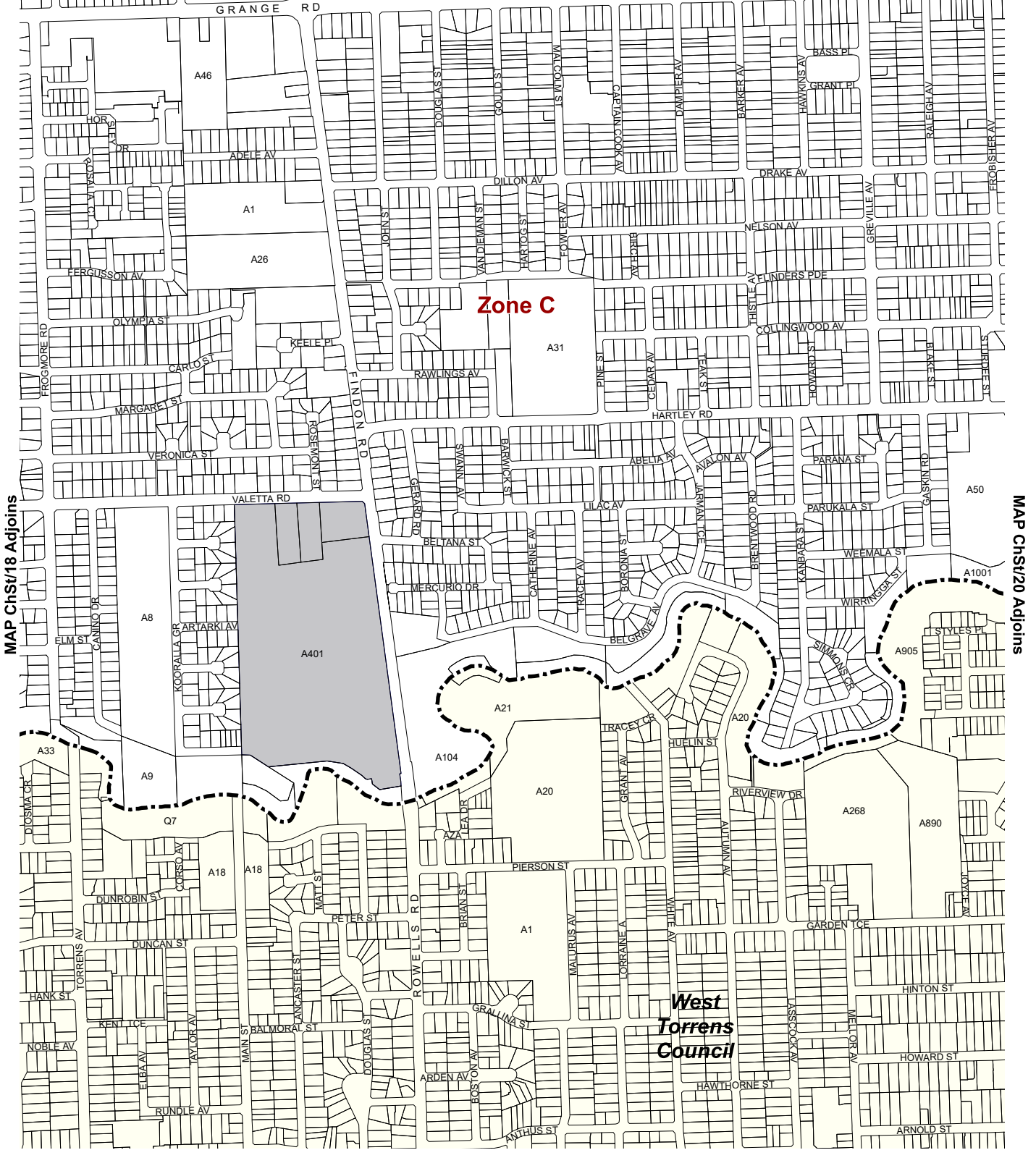
Overlay Map ChSt/19

TRANSPORT

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/13 Adjoins

MAP ChSt/14 Adjoins



MAP ChSt/1 Adjoins

Airport Building Heights
Zone C All Structures Exceeding 15 metres above existing ground level

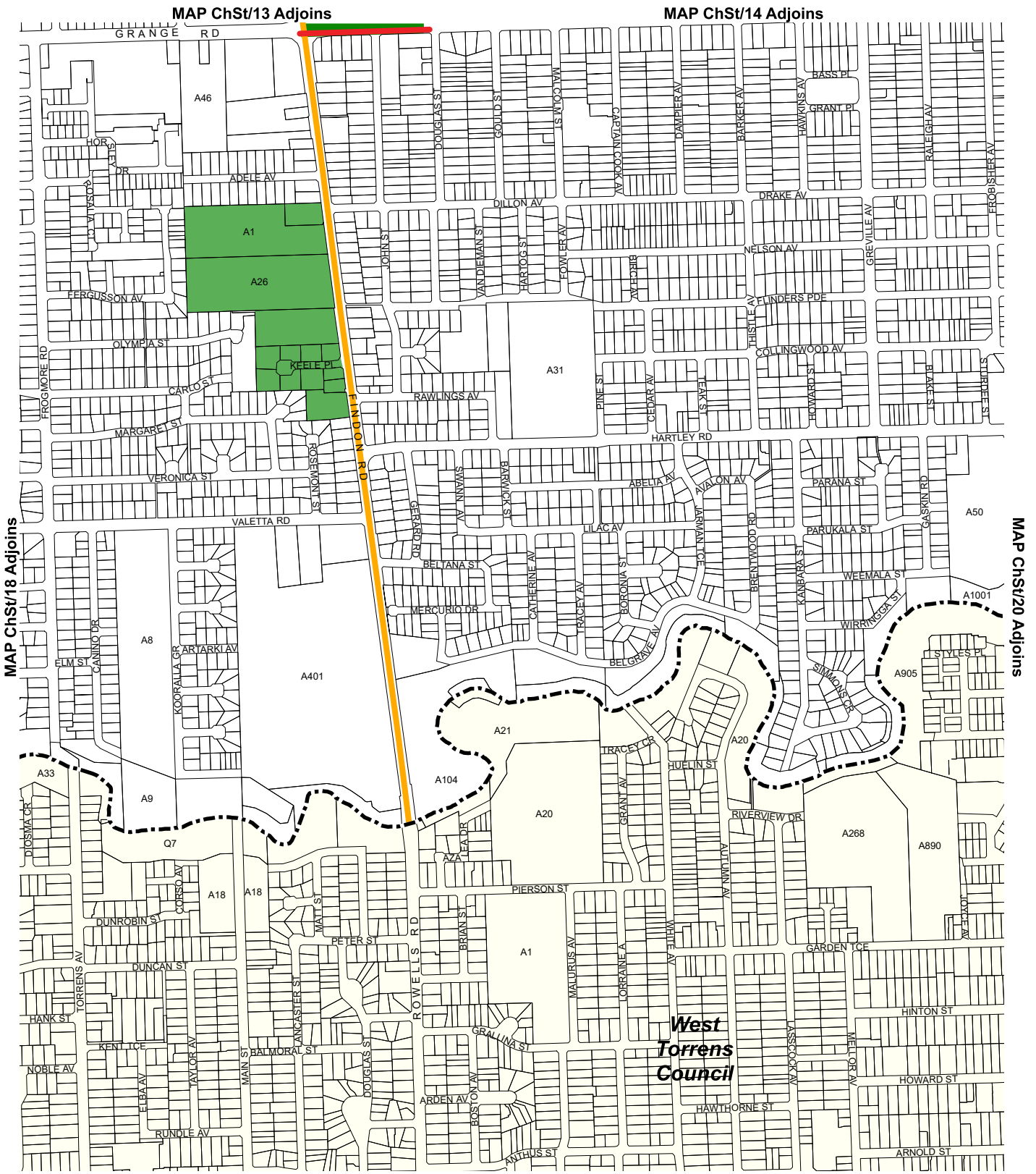


0 500m

Overlay Map ChSt/19

DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Industry Interface Area
- Development Plan Boundary



MAP ChSt/13 Adjoins

MAP ChSt/14 Adjoins

MAP ChSt/18 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/1 Adjoins



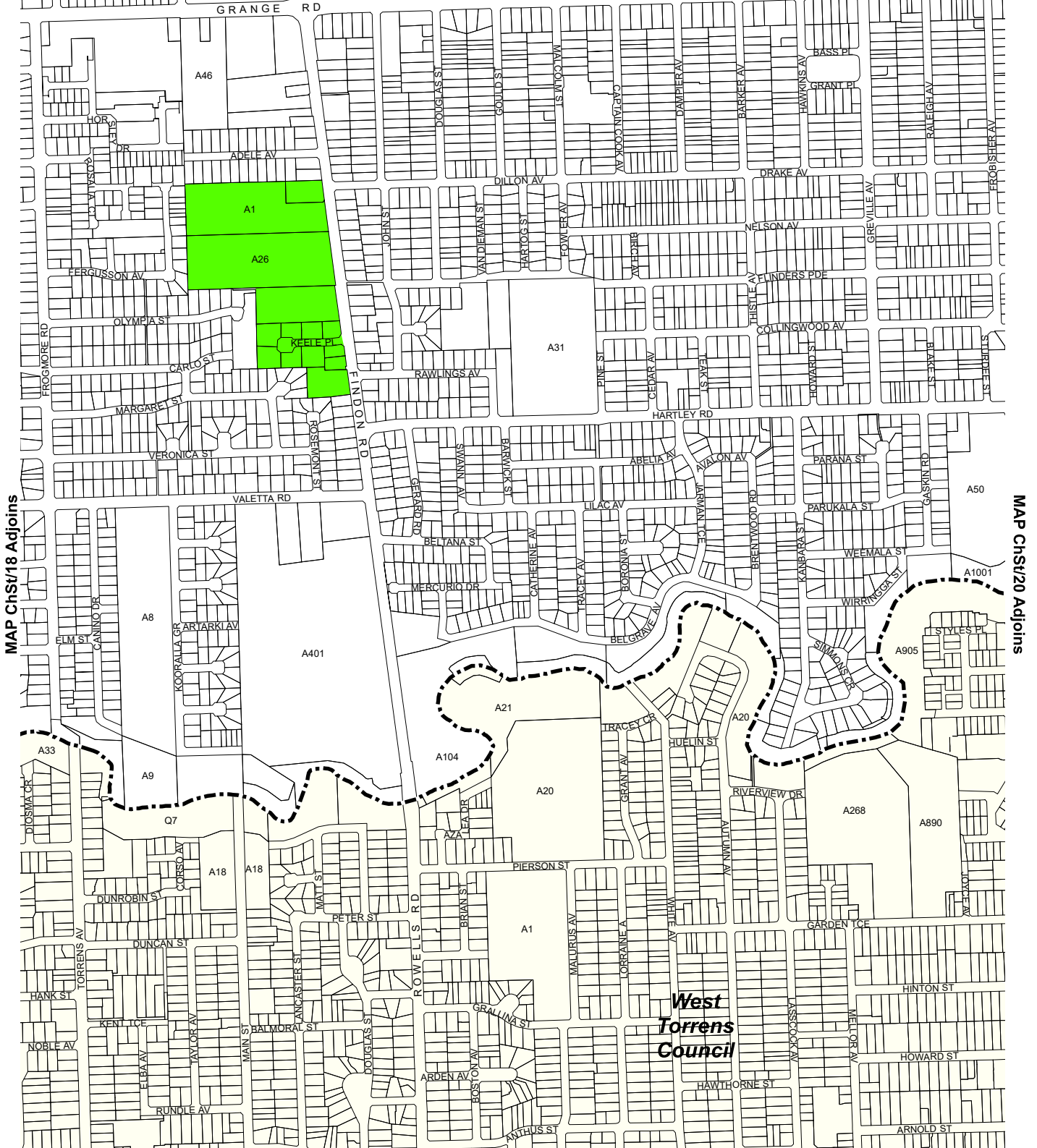
- Designated Road: type A road
- Designated Road: type B road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

Overlay Map ChSt/19

NOISE AND AIR EMISSIONS

MAP ChSt/13 Adjoins

MAP ChSt/14 Adjoins



MAP ChSt/1 Adjoins

MAP ChSt/20 Adjoins

MAP ChSt/18 Adjoins

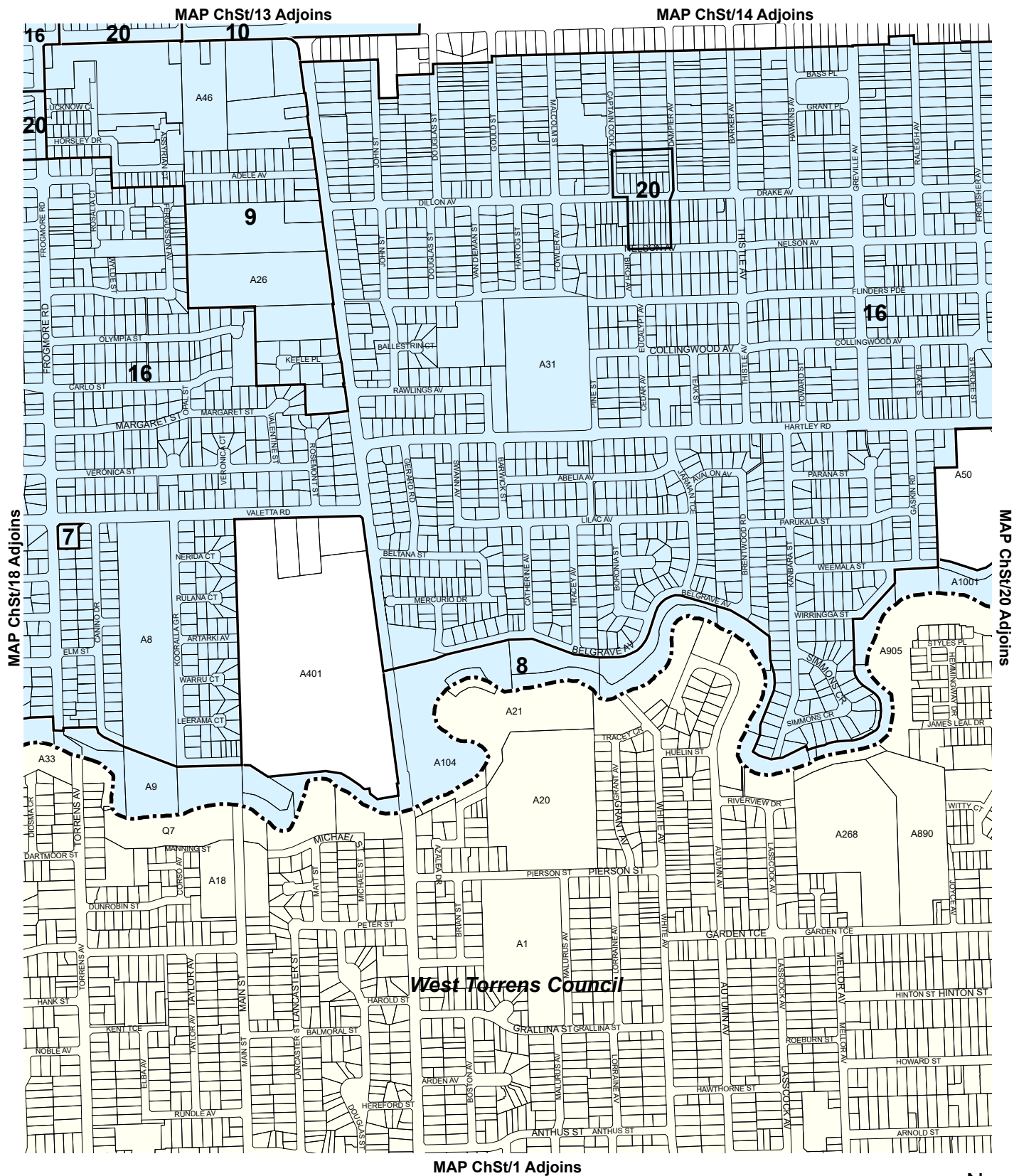


0 500m

Overlay Map ChSt/19

AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary



Lamberts Conformal Conic Projection, GDA94

Policy Area

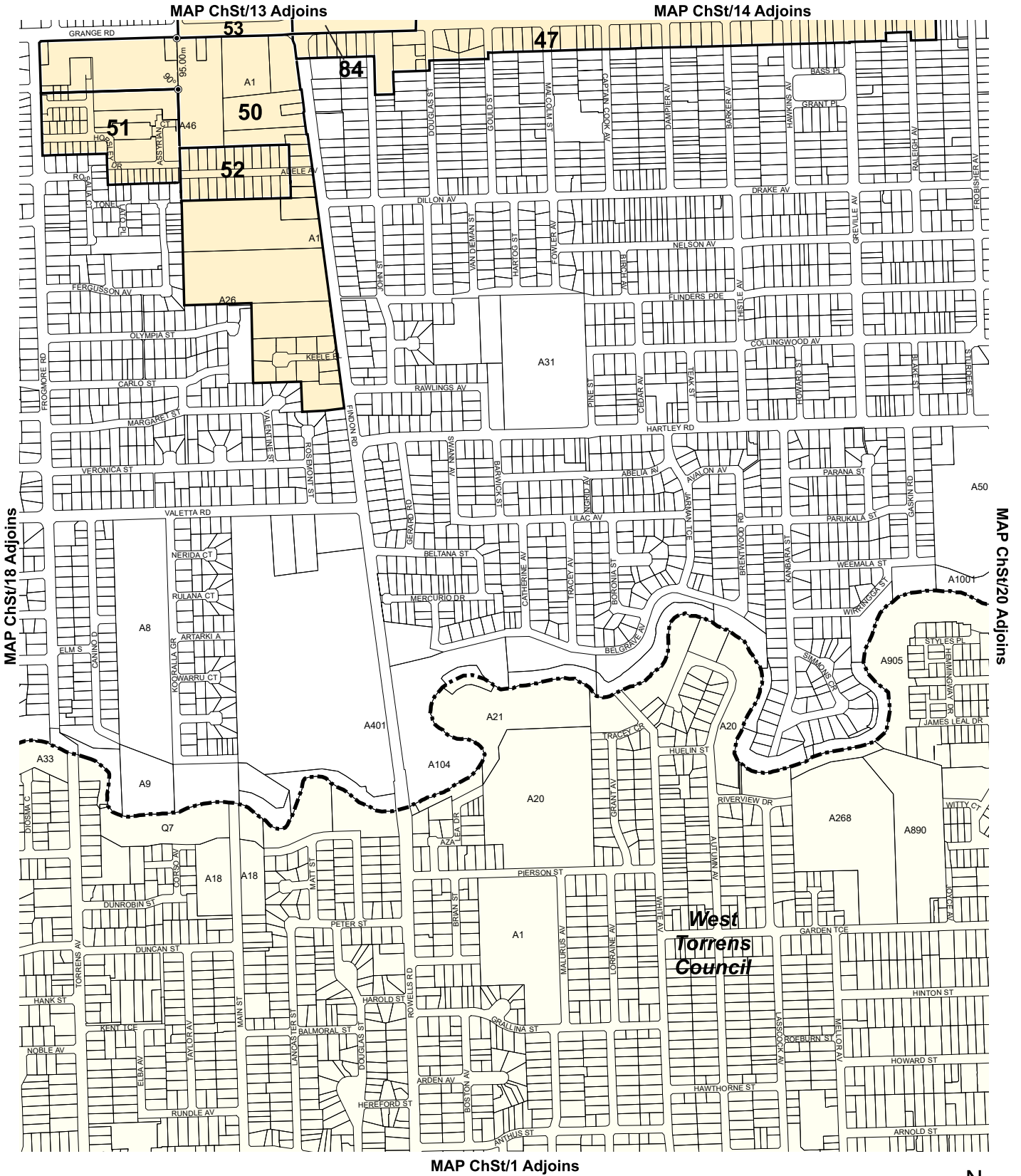
- 7 Local Shopping
- 8 Linear Park(River Torrens/Karrawirra Parri)
- 9 Urban Village Findon
- 16 Findon
- Mid Suburban
- 20 Integrated Medium Density



MAP ChSt/1 Adjoins

Policy Area Map ChSt/19

- Policy Area Boundary
- Development Plan Boundary



Lamberts Conformal Conic Projection, GDA94

Precinct

- 47 Mixed Use
- 50 Village Employment and Living
- 51 Village Living Medium
- 52 Village Living Low
- 53 Retail Core Findon
- 84 Findon Centre East



- Precinct Boundary
- Development Plan Boundary

Precinct Map ChSt/19

MAP ChSt/14 Adjoins

MAP ChSt/15 Adjoins



MAP ChSt/1 Adjoins

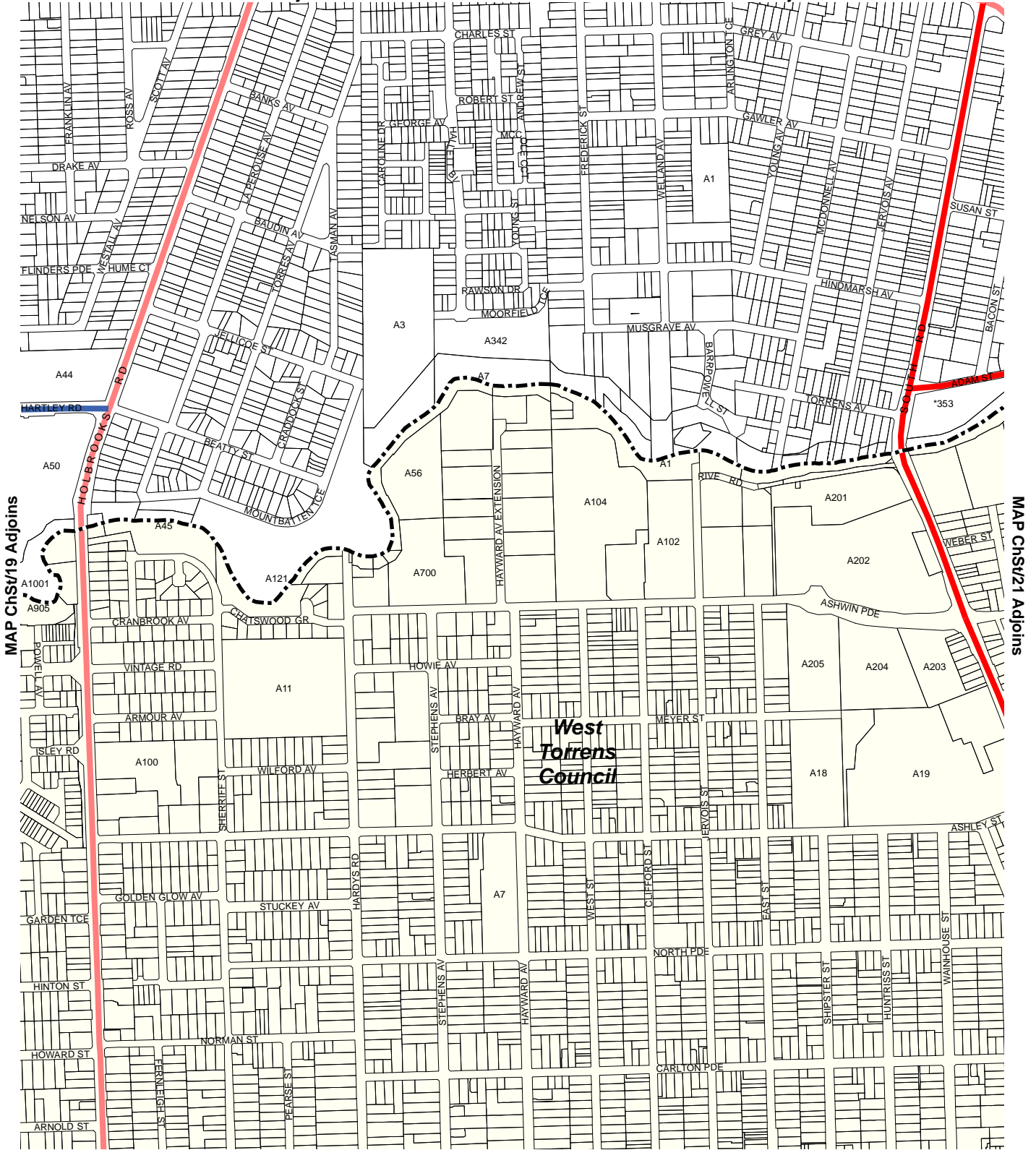


Location Map ChSt/20

- S School
- Local Reserves
- Development Plan Boundary

MAP ChSt/14 Adjoins

MAP ChSt/15 Adjoins



MAP ChSt/1 Adjoins



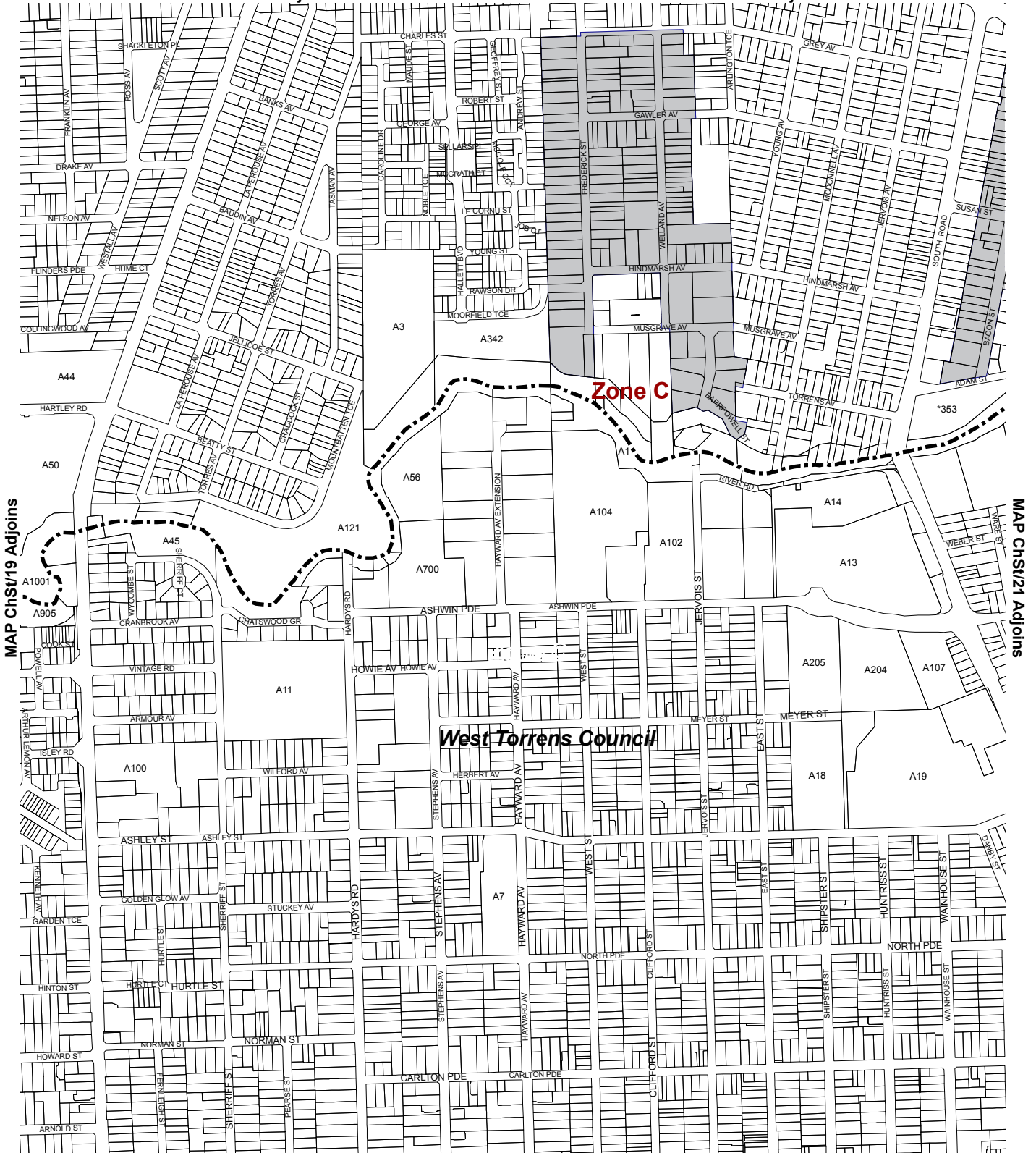
Overlay Map ChSt/20

TRANSPORT

- Primary Arterial Roads
- Secondary Arterial Roads
- Distributor Roads
- - - - Development Plan Boundary

MAP ChSt/14 Adjoins

MAP ChSt/15 Adjoins



Airport Building Heights
Zone C All Structures Exceeding 15 metres above existing ground level

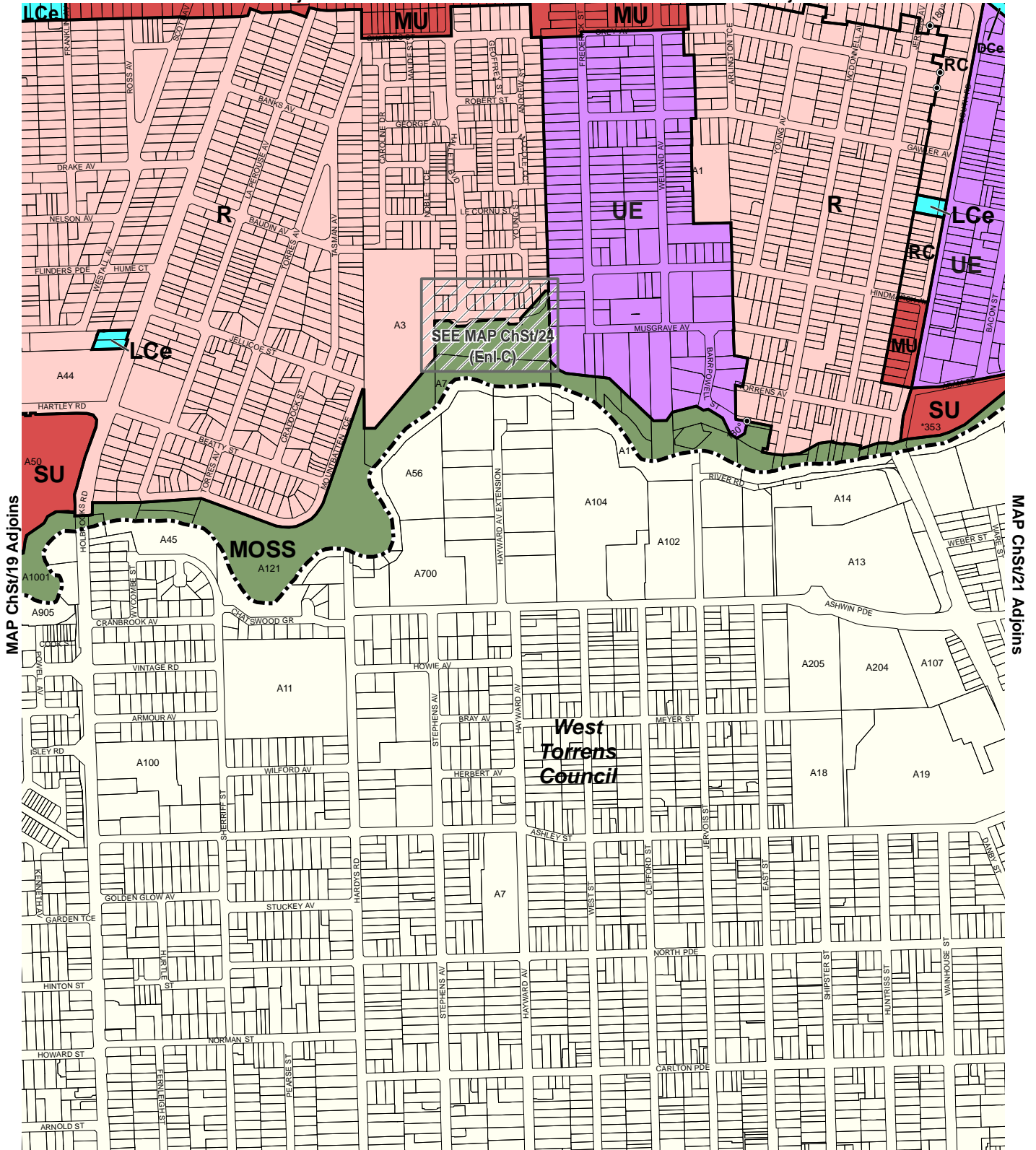


Overlay Map ChSt/20 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Industry Interface Area
- Development Plan Boundary

MAP ChSt/14 Adjoins

MAP ChSt/15 Adjoins



MAP ChSt/1 Adjoins

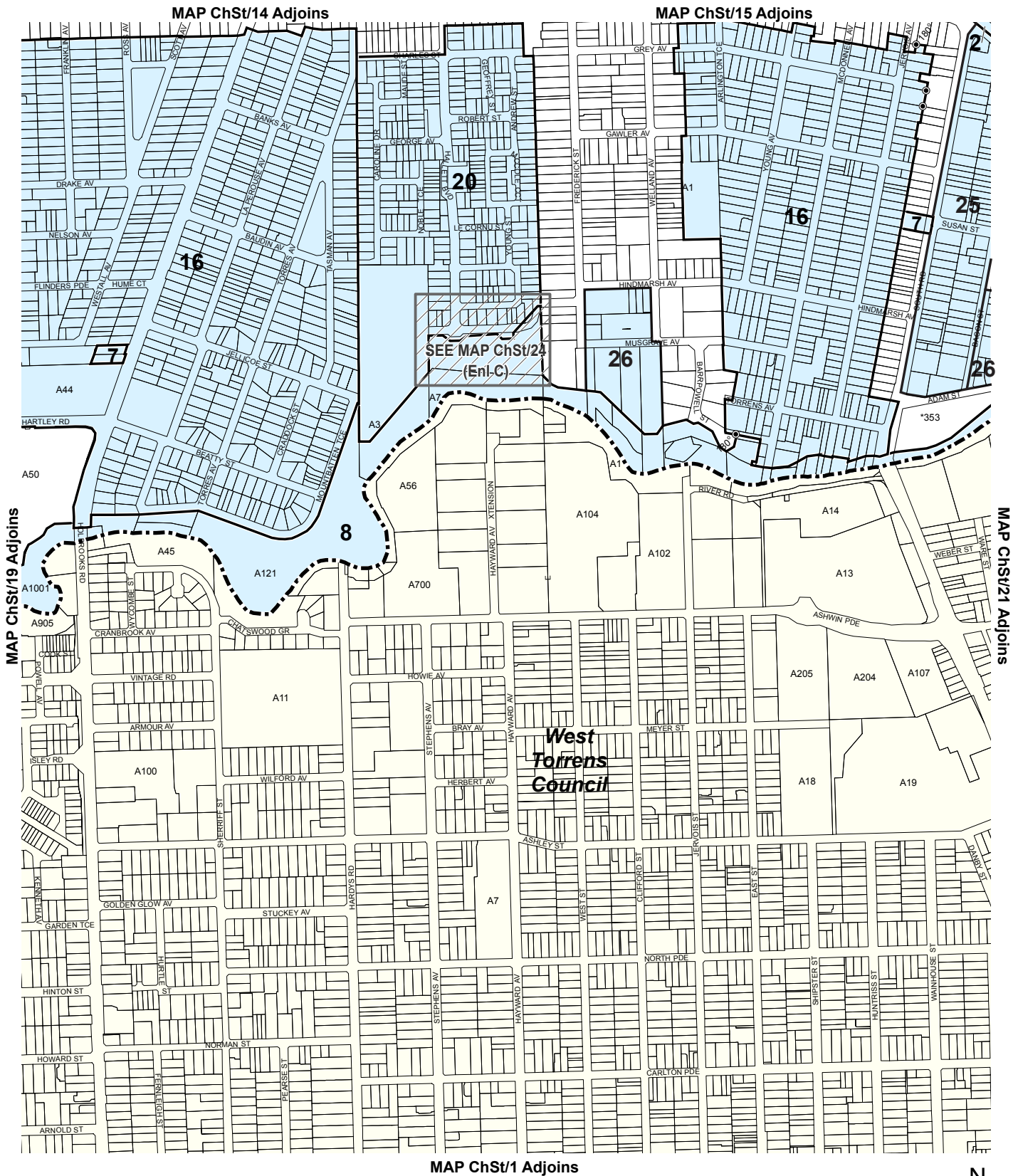
See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Zones

- DCe District Centre
- LCe Local Centre
- MOSS Metropolitan Open Space System
- MU Mixed Use
- R Residential
- RC Residential Character
- SU Special Use
- UE Urban Employment
- Zone Boundary
- Development Plan Boundary



Zone Map ChSt/20



See enlargement map for accurate representation.
 Lamberts Conformal Conic Projection, GDA94

- Policy Area**
- 2 Hindmarsh
 - 7 Local Shopping
 - 8 Linear Park(River Torrens/Karrawirra Parri)
 - 16 Mid Suburban
 - 20 Integrated Medium Density
 - 25 Main Road Commercial
 - 26 Core Industry

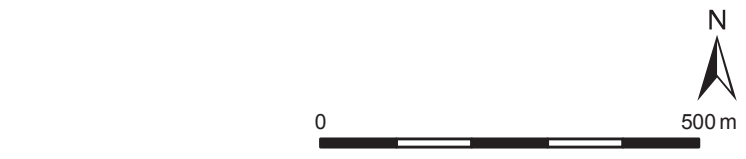


Policy Area Map ChSt/20

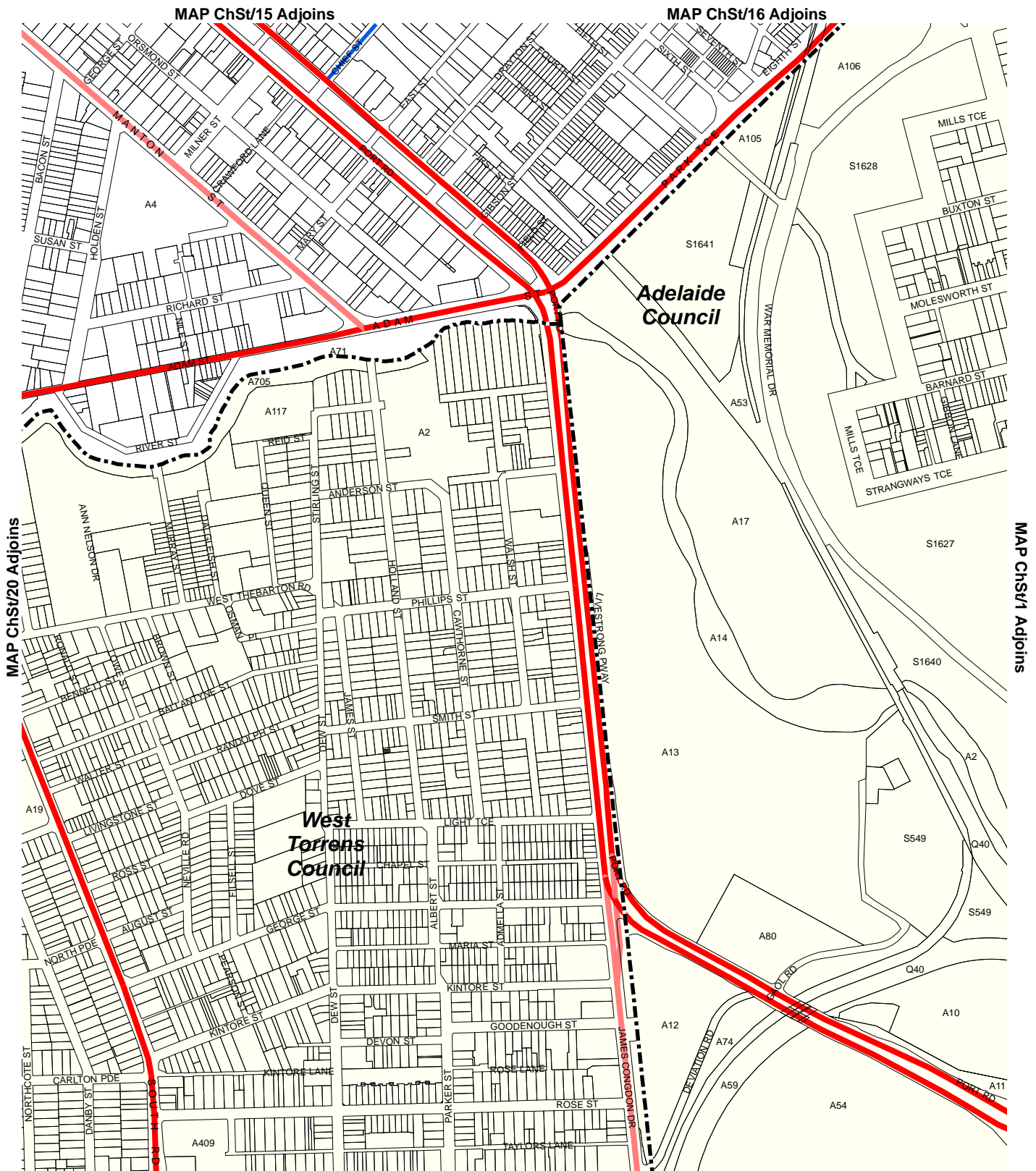
- Policy Area Boundary
- Development Plan Boundary



- S** School
- L** Public Library
- P** Post Office
- H** Other Health Services
- Railways
- Local Reserves
- Development Plan Boundary



Location Map ChSt/21



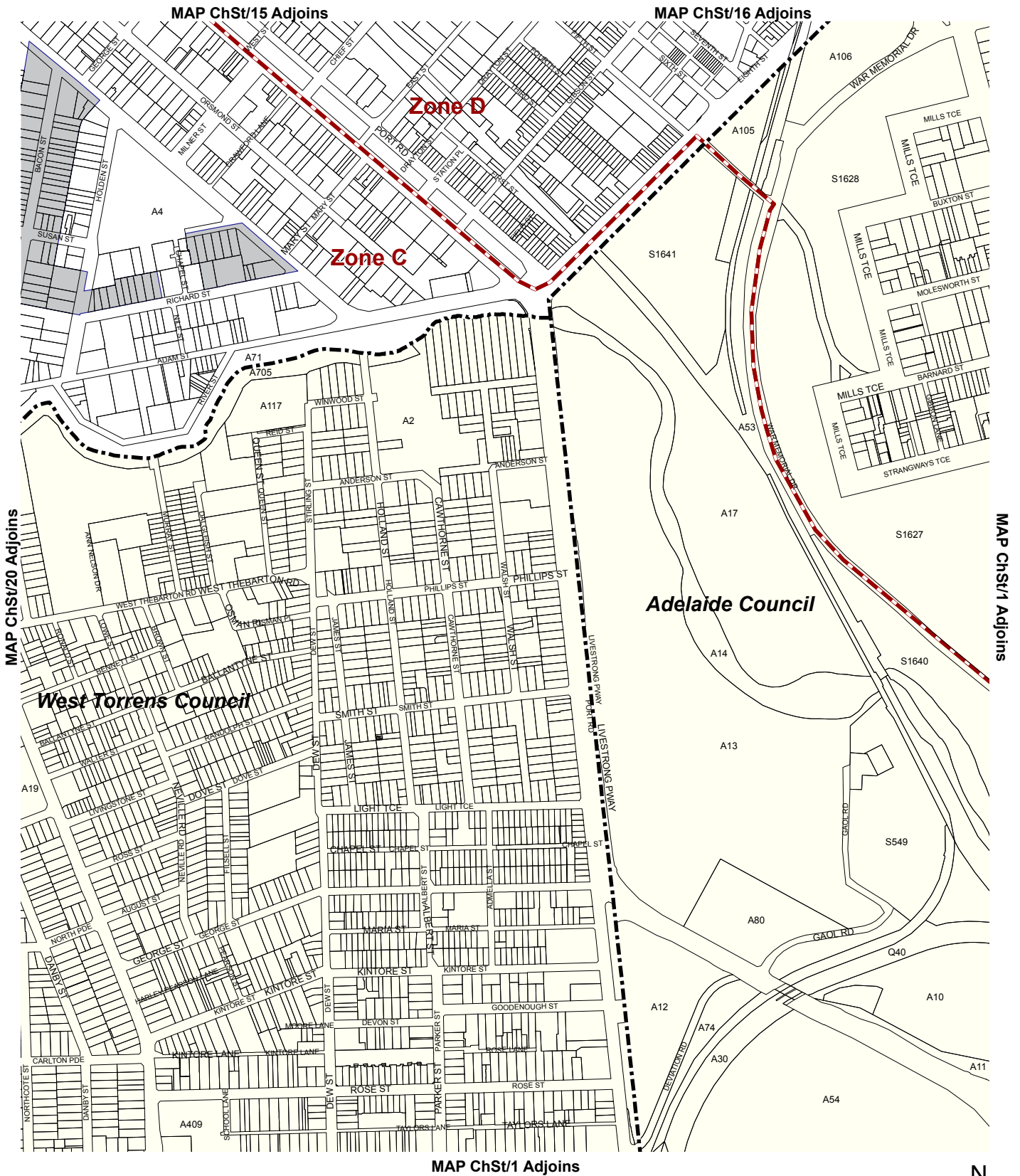
MAP ChSt/1 Adjoins

- ▬ Primary Arterial Roads
- ▬ Secondary Arterial Roads
- ▬ Distributor Roads
- Development Plan Boundary

Overlay Map ChSt/21

TRANSPORT





Airport Building Heights

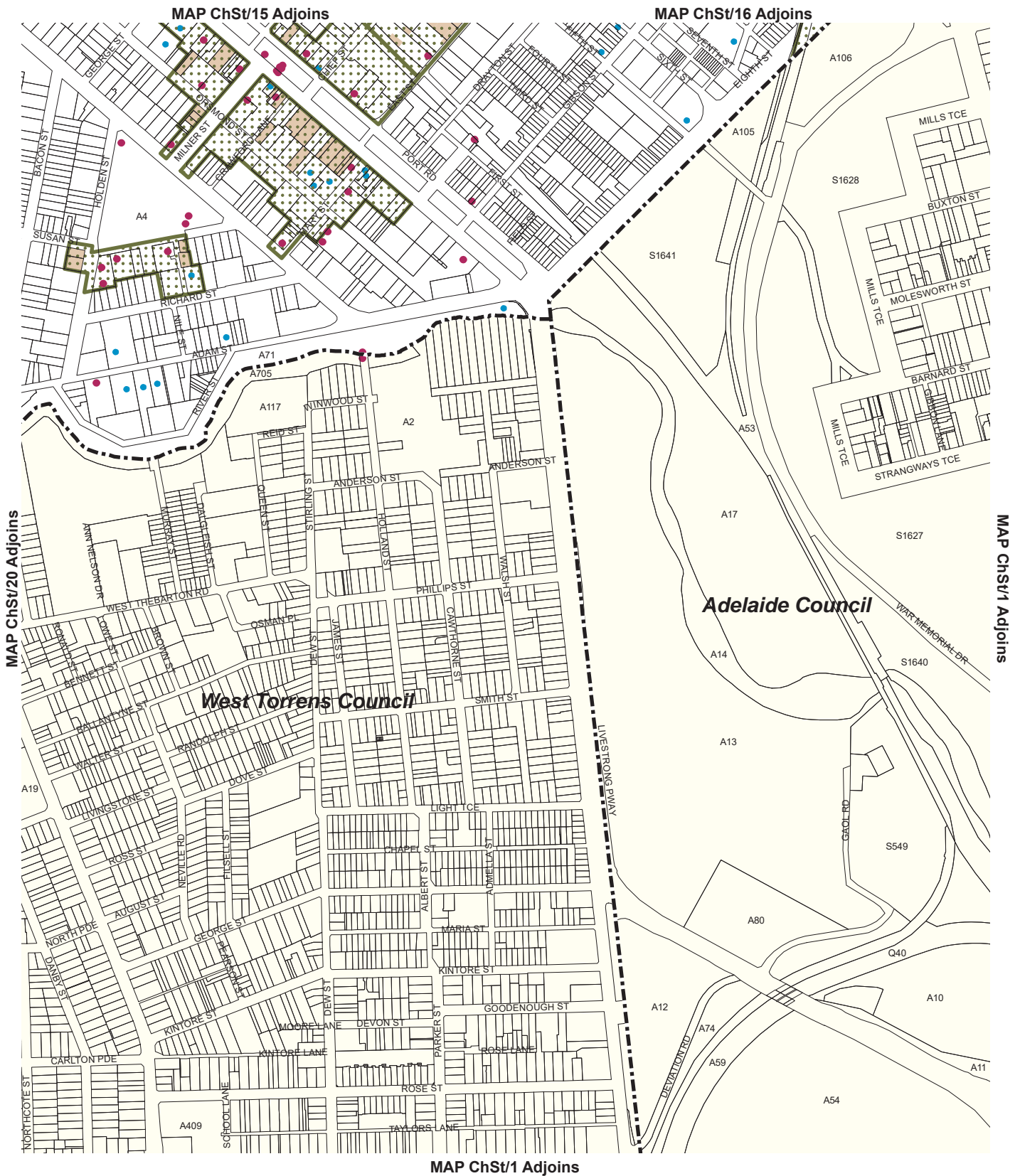
- Zone C** All Structures Exceeding 15 metres above existing ground level
- Zone D** All Structures Exceeding 45 metres above existing ground level

- Airport Building Heights
- Industry Interface Area
- Development Plan Boundary



Overlay Map ChSt/21

DEVELOPMENT CONSTRAINTS



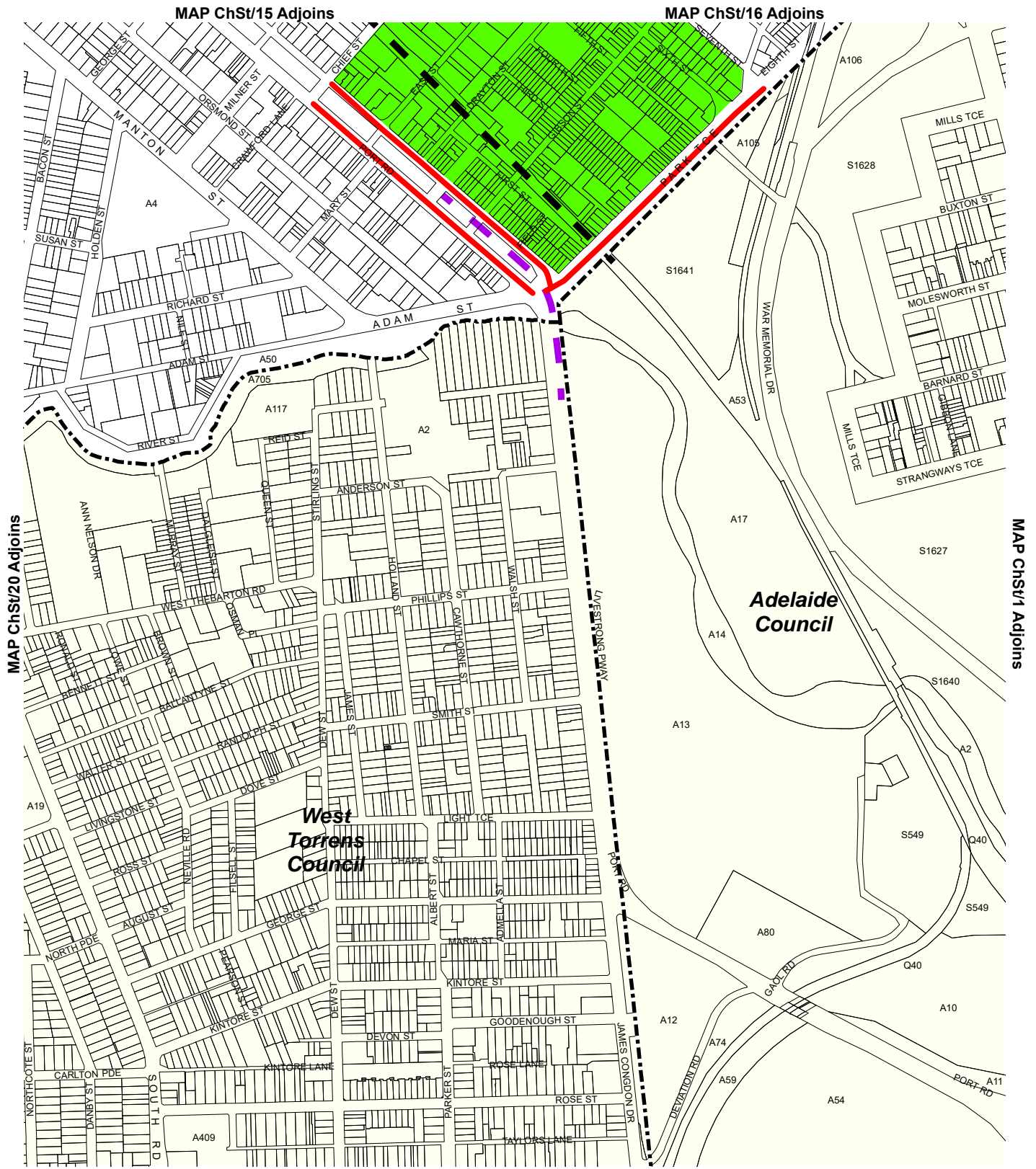
Heritage points are indicative only.
 For further information on State and Local Heritage Places and Contributory
 Items please refer to the relevant tables within this document.



- Local heritage place
- State heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/21

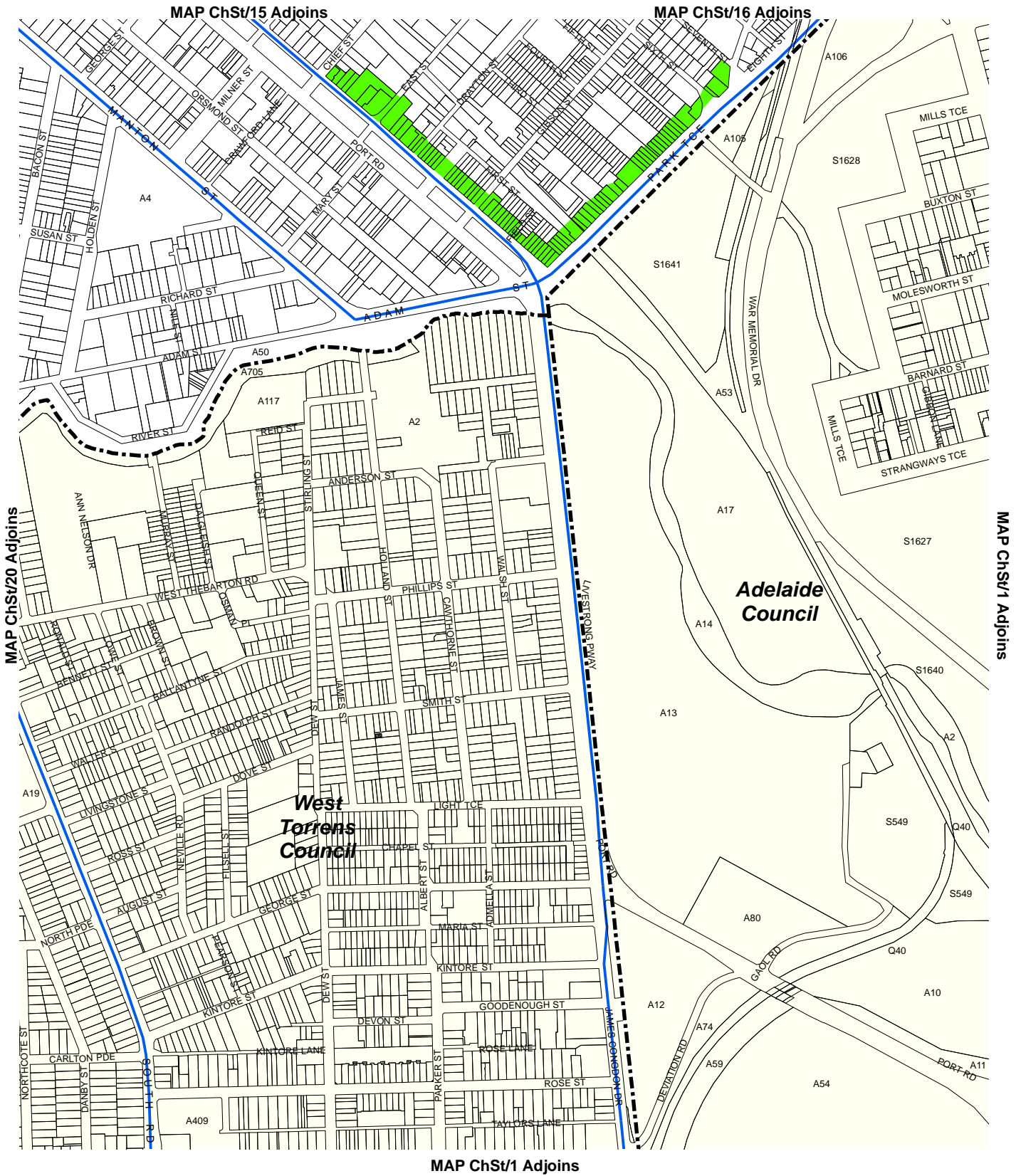
HERITAGE



- Tram Line
- Train Line
- Designated Road: type A road
- Noise and Air Emissions Designated Area
- Development Plan Boundary

Overlay Map ChSt/21

NOISE AND AIR EMISSIONS



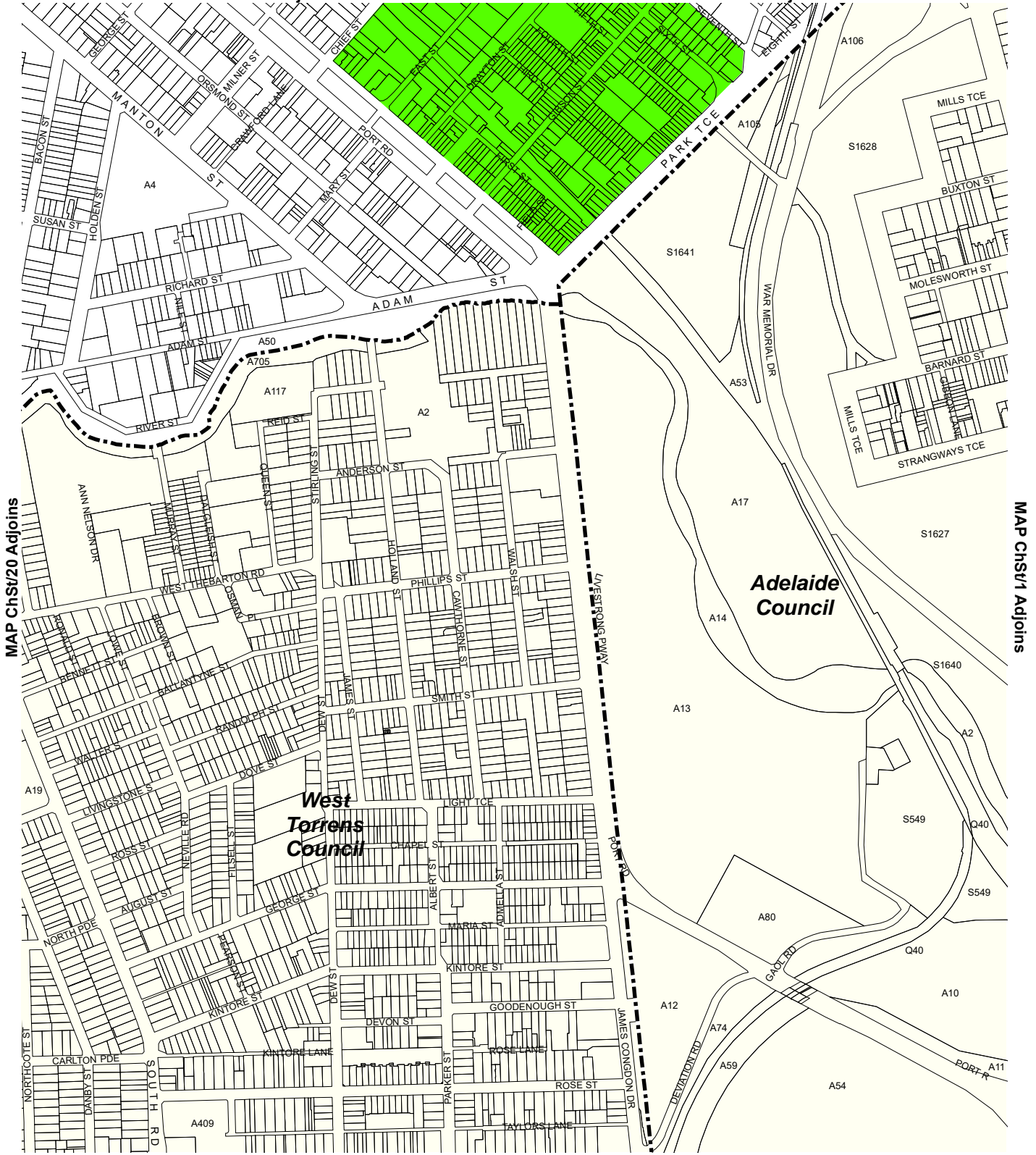
Overlay Map ChSt/21

STRATEGIC TRANSPORT ROUTES

- Strategic Transport Route
- Strategic Transport Routes Designated Area
- Development Plan Boundary

MAP ChSt/15 Adjoins

MAP ChSt/16 Adjoins



MAP ChSt/1 Adjoins



0 500m

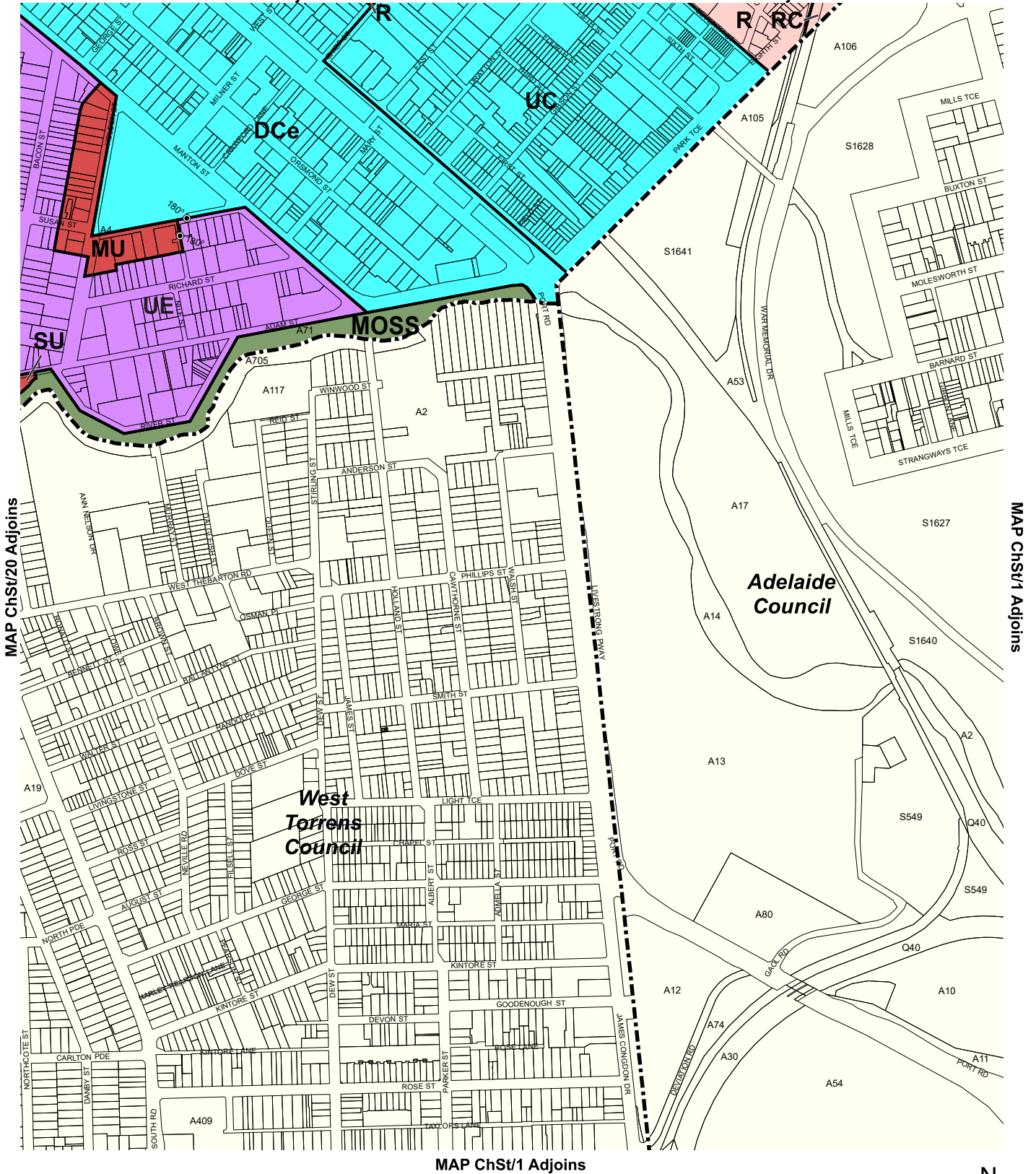
Overlay Map ChSt/21

AFFORDABLE HOUSING

- Affordable Housing Designated Area
- Development Plan Boundary

MAP ChSt/15 Adjoins

MAP ChSt/16 Adjoins



MAP ChSt/1 Adjoins

Lamberts Conformal Conic Projection, GDA94

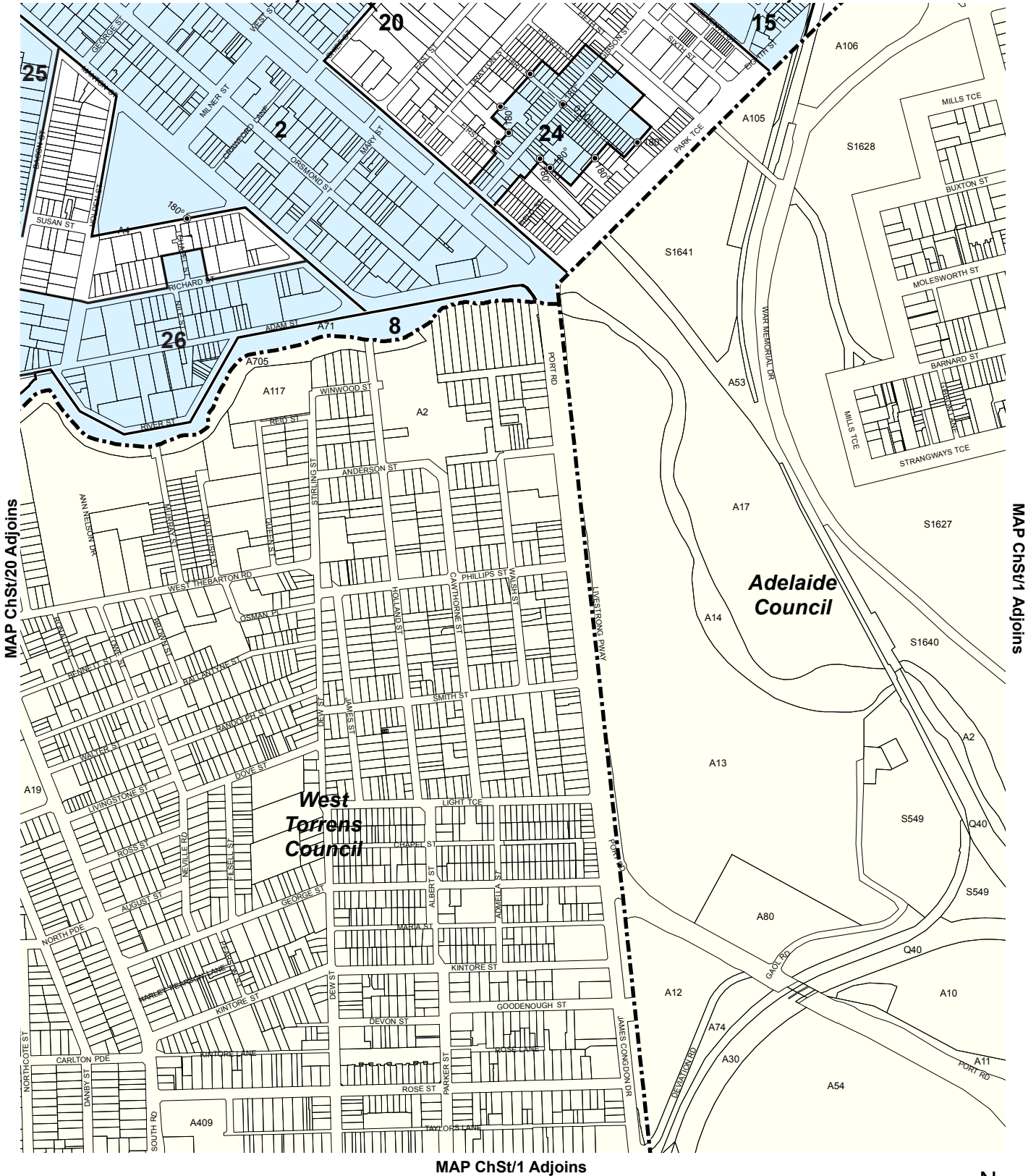
- Zones**
- DCe** District Centre
 - MOSS** Metropolitan Open Space System
 - MU** Mixed Use
 - R** Residential
 - RC** Residential Character
 - SU** Special Use
 - UC** Urban Core
 - UE** Urban Employment
 - Zone Boundary
 - - - -** Development Plan Boundary



Zone Map ChSt/21

MAP ChSt/15 Adjoins

MAP ChSt/16 Adjoins



MAP ChSt/20 Adjoins

MAP ChSt/1 Adjoins

Lamberts Conformal Conic Projection, GDA94

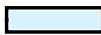

Policy Area

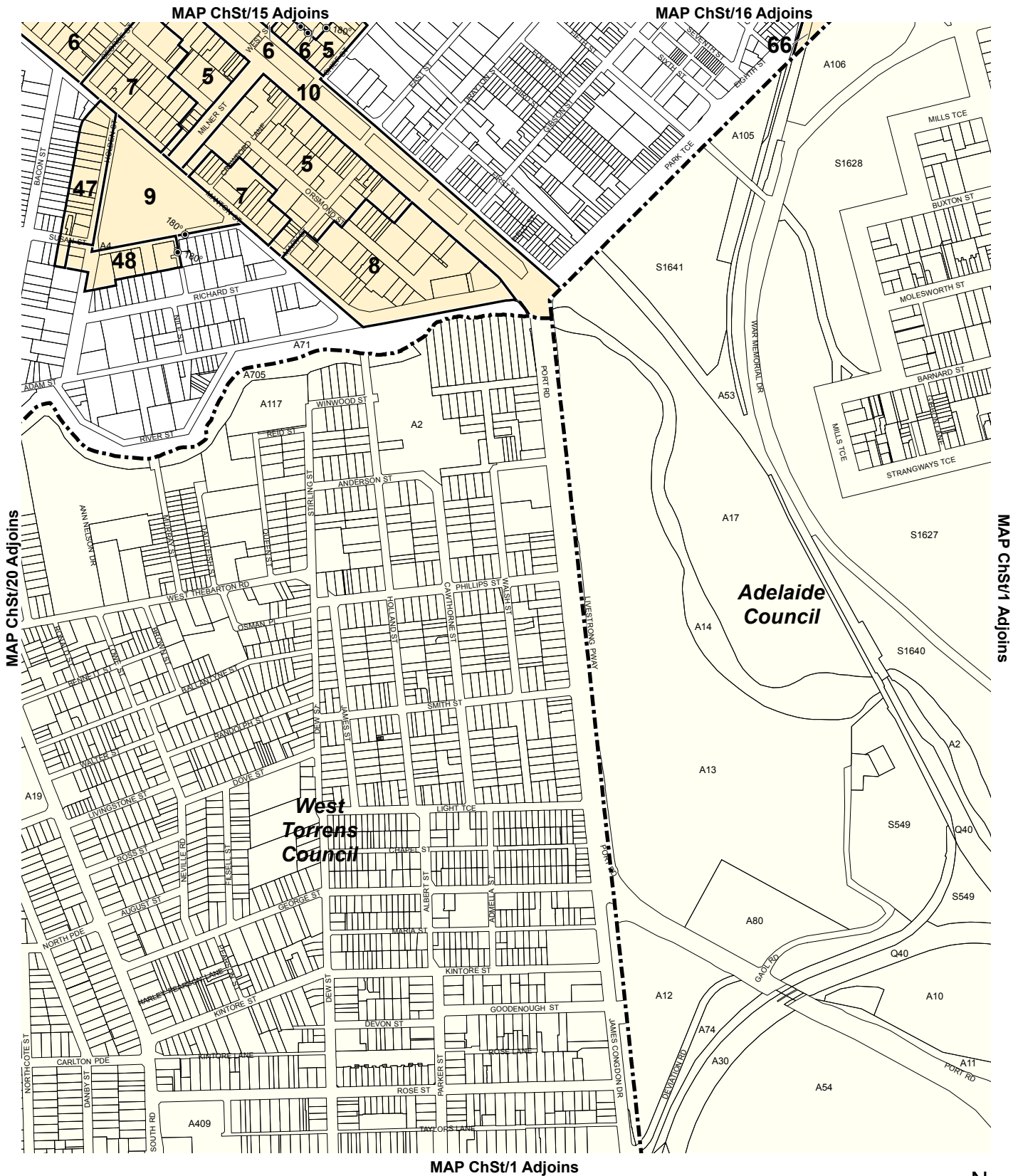
- 2 Hindmarsh
- 8 Linear Park(River Torrens/Karrawirra Parri)
- 15 Inner Suburban
- 20 Integrated Medium Density
- 24 Main Street
- 25 Main Road Commercial
- 26 Core Industry



0 500m

Policy Area Map ChSt/21

-  Policy Area Boundary
-  Development Plan Boundary



Lamberts Conformal Conic Projection, GDA94

- Precinct**
- 5 Historic Hub
 - 6 Port Road Office
 - 7 Manton Street
 - 8 Gateway
 - 9 Recreation Hindmarsh
 - 10 Port Road Gardens
 - 47 Mixed Use
 - 48 Hindmarsh Place
 - 66 Bowden/Brompton



- Precinct Boundary
- Development Plan Boundary

Precinct Map ChSt/21

MAP ChSt/17 Adjoins

HENLEY BEACH

West
Torrens
Council

HENLEY BEACH SOUTH

FULHAM

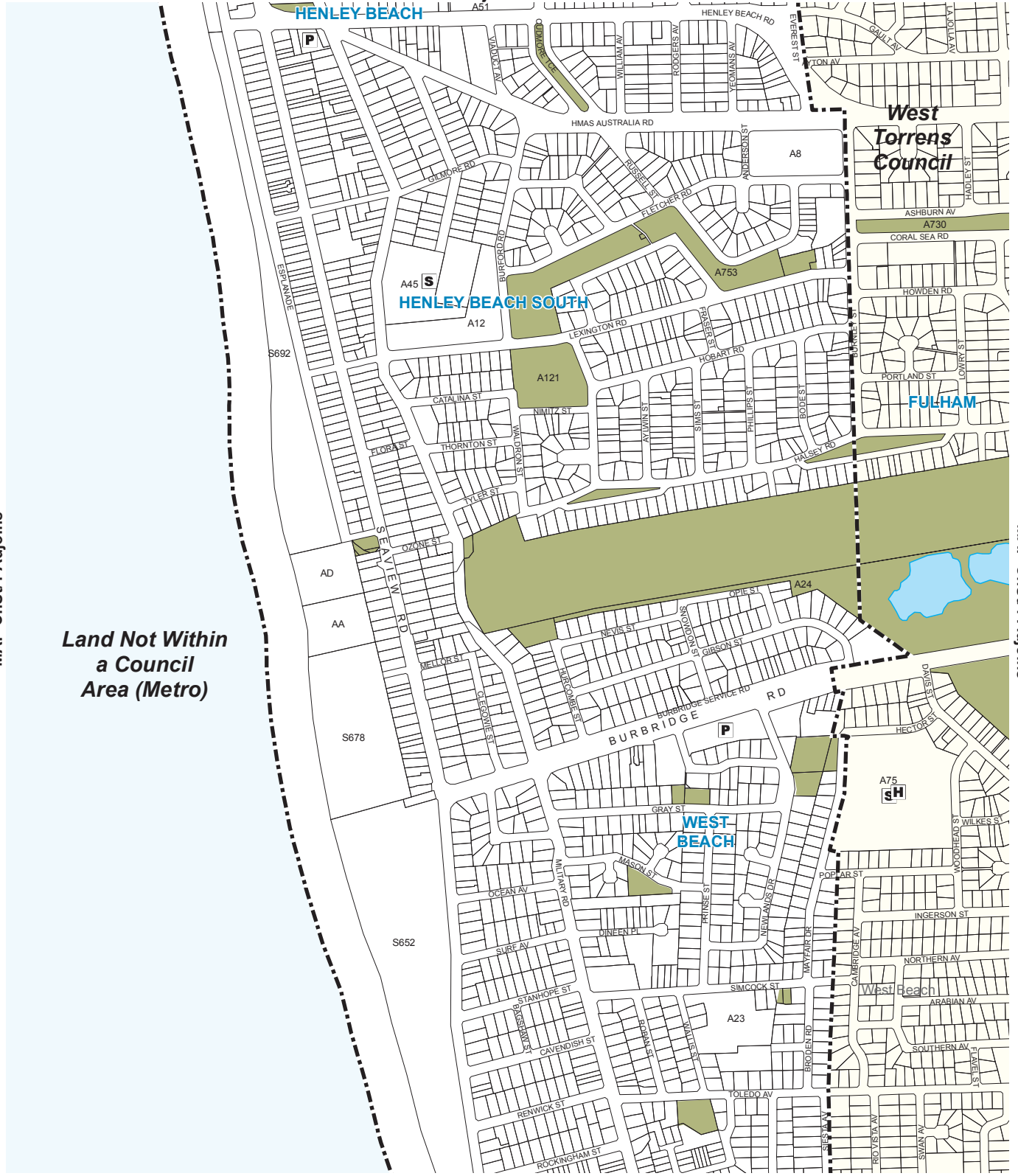
WEST
BEACH

MAP ChSt/23 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/1 Adjoins

Land Not Within
a Council
Area (Metro)



- S School
- P Post Office
- H Other Health Services
- Local Reserves
- Waterbodies
- Development Plan Boundary

Location Map ChSt/22

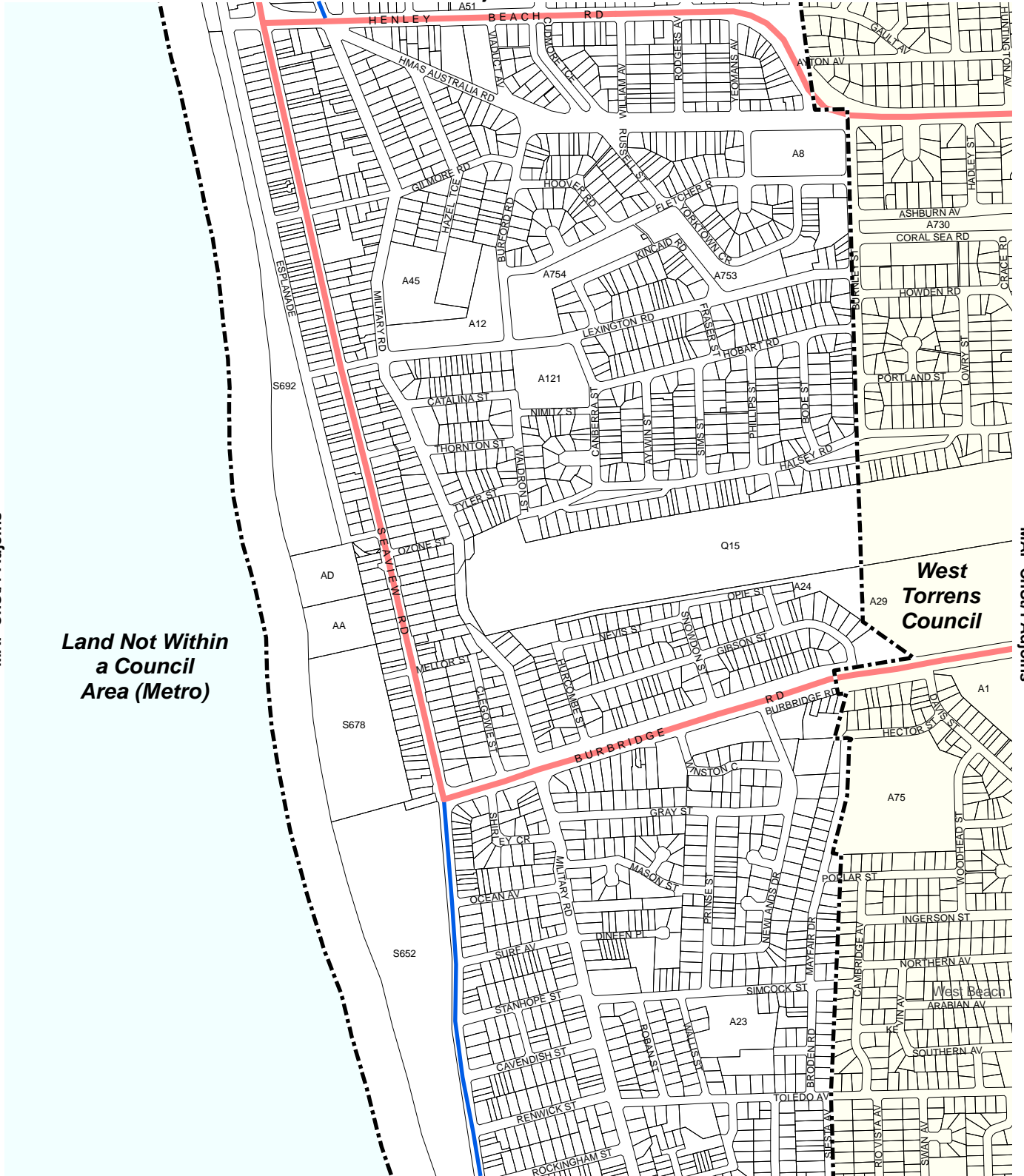
MAP ChSt/17 Adjoins

MAP ChSt/1 Adjoins

MAP ChSt/23 Adjoins

Land Not Within a Council Area (Metro)

MAP ChSt/1 Adjoins

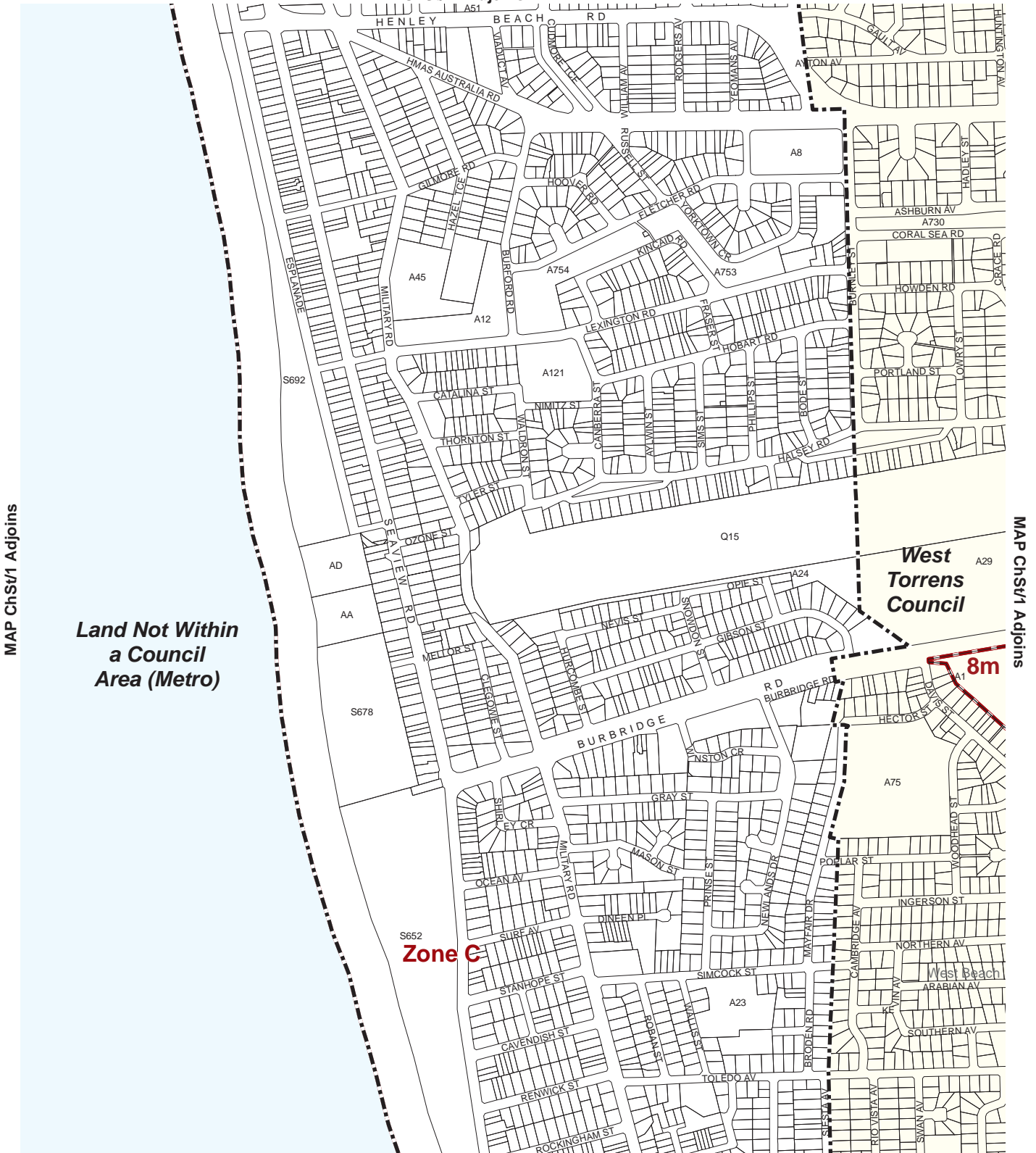


Overlay Map ChSt/22

TRANSPORT

- Secondary Arterial Roads
- Distributor Roads
- Development Plan Boundary

MAP ChSt/17 Adjoins



Land Not Within
a Council
Area (Metro)

Zone C

West
Torrens
Council

8m

Airport Building Heights

8m - 14m All Structures restricted to height identified on maps
(above existing ground level, measured from the top of the nearest roadside curb)

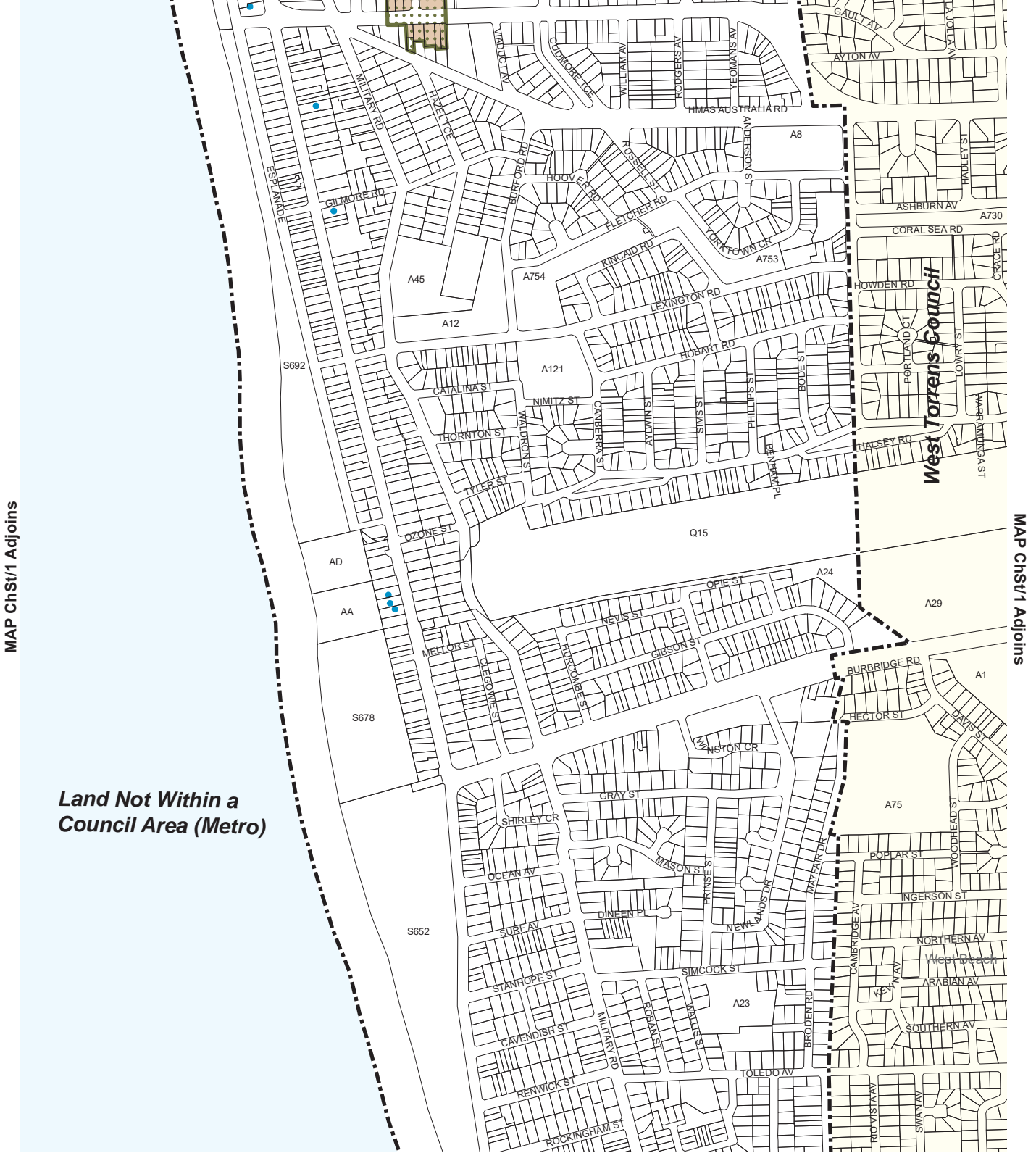
Zone C All Structures Exceeding 15 metres above existing ground level



Overlay Map ChSt/22 DEVELOPMENT CONSTRAINTS

- Airport Building Heights
- Development Plan Boundary

MAP ChSt/17 Adjoins



MAP ChSt/1 Adjoins

MAP ChSt/1 Adjoins

Land Not Within a Council Area (Metro)

MAP ChSt/23 Adjoins

Heritage points are indicative only. For further information on State and Local Heritage Places and Contributory Items please refer to the relevant tables within this document.

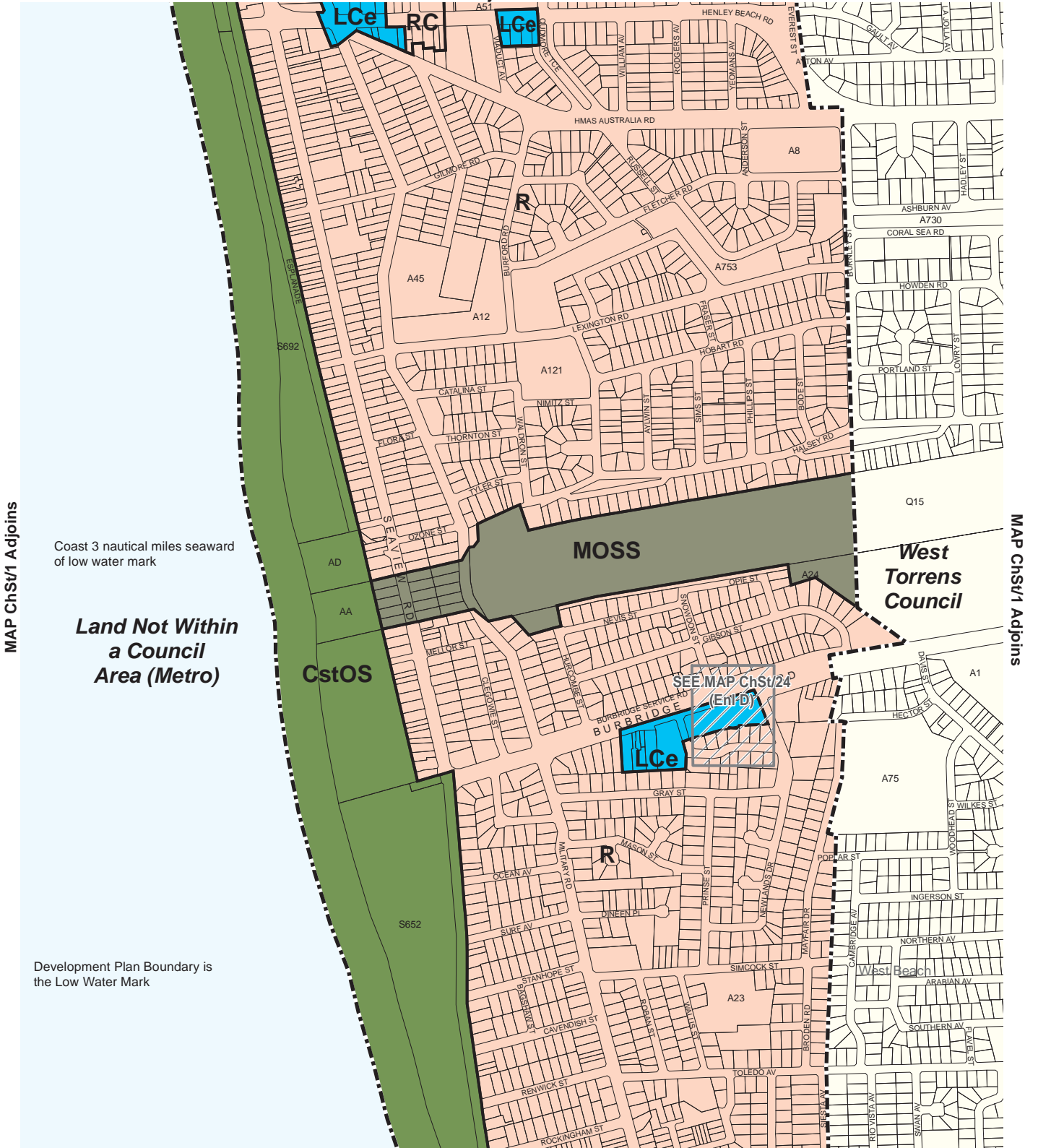


- Local heritage place
- Contributory item
- Historic Conservation Area
- Development Plan Boundary

Overlay Map ChSt/22

HERITAGE

MAP ChSt/17 Adjoins



Coast 3 nautical miles seaward of low water mark

Land Not Within a Council Area (Metro)

CstOS

MOSS

West Torrens Council

SEE MAP ChSt/24 (Enf'd)

LCe

R

West Beach

MAP ChSt/23 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

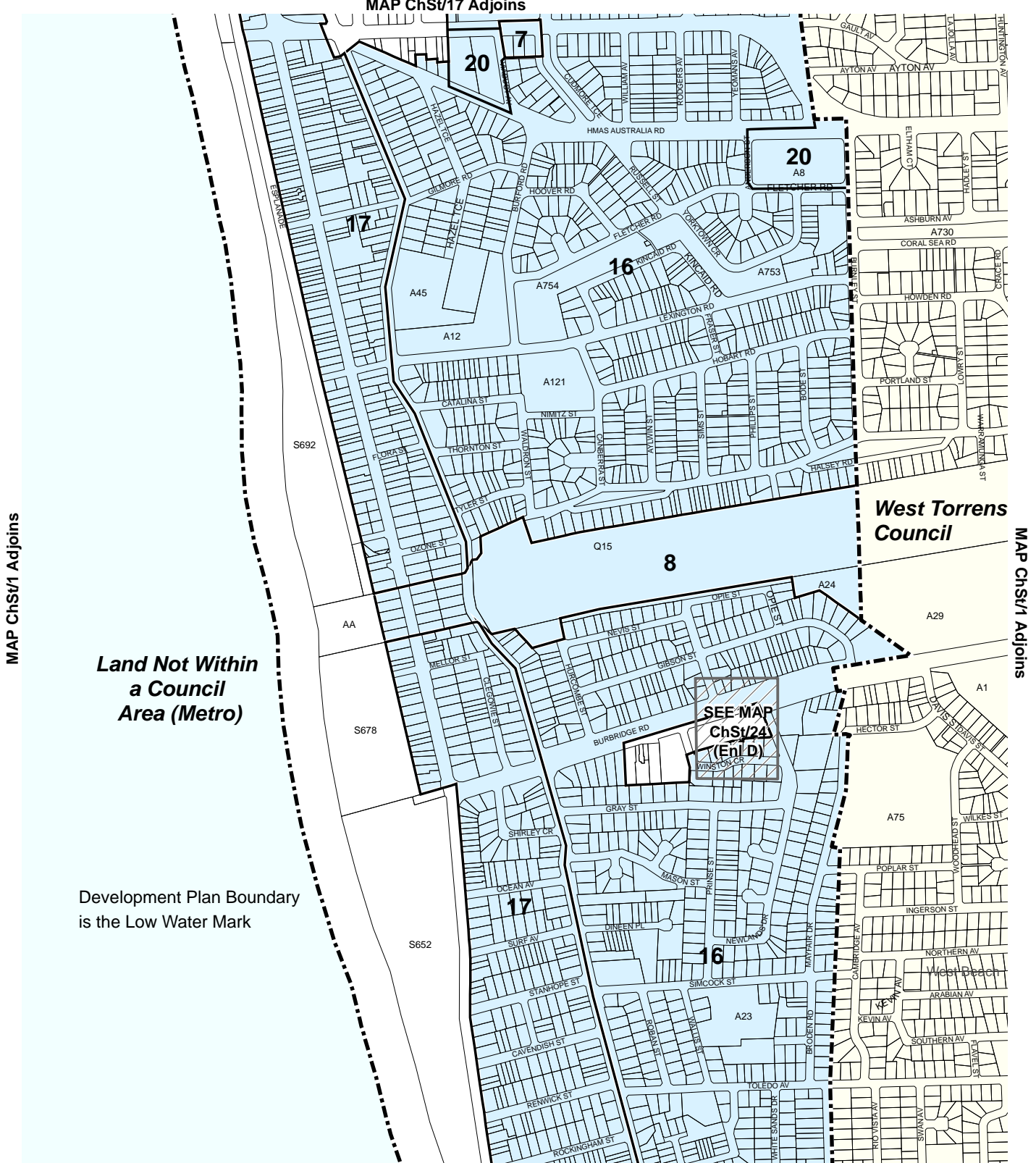


Zones

- CstOS Coastal Open Space
- LCe Local Centre
- MOSS Metropolitan Open Space System
- R Residential
- RC Residential Character
- Zone Boundary
- Development Plan Boundary

Zone Map ChSt/22

MAP ChSt/17 Adjoins



MAP ChSt/1 Adjoins

MAP ChSt/1 Adjoins

Land Not Within a Council Area (Metro)

Development Plan Boundary is the Low Water Mark

MAP ChSt/23 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

Policy Area

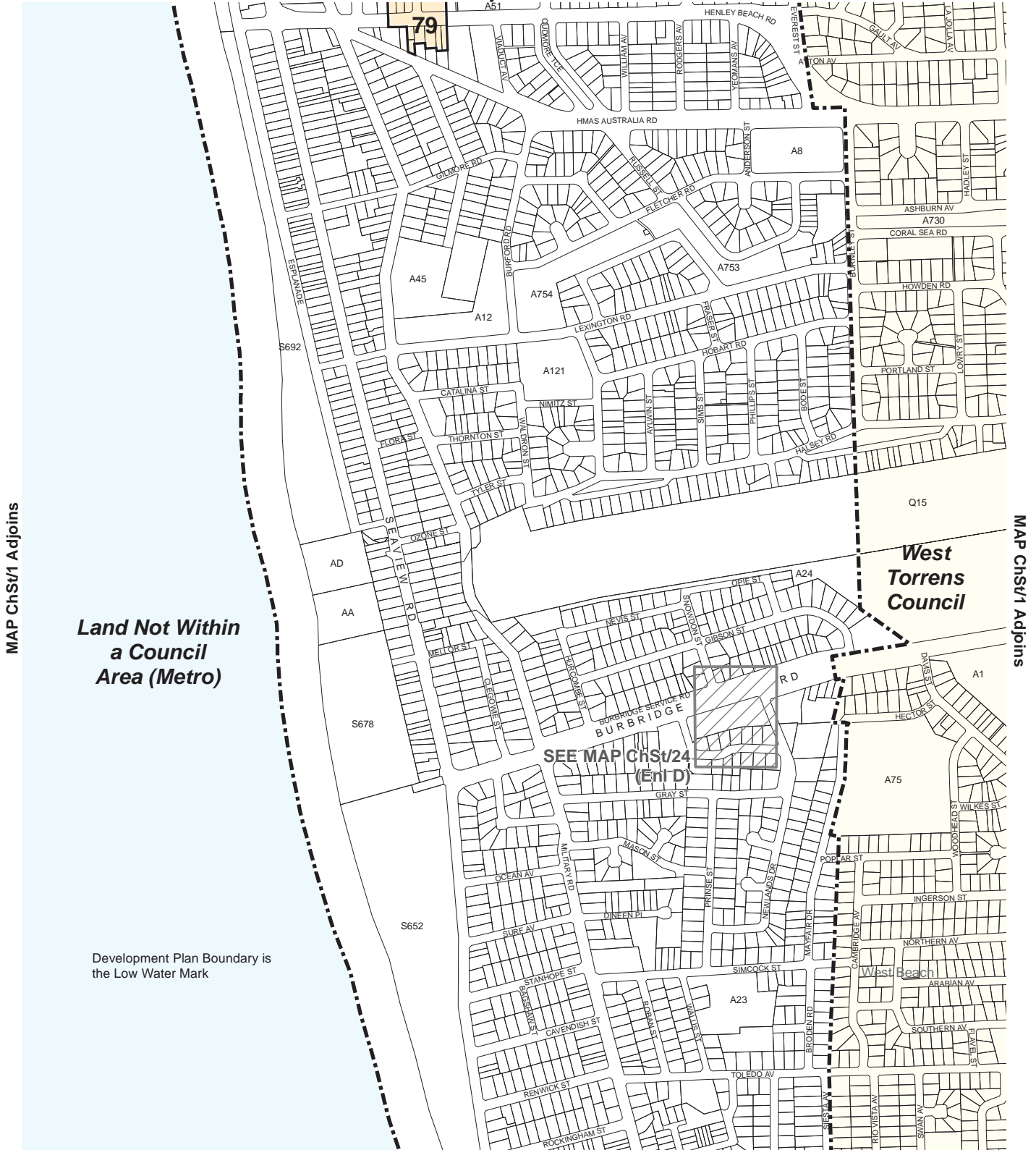
- 7 Local Shopping
- 8 Linear Park(River Torrens/Karrawirra Parri)
- 16 Mid Suburban
- 17 Western Edge
- 20 Integrated Medium Density



Policy Area Map ChSt/22

- Policy Area Boundary
- Development Plan Boundary

MAP ChSt/17 Adjoins



*Land Not Within
a Council
Area (Metro)*

Development Plan Boundary is
the Low Water Mark



MAP ChSt/23 Adjoins

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

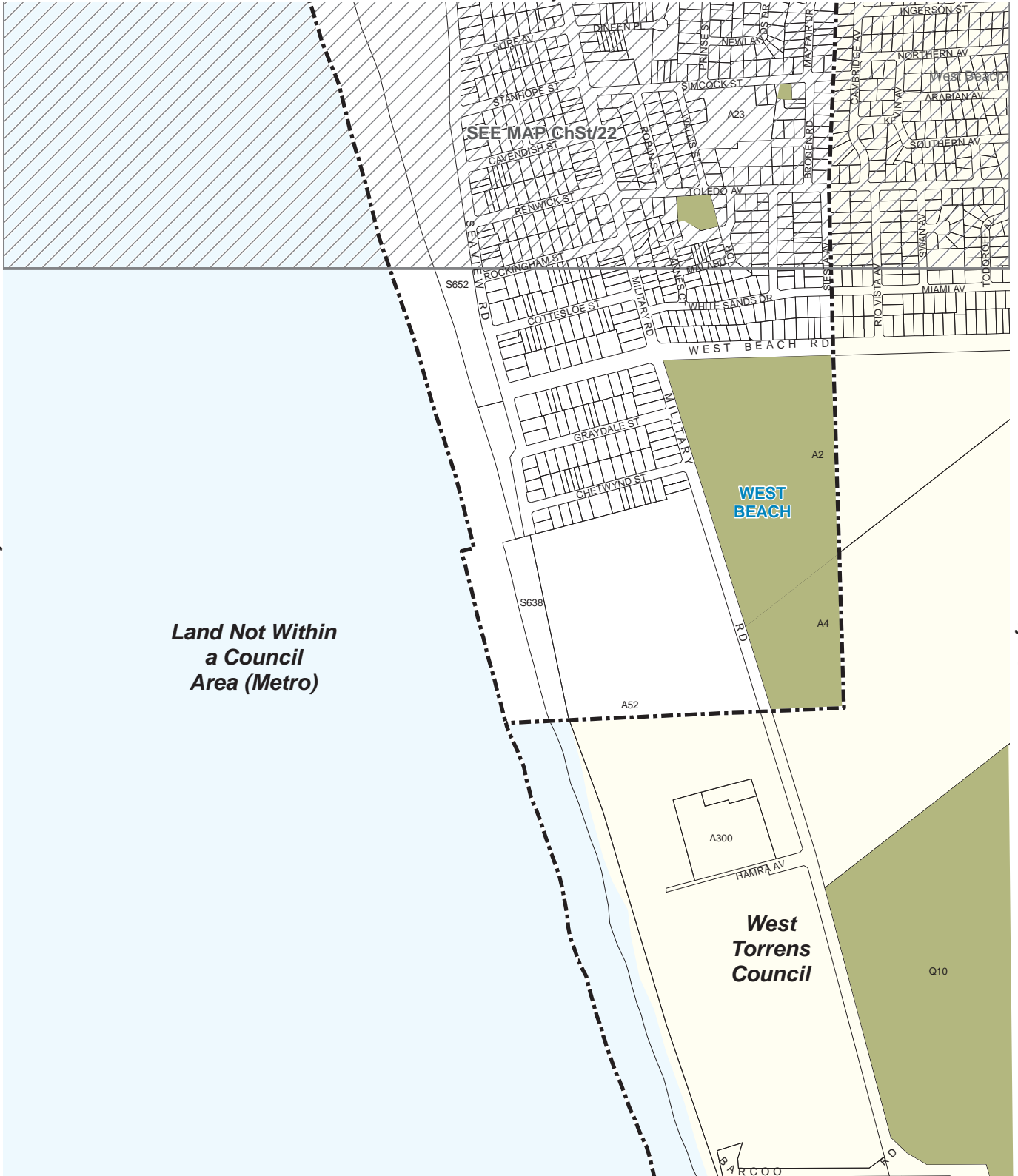
Precinct
79 Henley Beach



Precinct Map ChSt/22

-  Precinct Boundary
-  Development Plan Boundary

MAP ChSt/22 Adjoins



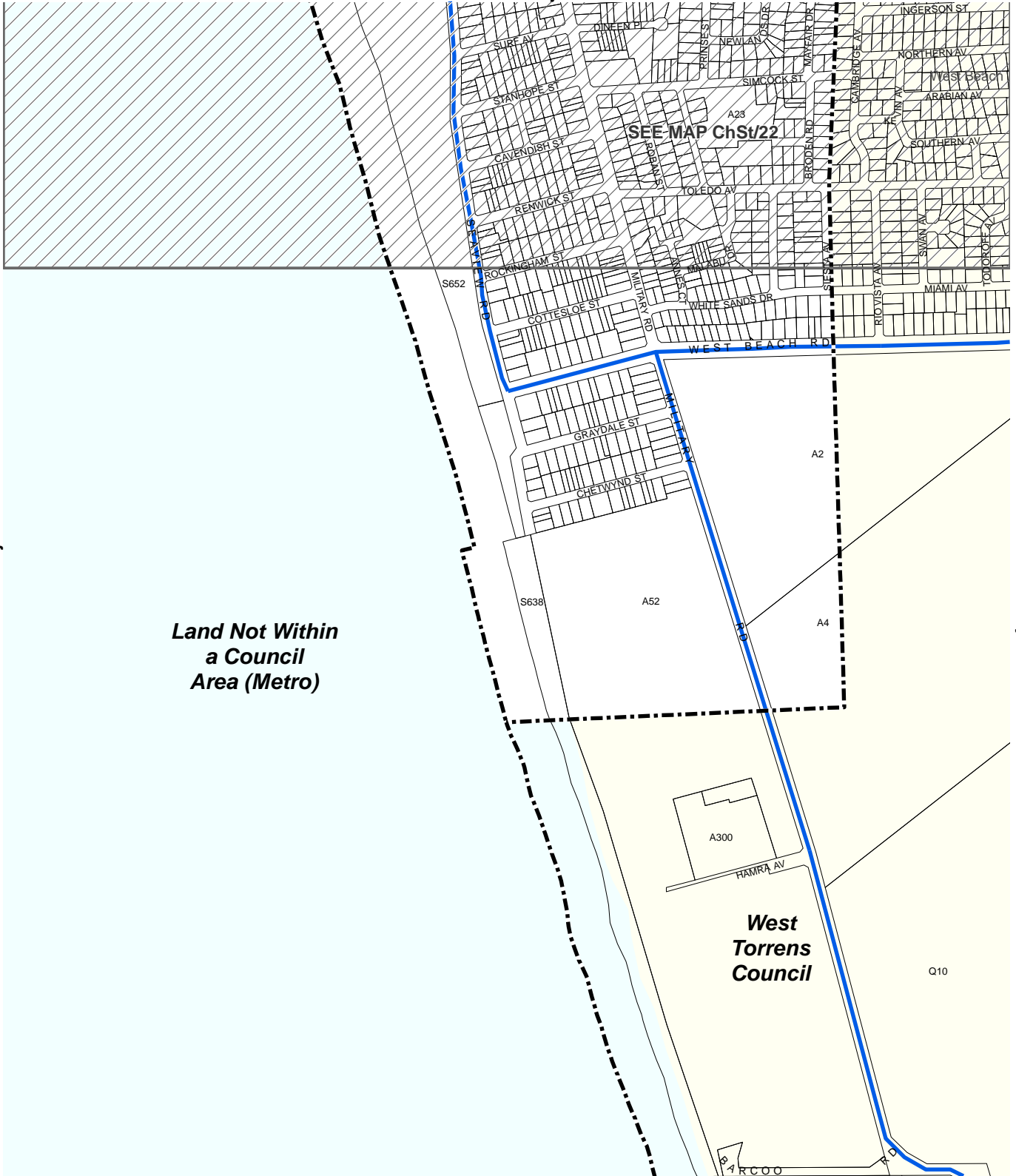
MAP ChSt/1 Adjoins



Location Map ChSt/23

- Local Reserves
- Development Plan Boundary

MAP ChSt/22 Adjoins



MAP ChSt/1 Adjoins

Land Not Within
a Council
Area (Metro)



MAP ChSt/1 Adjoins

MAP ChSt/1 Adjoins

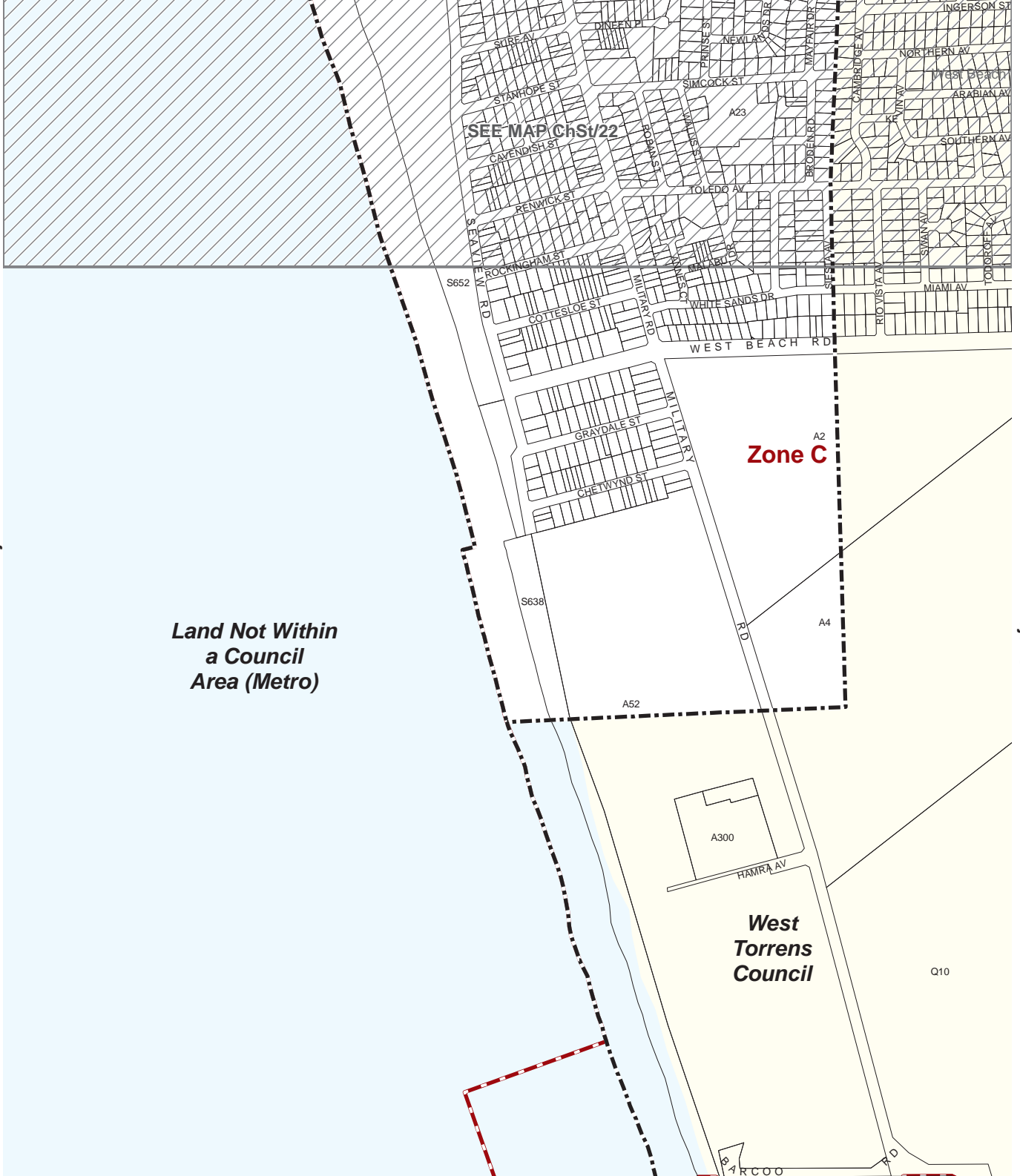


0 500m

Overlay Map ChSt/23 TRANSPORT

-  Distributor Roads
-  Development Plan Boundary

MAP ChSt/22 Adjoins



Land Not Within a Council Area (Metro)

Zone C

West Torrens Council



MAP ChSt/1 Adjoins

Airport Building Heights

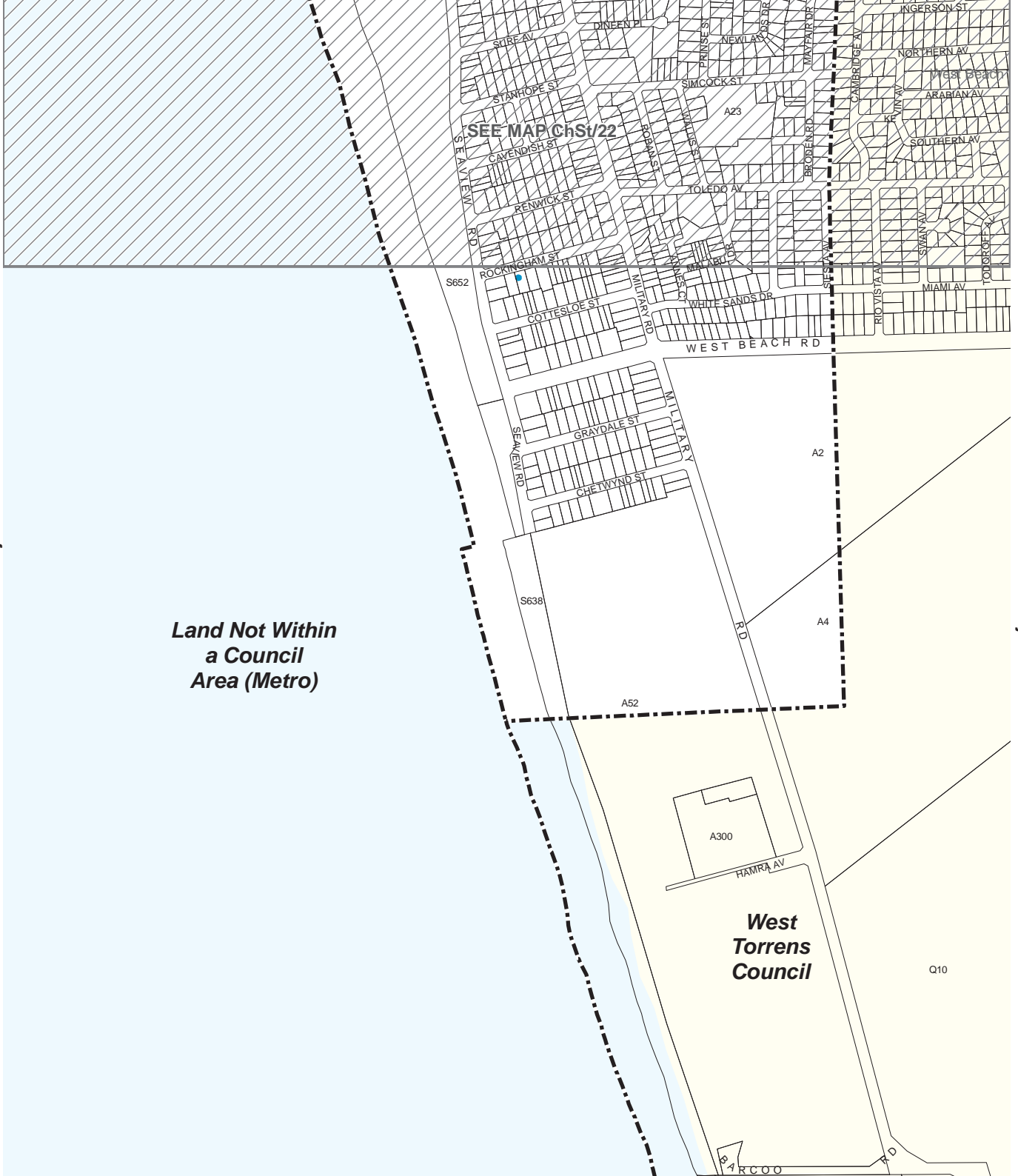
Zone C All Structures Exceeding 15 metres above existing ground level



Overlay Map ChSt/23 DEVELOPMENT CONSTRAINTS

-  Airport Building Heights
-  Development Plan Boundary

MAP ChSt/22 Adjoins



**Land Not Within
a Council
Area (Metro)**

**West
Torrens
Council**

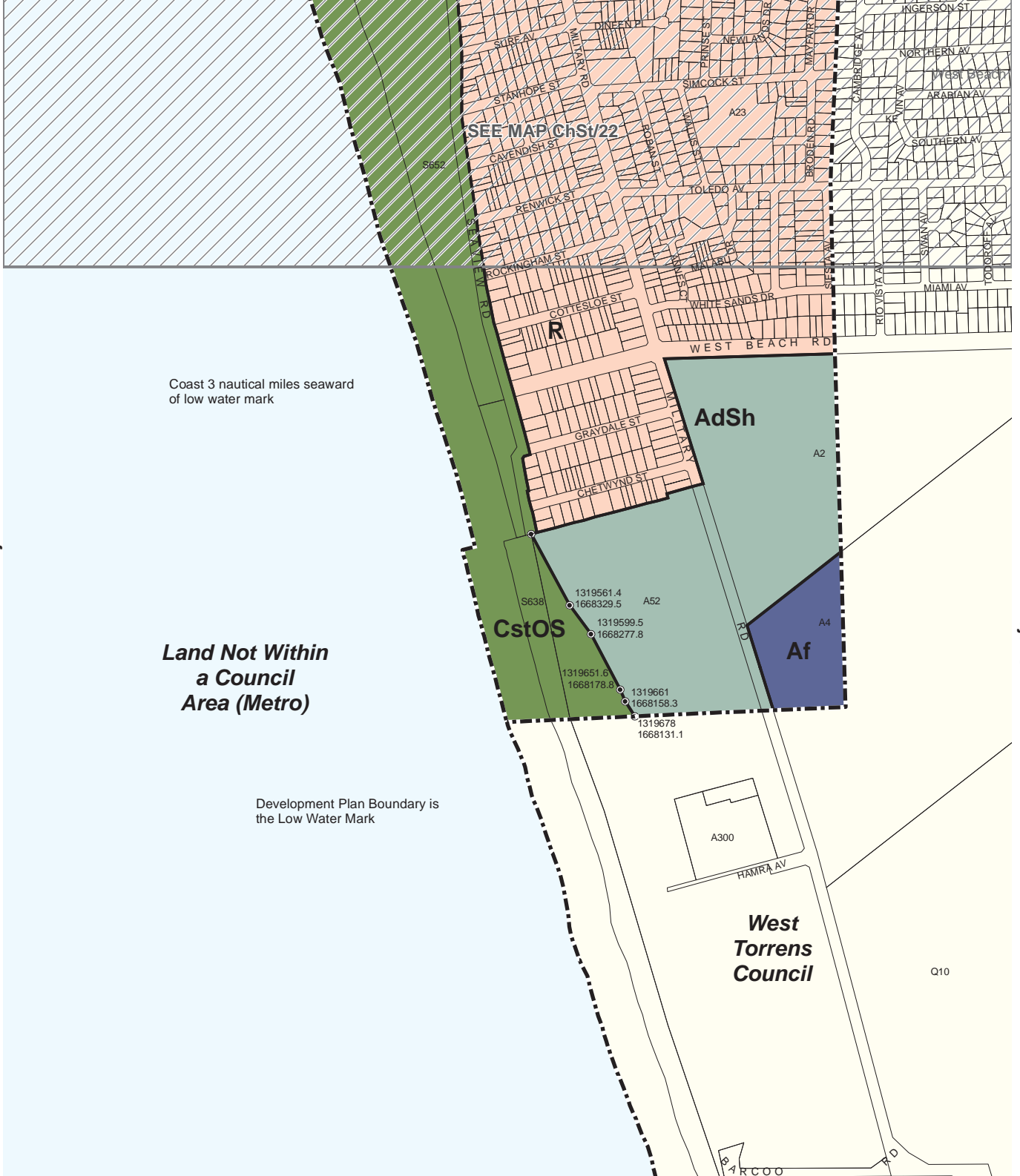
Heritage points are indicative only.
For further information on State and Local Heritage Places and Contributory
Items please refer to the relevant tables within this document.



Overlay Map ChSt/23 HERITAGE

- Local heritage place
- - - - - Development Plan Boundary

MAP ChSt/22 Adjoins



See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

MAP ChSt/1 Adjoins



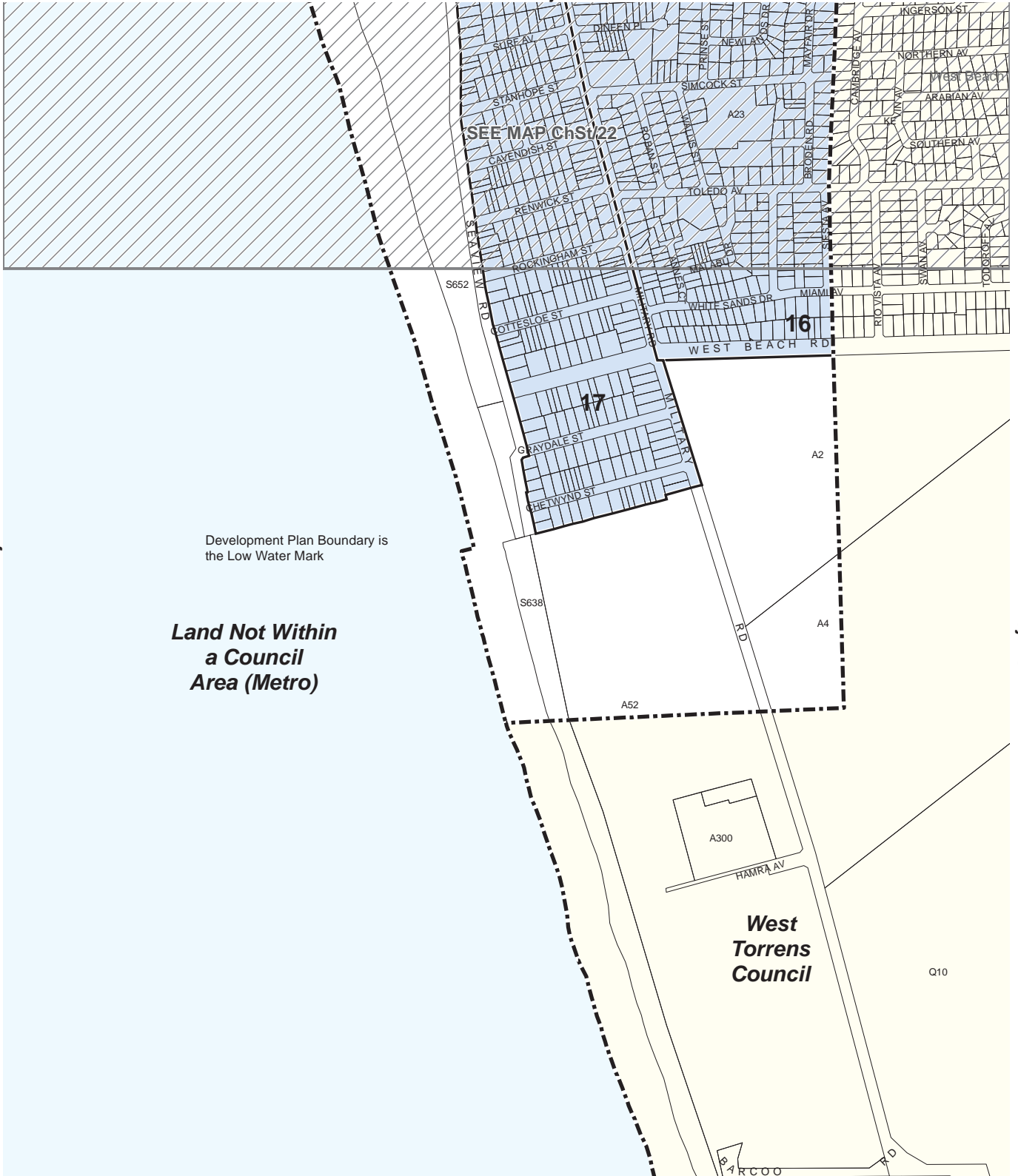
Zones

- AdSh Adelaide Shores
- Af Airfield
- CstOS Coastal Open Space
- R Residential

- Zone Boundary
- Development Plan Boundary

Zone Map ChSt/23

MAP ChSt/22 Adjoins



Development Plan Boundary is the Low Water Mark

Land Not Within a Council Area (Metro)

West Torrens Council

See enlargement map for accurate representation.
Lamberts Conformal Conic Projection, GDA94

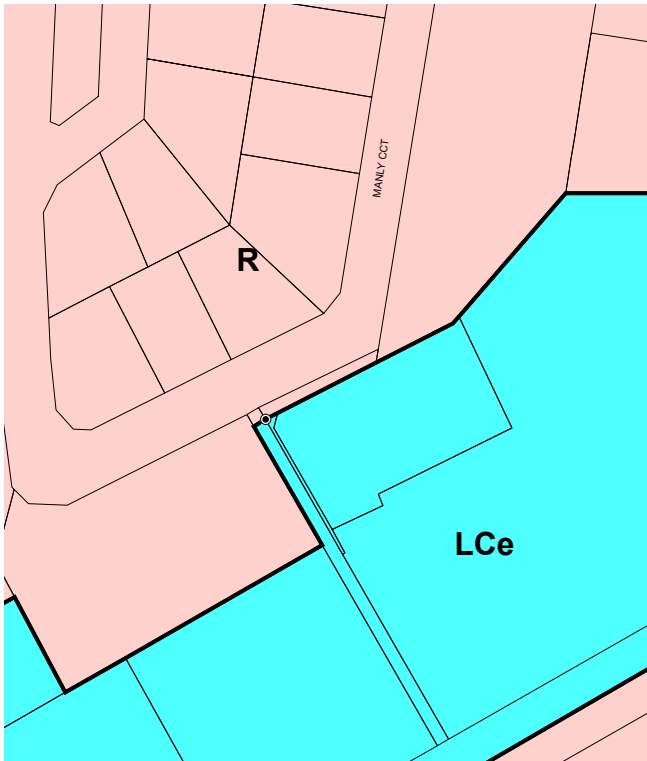
- Policy Area**
 16 Mid Suburban
 17 Western Edge



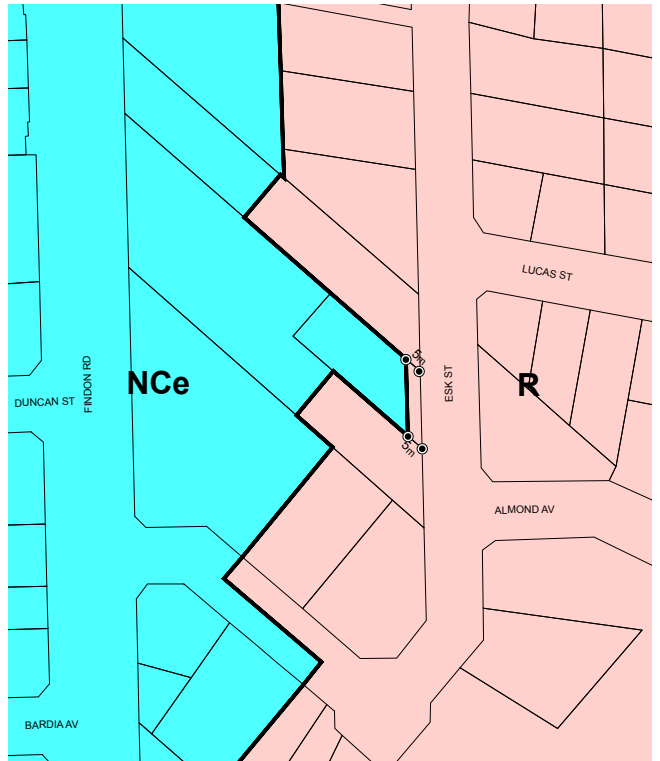
MAP ChSt/1 Adjoins

Policy Area Map ChSt/23

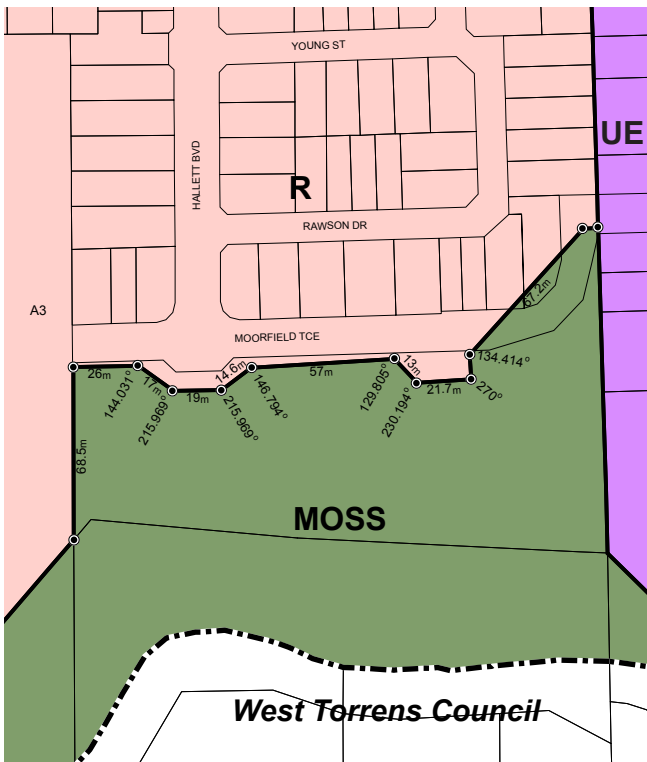
- Policy Area Boundary
- Development Plan Boundary



Enlargement A 0 60m

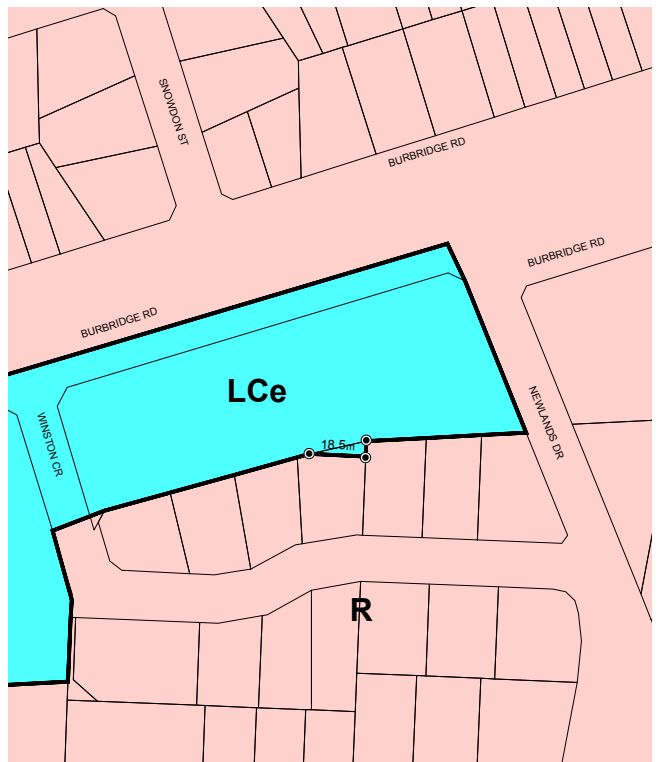


Enlargement B 0 60m



Enlargement C 0 75m

Lamberts Conformal Conic Projection, GDA94

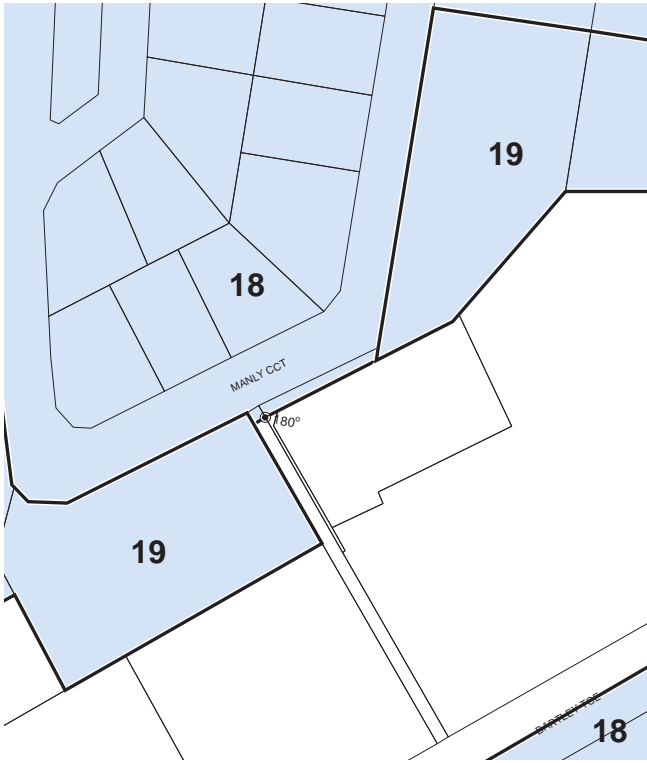


Enlargement D 0 75m



Zone Map ChSt/24

- Zone Boundary
- Development Plan Boundary



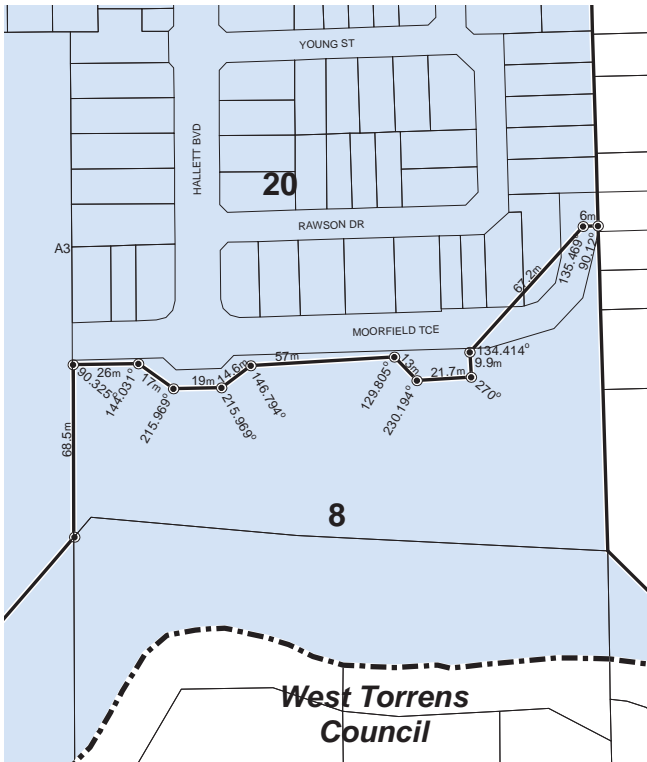
Enlargement A

0 60m



Enlargement B

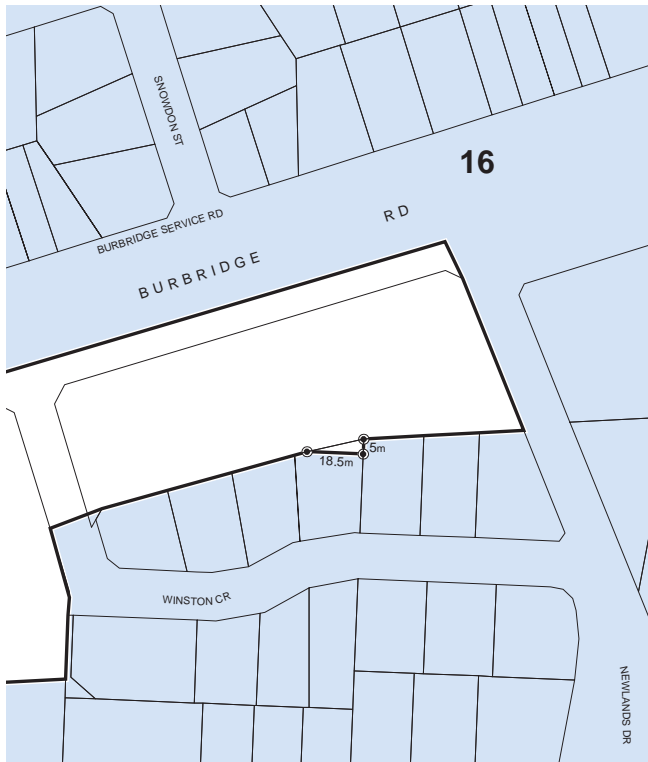
0 60m



Enlargement C

0 75 m

Lamberts Conformal Conic Projection, GDA94



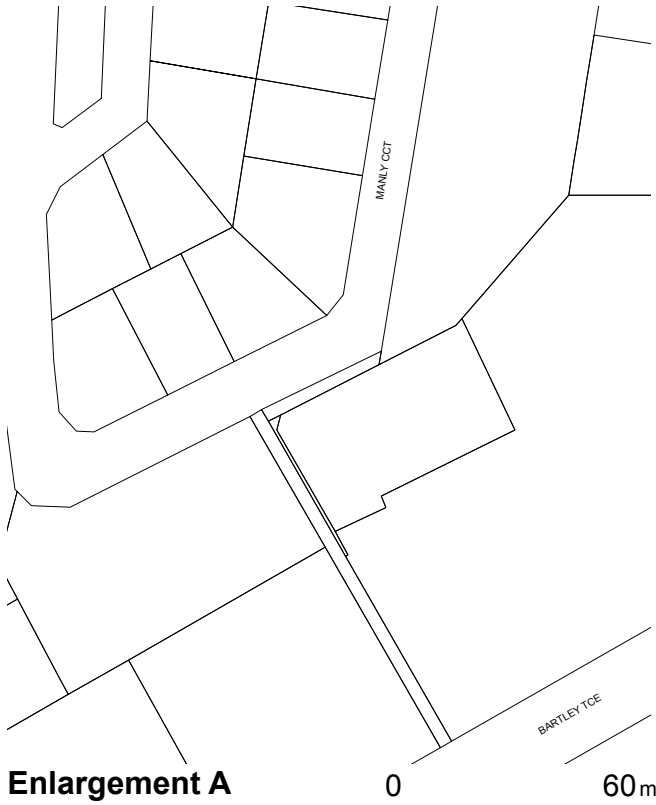
Enlargement D

0 75 m



Policy Area Map ChSt/24

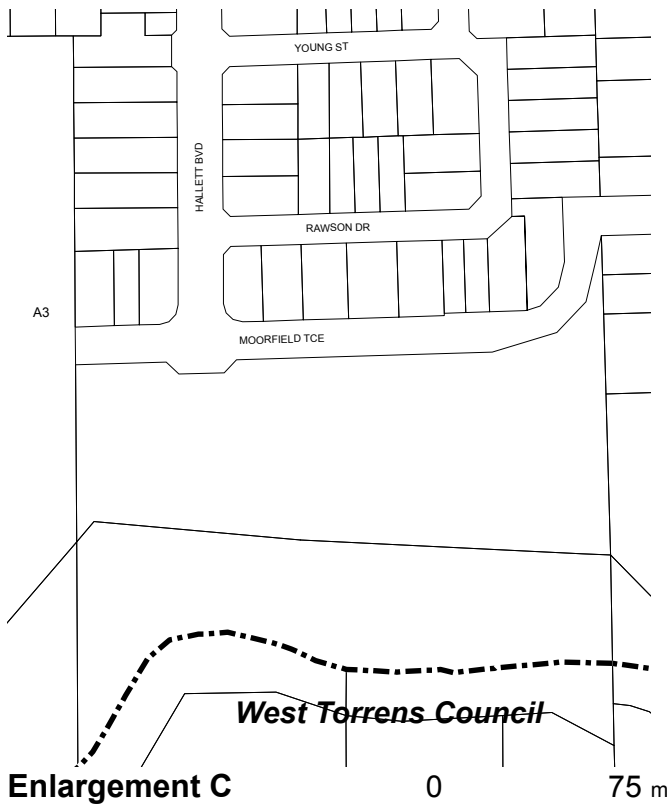
- Policy Area Boundary
- Development Plan Boundary



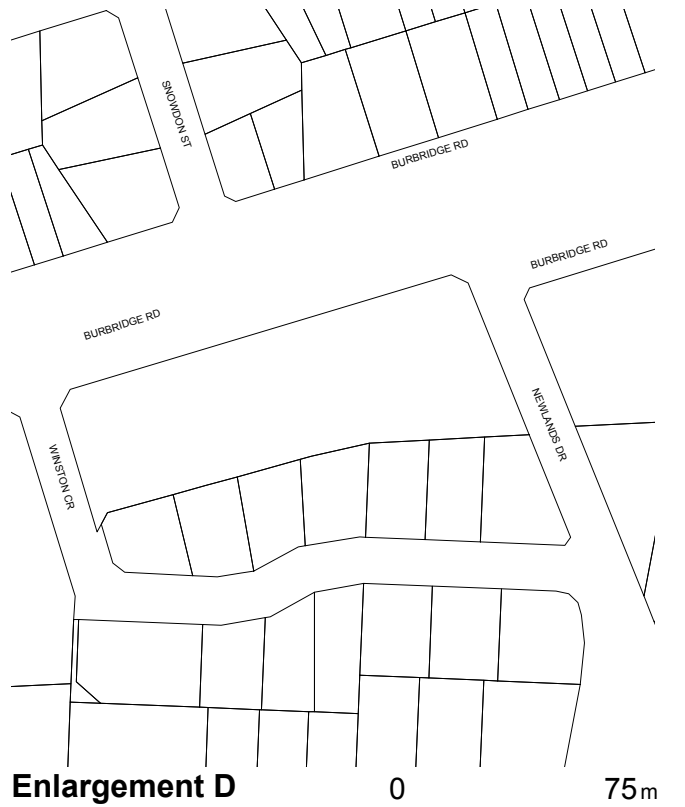
Enlargement A



Enlargement B



Enlargement C



Enlargement D

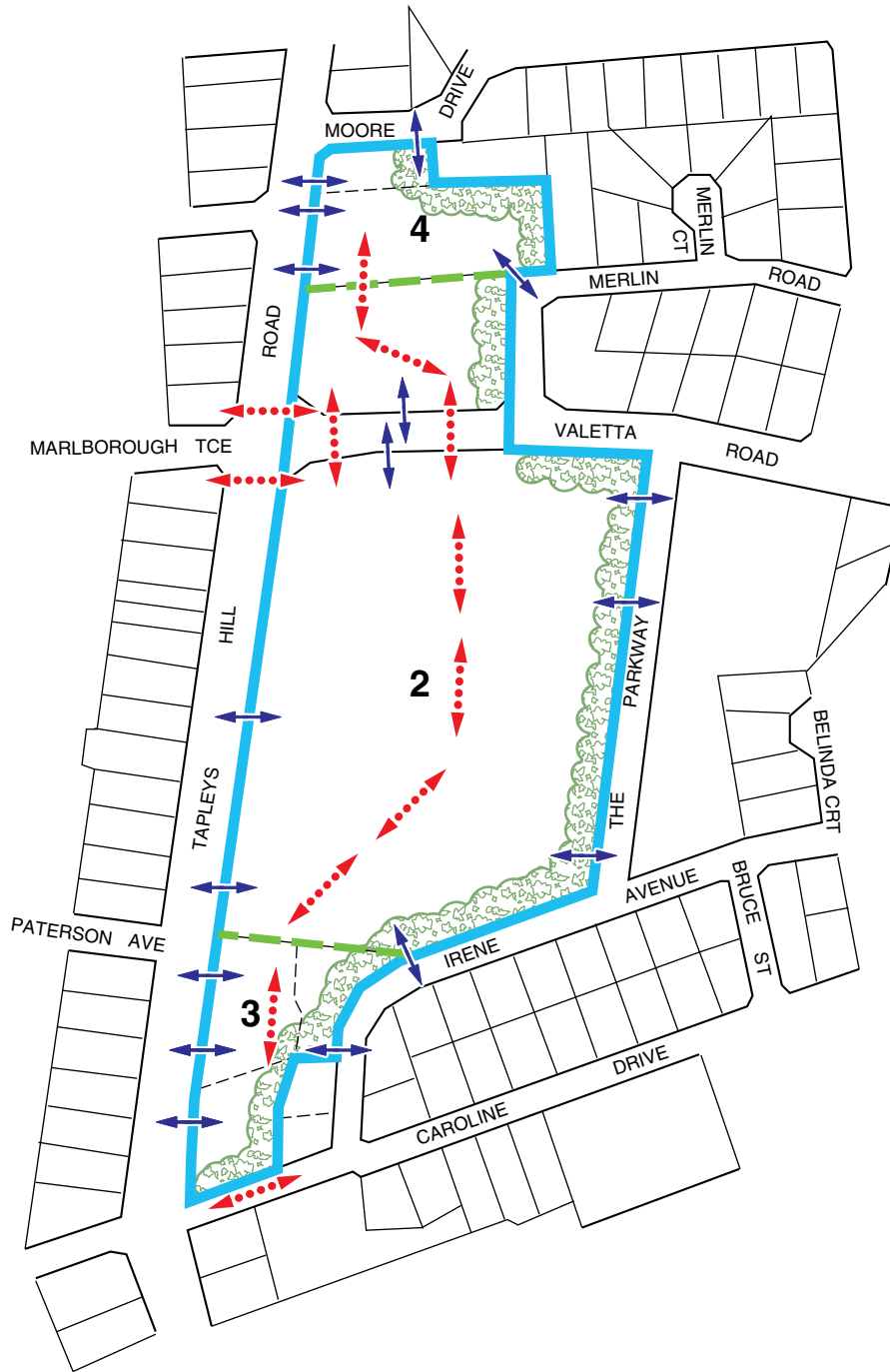
Lamberts Conformal Conic Projection, GDA94



Precinct Map ChSt/24

- Precinct Boundary
- Development Plan Boundary

Concept Plan Maps



- 2** Precinct 2 Retail Core Fulham Gardens
- 3** Precinct 3 Commercial Fringe Fulham Gardens
- 4** Precinct 4 Recreation Fulham Gardens



Intensive Landscape Buffer



Vehicle Access Points



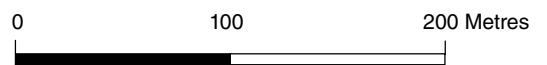
Pedestrian Links



Policy Area Boundary



Precinct Boundary



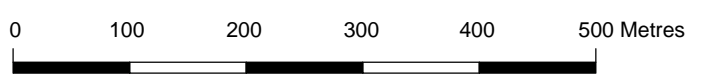
FULHAM GARDENS

Concept Plan Map ChSt/1

FULHAM GARDENS POLICY AREA 1



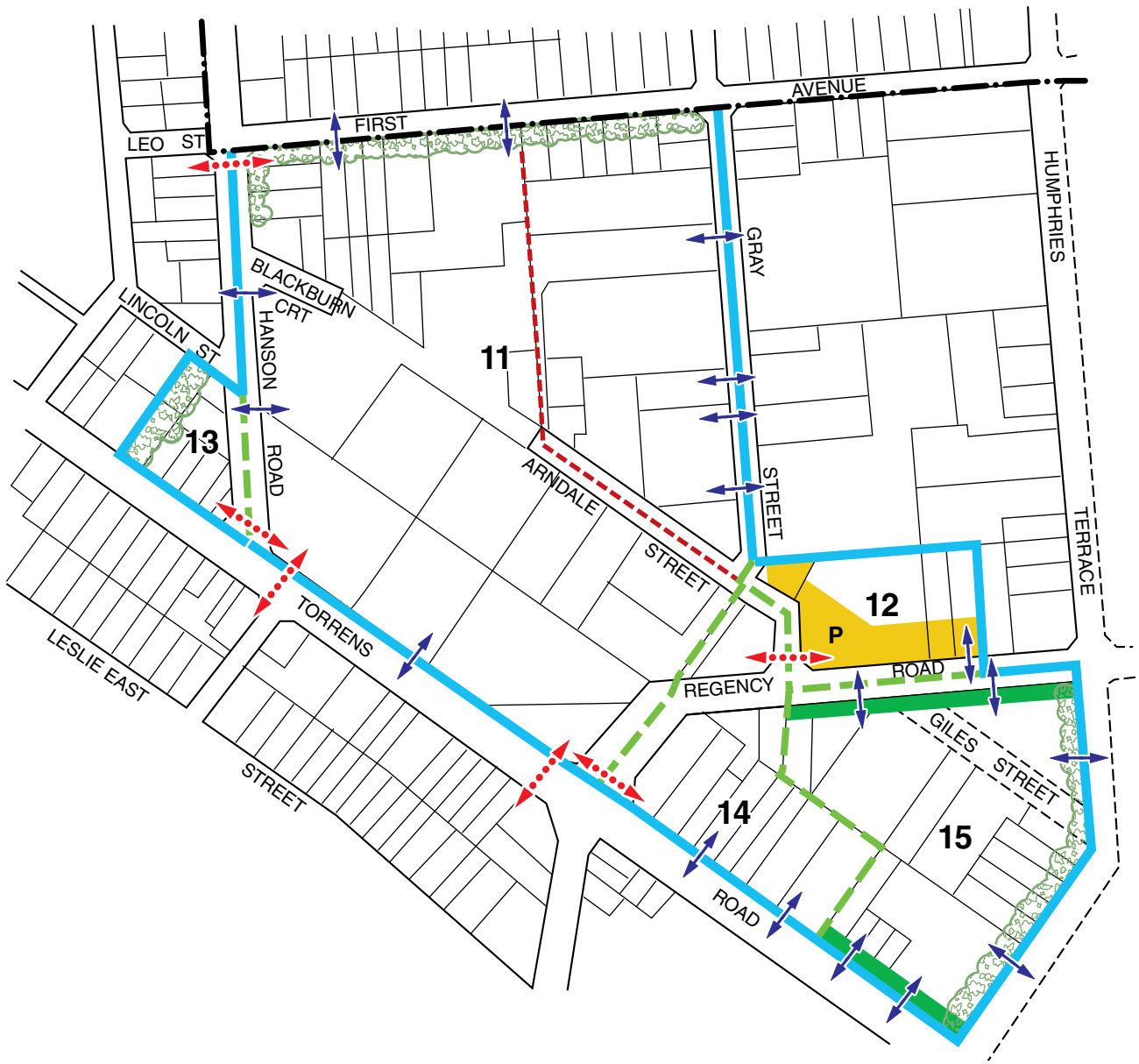
- 5 Precinct 5 Historic Hub
- 6 Precinct 6 Port Road Office
- 7 Precinct 7 Manton Street
- 8 Precinct 8 Gateway
- 9 Precinct 9 Recreation Hindmarsh
- 10 Precinct 10 Port Road Gardens
- Major Townscape Focus
- State Heritage Place
- ▲ Local Heritage Place
- ▨ Places contributing to the Historic Character of the area
- Policy Area Boundary
- - - Precinct Boundary
- - - Development Plan Boundary



HINDMARSH

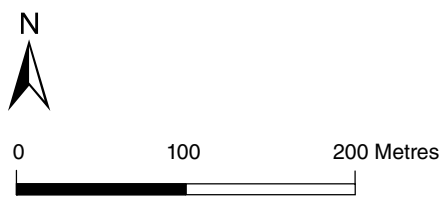
Concept Plan Map ChSt/2

HINDMARSH POLICY AREA 2



- 11 Precinct 11 Retail Core Kilkenny
- 12 Precinct 12 Regency Road
- 13 Precinct 13 Hanson Road
- 14 Precinct 14 Commercial Fringe Kilkenny
- 15 Precinct 15 Residential

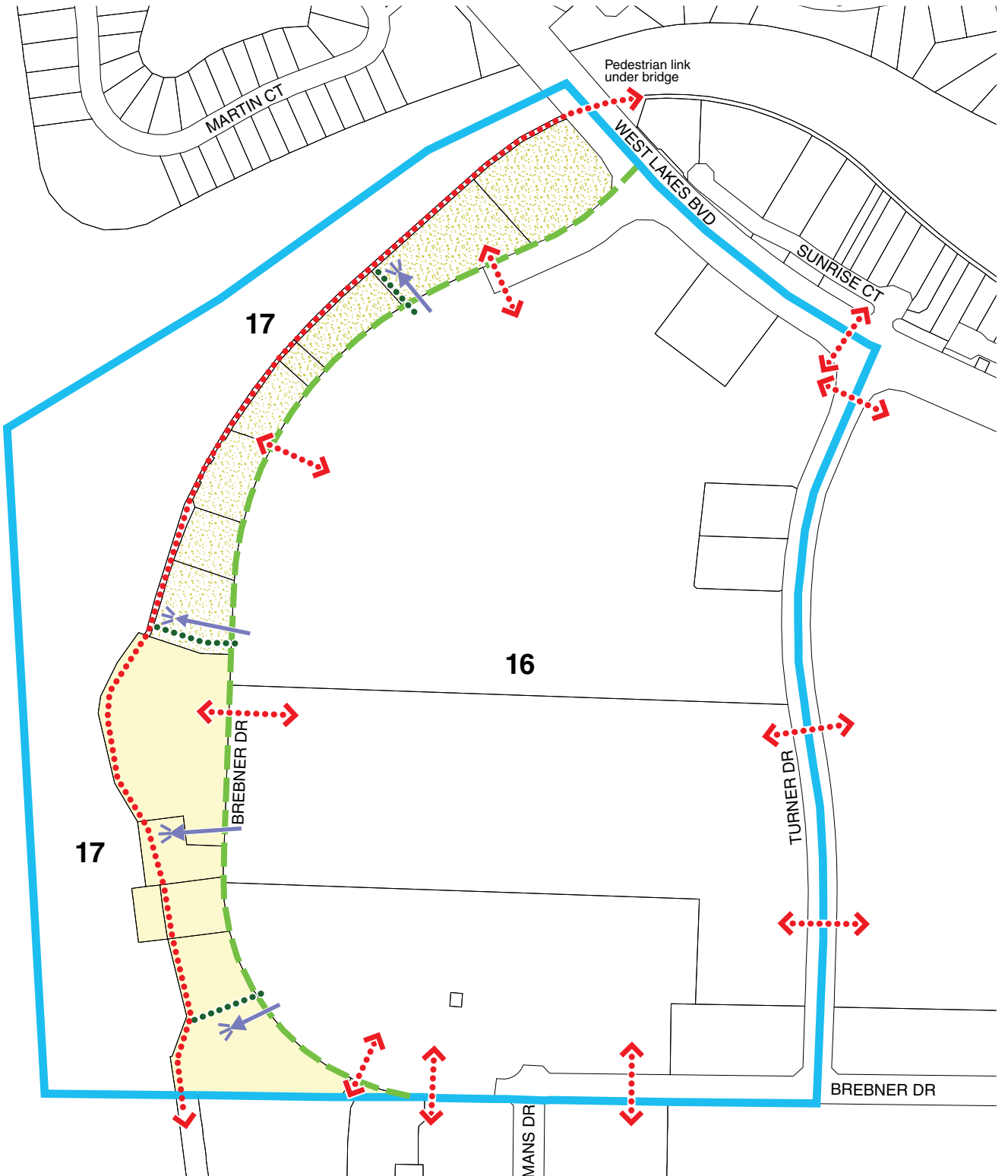
- Intensive Landscape Buffer
- Vehicle Access Points
- Pedestrian Links
- Public Transport Interchange / Access
- Shared Parking Area
- Development Incorporating Noise Attenuation Measures
- Policy Area Boundary
- Precinct Boundary
- Development Plan Boundary



KILKENNY

Concept Plan Map ChSt/3

KILKENNY POLICY AREA 3




16 Precinct 16 Retail Core West Lakes

17 Precinct 17 Lakefront

 Maximum 6 Storeys (21metre) Height Limit


 Maximum 4 Storeys (15metre) Height Limit

 Lakefront Public Pathway and Pedestrian Links to Brebner Drive

 Pedestrian Links to District Centre

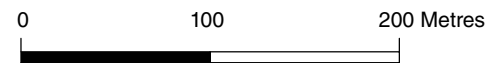
 Sightlines to Lakefront from Brebner Drive

 Policy Area Boundary

 Precinct Boundary



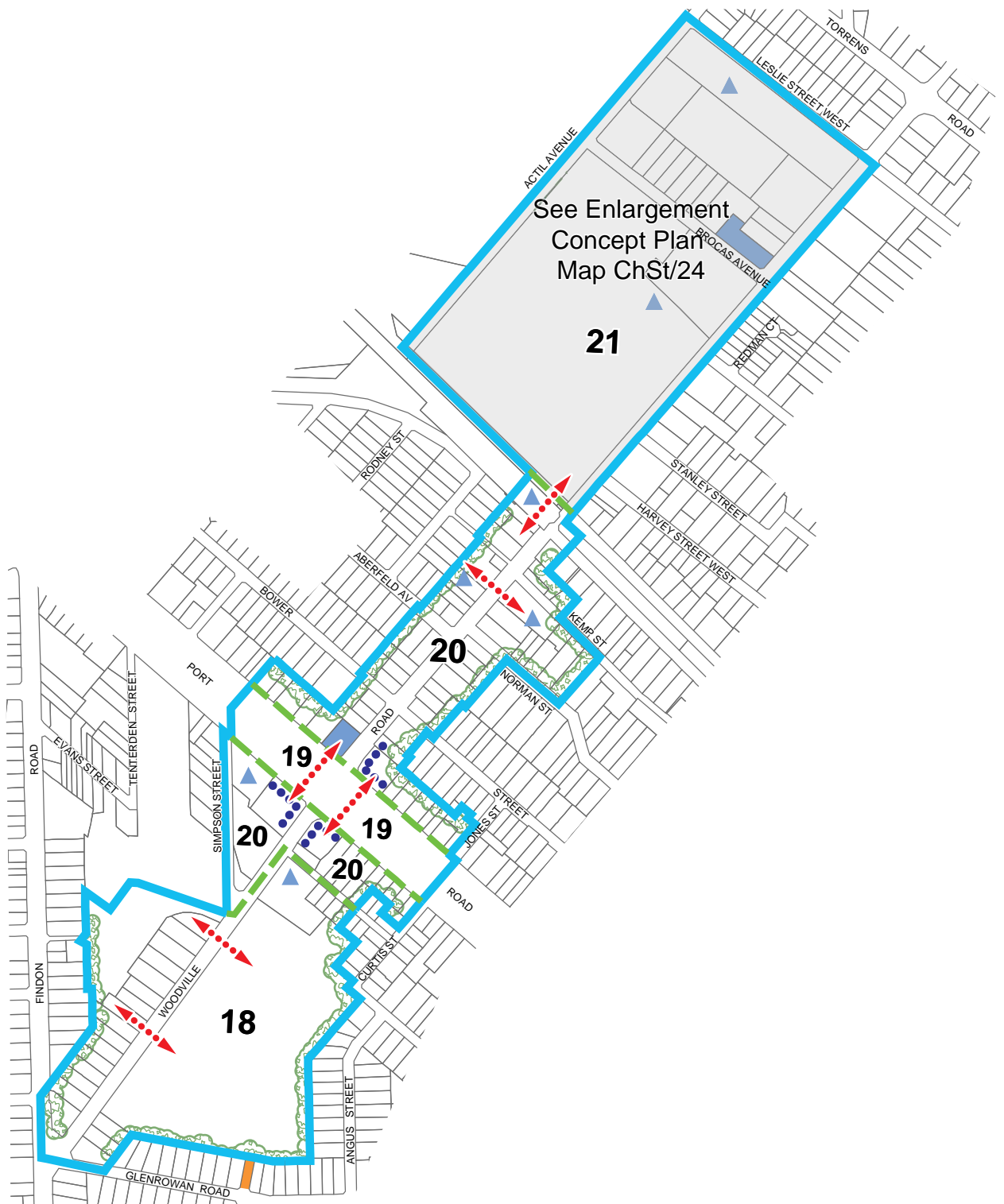
Scale 1:4000



WEST LAKES

Concept Plan Map ChSt/4

WEST LAKES POLICY AREA 4



- 18** Precinct 18 Woodville Road Medical
- 19** Precinct 19 Woodville Road Gardens
- 20** Precinct 20 Civic
- 21** Precinct 21 Railway Station



Intensive Landscape Buffer



Pedestrian Links



Possible Future Traffic Restriction Measures



State Heritage Place



Local Heritage Place



Major Townscape Focus



Policy Area Boundary



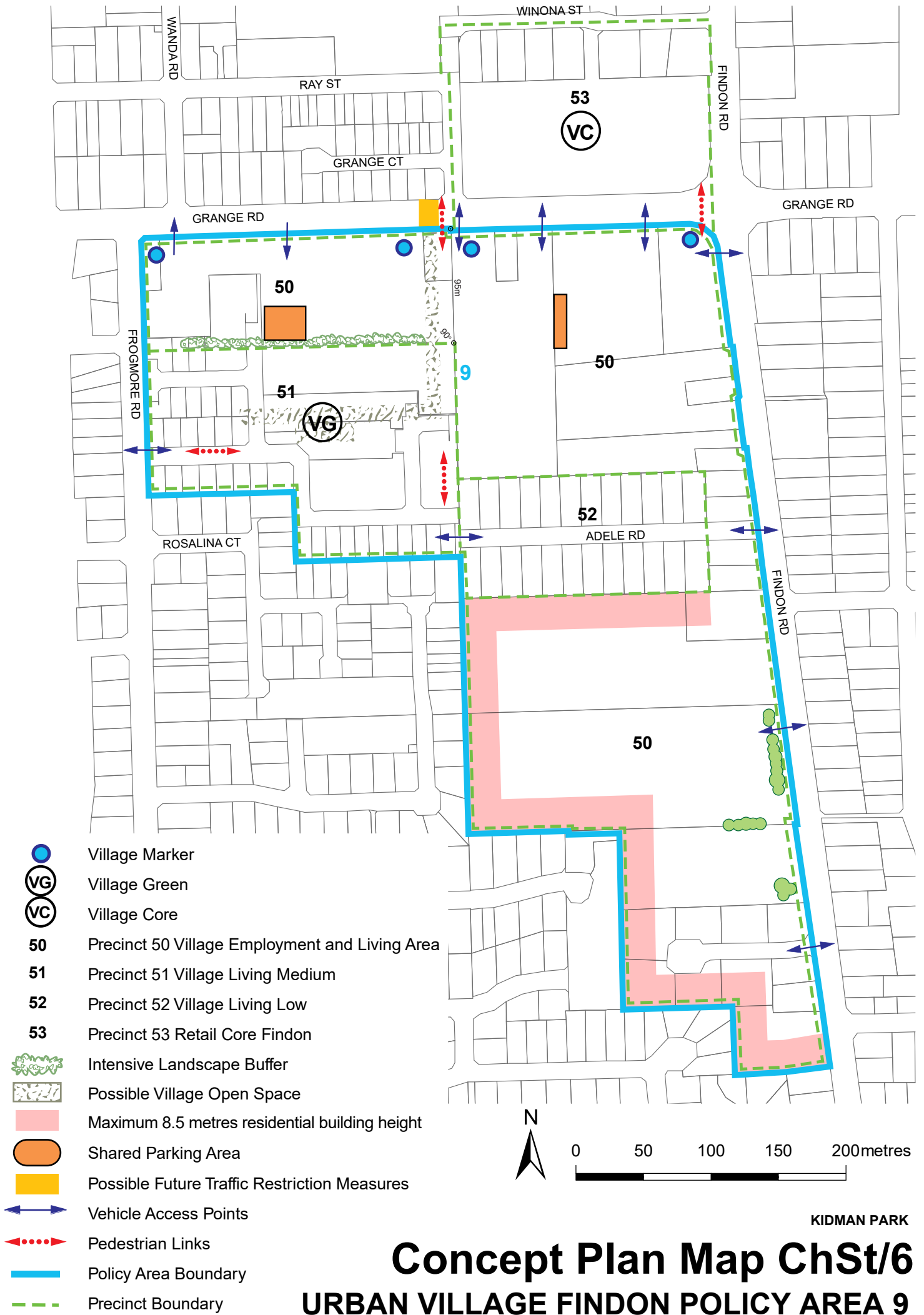
Precinct Boundary



WOODVILLE

Concept Plan Map ChSt/5

WOODVILLE POLICY AREA 5

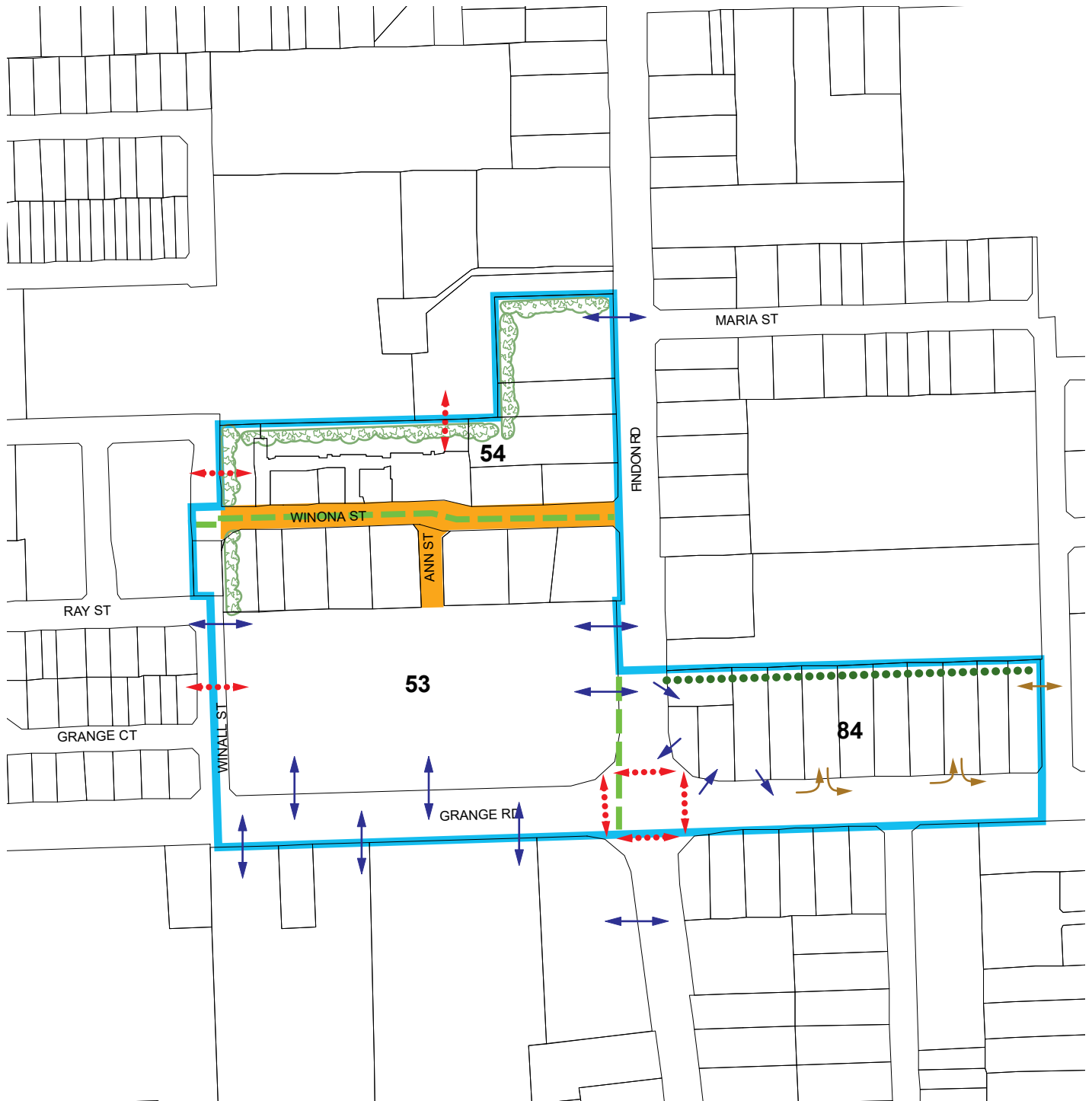


-  Village Marker
-  Village Green
-  Village Core
- 50** Precinct 50 Village Employment and Living Area
- 51** Precinct 51 Village Living Medium
- 52** Precinct 52 Village Living Low
- 53** Precinct 53 Retail Core Findon
-  Intensive Landscape Buffer
-  Possible Village Open Space
-  Maximum 8.5 metres residential building height
-  Shared Parking Area
-  Possible Future Traffic Restriction Measures
-  Vehicle Access Points
-  Pedestrian Links
-  Policy Area Boundary
-  Precinct Boundary

KIDMAN PARK

Concept Plan Map ChSt/6

URBAN VILLAGE FINDON POLICY AREA 9



- 53** Precinct 53 Retail Core Findon
- 54** Precinct 54 Winona Street
- 84** Precinct 84 Findon Centre East



Intensive Landscape Buffer



Existing Vehicle Access Points



Desired Vehicle Access Points



Interface Treatment



Pedestrian Links



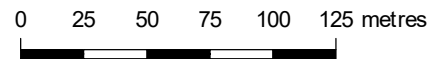
Possible Future Traffic Restriction Measures



Policy Area Boundary



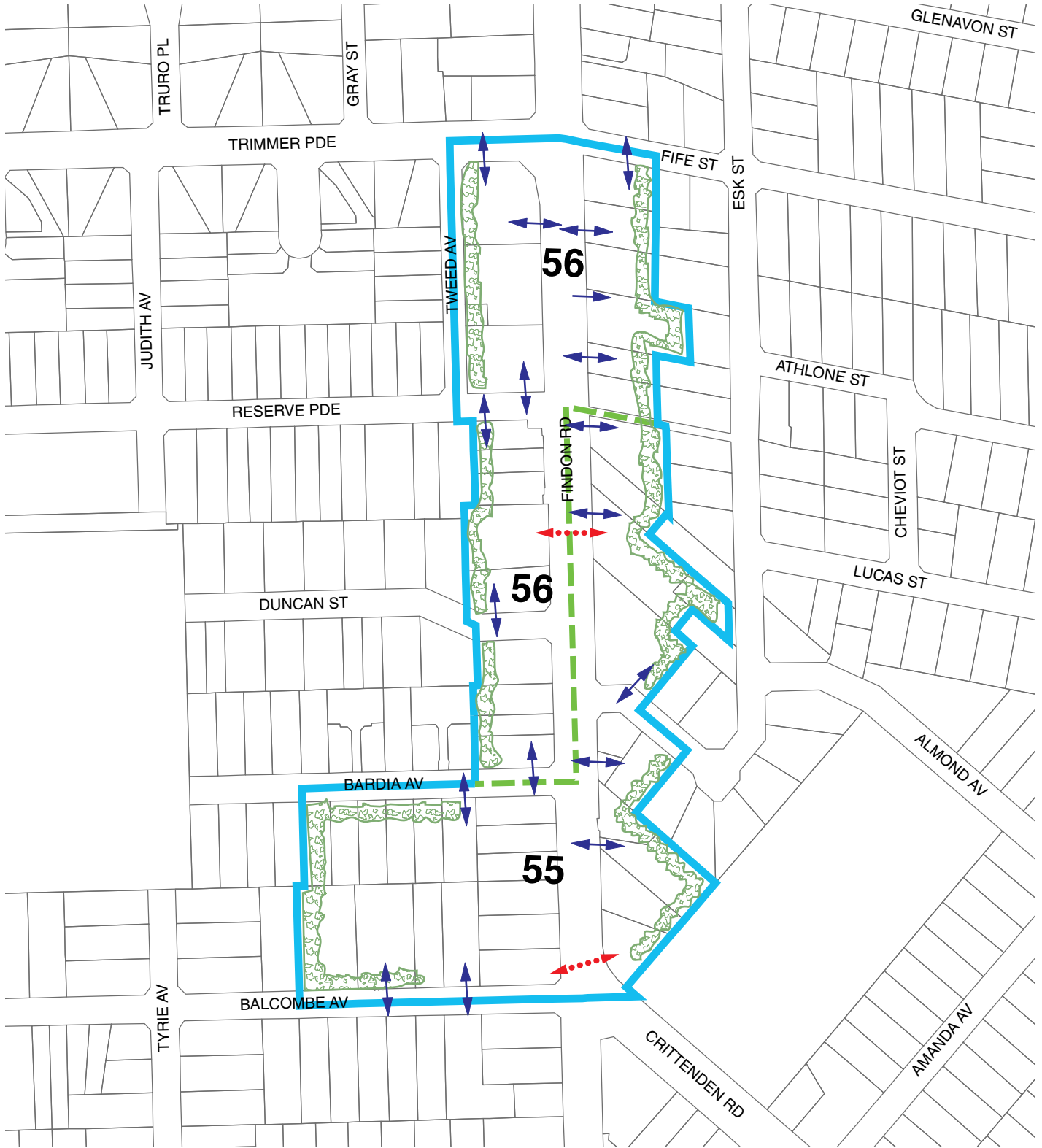
Precinct Boundary



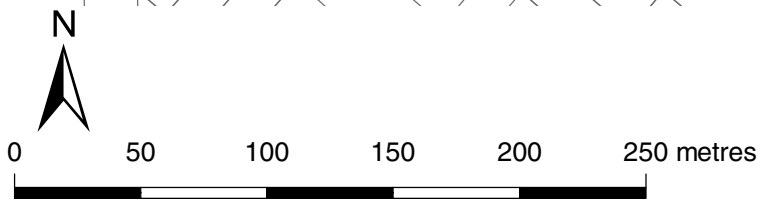
FINDON

Concept Plan Map ChSt/7

FINDON POLICY AREA 10



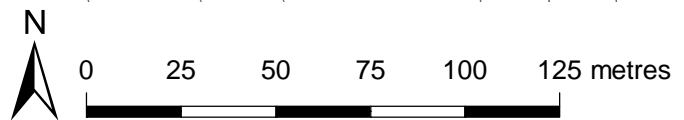
- 55** Precinct 55 - Retail Core Findon Road
- 56** Precinct 56 - Commercial Fringe Findon Road
- Intensive Landscape Buffer
- Vehicle Access Points
- Pedestrian Links
- Policy Area & Precinct Boundary
- Precinct Boundary








FINDON

Concept Plan Map ChSt/8

FINDON ROAD POLICY AREA 11










- 57 Precinct 57 Henley Square
- 58 Precinct 58 Seaview Road
-  State Heritage Place
-  Local Heritage Place
-  Metropolitan Open Space System
-  Policy Area Boundary
-  Precinct Boundary

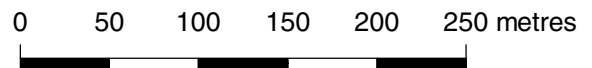
HENLEY SQUARE

Concept Plan Map ChSt/9

HENLEY SQUARE POLICY AREA 12



- 59** Precinct 59 Retail Core Royal Park
- 60** Precinct 60 Commercial Fringe Royal Park
-  Intensive Landscape Buffer
-  Possible Future Traffic Restriction Measures
-  Vehicle Access Points
-  Pedestrian Links
-  Shared Parking Area
-  Policy Area Boundary
-  Precinct Boundary



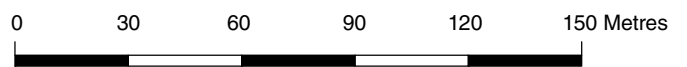
ROYAL PARK

Concept Plan Map ChSt/10

ROYAL PARK POLICY AREA 13



- 61** Precinct 61 Retail Core Welland
- 62** Precinct 62 Commercial Fringe Welland
-  Intensive Landscape Buffer
-  Vehicle Access Points
-  Pedestrian Links
-  Possible Future Traffic Restriction Measure
-  Policy Area Boundary
-  Precinct Boundary




WELLAND

Concept Plan Map ChSt/11

WELLAND POLICY AREA 14



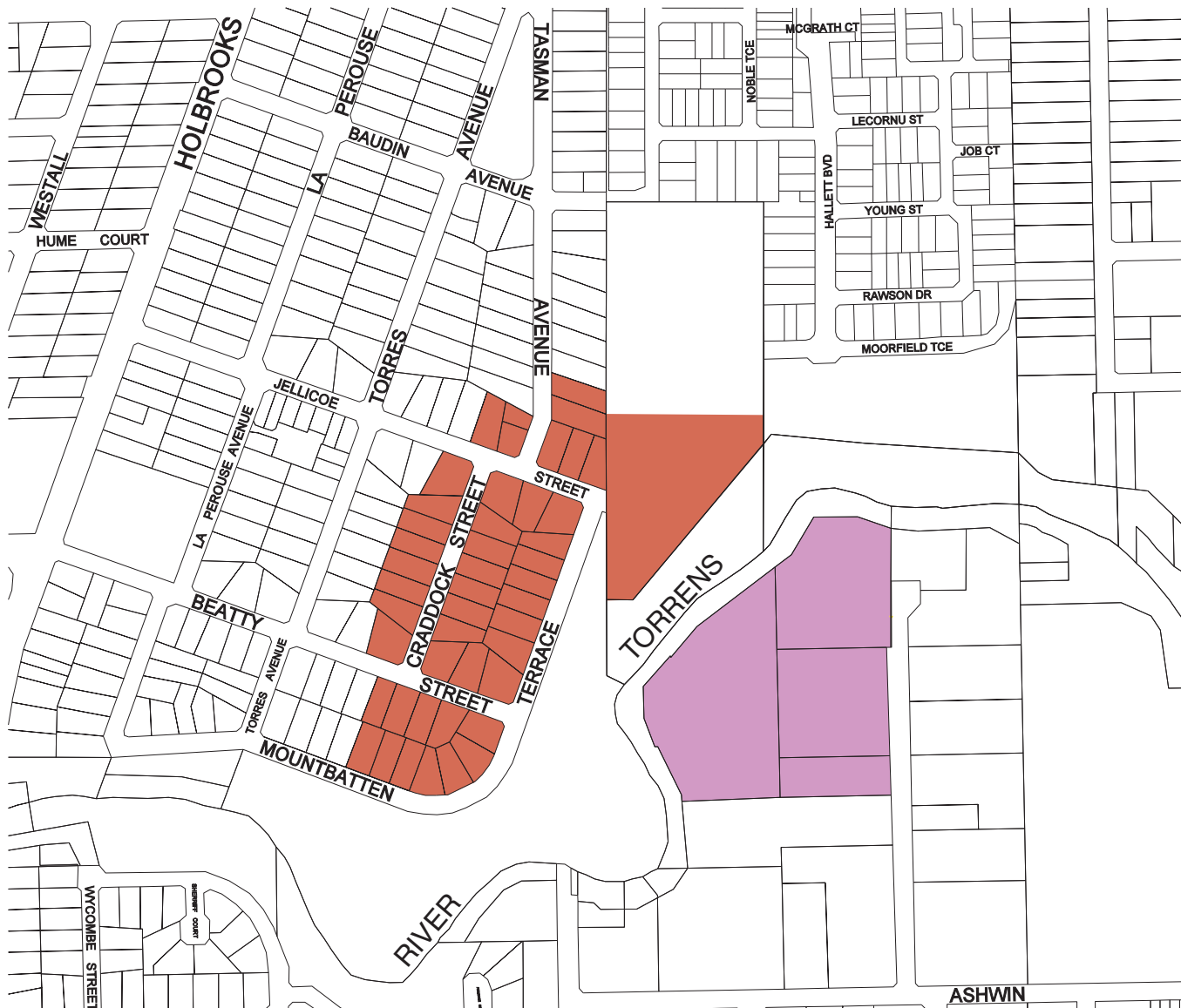
 Allotments affected by Adelaide Airport Flightpath noise



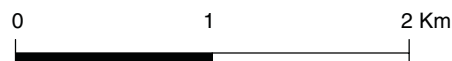
CHARLES STURT

Concept Plan Map ChSt/12

ALLOTMENTS AFFECTED BY AIRCRAFT NOISE



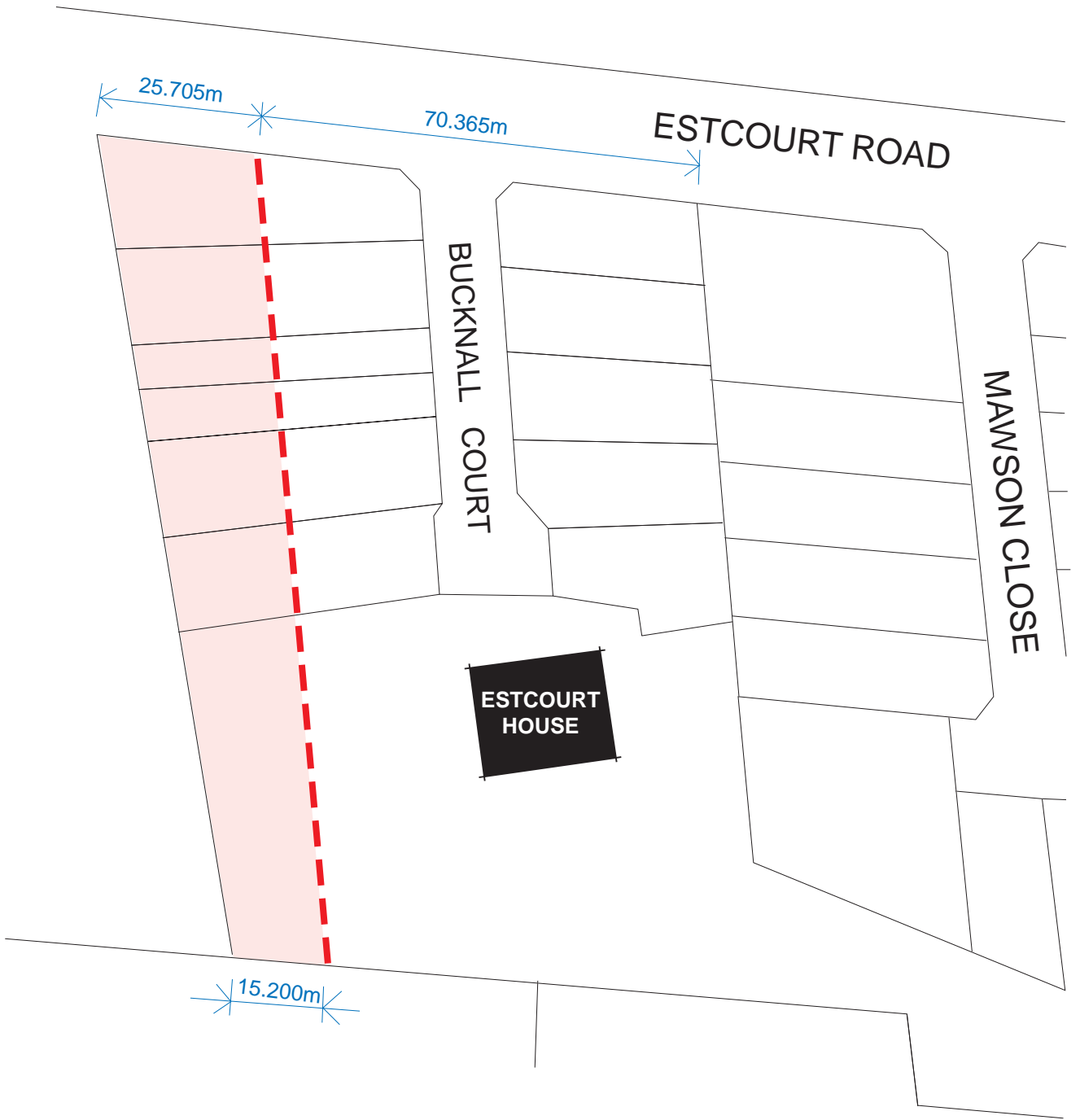
- Allotments in Allenby Gardens and Flinders Park Affected by Foundry Noise from south of the River Torrens
- Foundry



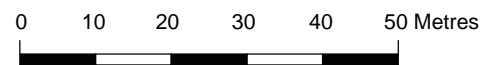
CHARLES STURT

Concept Plan Map ChSt/13

ALLOTMENTS AFFECTED BY FOUNDRY NOISE



Scale 1:1000



CHARLES STURT

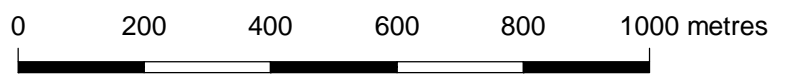
Concept Plan Map ChSt/14

BUILDING RESTRICTION AREA



Dwelling Prohibition Line
Dwelling Restriction Area

CHARLES STURT COUNCIL
Consolidated - 14 January 2021



Concept Plan Map ChSt/15


BUILDING RESTRICTION AREAS

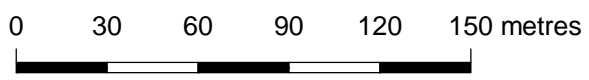
- Minimum Coastal Set-back - 4m
- Minimum Coastal Set-back - 8m



Concept Plan Map ChSt/16

BUILDING RESTRICTION AREAS

 Minimum Coastal Set-back - 8m

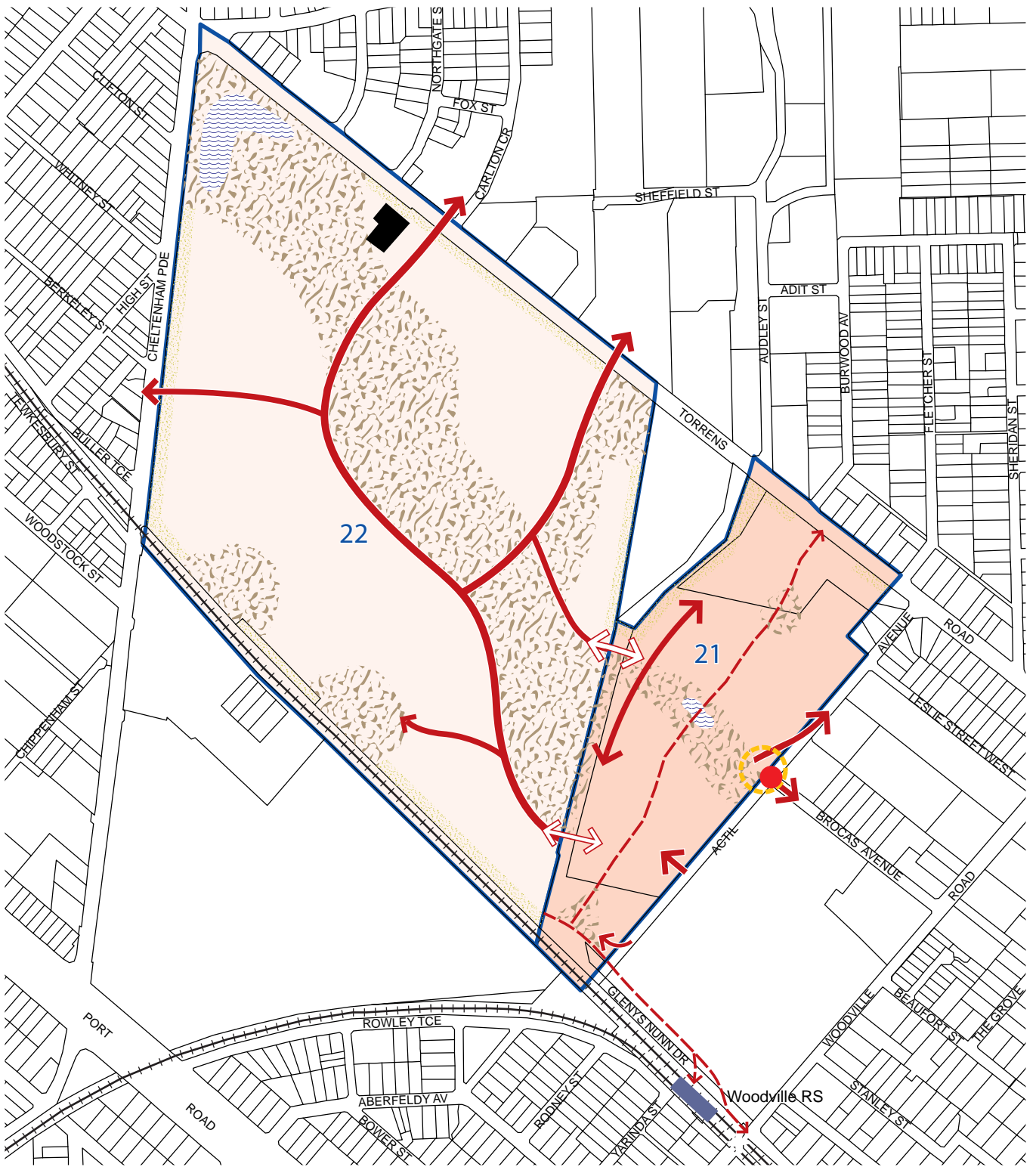



Concept Plan Map

ChSt/17

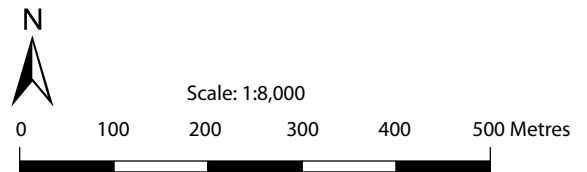
BUILDING RESTRICTION AREAS

- Minimum Coastal Set-back - 8m
- Minimum Coastal Set-back - 15m
- Maximum Seaview Road Set-back - 45m



-  Possible Roundabout
-  Railway Station
-  Railway
-  Major Arrival Focus
-  Noise & Visual Buffer
-  Possible Stormwater Detention
-  Major Two Way Road Access
-  Potential Future Link
-  Shared Pedestrian/Bicycle Link
-  Woodville Medium Density Policy Area 21
-  Cheltenham Park Policy Area 22
-  Recreation and Sports Club

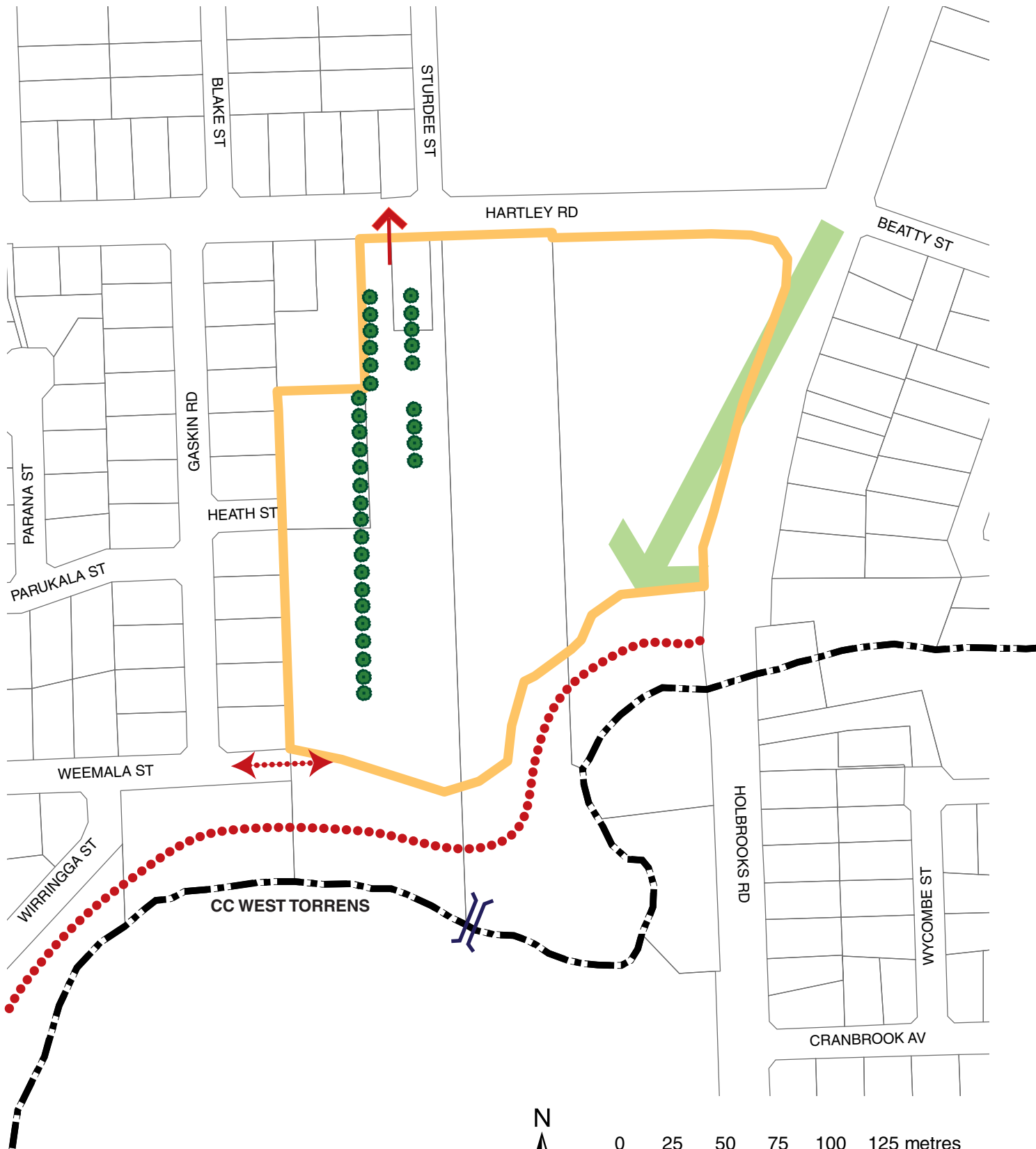
Note: This Concept Plan is for indicative purposes only. The final location of the open space may be subject to refinement following the outcomes of investigations into intended stormwater management / aquifer storage and recovery.










CHELTENHAM PARK AND WOODVILLE

Concept Plan Map ChSt/18

CHELTENHAM PARK AND WOODVILLE MEDIUM DENSITY

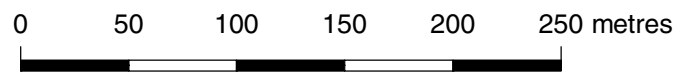
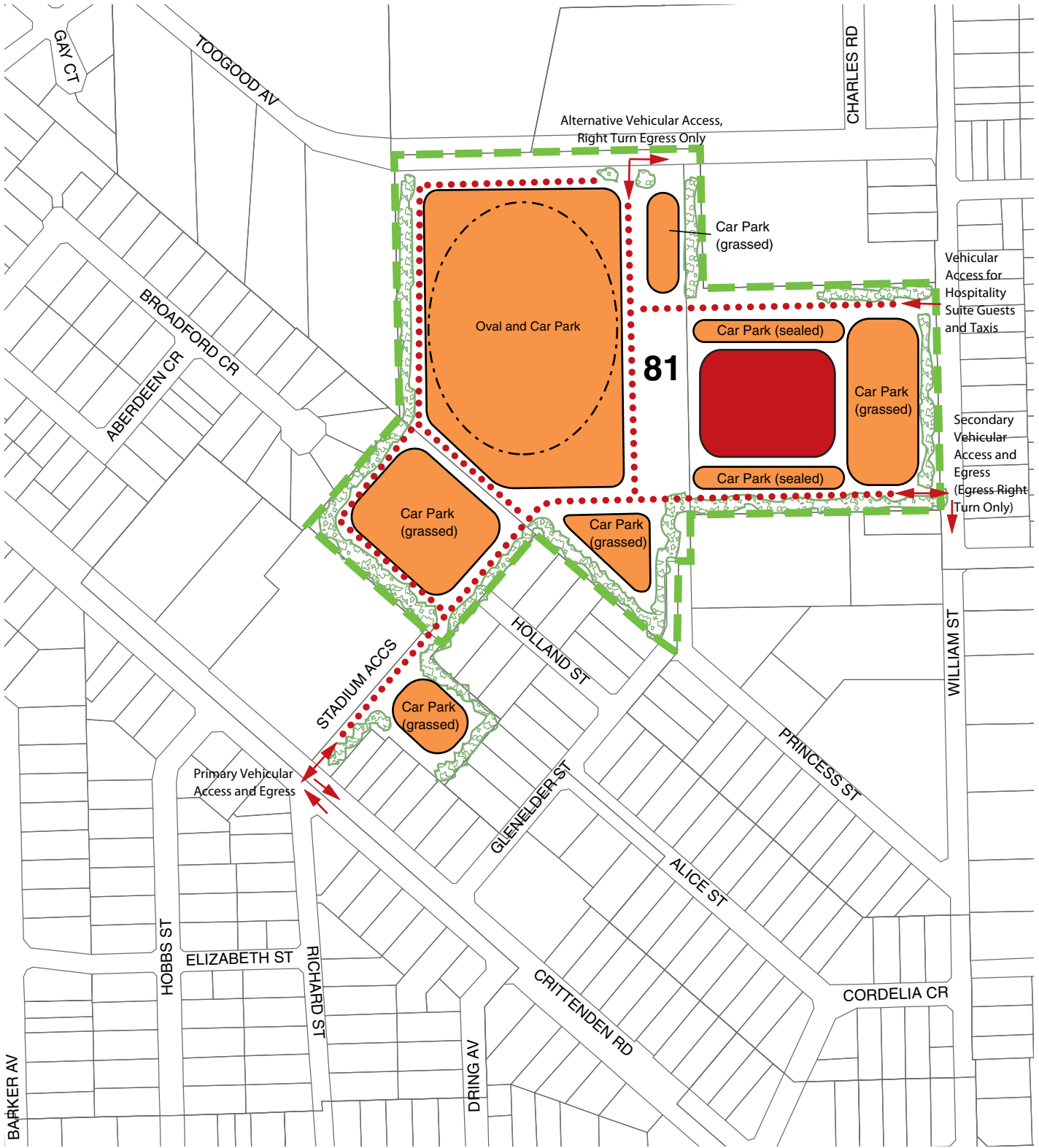









-  Linear Park Recreational Path
-  Pedestrian Bridge
-  Vehicle Access
-  Pedestrian Access
-  Tree Lined Avenues
-  Important Viewline / Open Space
-  Former Underdale Campus Site Boundary

UNDERDALE

Concept Plan Map ChSt/19

FORMER UNDERDALE CAMPUS SITE

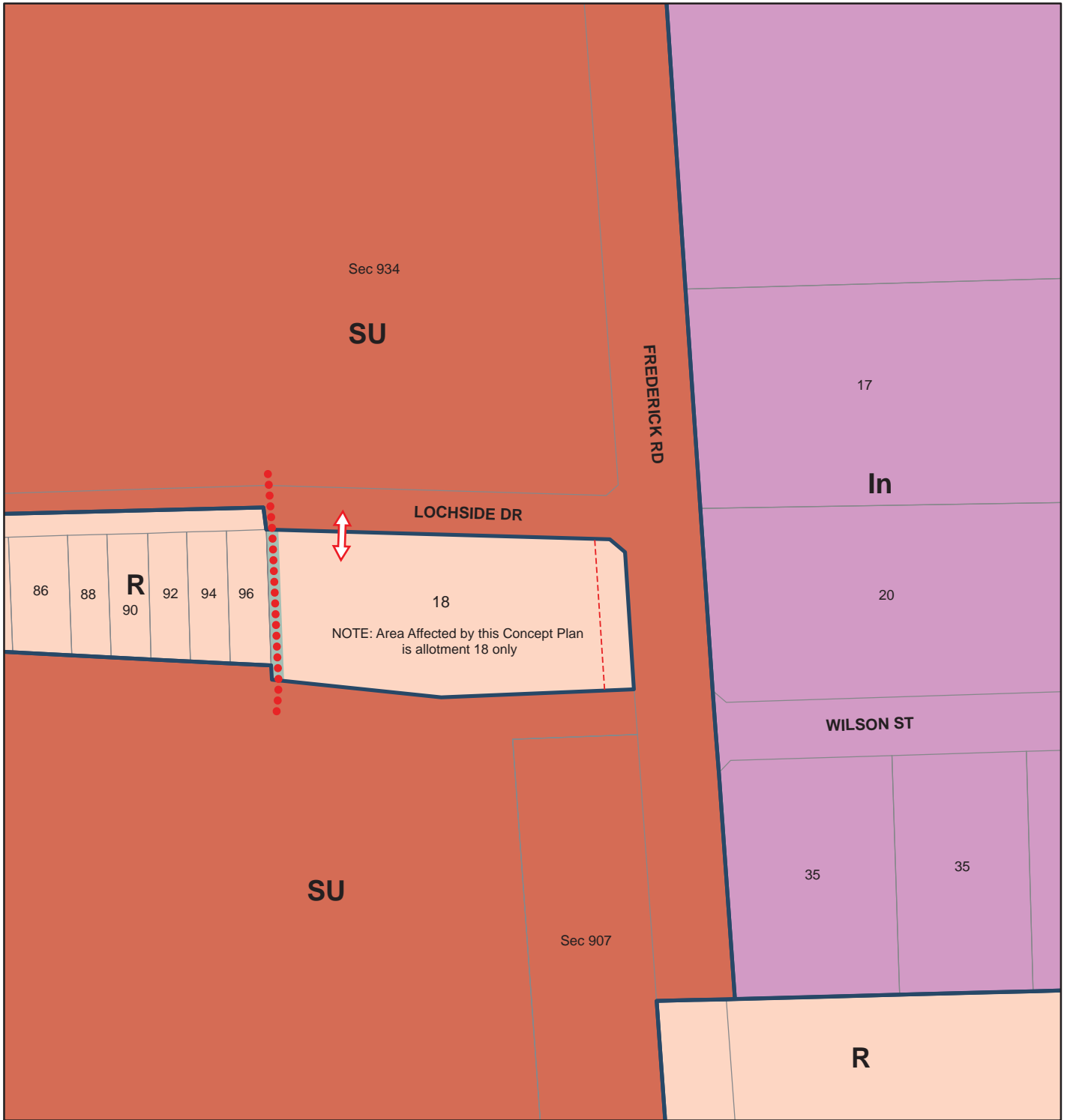


-  Building Area
-  Carparking Area
-  Perimeter Mounding and Planting
-  Recreation Oval
-  Vehicular Movement
-  Vehicular Access
-  Precinct 81 Stadium Beverley

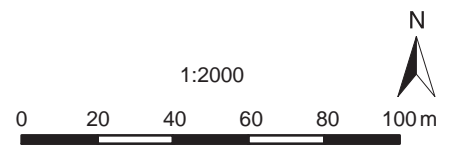
BEVERLEY

Concept Plan Map ChSt/20

PRECINCT 81 STADIUM BEVERLEY



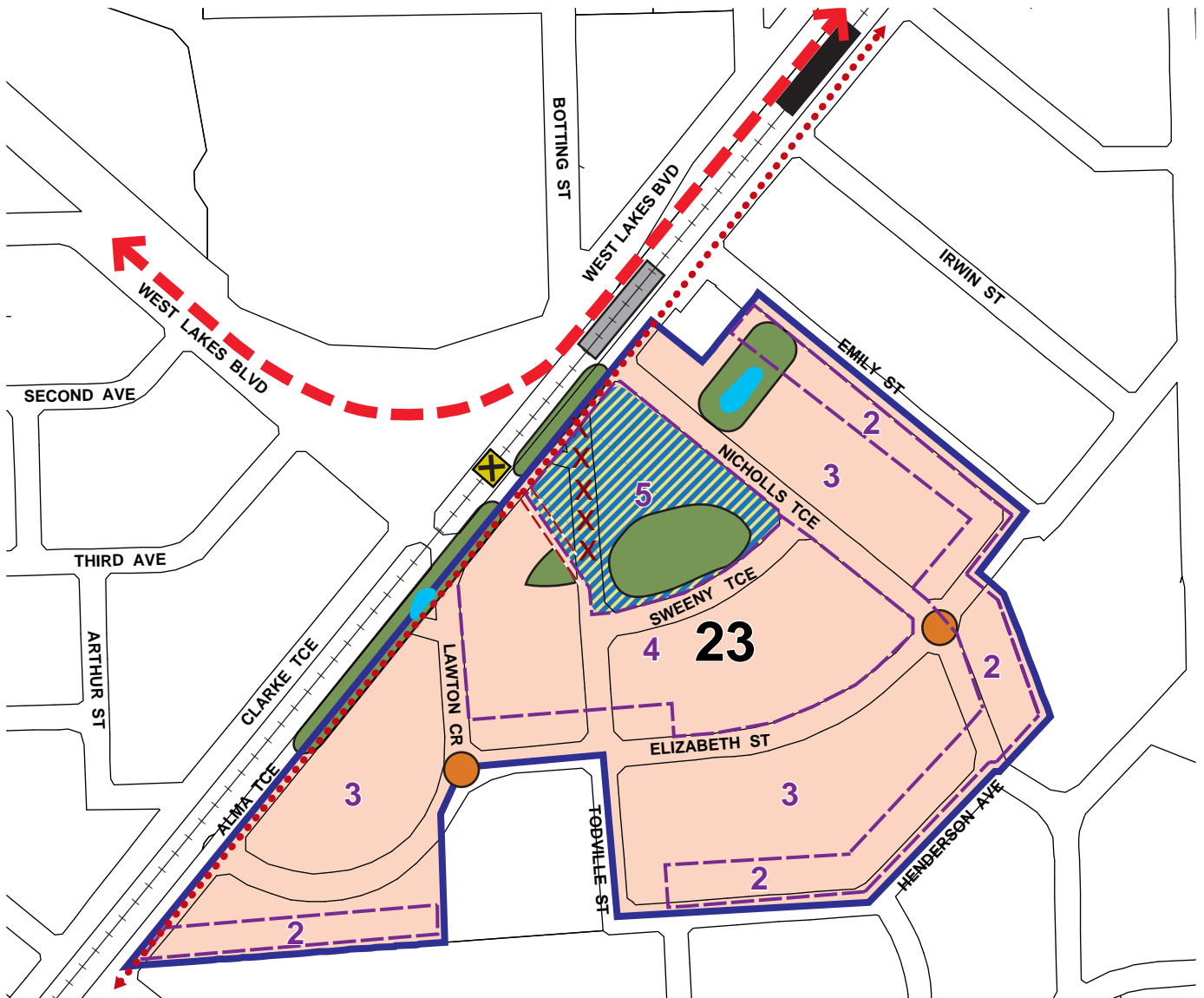
- Pedestrian Access
- ↔ Vehicle Access
- - - 10 metre setback from Frederick Road for Dwellings
- 5 metre wide Right Of Way for pedestrians and Golf Course machinery
- Residential
- Industrial
- Special Use
- Zone Boundary


















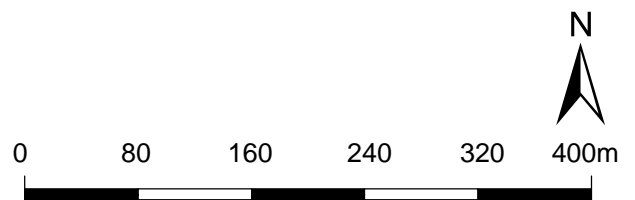
WEST LAKES

Concept Plan Map ChSt/21

FORMER WEST LAKES COUNCIL DEPOT LAND



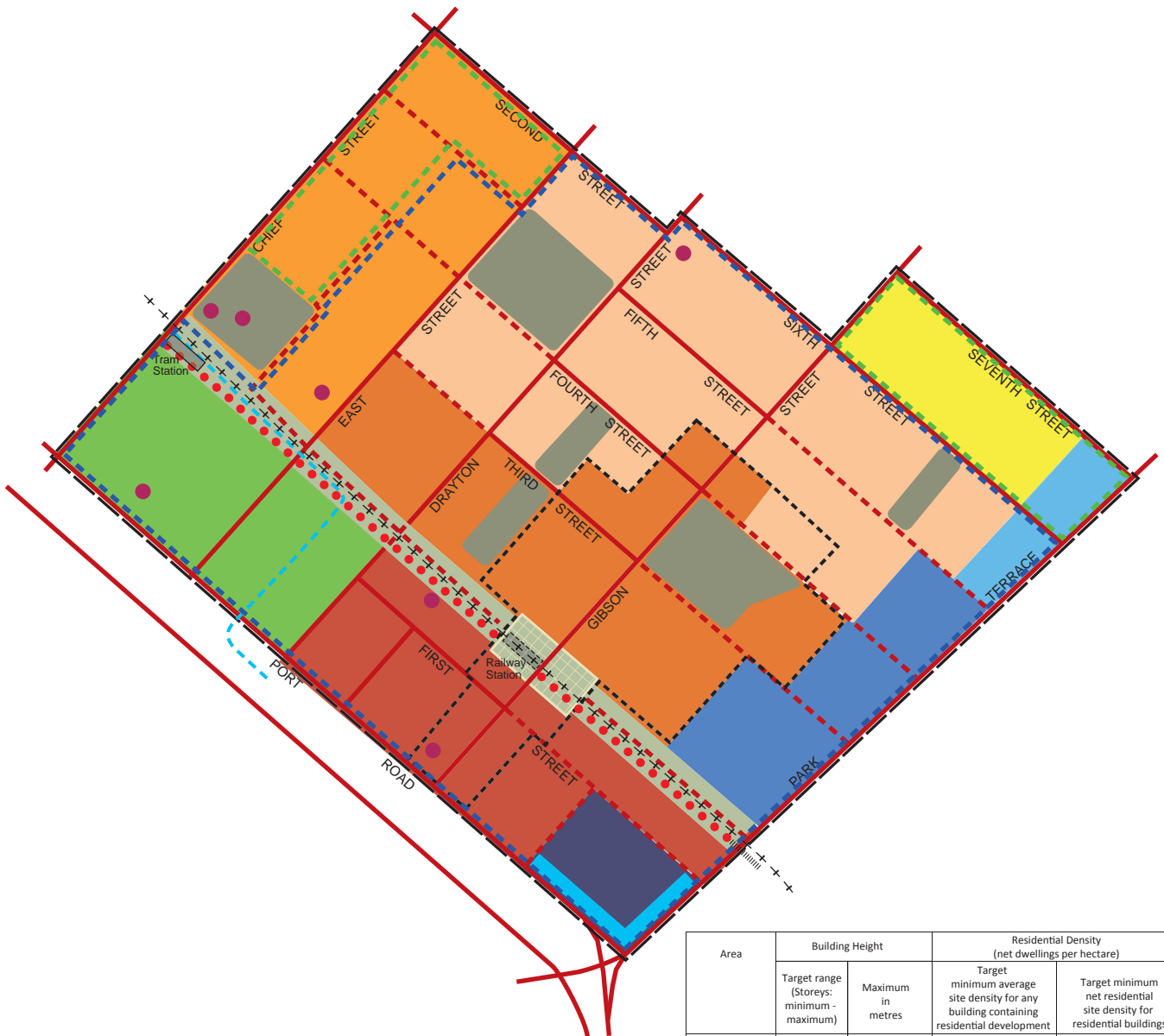
-  Concept Plan Boundary
-  Railway
-  Railway Station
-  Potential Railway Station
-  Potential mass transit
-  Existing Rail Crossing
-  Possible roundabout
-  Shared pedestrian / bicycle link
-  Possible stormwater detention
-  Future road alignment
-  Future road closure
-  Public reserve (indicative)
-  Core Precinct
-  Woodville West Policy Area 23
-  Maximum Building Height (Storeys)



WOODVILLE WEST

Concept Plan Map ChSt/22

WOODVILLE WEST



- Core
- Transition
- Greenway and bike/pedestrian path
- Piazza
- Proposed Pedestrian/Bicycle Overpass
- State Heritage Place
- Landmark Site
- Open Space
- Urban Core Zone
- Main Street Precinct boundary
- Potential extension of Tram Way
- Future Tram Station
- Future Underground Railway Station
- Potential for undergrounding and realignment of rail line
- Road
- Proposed new street

Area	Building Height		Residential Density (net dwellings per hectare)	
	Target range (Storeys: minimum - maximum)	Maximum in metres	Target minimum average site density for any building containing residential development	Target minimum net residential site density for residential buildings
	2 - 8	32.5	230	100
	3 - 4	16.5	90	60
	3 - 6	24.5	220	60
	3 - 10	40.5	260	130
	3 - 10	40.5	230	200
	4 - 6	24.5	220	150
	6 - 12	48.5	230	200
	6 - 12	48.5	270	220
	12 - 16	64.5	230	200



Concept Plan Map ChSt/23

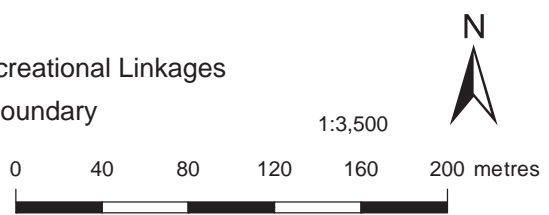
BOWDEN URBAN VILLAGE

URBAN CORE ZONE



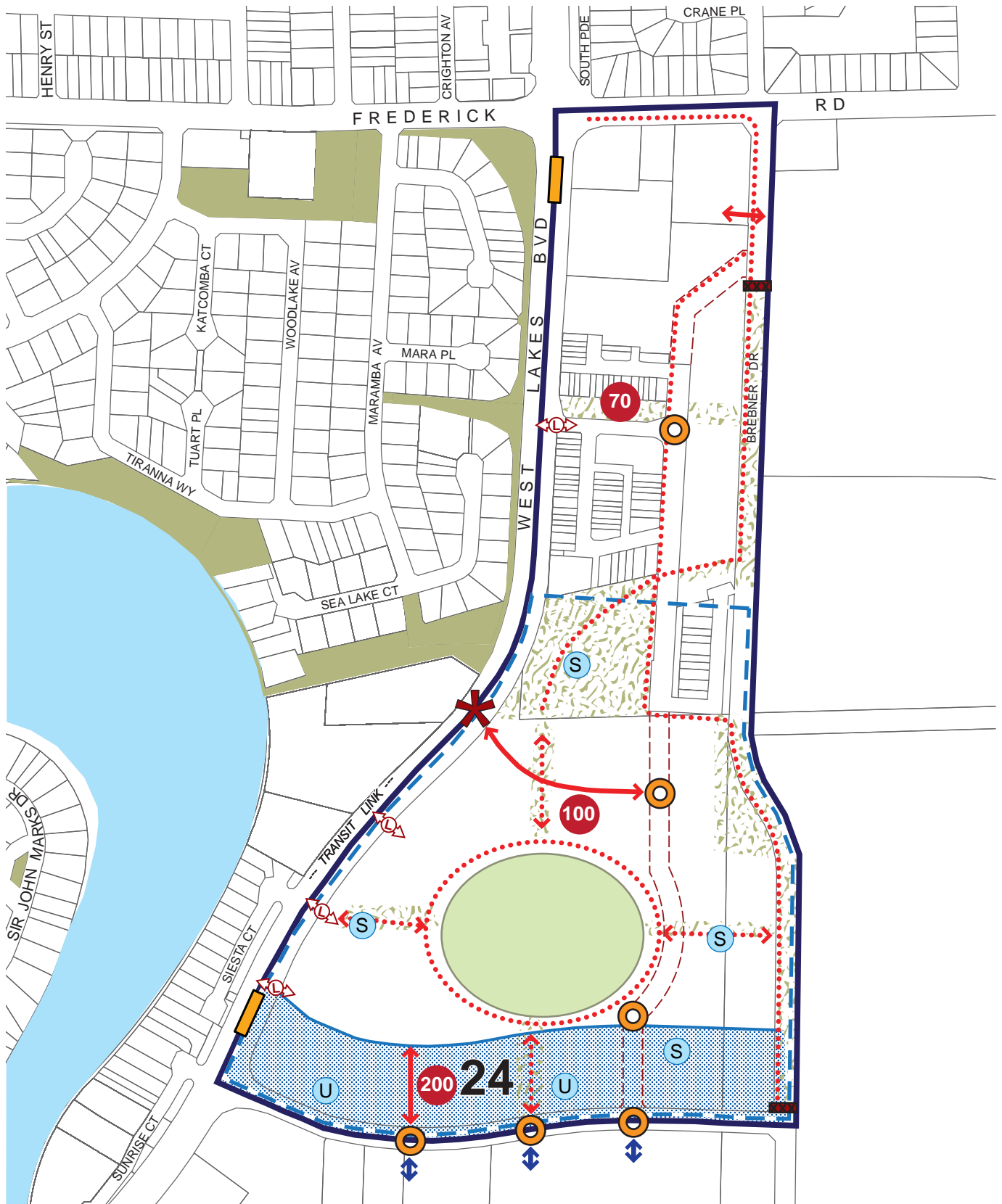
- Educational/Woodville High School
- Core Area
- Transition Area
- St Clair Recreation Centre
- Open Space
- State Heritage Place
- Railway Station
- Railways
- XX Closed Road
- <--> Indicative Road Alignment

- Roads
- Shared-use Recreational Linkages
- Concept Plan Boundary



Concept Plan Map ChSt/24

PRECINCT 21 RAILWAY STATION

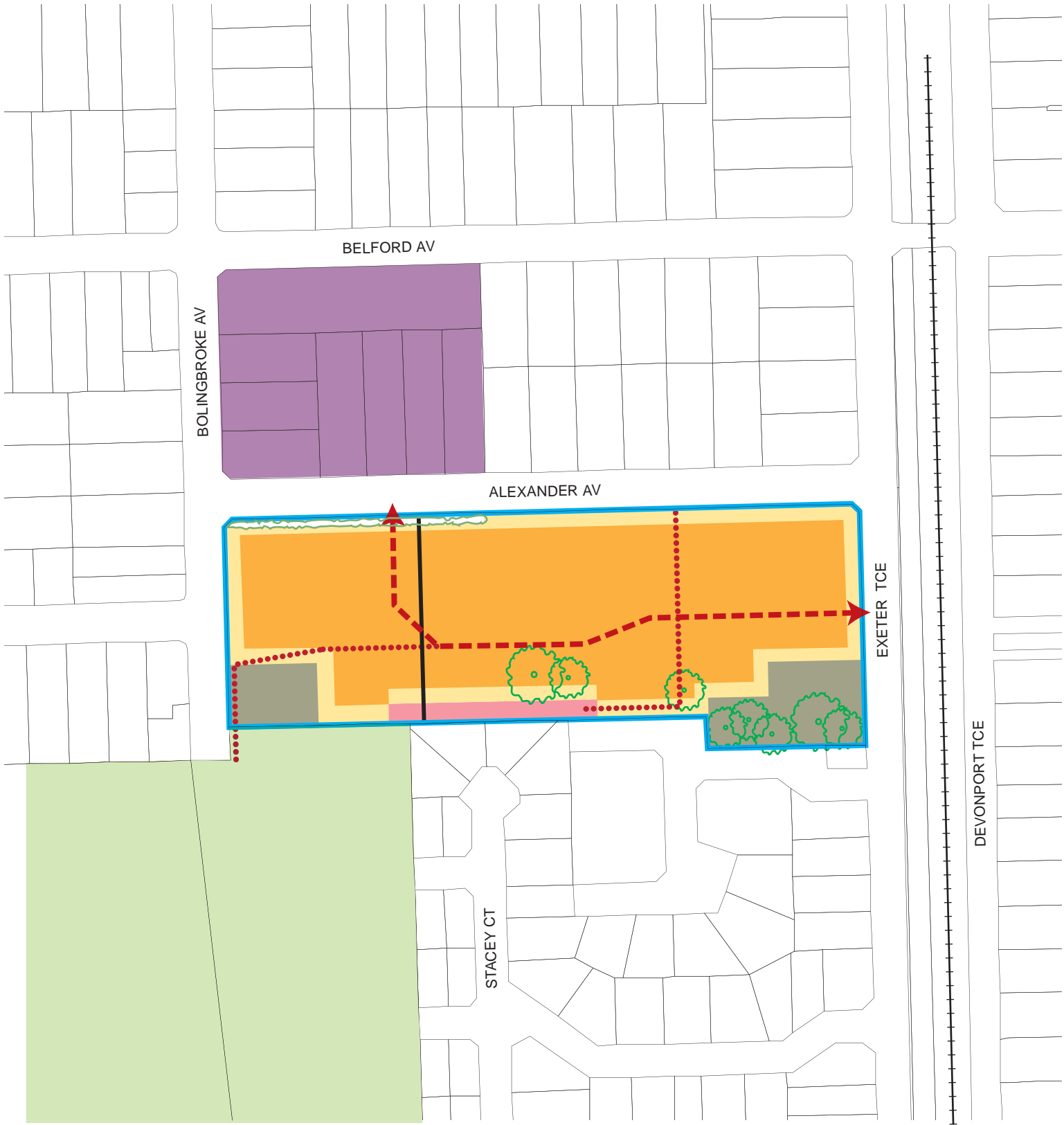


- Concept Plan Boundary
- Core Area of the Urban Core Zone
- 24 Main Street Policy Area
- 100 Housing Density (per ha)
- ↔ Road Linkage
- ⋯ Shared Use Paths (Cycling / Pedestrian)
- ✱ Potential Signalised Intersection
- ⊕ Left In / Left Out
- S Stormwater Detention
- U Underground Water Storage
- Sports Facilities
- ✱ Potential Road Closure
- Possible Mass Transit Stop
- Potential Secondary Road
- Intersection Treatment (Traffic Signals/Roundabout)
- ↕ Access Egress to Centre Zone
- Local Reserves
- Open Space / Reserves
- Waterbodies



Concept Plan ChSt/25

WEST LAKES URBAN CORE ZONE

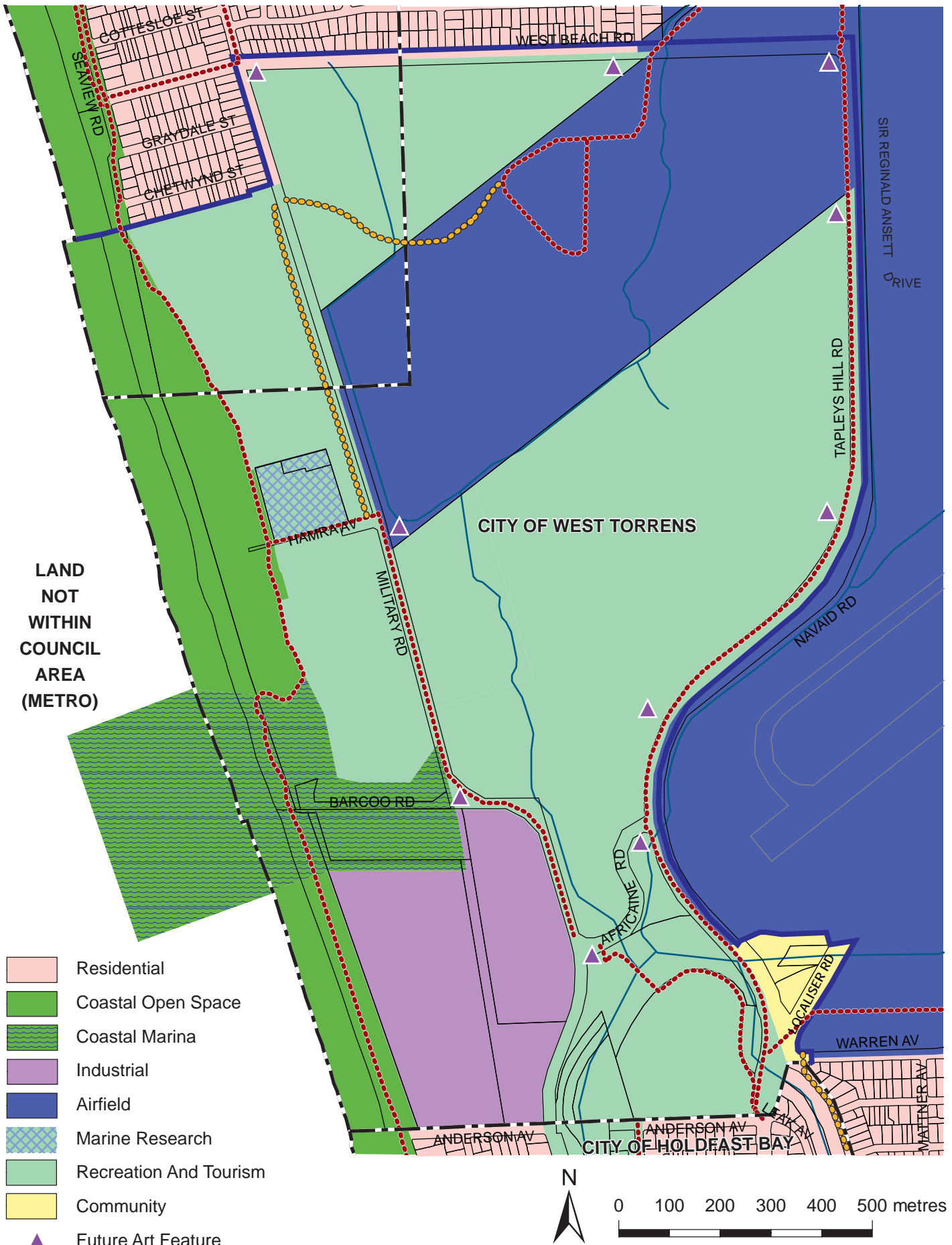


- 3 Storeys
- Up to 2 Storeys
- 1 Storey
- Internal Roadway
- Pedestrian Connection
- Industry
- Local Reserve
- Open Space (Indicative)
- Indicative Location of Significant Trees
- Landscaping
- Property Owner Boundary
- Concept Plan boundary

Concept Plan Map ChSt/26

DEVON PARK MEDIUM DENSITY REDEVELOPMENT SITE

CHARLES STURT COUNCIL
Consolidated - 14 January 2021



LAND NOT WITHIN COUNCIL AREA (METRO)

- Residential
- Coastal Open Space
- Coastal Marina
- Industrial
- Airfield
- Marine Research
- Recreation And Tourism
- Community

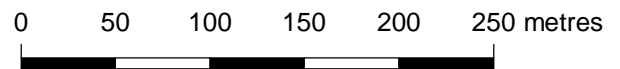
- Future Art Feature
- Existing Bike and Pedestrian Path
- Future Bike and Pedestrian Path
- Water Courses
- Concept Plan Boundary
- Development Plan Boundary





Concept Plan Map ChSt/27

ADELAIDE SHORES



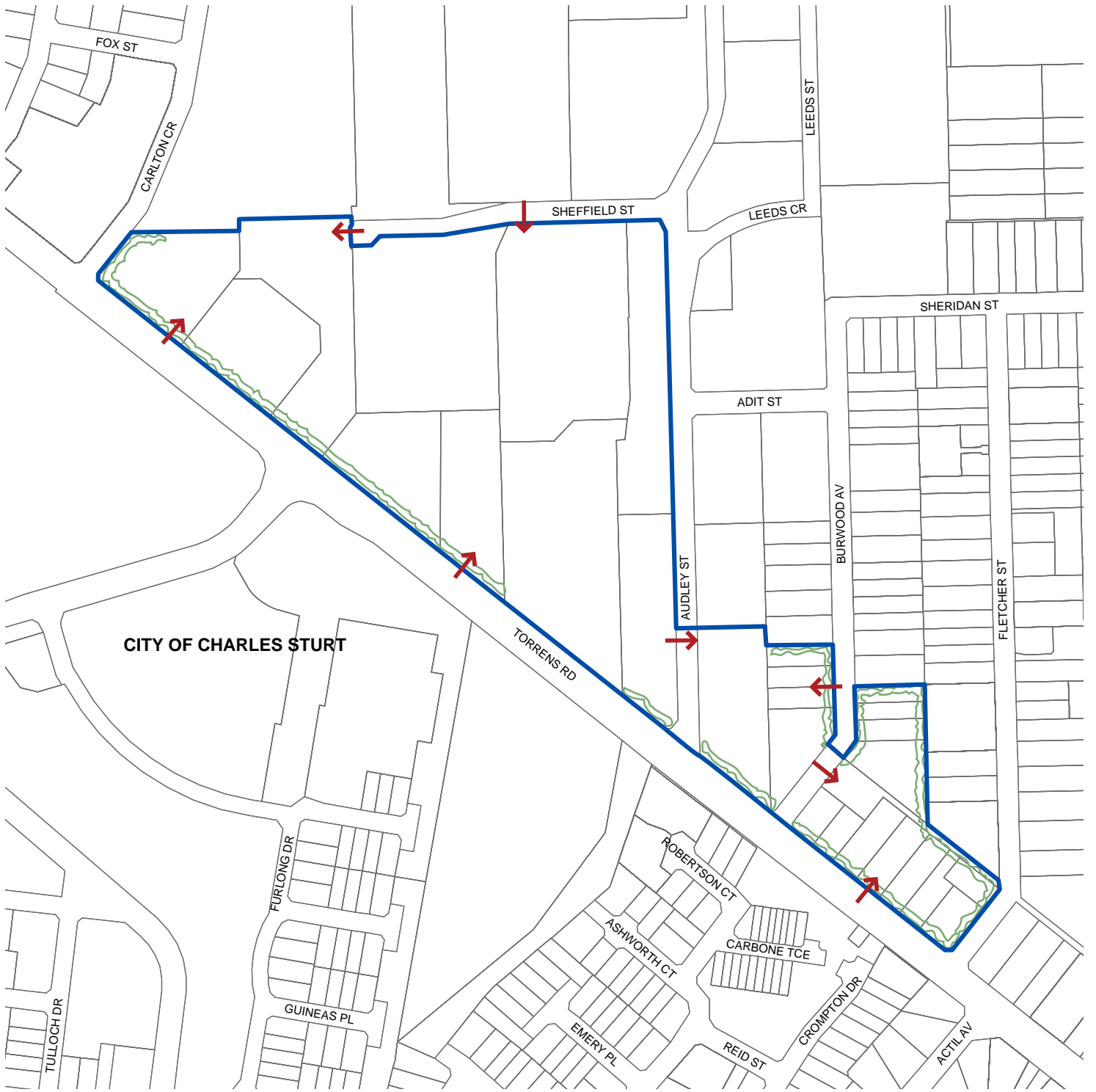
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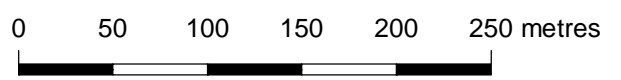
-  Vehicle Access
-  Landscaping
-  Concept Plan boundary
-  Local Government Area Boundary





Concept Plan Map ChSt/28

OLD PORT ROAD, ROYAL PARK



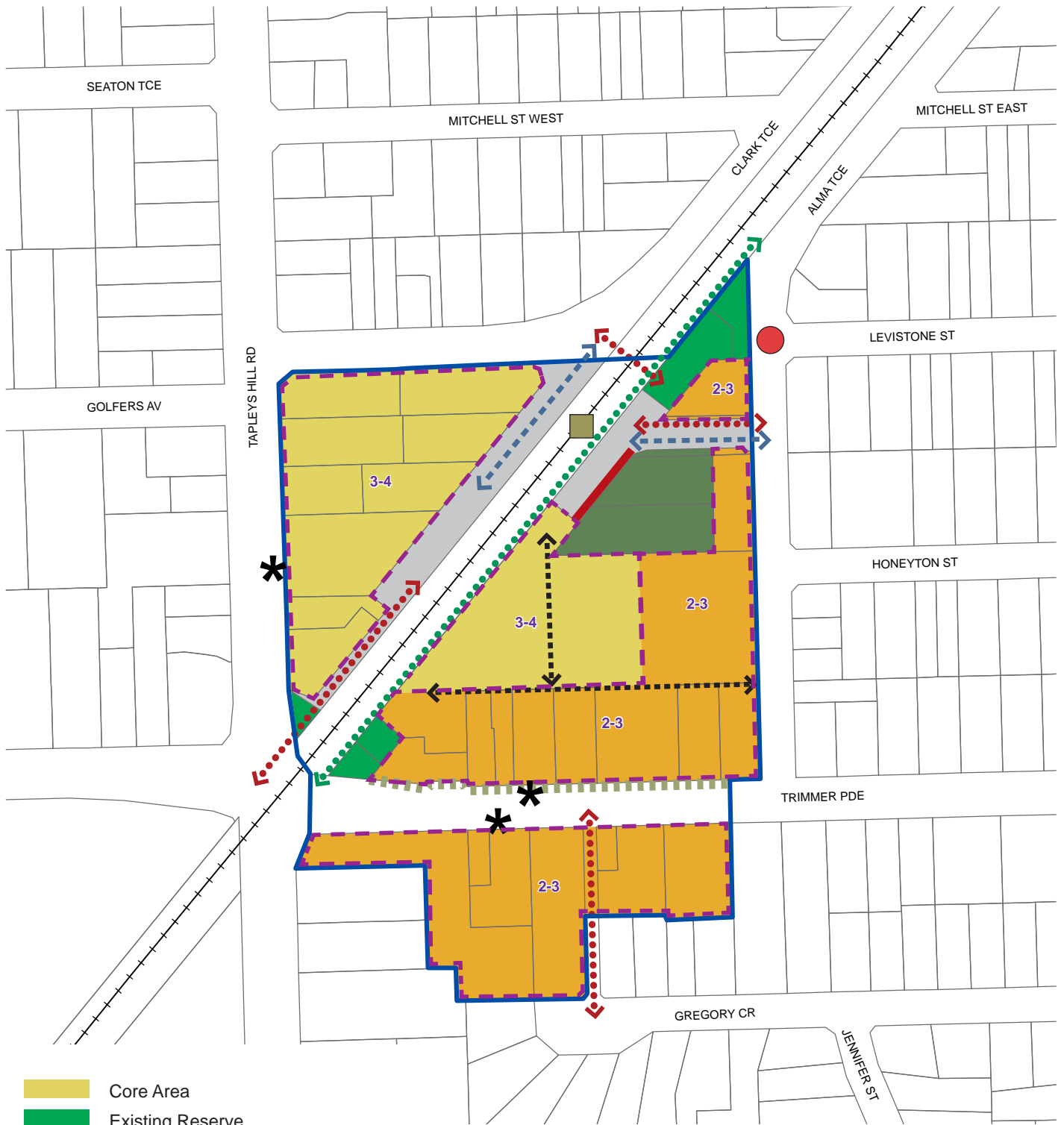
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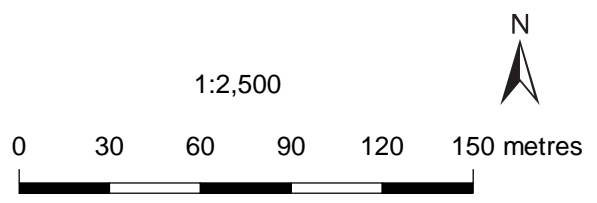
-  Vehicle Access
-  Landscaping
-  Concept Plan boundary
-  Local Government Area Boundary

Concept Plan Map ChSt/29

TORRENS ROAD, WOODVILLE NORTH



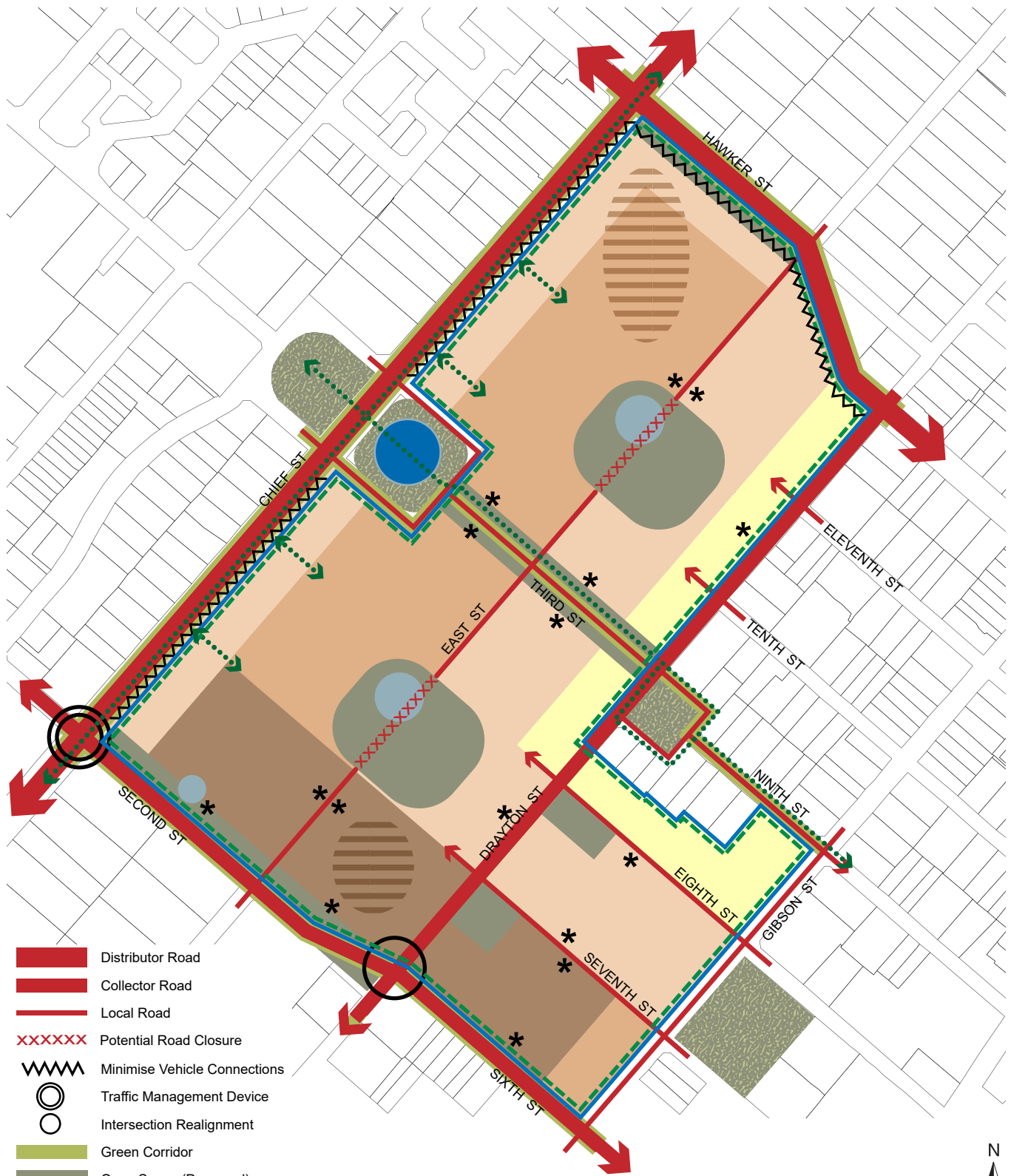
- Core Area
- Existing Reserve
- Public Open Space (indicative)
- Transition Area
- Existing Public Road
- 3-4 Minimum - Maximum Building Height (Storeys)
- Pedestrian / Cycle Access
- Vehicle Access
- Restricted Direct Vehicle Access
- Formalised Public Parking
- Internal Roadway (indicative)
- Proposed Grange Greenway
- Seaton Park Station
- Roundabout
- * Subject to Road Widening Requirements
- Railway
- Concept Plan Boundary



Concept Plan Map ChSt/30

SUBURBAN ACTIVITY NODE ZONE

SEATON



- Distributor Road
- Collector Road
- Local Road
- Potential Road Closure
- Minimise Vehicle Connections
- Traffic Management Device
- Intersection Realignment
- Green Corridor
- Open Space (Proposed)
- Open Space (Existing)
- Proposed Pedestrian / Cycle Access
- Pug holes (indicative)
- Stormwater Detention Basin
- Underground Stormwater Storage
- 2 Storey maximum (8.5 metres)
- 3 Storey maximum (12 metres)
- 5 Storey maximum (21 metres)
- 6 Storey maximum (24.5 metres)
- * Subject to Road Widening
- Concept boundary
- Transition Area



Concept Plan Map ChSt/31

BOWDEN-BROMPTON URBAN CORE ZONE

CHARLES STURT COUNCIL

Consolidated - 14 January 2021